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Preliminary Not For Construction

Revision:

Date:

Introduction

We understand that the Infill Incentive District is designed to encourage and facilitate sustainable development within specific and targeted zones in and around downtown Tucson. Our proposed development is located within the Greater Infill Incentive Sub-District that stretches south along Stone Avenue as it begins to angle towards the intersection of South 6th Avenue and Eighteenth Street (Five Points). Due to its proximity to downtown and major transit routes, it seems clear that increased density along this corridor is being encouraged, provided that it contributes to the economic vitality and established character of the surrounding neighborhoods.

In its current state, this property is vacant and has been for decades. It detracts from the historic character of the surrounding area. Both the zoning (C-3) and its inclusion in the IID zone indicate that there is an intention to bring greater activity to this site and the area overall. Existing conditions are complex in that the site is sized similarly to a single family residential lot but can support development of much greater density. The existing neighborhood suggests that a collection of detached structures would best fit the development pattern, as would areas of landscaping and tree lined sidewalks. Our strategy has been to incorporate these characteristics into a closely grouped collection of individual buildings with an emphasis on sustainable strategies, mixed commercial and residential activities, and pedestrian environments.

We are pursuing the Greater Infill Incentive Subdistrict (GIS) zoning in order to achieve these objectives. We feel that positioning the proposed buildings at or near the street edge and placing commercial activity at ground level is a positive strategy for the area. However, it requires that parking, setbacks, and landscaping buffers be thought of in ways the UDC does not allow.

In summary, our proposed subdivision will have five separate lots, each with one structure (see IID Plan). Units 1 and 3 are to be mixed use with commercial use on the ground floor, and single family residential above. Units 2, and 4 are to be single family residential only. Unit 5 will have a home office component integrated into a single family residence.

UDC 5.12.8 IID Design Standards

A. Streetscape Design

1. Pedestrian Orientation
 - a. All structures will have architectural articulation on all floors, including the first and second.
 - b. Unit 1 will have glazing and/or display on its exposed street frontages, but in total will not reach the 50% required opening amount. We believe that the building orientation is not conducive to more open area due to excessive heat gain along the west face of the building. Units 3 and 5 will have 50% or more of their street frontages exposed. Units 2 and 4 are residential and are not open to the street for privacy considerations.
 - c. No structures have a street facing façade of fifty feet or more.
 - d. The Unit 1 commercial space has an entry facing east and is clearly visible from both 17th Street and Stone Avenue (through glazing along Stone). The Unit 3 commercial space has an entrance clearly visible from 17th Street. Unit 5 has an entry to the home office directly visible from 17th Street.
 - e. All commercial activity is limited to the ground floor.
 - f. Sidewalks are to be existing where possible. Where sidewalk replacement is necessary, they will match the existing width.
 - g. n/a
 - h. n/a

2. Shade
 - a. There will be new trees planted along the south and west edges of the property between the sidewalk and street. Refer to the Development Package for more information.
 - b. Given the amount of curb cuts and infrastructure, it is not possible to plant enough trees to reach 50% coverage of the sidewalk. We are therefore requesting an exception be made in this case.

B. Development Transition Standards

1. Applicability

All adjacent properties are similarly zoned C-3. Across the street and diagonally are lots that are not being used for residential purposes. Immediately to the east and north are single family residences being used as such. These properties may influence the height of the proposed development. It should be noted that the property to the east has a historic residence that is taller than 25'-0".

2. Mitigation of Taller Structures

- a. Proposed individual building heights are as follows:

- Unit 1: 37'-6"
- Unit 2: 25'-0"
- Unit 3: 25'-0"
- Unit 4: 25'-0"
- Unit 5: 35'-6"

All units are within 30'-0" of the north or east property lines. Units 2, 3, and 4 comply with the 25'-0" height limit. Units 1 and 5 are taller than the 25'-0" allowed within 30'-0" of the adjacent single family residential properties and are therefore in violation.

There are several ways to approach resolving this. The first is that the adjacent single family residences are zoned C-3 and have the potential to be used as something other than residential, and could be developed to much greater density in the future. Second, it would be possible to consolidate the building volumes at the corner of 17th in a way which would comply the height restriction. However, we feel that this would result in an excessively large volume at the corner which would be out of character with the surroundings. We feel that the current proposal with its overall volume interrupted by an internally-focused court is the most consistent surrounding development pattern.

Thus, we are asking for special consideration on this issue because we feel it is a better fit with the neighborhood.

- b. Proposed building massing has intentionally been broken up in response to similar development patterns in the surrounding historical context.
 - c. Windows have been carefully placed with particular consideration given to privacy and views into the adjacent historic single family residential property to the north (refer to Exterior Elevations on Sheet A0.2).
 - d. Outdoor terraces are enclosed and screened for privacy.
 - e. Wherever possible, building orientation has been designed to minimize exposure between developing properties and the property to the north.
 - f. Site walls around landscaped areas are configured to prevent views between developing properties and existing single family residential property.
3. Mitigation of Service Areas

Trash and recycling are to be addressed by each property separately. There is no proposed collective service area.

4. Mitigation of Parking Facilities and Other Areas

Each subdivided lot will have its own on-site parking. With the exception of Unit 1, all on-site parking is behind gates to shield it from view. See IPP Plan for additional information.

C. Alternative Compliance

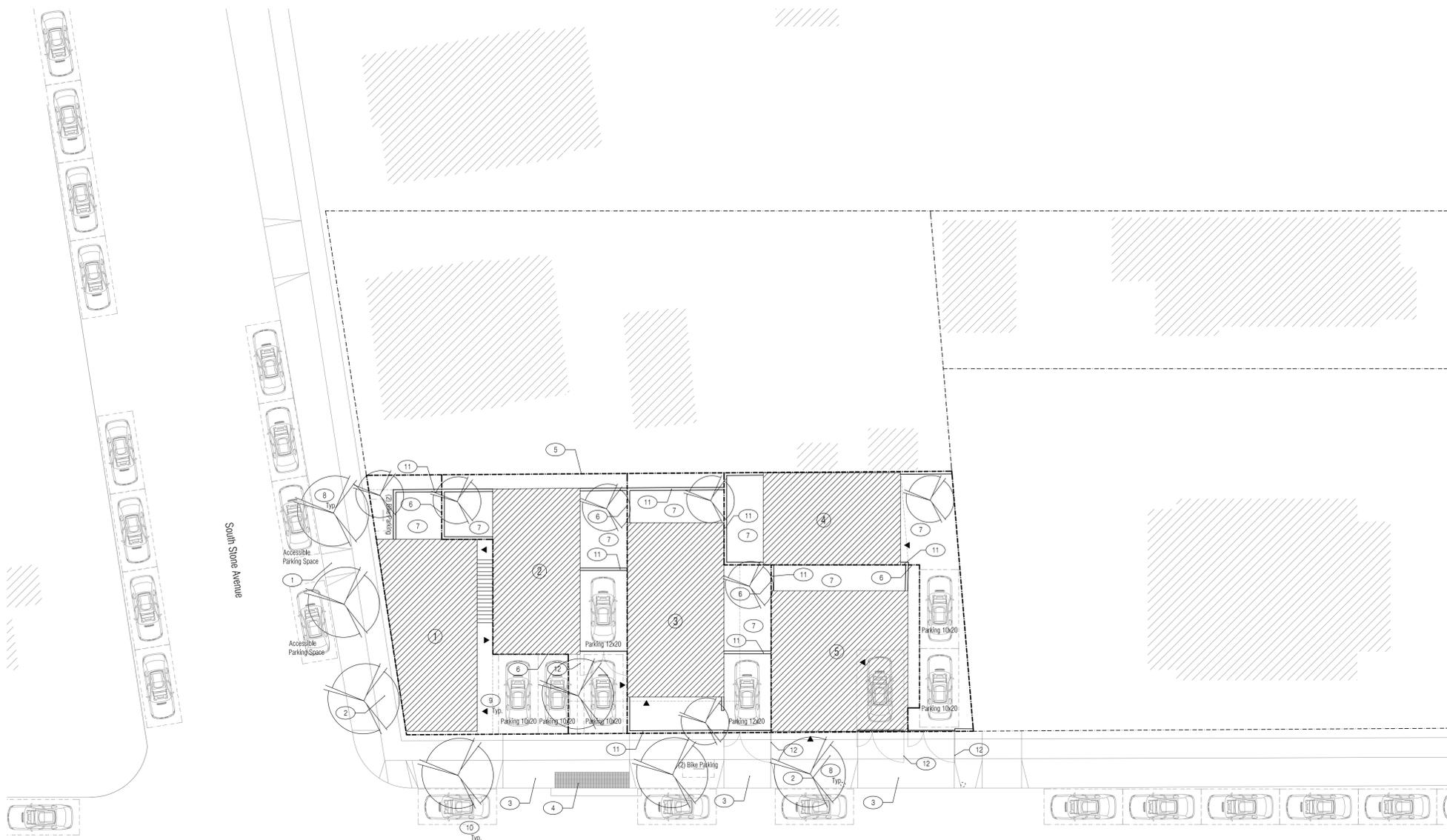
Within the above, we are asking for Alternative Compliance rulings on three things. The first relates to shade. We are planting as many trees along the sidewalks as is possible, but we have not reached 50% shade coverage. The second relates to Pedestrian Orientation. Unit 1 has commercial activity on the ground floor. We feel that opening up 50% of the façade would be detrimental due to the west facing orientation of its dominant street frontage. This would cause excessive heat gain throughout much of the year. We are proposing a lesser amount in response to this concern. The third item relates to building height. There is currently a single family residence adjacent to the north edge of the developing property. However, this residence is zoned C-3 and potentially could be used in alternative ways and/or be developed to a greater density than its current use.

D. Utilities

All utilities are to be sub-grade. Refer to the Development Plan for utility routing information.

E. Parking

All on-site parking shall be at grade. See IPP Plan for additional information.



IID Plan

UDC 5.12.9 GIS Greater Infill Incentive Sub-District

- A. GIS Land Uses
All proposed uses are to be Commercial, Retail Trade (excluding food and beverage service), and Residential.

- B. Modifications to Underlying Development Standards (see Exceptions below)

- C. Exceptions
1. Article 6, Dimensional Standards and Measurements
 - a. Building Height is addressed above.
 - b. We are requesting that the requirement of a street perimeter yard be waived. We believe that positioning some of our buildings along the street edge with commercial activity at the ground level is appropriate for the surrounding area, and consistent with other development along South Stone Avenue (both new and historic). We also maintain that the proposed development will not adversely impact site visibility along the major route.

2. Section 7.4, Motor Vehicle and Bicycle Parking
 - a. Parking requirements per the UDC are as follows:
Residences: 5 units at 2.25 spaces per unit equals 12.5 spaces
Commercial / Retail: Unit 1 at 4 spaces, Unit 5 at 1 space equals 5
Total 17.5 spaces.
Applying a 25 % reduction per IID equals 13.25, rounded down to 13.
There are 8 spaces provided on-site (all residential). 5 spaces remain and can be accommodated on-street (mixture of residential and commercial), 4 of them immediately in front of the development property.
See IPP Plan for additional information.

- b. There is one van accessible space required for the development to serve the commercial spaces. We are proposing that this be provided on-street at the northwest corner of the development property. There is precedent for this along South Stone Avenue. There will be (4) short term bicycle parking spaces provided. Long term spaces are to be provided within the individual buildings.
 - c. The location of parking spaces is shown on IID Plan.

3. Section 7.5, Off-Street Loading

There are no off-street loading requirements for the proposed development.

4. Solid Waste Collection

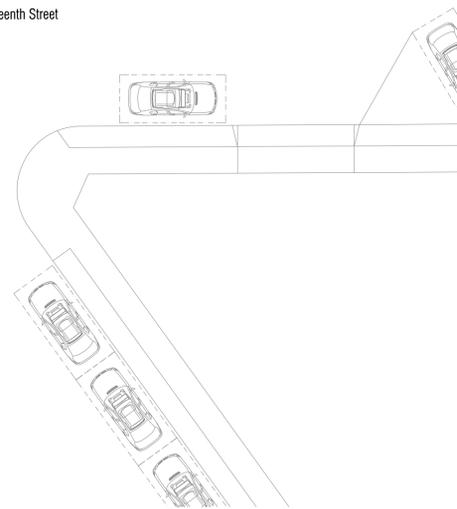
Each individual property will have their own trash and recycling containers, and will assume the responsibility of placing them along the curb on trash collection days.

5. Section 7.6, Landscaping and Screening
 - a. The proposed development property is currently a vacant lot containing no significant native plant growth.
 - b. We are requesting that the landscape buffer requirement along the two street edges be waived.
 - c. Shade for pedestrians will be provided by planting new trees in the public right-of-way. See IID Plan and Development Plan for tree locations and species.

IID Plan Key Notes

- 1 Remove existing curb cut. Replace with new curb, accessible ramp, and landscaping.
- 2 Existing sidewalk to remain where possible. Replace sidewalk where necessary to match existing.
- 3 New curb cut.
- 4 Modified storm water catch basin (refer to Civil Drawings)
- 5 Existing property line.
- 6 New subdivision property line.
- 7 Private court.
- 8 New shade trees in public right-of-way.
- 9 Unit entry.
- 10 On-street parking space.
- 11 Site wall.
- 12 Gate to parking space.

East Seventeenth Street



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Project:

The Stones

601 South Stone Avenue
Tucson, Arizona

Drawing Title:

IID Plan

Date: November 18, 2019

Issued For: Pre-Submittal Meeting

Drawn By: PR

Scale: 1/16"=1'-0"

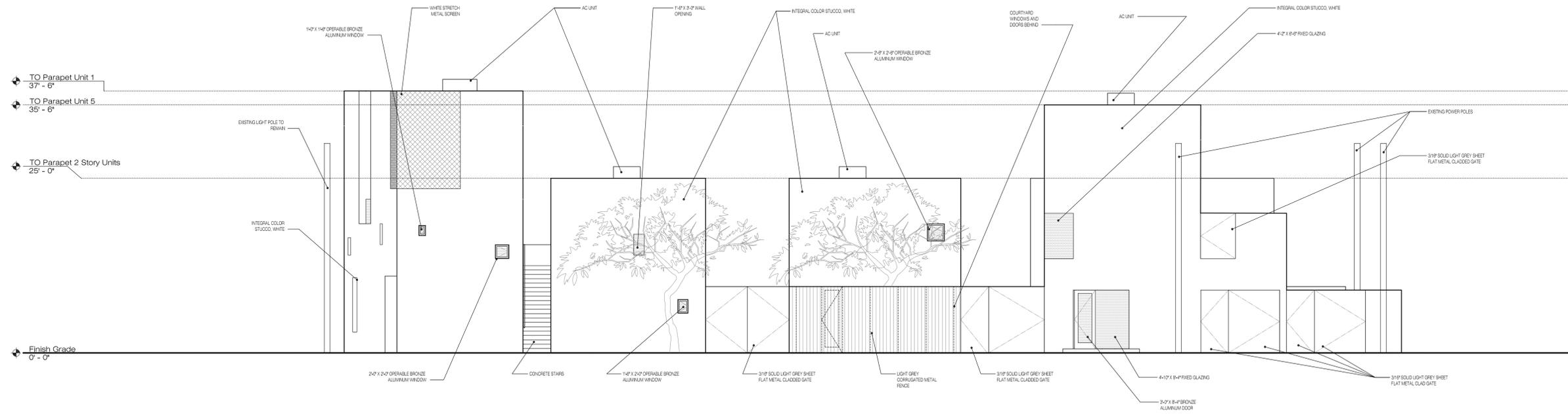
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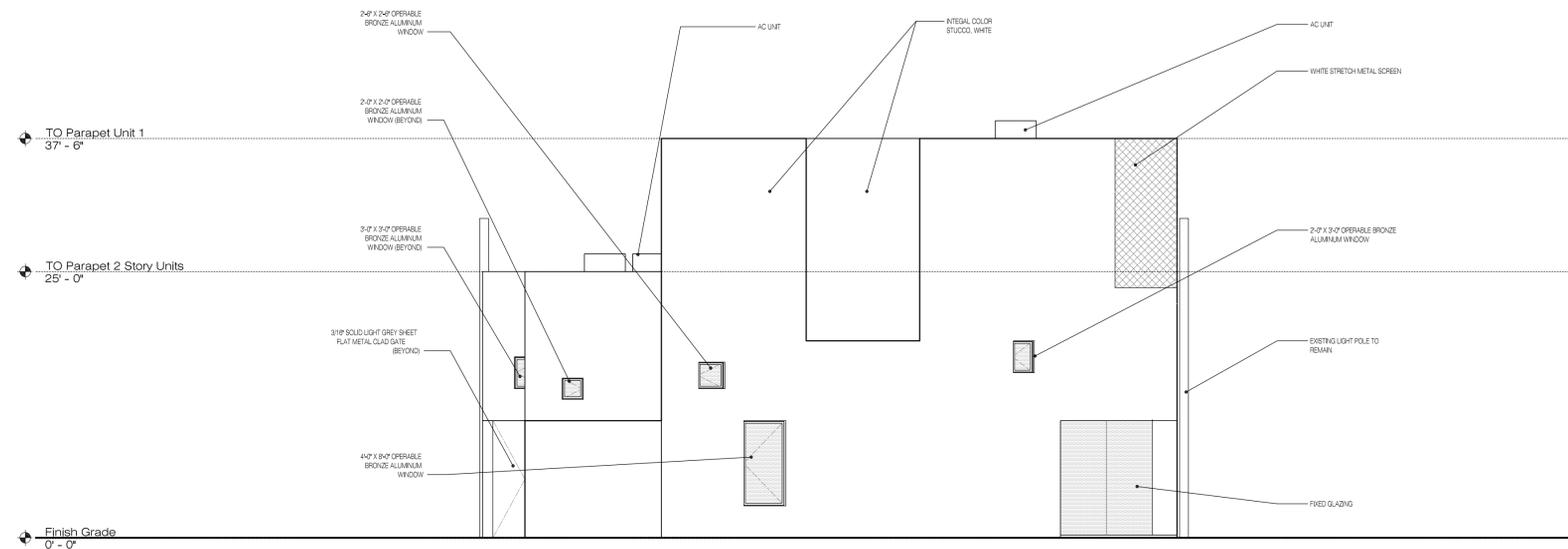
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South Street Elevation



West Street Elevation

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Project:

The Stones

601 South Stone Avenue
Tucson, Arizona

Drawing Title:

IID Elevations

Date: November 18, 2019

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Sheet No.:

A.02

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We are pursuing the Greater Infill Incentive Subdistrict (GIIS) zoning in order to achieve these objectives. We feel that positioning the proposed buildings at or near the street edge and placing commercial activity at ground level is a positive strategy for the area. However, it requires that parking be thought of in ways the UDC does not allow. We are applying for IPP to address this. In summary, our proposed subdivision will have five separate lots, each with one structure (see IID Plan). Units 1 and 3 are to be mixed use with commercial use on the ground floor, and single family residential above. Units 2, and 4 are to be single family residential only. Unit 5 will have a home office component integrated into a single family residence.

UDC 7.4.5 Motor Vehicle Parking Reductions and Exception

A. Individual Parking Plan

1. Applicability

This will be a proposed development of the site.

2. Permitted Uses and Types of Development

This will be a combined residential and non-residential development.

3. Individual Parking Plan Requirement

a. Number of required spaces:

Parking requirements per the UDC are as follows:

Residences: 5 single family units at 2.25 spaces per unit. Total equals 12.5 residential spaces.

Commercial / Retail:

Unit 1 is 1000 gsf of ground floor space plus 200 gsf of outdoor eating space. Food Service Use is 1 space per 100 sf. Equals 12 spaces.

Unit 3 is 900 gsf of ground floor space plus 450 gsf of outdoor eating space. Food Service Use is 1 space per 100 sf. Equals 13.5 spaces.

Unit 5 has 200 sf of home office space. Commercial Use is 1 space per 300 sf. Equals 1 space.

Total equals 26.5 commercial spaces.

Overall Total equals 39 spaces.

Applying a 25% reduction per IID equals 29.25, rounded down to 29 required spaces.

Number of space provided:

On-Site: There are 8 spaces provided on-site (all residential).

On-Street: 21 spaces remain and can be accommodated on-street (see IPP Plan), 5 of them immediately in front of the development property.

There is 1 van accessible space and 1 accessible space required for the development to serve the commercial spaces. We are proposing that they be provided on-street along the west edge of the development property. There is precedent for this along South Stone Avenue. There will be 4 short term bicycle parking spaces provided. Long term spaces are to be provided within the individual buildings.

b. See IPP Plan for parking space locations.

c. The site is currently vacant. New development will be a mixture of commercial, retail, and residential uses. Refer to IPP Plan for on-street parking space locations in the immediate vicinity of the development property.

d. Development site is located at the intersection of South Stone Avenue and East 17th Street. Stone is an MS+R and 17th is a residential street.

e. The development property is located adjacent to the Barrio Veljo neighborhood, and along the western edge of the Armory Park neighborhoods. Both are residential neighborhoods.

f. Although there are areas within each of the adjacent neighborhoods with residential permit programs, there are no restricted parking areas within a block of the development property.

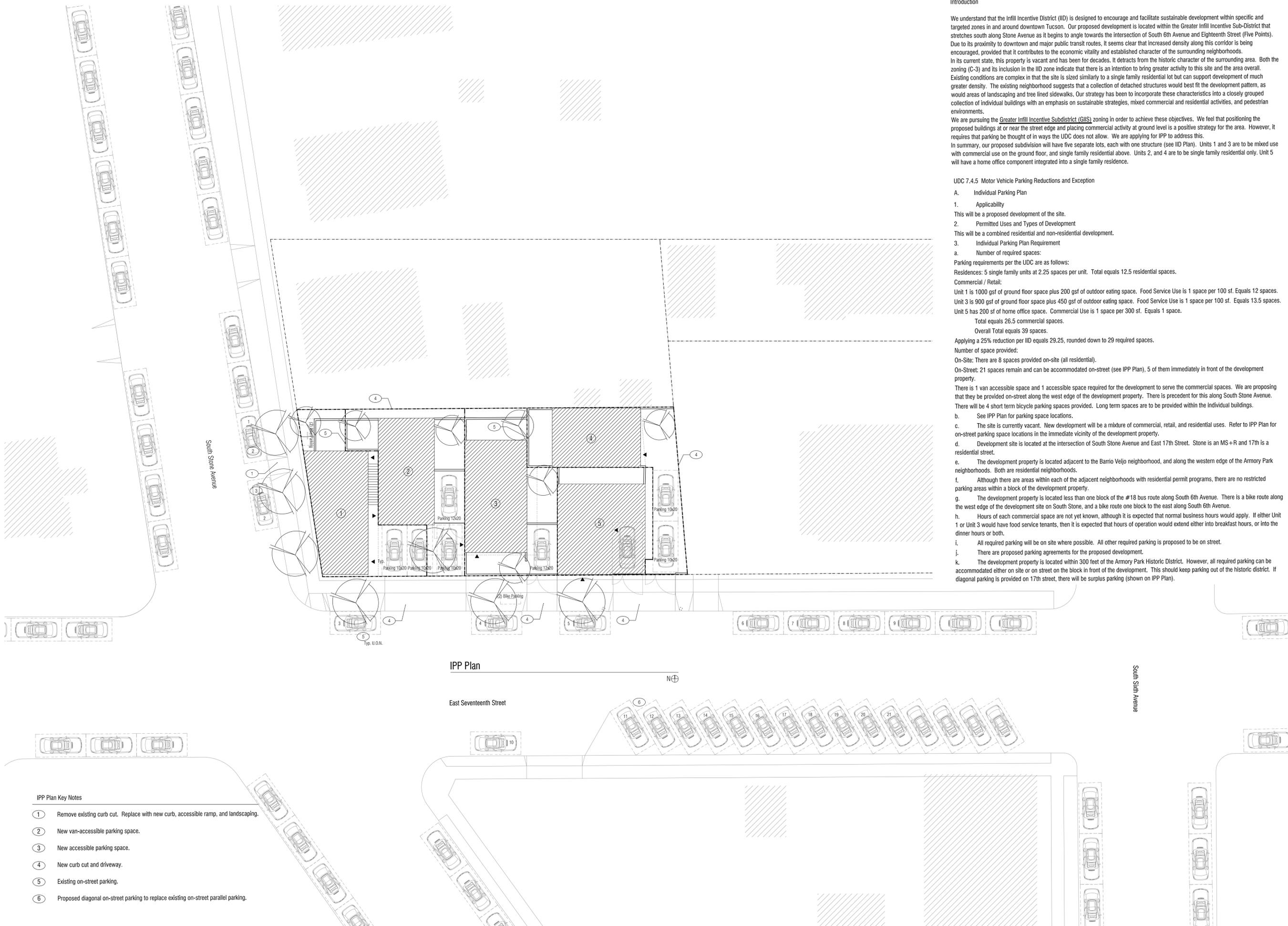
g. The development property is located less than one block of the #18 bus route along South 6th Avenue. There is a bike route along the west edge of the development site on South Stone, and a bike route one block to the east along South 6th Avenue.

h. Hours of each commercial space are not yet known, although it is expected that normal business hours would apply. If either Unit 1 or Unit 3 would have food service tenants, then it is expected that hours of operation would extend either into breakfast hours, or into the dinner hours or both.

i. All required parking will be on site where possible. All other required parking is proposed to be on street.

j. There are proposed parking agreements for the proposed development.

k. The development property is located within 300 feet of the Armory Park Historic District. However, all required parking can be accommodated either on site or on street on the block in front of the development. This should keep parking out of the historic district. If diagonal parking is provided on 17th street, there will be surplus parking (shown on IPP Plan).



IPP Plan

East Seventeenth Street

South Sixth Avenue

IPP Plan Key Notes

- 1 Remove existing curb cut. Replace with new curb, accessible ramp, and landscaping.
- 2 New van-accessible parking space.
- 3 New accessible parking space.
- 4 New curb cut and driveway.
- 5 Existing on-street parking.
- 6 Proposed diagonal on-street parking to replace existing on-street parallel parking.

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