



# BROADWAY VILLAGE

— EST. 1939 —



## Planned Area Development



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# Broadway Village Planned Area Development

Broadway Boulevard and Country Club Road  
Tucson, Arizona

*Submitted to:*

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- Appendix B: Architectural Design Guidelines
- Appendix C: Memorandum of Understanding Regarding Public Right-of-Way
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# Part 1: Introduction



## A. Background

Broadway Village is a timeless collaboration between local real estate developers, John and Helen Murphey, and Swiss architect, Josiah Joesler. Built in 1939, Broadway Village captures Spanish Mission Revival architecture in Arizona's first shopping center. Joesler's use of arches, breezeways, and patios elicit an outdoor lifestyle and Mexican antiquity that convey the romantic cultural heritage of Tucson. Joesler's prolific 30-year career with the Murphey's and their entrepreneurial legacy continue to define architectural standards in the Southwest.

John Murphey, as a young developer and visionary, and his wife Helen began creating building trends in Tucson in the early 1920's. Helen Murphey contributed greatly to the overall aesthetic image of their projects through her study of Mexican art and architecture. John went to California many times to solicit a Spanish Revival architect to be his company architect and finally recruited Josias Joesler in 1927, then working in Los Angeles.



*John & Helen Murphey*



*Josias & Nati Joesler*

Murphey-Keith Building Company supplied Joesler with 56 jobs in 1928 alone, initiating a patron-architect relationship that lasted for more than 30 years. Together they produced over 400 buildings in Tucson. Joesler's ability to speak fluent Spanish to primarily Mexican craftsmen led to pride in their work, as exhibited in the quality of their final products. He commanded great respect from everyone who worked with him, and he valued the work of his craftsmen. Joesler's work decided the stylistic imagery used to promote Tucson as a resort destination and subsequently America's romance with the Southwest.

Broadway Village Shopping Center was Joesler's most successful and enduring commercial building. The use of "village" in the title is



quite intentional, as Joesler tried to create the scale and character of a Mexican village. The Murphey's found the impetus for Broadway Village on a visit to Patzcuaro, a quaint village in Central Mexico. They decided to recreate the village square with Joesler's expertise, using its Spanish Colonial Revival forms, low-pitched clay tile roofs, arched openings, carved niches, arcades, stairways, mortar-washed brick, and colored ceramic tile. Together they crafted a romantic sense of place in Broadway Village for locals and visitors alike to admire and enjoy.



Although cherished for its iconic Spanish Revival architecture, Broadway Village was also a pioneering retail concept. In 1939, although far from downtown shops, its innovative plaza-style site plan with parking in the rear exposed its sculpted outdoor spaces to Broadway. With a genuine understanding of the retail experience, the Murphey-Joesler team designed Broadway Village to invite neighbors to shop and socialize throughout the scenic village plaza.



*Broadway Village, ca 1940*

Records also show that Broadway Village was the first shopping center in Arizona. Even more impressive, Broadway Village merged a village shopping center with a neighborhood grocery store. The vision of the Murphey-Joesler partnership created Arizona's first neighbor shopping center while capturing a timeless quality of antiquity.

In January 2008, CRI Broadway Village Partners, LLC (the "Owner") acquired Broadway Village from the Robert C. Murphey Trust. The Trust selected the Owner to purchase the historic shopping center based on its expertise as a retail specialist and its commitment to serve as a responsible steward for a City landmark. The Owner's vision is to restore the scenic beauty of Broadway Village, return it to the bustling retail hub reminiscent of the 1940's and preserve Joesler's iconic Spanish Mission Revival architecture for generations to come. Repositioning and enhancing Broadway Village will preserve the center for the neighborhoods of Broadmoor, Sam Hughes, Colonia Solana, and El Encanto, as well as all of Tucson's residences and visitors for many more decades.

## **B. Project Overview**

Broadway Village is situated on 2.68 acres at the southwest corner of Broadway Boulevard and Country Club Road (see *Exhibit I: Location Map*, page 5). Today there are 14 storefronts occupying over 37,000 square feet of retail space. The Owner has taken great care during the restoration process to return Broadway Village to its original look and feel. By updating and enhancing the shopping plaza's site, the Owner strives to make Broadway Village the successful retail hub that it once was for future generations to enjoy. In accordance with the Broadmoor-Broadway Village Neighborhood Plan, Broadway Village will once again provide a commercial anchor to the surrounding neighborhoods and continue to architecturally blend with the residential communities. Through its submittal of the PAD application, the Owner respectfully



requests City planning and zoning staff and the City Council to join the Owner's vision and plan to preserve a local landmark and to make it a viable retail enterprise.

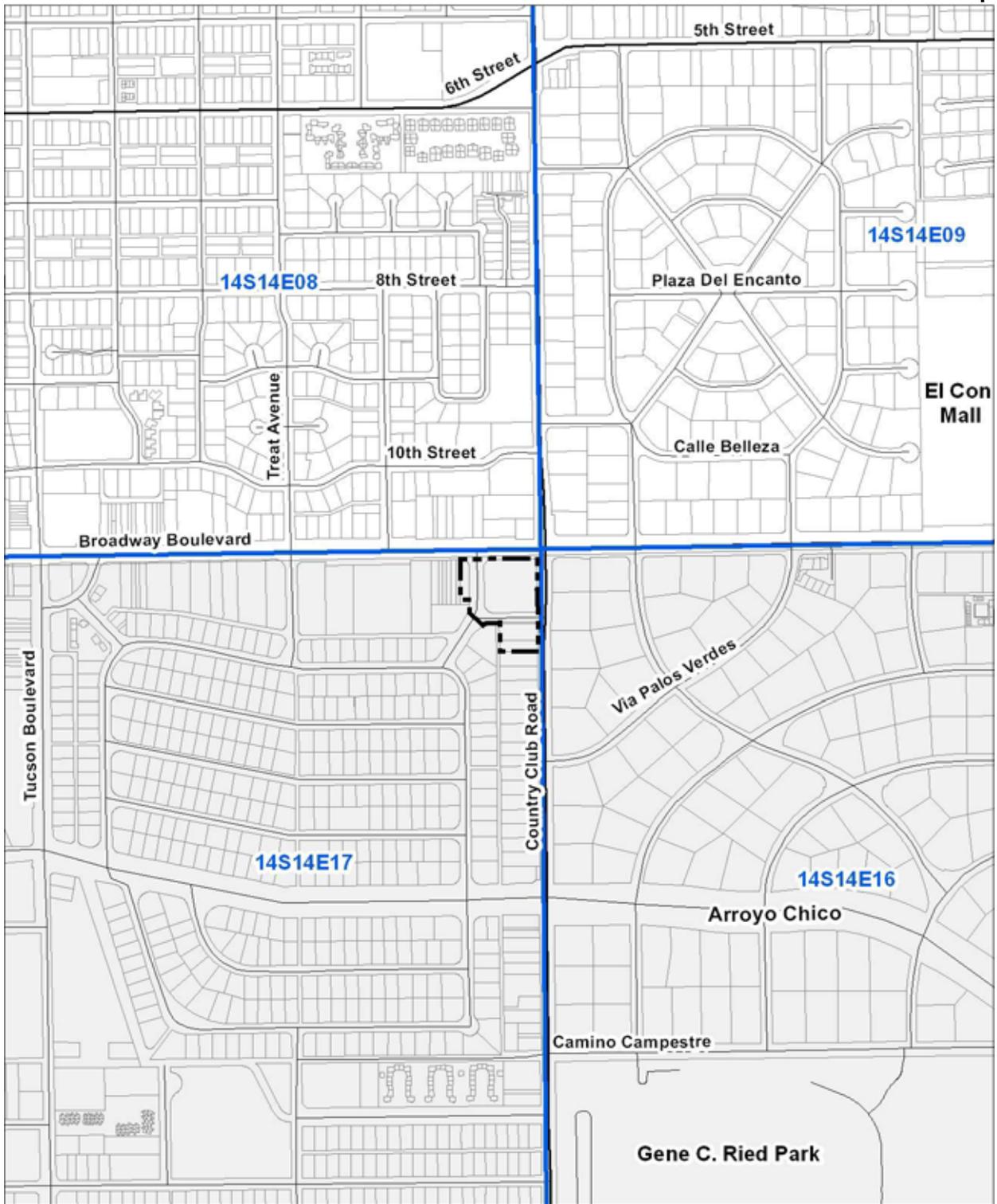
### **C. Rationale and Benefits for Use of a PAD**

The City of Tucson Planned Area Development (PAD) zoning designation is intended to be a vehicle to allow Broadway Village to renovate and expand its facilities, redesign parking and circulation, and provide more consistency with today's Land Use Code. Broadway Village is a historically established use since 1940 that does not fit neatly within any current zoning classification.

While the parcel acreage (under private ownership) totals 2.68 acres, the PAD boundary includes portions of the right-of-way, and thus, this PAD encompasses 3.68 acres. As Eastbourne Avenue and Manchester Street segregate the parcels that make up Broadway Village, including these rights-of-way in the master plan for this project allows for the cohesive design of circulation throughout Broadway Village.

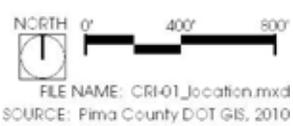


**Exhibit I: Location Map**



- LEGEND**
-  PAD Boundary
  -  Township, Range & Section
  -  Arroyo Chico Area Plan

**Notes:**  
 Project Site is located at:  
 Township 14S, Range 14E, and Section 17  
 PAD Acreage: Approx. 3.7 AC  
 Parcel ID #s:  
 129-02-0010, 129-02-0020, 129-02-0030,  
 129-02-0040 & 129-02-0510



## **D. Conformance with the General Plan and City Land Use Plans**

The proposed changes to Broadway Village are consistent with policy recommendations in the City's General Plan, the Broadmoor-Broadway Village Neighborhood Plan and the Arroyo Chico Area Plan. These plans emphasize compatible design and protection of the single-family residential character of the neighborhood.

### **1. City of Tucson General Plan**

#### **a. Shopping Center in Mid-City Growth Area**

The PAD is located within the Mid-City Growth area of the General Plan. The Mid-City Growth Area supports continued neighborhood investment and opportunities to support the rehabilitation of commercial nodes and centers. The goal of the Broadway Village PAD is to renovate and enhance the unique architectural and historical features of the Center to ensure its viability to attract new tenants and make it a desirable destination for the future.

#### **b. Access and Circulation**

In accordance with the Livable Tucson Goals, the alternatives to automobile transportation include improvement of public transportation, improvement of roadways (lighting, landscaping, sidewalks and bus stops), and promotion of alternatives to the automobile. The PAD redesign will improve:

- Accessibility of the existing bus stop on Country Club.
- Landscaping (including passive water harvesting), lighting and pedestrian accessibility will be incorporated in the PAD.
- Routes for walking and biking from adjacent residential areas to commercial services.

#### **c. Community Character and Design**

The community character and design element support infill and redevelopment projects that reflect sensitivity to the site and neighborhood conditions, and adhere to site and architectural design guidelines.

- The PAD supports the policies of the community character element of the General Plan, the Broadmoor-Broadway Village Neighborhood Plan and the Arroyo Chico Area Plan to assure single family residential architectural and design compatibility with adjacent residential uses.



**d. Cultural Heritage**

In accordance with the Livable Tucson Goals, the City General Plan supports the preservation and celebration of local landmarks, buildings and neighborhoods.

- Broadway Village is a local landmark that has been preserved for its location, architecture, design and Spanish Mission character.

**e. Economic Development**

The Economic Development element encourages preserving the special qualities and resources that contribute to Tucson's attractiveness and make the community unique.

- Broadway Village has historically integrated neighborhood commercial services with the surrounding neighborhood. The PAD furthers this historic relationship.
- Since the 1940's, Broadway Village has promoted the single family residential character of the neighborhood making it distinctive from other parts of Tucson.
- The PAD promotes the historic qualities of the development and enhances existing architectural and design qualities.

**2. Broadmoor-Broadway Village Neighborhood Plan**

The Broadmoor-Broadway Village Neighborhood Plan (BBVNP) was created to preserve the existing character of a one-quarter square mile area neighborhood. This area is bounded on the north by Broadway Boulevard, on the east by Country Club, on the west by Tucson Boulevard and on the south by Broadmoor subdivision boundary, south of Stratford Drive. The main land use within the interior boundaries of the neighborhood plan is single family residential. Closer to Broadway Boulevard, a major arterial, there are a few areas of apartments and duplex units, and along Broadway, there are a variety of commercial uses. Broadway Village provides a commercial anchor to this residential subdivision and architecturally blends in with residential structures.

The overall goal of the plan is to maintain a safe and enjoyable living environment that enhances the residential, historical and architectural elements of the area. Since the development of these areas, the neighborhood has maintained a stable and proud micro-community as a part of a larger Tucson community. The neighborhood promotes:

- Nurturance of old established large trees and drought resistant landscaping
- Absence of rezonings that would change the character of the area
- Continued enhancement of the Broadway and Country Club frontage that preserves the character of the neighborhood subject to strict height limitations, buffering and other constraints



More specifically, the Broadway Village PAD plans to conform with all the policies of the BBVNP; the major improvements include:

- Encourage new development on Country Club to be compatible with Southwestern architectural styles existing in the Broadway Village Shopping Center
- Encourage the integration of new office/commercial development compatible with adjacent existing or proposed office/commercial uses through shared access, common parking and maneuvering areas, shared on-site circulation, and shared refuse collection area
- Screen or enclose any outdoor storage areas or dumpsters from the view of all streets and adjacent properties
- Screen parking areas of more than four spaces from adjacent use and from the street with decorative walls or a dense screen of shrubs and canopy trees.
- Encourage consultation with immediate neighbors for the siting of buildings and additions to provide privacy, noise attenuation, and protection of view corridors for adjacent uses
- Require non-residential and high-density residential uses to provide access from a major street
- Building and site lighting shall be located, designed and oriented to prevent illumination or glare from extending to existing residential properties.

### **3. Arroyo Chico Area Plan**

The Arroyo Chico Area Plan adopted by Mayor and Council on March 24, 1986 provides policy direction for a 4.3 square mile area in the central portion of the Tucson Metro Area. The boundaries of the plan area include the subject property with Broadway Boulevard to the north, Alvernon Way on the east and Aviation Highway, Southern Pacific Railroad, Fairland Stravenue, and 36th Street to the south/southwest.

The Arroyo Chico Area Plan goals include providing for the commercial needs of the area by encouraging commercial development at appropriate locations. The Broadway Village PAD is an existing commercial amenity that needs renovation to maintain the viability of this historic landmark and improve the access and circulation of the development.



## **E. Compatibility with Adjoining Land Uses**

Special care will be taken to ensure that existing and new development within the Broadway Village PAD is compatible with the surrounding neighborhoods and the existing historic and architectural character of the neighborhoods. As stated above, adjacent residents in the Broadmoor-Broadway Village Neighborhood are proud of the single family residential character of their neighborhood and of Broadway Village as a source of cultural identity.





## Part 2: Site Analysis



## A. Existing On-Site Development

Broadway Village currently features a number of commercial and retail services, including restaurant space, personal services (such as hair and nail salons), and specialty retail stores for clothing, home accessories and miscellaneous merchandise. Several spaces within the center are currently vacant. In addition, an outdoor farmers market is held weekly at Broadway Village.

The shopping center consists of three commercial structures, associated parking, courtyards, covered walkways, loading area, pedestrian ways and two public rights-of-way, Eastbourne Avenue and Manchester Street, which are City of Tucson maintained roadways. The existing conditions of the development are depicted in *Exhibit II.A: Existing Development*, page 13.

*Table II.A: Building Inventory* provides a description of each of these buildings.

**Table II.A: Building Inventory**

Existing Building	Current Square Footage	Maximum Building Height (feet)	Building Setback
Building A	7,182	15-18	49 feet from north property line
Building B	18,165	12-22	10 feet from north property line
Building C	11,856	14-17	12 feet from north property line 0 feet from east property line

### 1. Pedestrian Ways

The sidewalk surrounding Building B and C at the corner of Broadway and Country Club is continuous with pavers along the north, west, and south side of the buildings. The east side of the building along Country Club is a recently repaved 6-foot colored concrete sidewalk. The small linear commercial structure (Building A) west of Eastbourne Avenue, does not have any sidewalks along Broadway Boulevard but there are sidewalks along the north and east side of the building. The west side of the building contains a party wall agreement with the commercial strip center directly to the west of the PAD.



## 2. Existing Loading and Parking

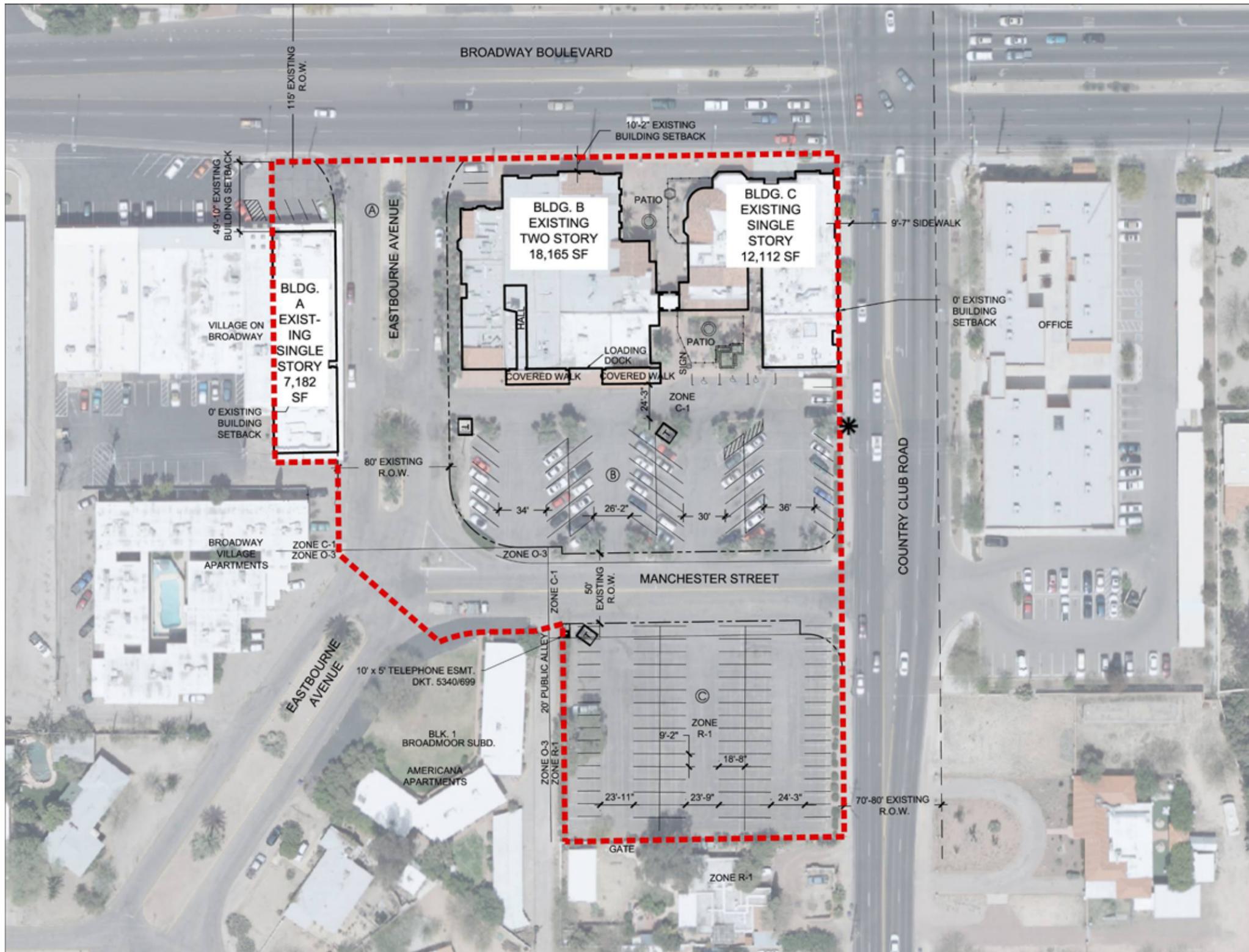
The existing loading dock is located within the interior of the south end of Building B.

There are a total of 177 vehicular parking spaces located within the PAD boundary. Parking is located in three general areas:

- **Parking Area A** includes 4 spaces on the north side of Building A, 3 spaces northwest of Building B (accessed from Eastbourne Avenue) and 10 parallel parking spaces within the Eastbourne Avenue right-of-way. There are no accessible spaces located within Parking Area A.
- **Parking Area B**, located south of Buildings B and C, north of Manchester Street and east of Eastbourne Avenue contains 64 spaces, 3 of which are accessible spaces.
- **Parking Area C**, which encompasses the area south of Manchester Street, has a total of 96 parking spaces. There are no accessible spaces located within Parking Area C.



Exhibit II.A: Existing Development



Legend

- IPAD Boundary
- Existing Building
- ✱ Bus Stop
- T Trash Dumpster
- ♿ Handicap Parking Space

Notes

PAD Area: 160,473 S.F. (3.68 acres)  
 Existing Building Square Footage: 37,203  
 Existing Zoning: C-1 and R-1  
 Maximum Building Height: 30'

Existing Parking Spaces: 167 Spaces  
 (A) 17 Spaces (No Accessible Spaces)  
 (B) 64 Spaces (3 Accessible Spaces)  
 (C) 96 Spaces (No Accessible Spaces)



FILE NAME: CR1-01-EXHIBITS (ALL 8.5X11).DWG / EX-Cond



## B. Existing Off-Site Development

Broadway Village is surrounded by a mixture of single- and multi-family residential uses, commercial uses and office uses, as shown in *Exhibit II.A: Existing Development*, page 13. Commercial and office uses are located adjacent to Country Club Road and Broadway Boulevard. Single-family homes located west of Country Club Road are on lots ranging in size from approximately 8,000 square feet to 11,000 square feet. Single-family homes located east of Country Club Road are on larger lots ranging in size from one-half acre to acre lots.

**Table II.B: Existing Land Uses**

<b>PAD District</b>	3 one- and two-story commercial buildings
<b>North</b>	Commercial, Financial Institution, Church
<b>South</b>	Single Family Residential
<b>East</b>	Large Lot Single Family Residential, Office
<b>West</b>	Commercial, Single and Multi Family Residential

(See *Exhibit II.B: Existing Off-Development*, page 15.)

## C. Zoning

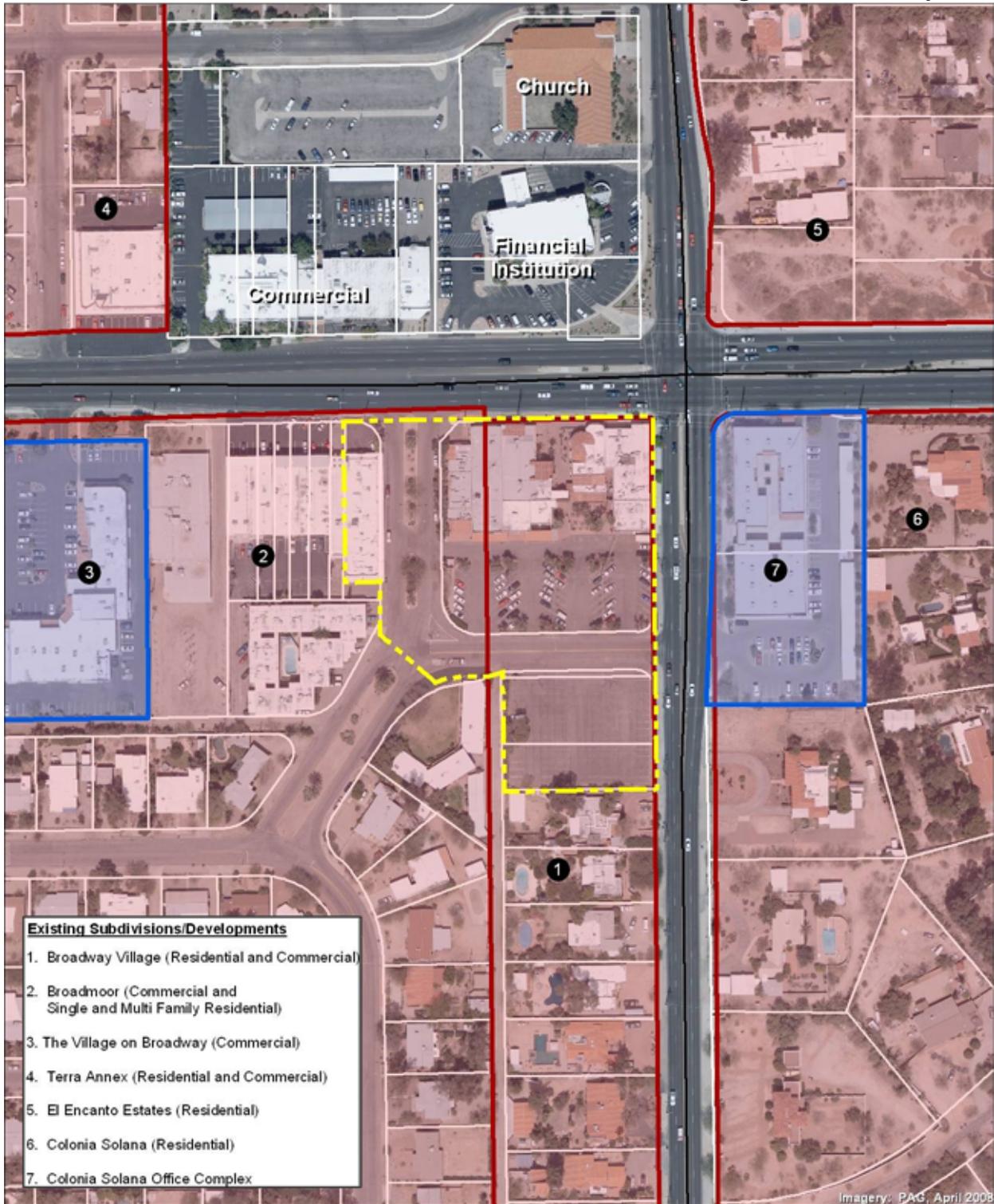
**Table II.C: Existing Zoning**

<b>PAD District</b>	C-1 (Commercial Zone) O-3 (Office Zone) and R-1 (Residence Zone)
<b>North</b>	C-1 (Commercial Zone) , R-2 (Residence Zone)
<b>South</b>	R-1 (Residence Zone), O-3 (Office Zone)
<b>East</b>	O-2 (Office Zone) and RX-1 (Residence Zone)
<b>West</b>	C-1 (Commercial Zone), O-3 (Office Zone)

(See *Exhibit II.C: Zoning*, page 16.)

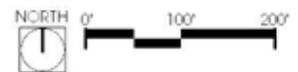


**Exhibit II.B: Existing Off-Site Development**



**LEGEND**

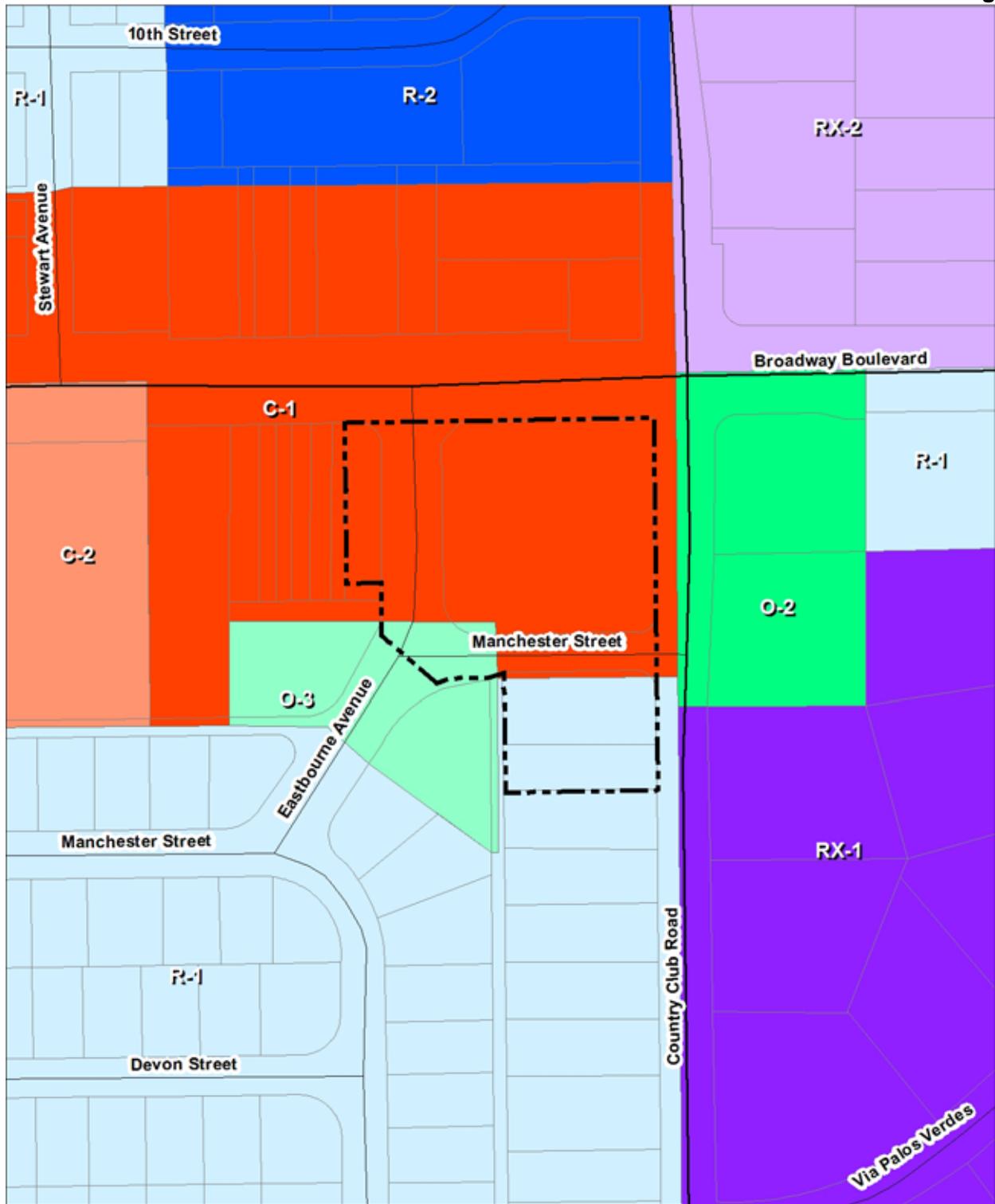
-  PAD Boundary
-  Approved Subdivision Plat
-  Approved Development Plan



FILE NAME: CRI-01\_existinglanduses.mxd  
SOURCE: Pima County DOT GIS, 2010



Exhibit II.C: Zoning



LEGEND		City of Tucson Zoning	
	PAD Boundary		R-1
	R-2		O-2
	RX-1		O-3
	RX-2		C-1
			C-2

NORTH 0 100' 200'
   
 FILE NAME: CRI-01\_zoning.mxd
   
 SOURCE: Pima County DOT GIS, 2010



## **D. Community Facilities**

### **1. Fire Stations**

The nearest fire station, station #3, is located within the Tucson Fire Department District at 24 North Norris, approximately one mile west of the project PAD District (see *Exhibit II.D: Community Facilities*, page 18).

### **2. Police Stations**

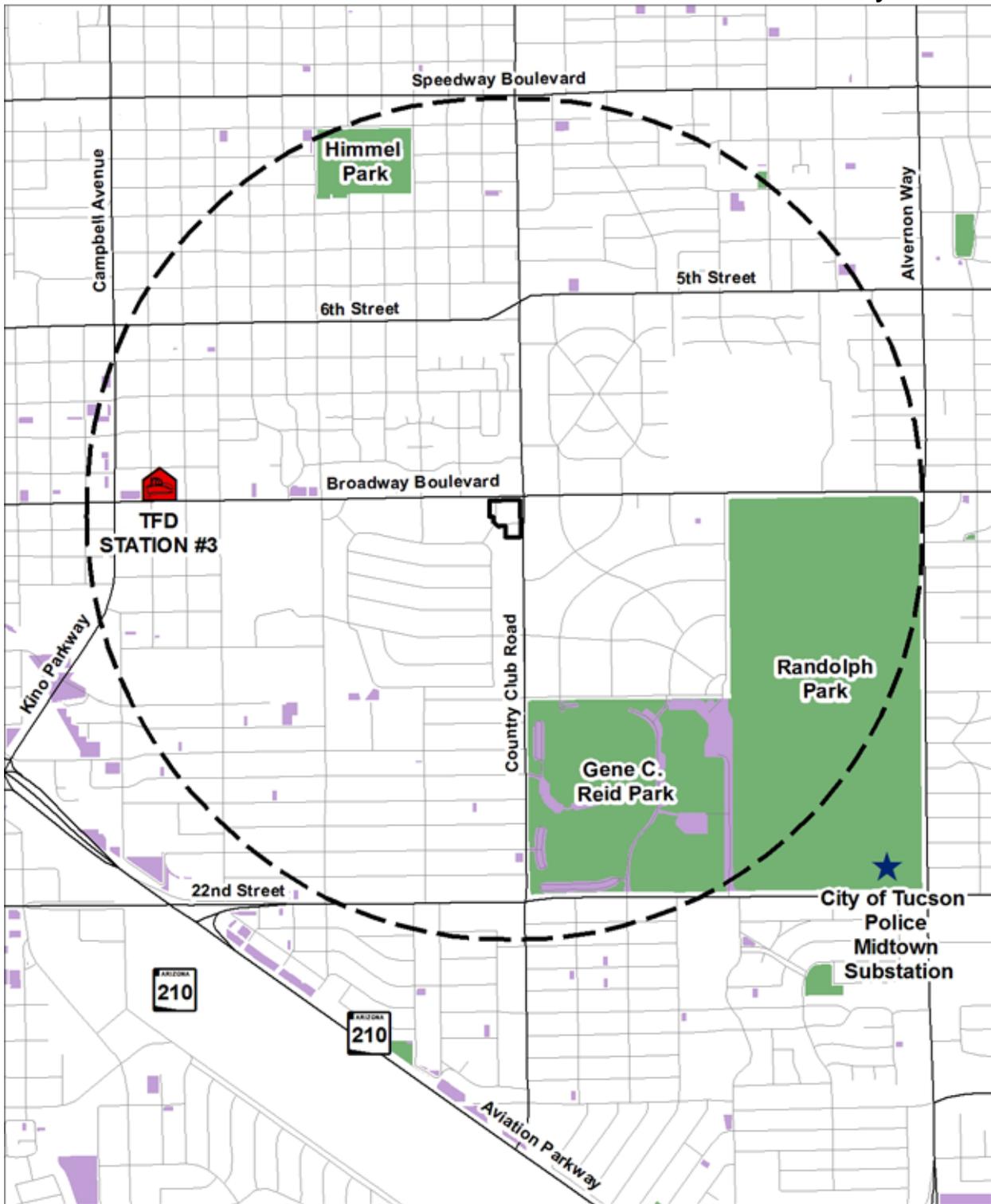
There is one police station located within the vicinity of the PAD. The Midtown Substation is located at 1100 South Alvernon Way, approximately 1.25 miles southeast of the PAD.

### **3. Hospitals**

There are no hospitals within one mile of the PAD District.



Exhibit II.D: Community Facilities



LEGEND

- PAD Boundary
- One-Mile Radius
- 🏠 Fire Station
- ★ Police Stations
- Existing Parks
- City of Tucson Owned Parcel



FILE NAME: CRI-01\_public\_facilities.mxd  
SOURCE: Pima County DOT GIS, 2010



## E. Existing Transportation and Circulation

A Traffic Impact Analysis (TIA) prepared by Kimley-Horn and Associates (submitted under separate cover) describes the existing traffic circulation patterns affecting the PAD.

### 1. Existing Streets Serving the PAD

The Broadway Village PAD is bordered by Broadway Boulevard on the north and Country Club to the east. Two local streets, Eastbourne Avenue and Manchester Street run through the project providing access to Broadway Village and other residential developments to the south and west of the site. Attributes of the roadways are summarized in *Table II.E: Roadway Characteristics*, page 20 and *Exhibit II.E: Traffic*, page 21 shows surrounding roadways within a one-mile context.

### 2. Current and Future Rights-of-Way

According the Major Streets and Routes (MS&R) Map, Broadway Boulevard and Country Club are Major Streets and Routes. Broadway Boulevard has a current right-of-way of 120 feet and future 150-foot right-of-way adjacent to the project PAD District. Country Club Road's current right-of-way varies 70-80 feet, and its future right-of-way is 100 feet. Eastbourne Avenue has an existing right-of-way of 80 feet. Manchester Street has an existing right-of-way of 50 feet.

### 3. Access Points

There are three vehicular access points from public roadways into the PAD District, one along Country Club at Manchester Street, another along Broadway Boulevard at Eastbourne Avenue, and the third entry point is from the residential neighborhood to the south via Eastbourne Avenue.

### 4. Program for Street Improvements

The City of Tucson has an ongoing project, Broadway Boulevard Corridor, which will result in the widening of Broadway Boulevard from Euclid Avenue to Country Club Road. The project is funded through the Regional Transportation Plan and Pima County Bonds at approximately \$71 million. While the design is still underway, preliminary information indicates that widening will result in an 8-lane roadway with one lane in each direction dedicated to transit. Right-of-way plans indicate that the alignment will shift to the north and the curb location along Broadway Village will not be significantly affected. Construction is not anticipated until 2014 or later. There are no scheduled improvements for Country Club in the vicinity of the PAD District.

### 5. Alternate Modes of Transportation

SunTran operates five bus routes within a one-mile radius of the PAD. There are two routes along Broadway, one route along Country Club, one route along 6<sup>th</sup> Street and the Campbell route. There is one existing bus stop near the southwest



corner of Country Club Road and Broadway Boulevard, just south of Building C (see *Exhibit II.A: Existing Development*, page 13).

## 6. Bicycle Routes and Bicycle Parking

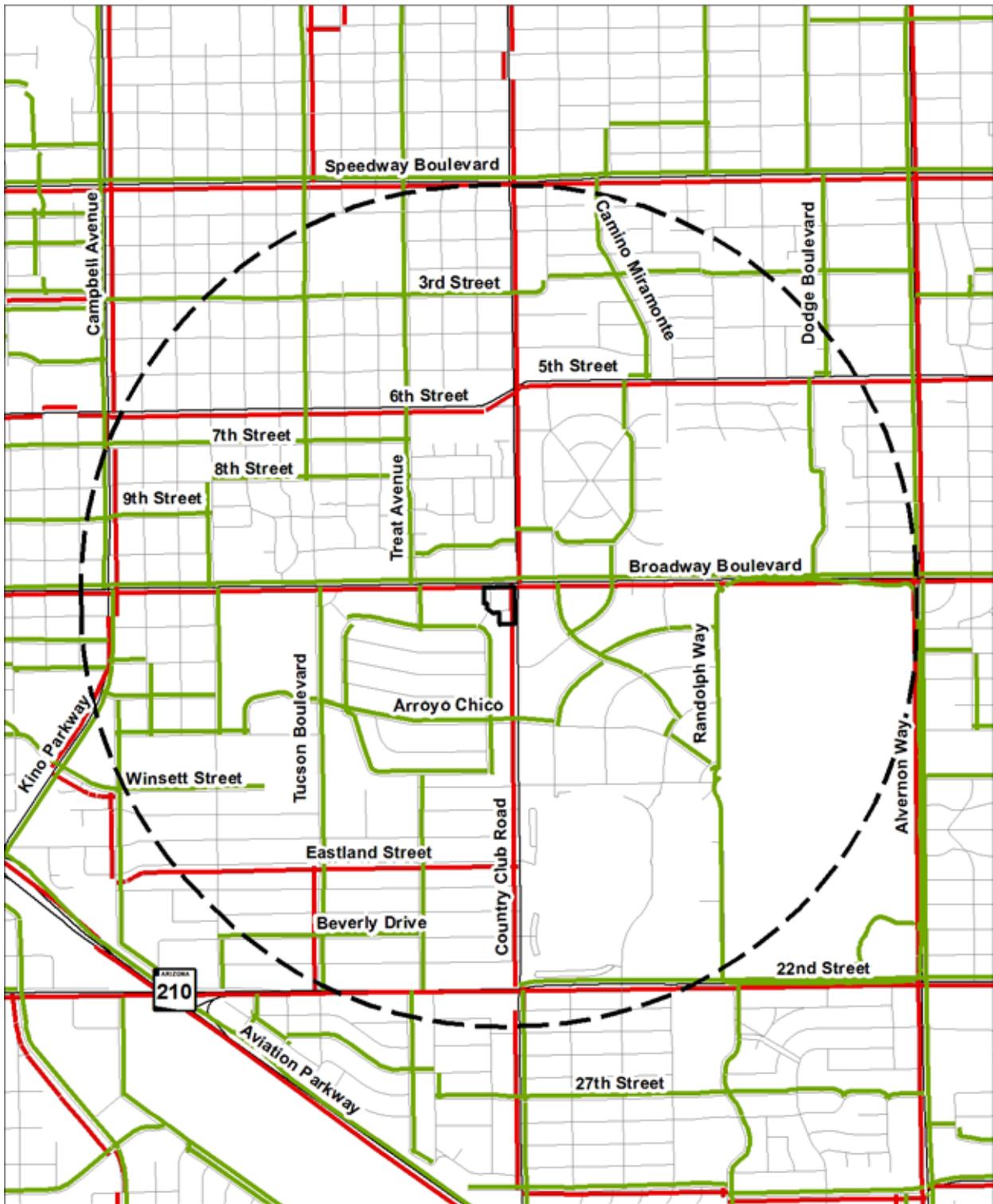
There are several bicycle routes within a one-mile radius serving the PAD District providing alternative access. Existing bicycle routes within a one-mile radius are displayed on *Exhibit II.E: Traffic*, page 21. Broadway Boulevard has a bike lane with a striped shoulder. Country Club does not have a bike lane. There is no existing bicycle parking located on the PAD District.

**Table II.E: Roadway Characteristics**

Roadway	Broadway Boulevard	Country Club Road	Eastbourne Avenue	Manchester Street
<b>Functional Class</b>	Urban Principal Arterial	Urban Minor Arterial	Local Street	Local Street
<b># Lanes</b>	5	4	2	2
<b>Divided</b>	No	No	Yes	No
<b>Bike Route</b>	Yes	No	No	No
<b>Bus Route</b>	Yes	Yes	No	No
<b>Curb &amp; Gutter</b>	Yes	Yes	Yes	Yes
<b>Sidewalks (continuous or intermittent)</b>	Yes (intermittent)	Yes	No	No
<b>Paved</b>	Yes	Yes	Yes	Yes
<b>Average Daily Trips (PAG, 2004)</b>	43,000	17,000	N/A	N/A
<b>Speed Limit</b>	35	35	25	25
<b>Sun Tran Route</b>	Yes (Route #8- Broadway/6 <sup>th</sup> Ave and Route #108X Broadway Express)	Yes (Route #17 Country Club/29 <sup>th</sup> )	No	No
<b>Ownership</b>	City of Tucson	City of Tucson	City of Tucson	City of Tucson

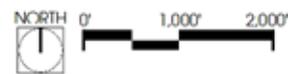


Exhibit II.E: Traffic



LEGEND

-  PAD Boundary
-  One-Mile Radius
-  Bicycle Routes
-  Bus Routes (Sun Tran)



FILE NAME: CRI-01\_traffic.mxd  
SOURCE: Pima County DOT GIS, 2010



## F. Existing Hydrology

### 1. FEMA

Per the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map (FIRM) No. 004019C2231K, effective date: February 8, 1999, the project site is located within an unshaded Zone X area, defined as an area outside of the 500-year event floodplain. No local floodplains or flood hazards are located within the project site.

### 2. Tucson Stormwater Management Study (TSMS)

As identified in the Tucson Stormwater Management Study (TSMS Report – 1995) the major watershed that impacts the parcel is the Tucson Arroyo Watershed. There are no TSMS nodes present on or near the site.

### 3. Pre-Development Hydrologic Conditions

The parcel is currently highly developed with structures and paved parking surfaces with very little natural, undisturbed area. The existing parcel has an average slope of 1.0%. No major washes run through the property.

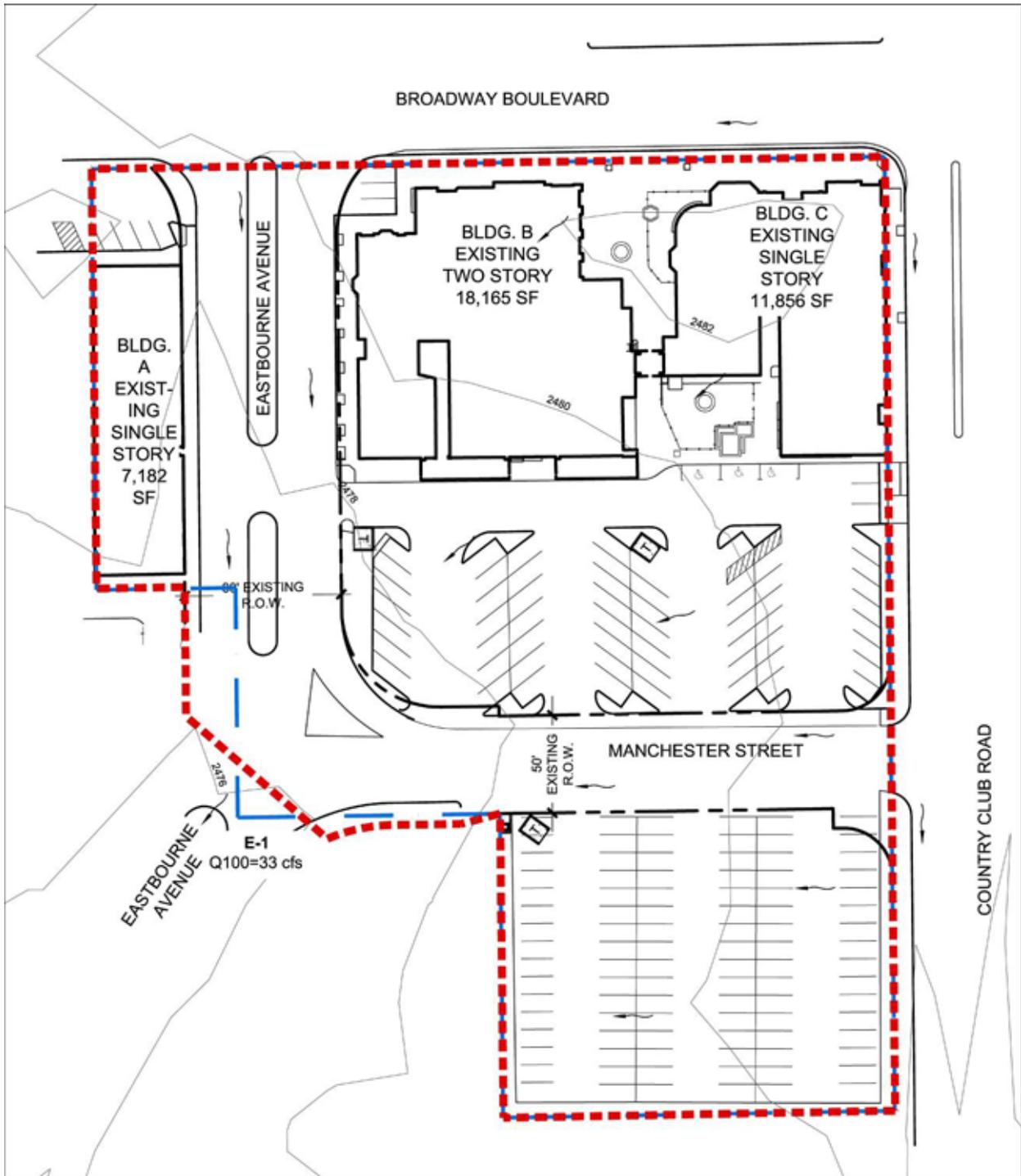
Onsite existing hydrologic conditions calculations have been provided in the Broadway Village Drainage Statement, which has been submitted under a separate cover. Existing sub-basins, areas, and discharges for the 2-, 10-, and 100-year discharge is shown on *Exhibit II.F: Existing Hydrology*, page 23. *Table II.F* also summarizes onsite existing discharges.

**Table II.F: Summary of Onsite Existing Discharges**

Sub-basin ID	Area (Acres)	2-Year Discharge (cfs)	10-Year Discharge (cfs)	100-Year Discharge (cfs)
E1	3.61	3.3	12.3	33.3



Exhibit II.F: Existing Hydrology



Legend

- IPAD Boundary
- 2' Elevation Contours
- Existing Building
- Flow Arrow
- Subbasin Limits



## **G. Views**

The proposed PAD District is located in a developed area, surrounded by single-family residential uses, offices, churches, a financial institution and other commercial uses fronting Broadway Boulevard. Currently, Broadway Village buildings vary in height from one to two stories. The project site is visible from all sides of the property. Photos of the project site are shown in *Exhibit II.G.2: Photos*, page 26. *Exhibit II.G.1: Photo Key Map*, page 25 indicates the locations from which the photos were taken.

### **1. Views of the PAD District**

All existing commercial structures are highly visible along Broadway Boulevard. Building C and the adjacent parking area is highly visible from Country Club Road and Manchester Street. The interior of the property is also highly visible from the public rights-of-way (Manchester Street and Eastbourne Avenue) supplying access to the property.

### **2. Views from the PAD District**

The PAD district is located in a developed area of the city surrounded by one and two-story structures. Views across the PAD district of surrounding mountain ranges are predominantly obstructed.

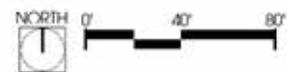


Exhibit II.G.1: Photo Key Map



**LEGEND**

-  PAD Boundary
-  Photo ID & location photo was taken



FILE NAME: CRI-01\_photokeymap.mxd  
SOURCE: Pima County DOT GIS, 2010



**Exhibit II.G.2: PAD District Photos**



Photo 1: Looking south toward Building A from the south side of Broadway Boulevard.



Photo 2: Looking east from Eastbourne Avenue toward the fronts of Building B and C.



Photo 3: A photo looking toward the courtyard on the south side of Buildings B and C.



Photo 4: Looking east toward the interior loading dock and storefronts on the south side of Building B.



Photo 5: A photo looking from Manchester Avenue towards the main parking lot behind buildings B and C.



Photo 6: Looking north along Eastbourne Avenue and towards building A.



# Part 3: PAD District Proposal

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## **A. Permitted and Excluded Uses**

The Broadway Village PAD shall include all those uses permitted by LUC Section 2.5.3 under C-1 zoning. Special Exception Land Uses, as provided for in C-1, shall be subject to the Zoning Examiner Full Notice Procedure.

A Conceptual Site Plan has been provided as *Exhibit III.A: Conceptual Site Plan*, page 30, which identifies existing and future development within the Broadway Village PAD. Existing and future development within the PAD shall conform to the regulations and standards set forth in this PAD. Where these regulations and standards vary from the LUC or other City standards, the PAD regulations and standards shall control.

### **1. Permitted Uses**

In addition to Permitted uses in C-1, the following shall be permitted in the Broadway Village PAD:

Commercial Services Use Group:

- Alcoholic Beverage Service
- Communications, limited to wireless communication antennae, provided:
  - a. The antennae are mounted on the wall or roof of a building and concealed within an architectural or structural element of the building.
  - b. The antennae and architectural or structural element, are architecturally and/or environmentally compatible with the building and general area.

Industrial Use Group:

- Craftwork
- Perishable Goods Manufacturing

Provisions of Section 2.5.3.6.B of the City of Tucson Land Use Code (General Restrictions) shall not apply to this PAD. Outdoor activities, including but not limited to patio dining, merchandise displays and farmer's markets, shall be permitted.



**2. Secondary Uses**

Land uses secondary to the Permitted Land Uses are permitted within the PAD, subject to the requirements of LUC Section 3.2.4 and to any additional requirements listed with the use. Secondary uses are limited to those identified in the C-1 Zone of the Land Use Code.

**3. Accessory Land Uses**

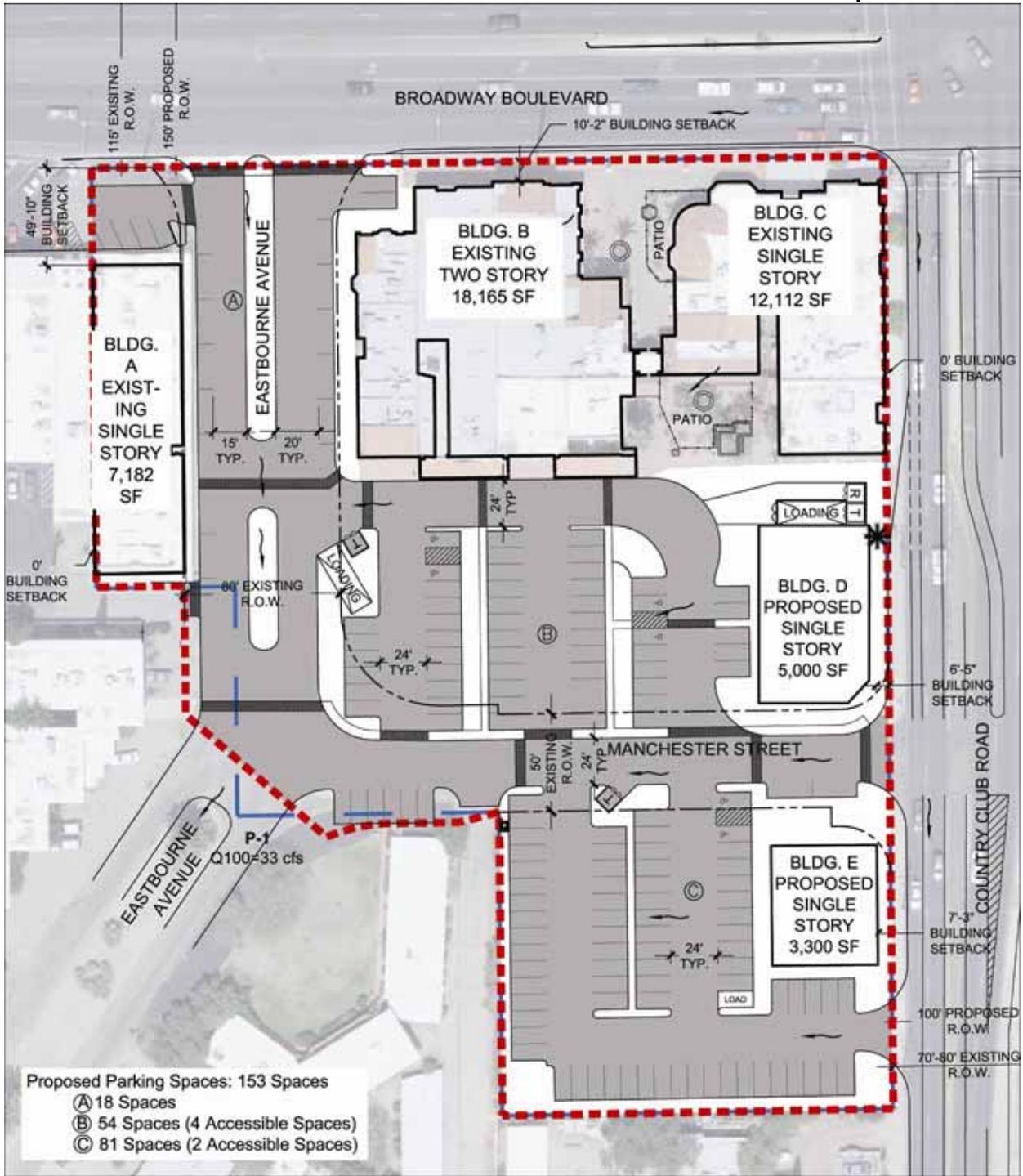
Land uses accessory to the Permitted or Secondary Land Uses are allowed within the PAD, subject to compliance of LUC Section 3.2.5.

**4. Excluded Land Uses**

Land uses not listed as a Primary or Secondary use, or land uses that are not an accessory to the Primary or Secondary use, are prohibited within the Broadway Village PAD.



Exhibit III.A: Conceptual Site Plan

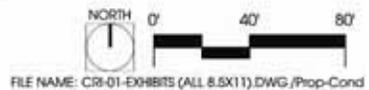


Legend

- - - PAD Boundary
- Flow Arrow
- Subbasin Limits
- Bus Shelter
- Existing Right of Way
- Trash Dumpster
- Handicap Space

Notes

PAD Area: 160,473 S.F. (3.68 acres)  
 Proposed Building Square Footage: 45,759  
 Maximum Proposed Building Height: 30'



## B. Development Standards

The Broadway Village PAD seeks to conform to the plan goals and policies established in the Arroyo Chico Area Plan and Broadmoor-Broadway Village Neighborhood Plan. In order to achieve those goals, the PAD will provide appropriate transitioning to surrounding development through the use of development standards. The following standards apply to the development of buildings, landscape borders, vehicle use areas and buffering for all permitted uses within the PAD. The entire site will meet the PAD criteria by the completion of the last new building.

For the purpose of setback and buffer requirements and other similar development standards that would otherwise apply to separately owned lots or parcels under the LUC and City Development Standards, the PAD shall be considered as a single parcel. All new development within the PAD shall conform to applicable building, fire and other life safety standards. Prior to development plan approval, the lots south of Manchester Street will be combined via the appropriate City of Tucson process.

These standards will supersede the standards in the Land Use Code (including but not limited to Article 3. Development Regulations, Division 2. Development Criteria and Division 5. Performance Criteria) in accordance with Section 2.6.3 of the Land Use Code, except where specific references to such standards are provided in this section of the document.

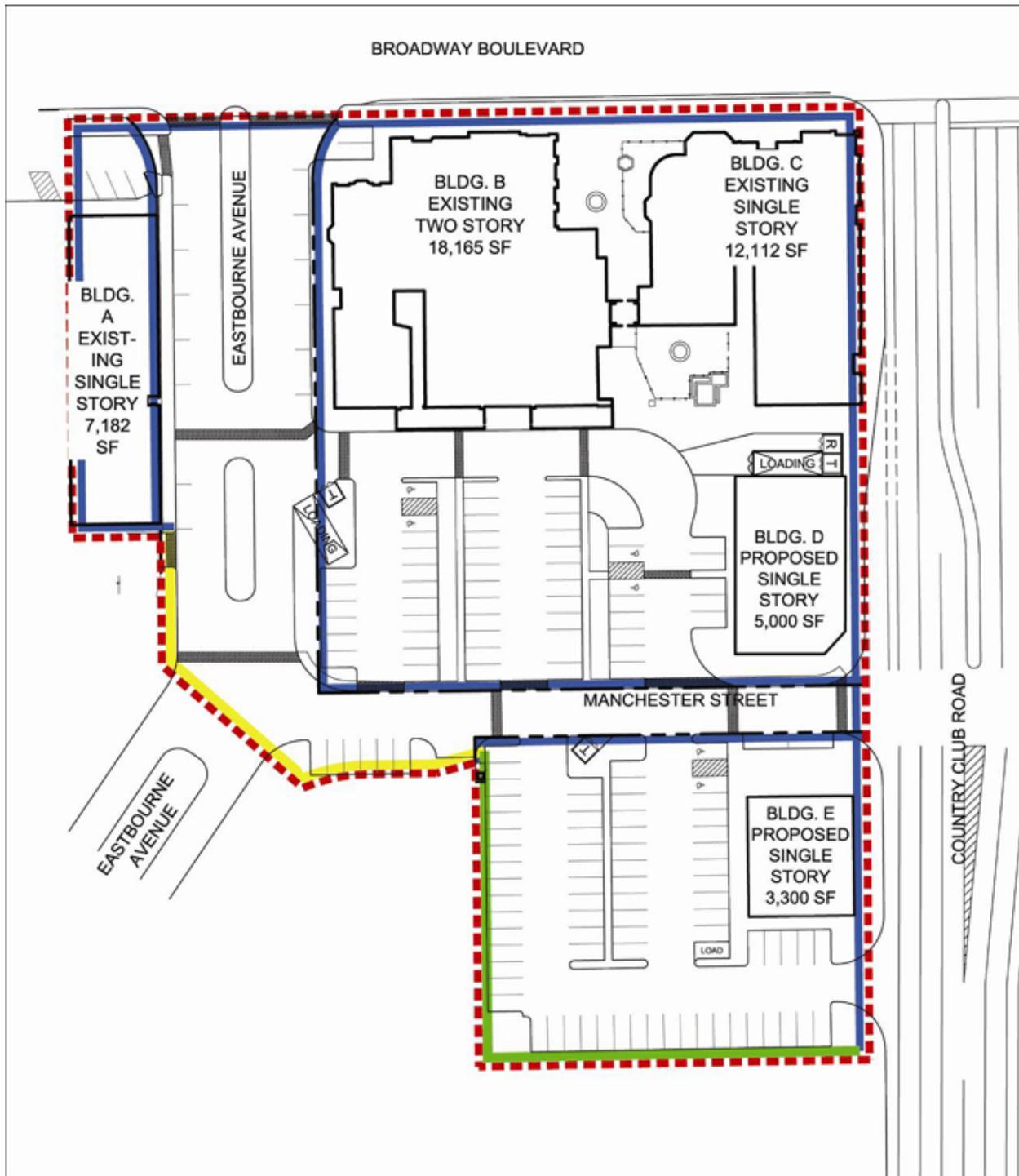
### 1. PAD District Development Standards

**Table III.B: Development Standards**

<b>Minimum Site Area</b>	None
<b>Maximum Lot Coverage</b>	None
<b>Maximum Floor Area Ratio</b>	0.75
<b>Maximum Building Height</b>	30 feet
<b>Minimum Building Separation</b>	Per Building and Fire Codes
<b>Minimum Perimeter Setbacks</b>	See <i>Exhibit III.B: Perimeter Setbacks</i> , page 32
<b>Minimum Setback from Public Rights-of-Way (Broadway, Country Club, Eastbourne &amp; Manchester)</b>	None
<b>Minimum PAAL Width</b>	20 feet



Exhibit III.B: Perimeter Setbacks



- Legend**
- PAD Boundary
  - Building Footprint
  - 0' Perimeter Setback
  - 15' Perimeter Setback
  - 40' Perimeter Setback



## **2. Vehicular Use Areas and Parking**

The Broadway Village PAD will provide one vehicular parking space per 300 square feet of building space. Vehicular parking spaces are not required for patio square footage, such as outdoor seating, farmers markets and other outdoor retail displays.

Vehicle use areas shall be constructed utilizing materials and construction techniques in accordance with recommendations of the geotechnical engineer and concurrence from City of Tucson staff. The minimum PAAL width within the PAD shall be 20 feet.

Handicapped parking will be provided in accordance with ADA requirements from the 2006 IDC, Chapter 11 and ICC/ANSI 117.1, 2003 Edition. Accessible spaces and “Van Accessible” spaces will connect to the accessible route as required by the 2006 IDC, Chapter 11 and ICC/ANSI 117.1, 2003 Edition. Newly constructed and modified sidewalks, detectable warnings and curb ramps will comply with accessibility requirements as required. The entire circulation system will meet these requirements by the issuance of the last Certificate of Occupancy for the last new building to be built on-site.

Cross-access and cross-parking agreements with adjacent commercial property owner(s) west of the PAD will be recorded prior to approval of the PAD.

Vehicular perpendicular parking and maneuvering may occur in public rights-of-way (Eastbourne Avenue and Manchester Street) within the PAD boundaries, subject to the execution of a maintenance agreement with the City of Tucson. Existing parallel parking along Eastbourne Avenue and Manchester Street may remain.

## **3. Bicycle Parking**

A minimum of five (5) bicycle parking spaces (Class 2) shall be provided for each existing and proposed building.

## **4. Loading Zones and Solid Waste Disposal**

Two 12 x 35 foot loading zones, one 8.5 x 18 foot loading zone and three trash dumpsters shall be provided within this PAD. The two 12 x 35 foot loading zones and trash dumpsters will be screened by masonry walls and are intended to blend into Broadway Village development.

Loading zones and trash dumpsters are located in close proximity to existing and proposed buildings and are accessible from Broadway Boulevard and Country Club Road. Loading zones, trash receptacles and recycling areas may have shared access within the PAD. In addition, access and maneuvering for loading and trash trucks may occur within the public rights-of-way within the PAD boundaries, subject to the execution of a maintenance agreement with the City of



Tucson. Dumpsters shall be located a minimum of 50 feet from existing residential structures.

## **5. Wastewater**

The owner/developer shall obtain written documentation from the Pima County Regional Wastewater Reclamation Department (PCRWRD) that treatment and conveyance capacity is available for any development within the rezoning area, no more than 90 days before submitting any tentative plat, development plan, sewer improvement plan or request for building permit for review. Should treatment and/or conveyance capacity not be available at that time, the owner/development shall have the option of funding, designing and constructing the necessary improvements to Pima County's public sewerage system at this or her sole expense or cooperatively with other affected parties. All such improvements shall be designed and constructed as directed by the PCRWRD.

## **C. Water Conservation Standards**

Conservation standards will be accomplished via four areas of emphasis – low water use plants, efficient irrigation, passive rainwater harvesting and historic landscape palette.

### **1. Low Water Use Plants**

The plant palette at Broadway Village will consist of predominately low water use, native and regionally adapted plants. (See *Appendix A: Broadway Village Plant Palette*.) The plants will be located relative to their functionality and the uses associated with the zones within which they are planted. The use of low water use plants in locations appropriate with their species characteristics provides for the conservation of potable water while assuring the survivability and long term health of such plant material.

### **2. Rainwater Harvesting**

Passive rainwater harvesting will be incorporated throughout Broadway Village. A water harvesting plan shall be included with the development plan submittal in compliance with Development Standards 10-03 illustrating 50 percent of estimated landscape water budget is met by water harvesting techniques.

There will be a net increase of pervious and semi-pervious areas within the PAD of 17,876 sq. ft. (see *Exhibit III.C.1: Existing Pervious and Impervious Conditions*, page 36 and *Exhibit III.C.2: Post-Development Rainwater Harvesting Concept*, page 37). A number of techniques will be employed to direct and capture rainfall for the benefit of the landscape: curb cuts, flush curbs, recessed planting areas, minimized compaction of planting areas, and semi-pervious pavers.

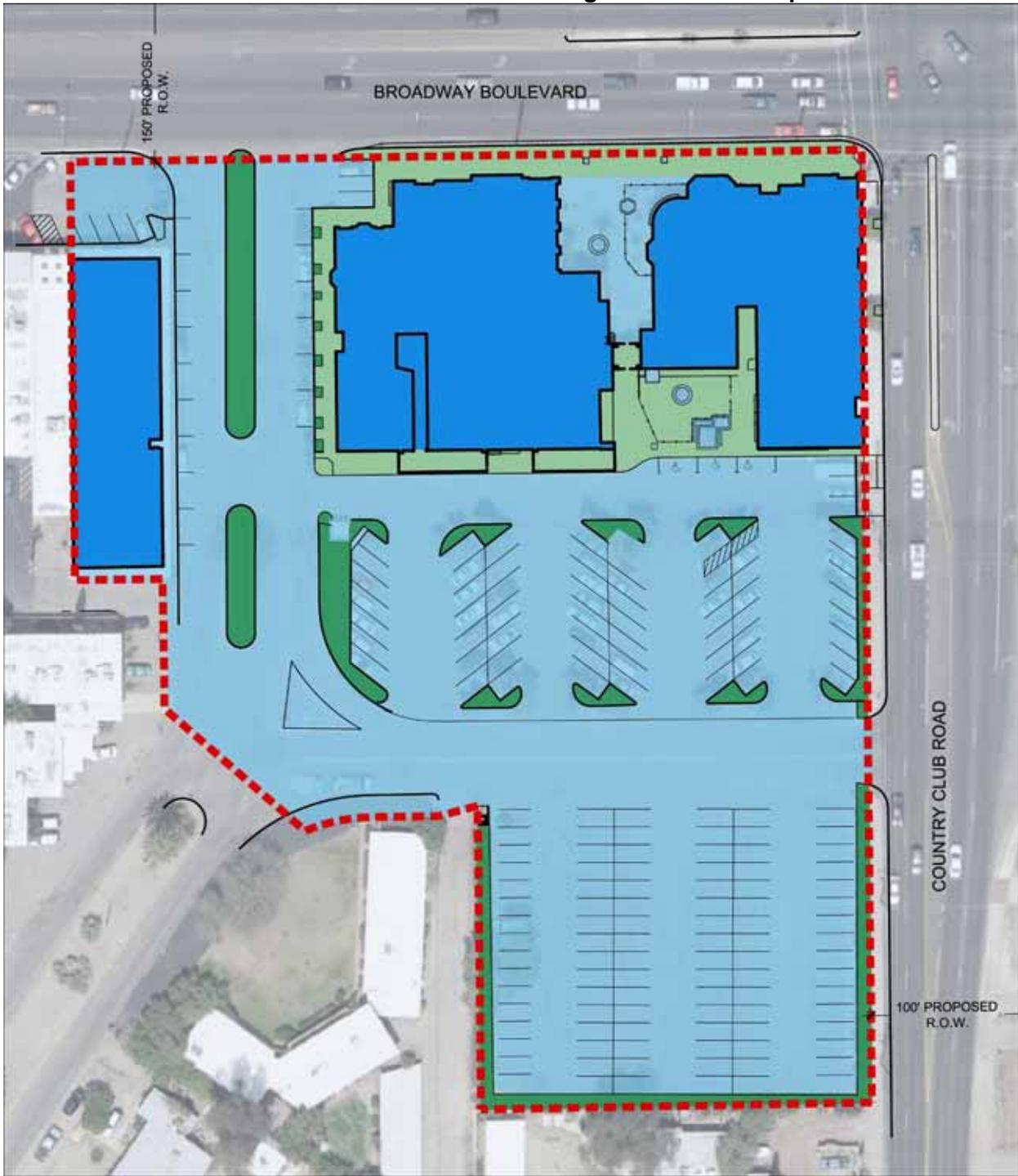
The whole of the site generally sheet drains from the northeast to the southwest. The site plan and grading will take advantage of this by directing sheet flow from



the impervious surfaces (buildings, plazas, and parking) and direct it to the landscape islands. The rainwater will enter the recessed planting areas via flush curbs and/or curbs cuts. The water movement will be slowed and permitted to percolate into the root zones of the planting areas benefiting the plant material and minimizing the amount of supplemental irrigation required. The grading of the landscape areas will permit excess water to exit during a major rain event.



**Exhibit III.C.1: Existing Pervious and Impervious Conditions**



**Legend**

PAD Boundary

**Areas**

Pervious	10,486 SF	Impervious	102,194 SF
Semi-Pervious	12,132 SF	Impervious Buildings	35,889 SF
<b>Total (14%)</b>	<b>22,618 SF</b>	<b>Total (86%)</b>	<b>138,083 SF</b>

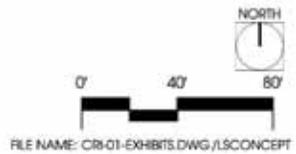


Exhibit III.C.2: Post-Development Rainwater Harvesting Concept



Legend

	PAD Boundary		Surface Flow (Passive Rainwater Harvesting)		
<b>Areas</b>					
	Pervious	11,925 SF		Impervious	71,064 SF
	Semi-Pervious	26,993 SF		Impervious Buildings	45,759 SF
Total (20%)		28,918 SF	Total (80%)		116,823 SF



## D. Landscape Program

The following landscape program constitutes the complete requirements for landscaping within the Broadway Village PAD. If there is a question regarding landscape requirements, the LUC regulations for landscape, screening and buffering shall apply. Any new landscaping associated with a new project and/or development should be consistent with the community need to conserve and retain water plus a desire to visually integrate with the existing historic Broadway Village landscaping.

### 1. Landscape Zones

The proposed landscape for Broadway Village consists of three major landscape zones: landscape borders, plaza landscape areas and parking landscape areas (see *Exhibit III.D.1 Landscape Zones*, page 41).

#### a. Landscape Borders

This PAD consists of three landscape borders: Broadway Boulevard, Country Club Road, and the south property line border, each with unique functional requirements and characteristics. No landscape border is required along the west/southwest property line, as indicated in *Exhibit III.D.2: Landscape Program Concept*, page 42.

##### i. Broadway Boulevard

There shall be no minimum width landscape border adjacent to Broadway Boulevard. The existing conditions of the PAD's border with Broadway Boulevard include no trees or other vegetation and consist of exclusively paved surfaces. Broadway Boulevard is a major arterial and as such the view from passing vehicles is relatively brief. The proposed landscape treatment will include Texas Mountain Laurel trees, minimum 36" box or 2" caliper, planted in tree grates within the existing paved (pavers) areas. Additional potted shrubs and/or accents will augment the Broadway Boulevard landscape border.

This concept will provide a variety of benefits: a net increase of plant material along Broadway Boulevard, permit ample room for pedestrian circulation east and west, and maintain visibility to the unique architecture of Broadway Village with the landscape architecture also integrated into the historic setting.

##### ii. Country Club Road

Due to the limitations of available landscape areas along Country Club, there is no minimum landscape border width. The existing conditions of Country Club landscape include Texas Mountain Laurel trees and a row of small shrubs adjacent to the parking areas. The proposed buildings will provide all required



parking area screening. Ingress and egress into the PAD will be landscaped, while the northern of the two entrances will be accentuated to a greater degree. While no additional shrubs or accents will be required along the Country Club landscape border, if an opportunity for potted plants/shrubs exists, it will be allowed. Pedestrian shade will be added with trees (3 additional trees maximum) or shade structure(s)/canopies from the buildings. Any trees within the public right-of-way will require City review and approval and a license agreement at the time of Development Plan approval.

iii. South Property Line

The south property landscape border will be 5 feet in width and include 1 tree per 33 feet of frontage and the existing 5-foot tall screen wall on the adjacent property. The landscape border west of the south property line (adjacent to existing apartments) will be 5 feet in width and include 1 tree per 33 feet of frontage and a 5-foot tall screen as indicated in *Exhibit III.D.2: Landscape Program Concept*, page 42.

**b. Plaza Area Landscape Zone**

The plaza landscape zone is located almost exclusively in the areas of existing development between buildings and consisting of paved surface (brick pavers). The unique character of this area will be maintained through the preservation and continued use of potted plants and accents and the introduction of Palo Brea trees (*Parkinsonia praecox*) where there is expected to be greater amount of people congregating and sitting. The Palo Brea trees will be located within tree grates (among the pavers) and as such will permit ample pedestrian circulation movement, shade for visitor and customers, and meet parking shade tree requirements west of the existing building.

**c. Parking Area Landscape Zone**

Landscape within the parking area zone, which includes the landscape treatment along Manchester Street and portions of Eastbourne Avenue within the PAD, will be designed and planned with four goals in mind: efficient and safe circulation (pedestrian and vehicular), urban heat island mitigation, rainwater harvesting, and screening from adjacent residential neighbors. The predominant tree in this zone will be a non-fruiting Olive tree (*Olea europea* 'Swan Hill' or "Wilsoni"). This tree has a moderate growth rate, thrives in high reflectivity areas such as parking lots, and is low water use. This tree also has been historically used on the site and within the surrounding established neighborhoods. Parking area shade trees will be provided at a rate of 1 tree per four stalls.



Parking screening (in the form of plants and/or low walls) will be provided for the areas adjacent to and visible from Broadway Boulevard and Country Club Road (accessed from Eastbourne Avenue and Manchester Street). No screening will be provided within sight visibility triangles.

All new tree plantings will provide a minimum root zone area of 300 cubic feet (maximum 3-foot depth) with soil compaction of 90 percent or less; use of structured soil will allow compactions of 95 percent or greater. Alternatively, trees shall be irrigated by means of a deep root watering system such as, but not limited to, a 3-inch diameter, 30-inch deep perforated pipe with drip emitters in pipe and on surface (two pipes per tree). Other measures that may ensure deep and healthy tree root zone development may also be employed at the approval of the City of Tucson.

## **2. Plant Palette**

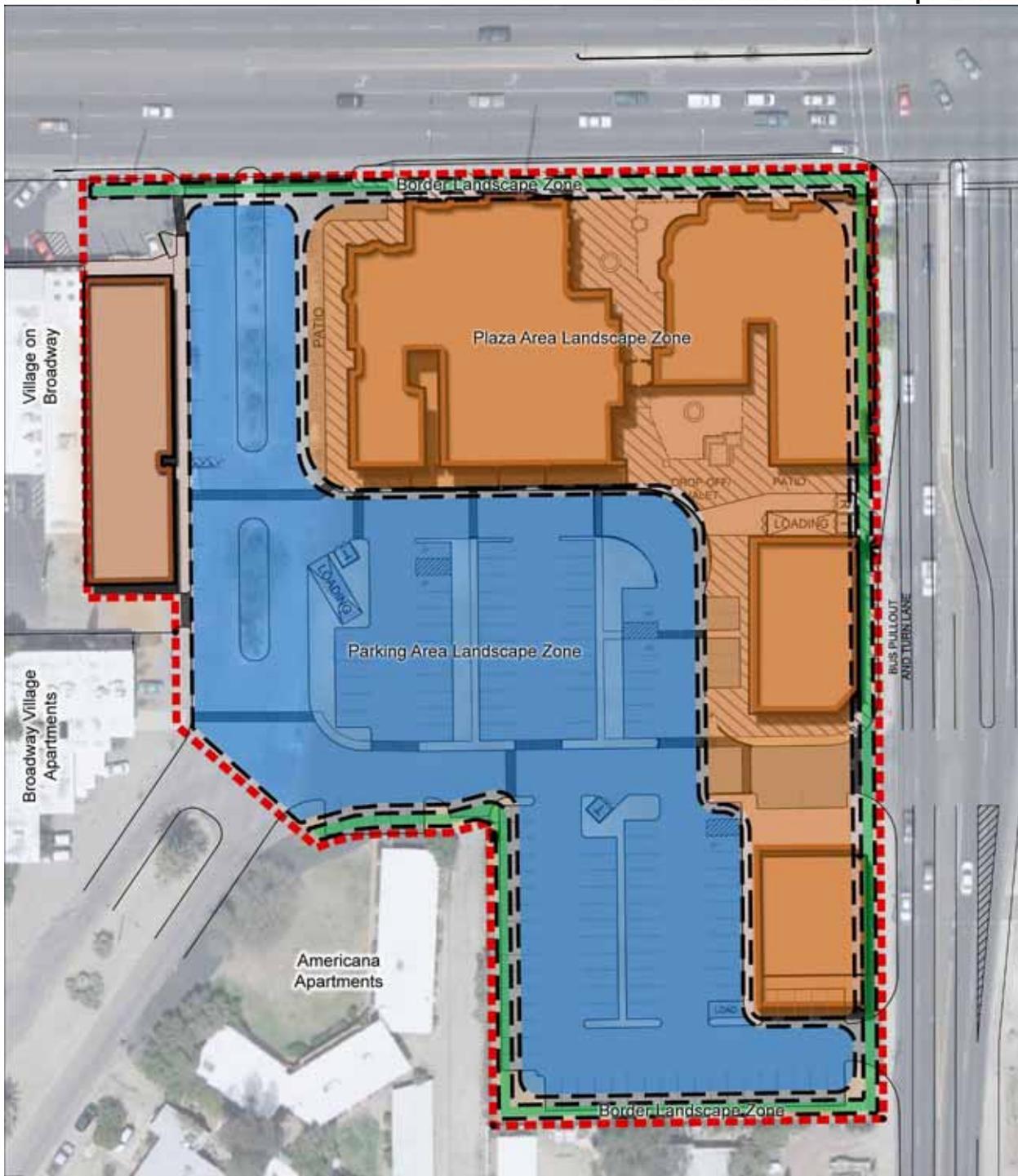
Plant material will be chosen from the Broadmoor-Broadway Village Neighborhood Plan (See *Appendix A: Broadmoor-Broadway Suggested Plant Material*) and the Arizona Department of Water Resources (ADWR) Tucson Active Management Area low water use plant list. Except in oasis areas, all proposed plants will comply with the ADWR low water use list or include an application for approval to the list.

### **a. Historic Landscape**

The existing Broadway Village (and adjacent Broadmoor Neighborhood) landscape consists of a unique and historic plant palette. To the extent feasible specific accent plants from this existing palette will be used within the PAD. The most identifiable plants are the Canary Island Palm (*Phoenix canariensis*) and the Olive Tree (*Olea sp.*). A non-fruiting (non-pollen producing) variety of the Olive Tree will be used along pedestrian routes within Broadway Village (see Exhibit III.D.2: Landscape Program Concept, page 42). Non-fruiting Olive Trees may include the 'Swan Hill' or 'Wilsoni' varieties. The Canary Island Palm will be used to identify major entries into the PAD and will provide a continuation of the existing palms in the median on Eastbourne Avenue. Texas Mountain Laurels are currently planted along Country Club Avenue and will also be used along Broadway Boulevard.



Exhibit III.D.1: Landscape Zones



Legend

- PAD Boundary
- Landscape Border Zone
- Plaza Area Landscape Zone
- Parking Area Landscape Zone

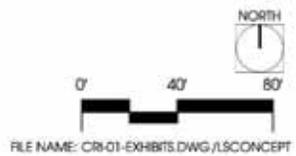


Exhibit III.D.2: Landscape Program Concept



Legend

- PAD Boundary
- Potted Landscape Area
- Specialty Paving
- Existing Right of Way

- Swan Hill Olive (Fruitless)(Pedestrian Circulation Tree)
- Palo Brea (Plaza Tree)
- Canary Island Palm (Historic Neighborhood Tree)
- Texas Mountain Laurel (Broadway and Country Club)
- Parking Shade Tree



FILE NAME: CR1-01-EXHIBITS.DWG /LSCONCEPT



## E. Post-Development Hydrology

### 1. Watershed Description

Post-development peak discharges were calculated using the City of Tucson Standards Manual for Drainage Design and Floodplain Management in Tucson, Arizona, Chapter IV. Post-development conditions consist of the parcel being redeveloped with additional buildings and reconfigured parking areas. The existing structures will stay in place. For a highly urban type development an impervious value of 90% was used. Delineation and peak discharge is shown in *Exhibit III.E: Post-Development Hydrology*, page 44 and in *Table III.E: Summary of Post-Development Discharges*.

**Table III.E: Summary of Post-Development Discharges**

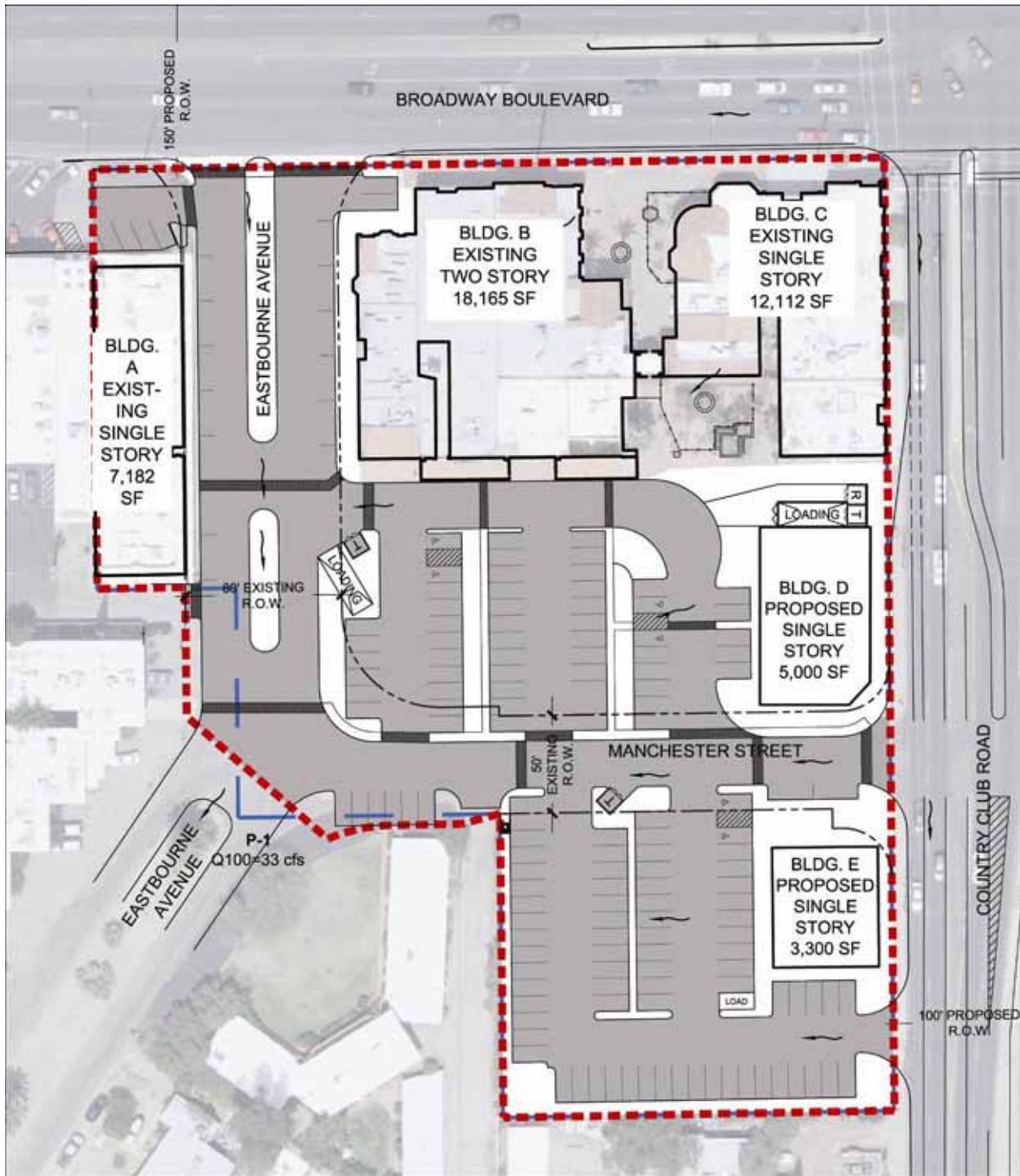
Sub-basin ID	Area (Acres)	2-Year Discharge (cfs)	10-Year Discharge (cfs)	100-Year Discharge (cfs)
P-1	3.61	3.3	12.3	33.3

### 2. Post-Development Conditions Hydrologic Model

Retention/detention will not be required due to the pre-development conditions of the parcel. The pre-development and post-development site calculations result in identical discharges due to the similarity in impervious area. Some passive water harvesting within the property will be utilized, which will retain a portion of onsite runoff. Water harvesting will be ignored within the post-development calculations due to the minor effect it will have on the post-development calculations.



Exhibit III.E: Post-Development Hydrology



Legend

- IPAD Boundary
- Building Footprint
- Flow Arrow
- Subbasin Limits
- Existing Right of Way



## F. Circulation Plan

### 1. Traffic Circulation

A Traffic Impact Analysis (TIA) prepared by Kimley-Horn and Associates (submitted under separate cover) contains recommendations for improvements for the redevelopment of Broadway Village. Following are the major conclusions of this analysis:

- The existing access points along Broadway Boulevard and Country Club Road will remain. In addition, a new right-in, right-out driveway is proposed on Country Club Road south of Manchester Street. The driveway spacing between Manchester Street and the new driveway meets the City of Tucson driveway spacing requirements.
- All study area intersections currently operate at acceptable levels of service during the peak hours. It should be noted that several left-turn movements at the intersection of Broadway Boulevard and Country Club Road operate at unacceptable levels of service during the PM peak hour.
- Trip generation for the addition of approximately 10,000 square feet of retail/office within the PAD results in 32 new trips in the AM peak hour and 49 new trips in the PM peak hour.
- It is anticipated that all study area intersections and driveways will operate at acceptable levels of service with the exception of the Country Club Road/Manchester Street intersection. The eastbound left-turn movement at Country Club Road/Manchester Street is anticipated to operate at LOS E which is typical of unsignalized intersections that allow left-turn movements onto arterials.
- The City of Tucson has an ongoing project, Broadway Boulevard Corridor, which will result in the widening of Broadway Boulevard from Euclid Avenue to Country Club Road. Preliminary information indicates that widening will result in an 8-lane roadway with one lane in each direction dedicated to transit.
- It is recommended that a dedicated southbound right-turn lane be provided at Country Club Road/Manchester Street with a minimum length of 75 feet. A dedicated right-turn lane is not warranted along Country Club Road at the new driveway.



**a. Manchester Street and Eastbourne Avenue**

Within the PAD, Manchester Street and Eastbourne Avenue will remain public rights-of-way. The developer will enter into a maintenance agreement with the City of Tucson regarding any improvements in the public right-of-way, which may include maintenance and irrigation of newly landscaped areas and Vehicular parking and maneuvering, including that required for loading and solid waste disposal. Improvements to Manchester will be required at the time of construction of either Building D or E, whichever is constructed first.

*Exhibit III.F.1.a* (page 47) illustrates the PAD's onsite circulation, and *Exhibit III.F.1.b* (page 48) depicts the cross-sections for Eastbourne Avenue and Manchester Street.

**b. Country Club Road**

The existing right-of-way for Country Club Road varies between 70 feet south of Manchester to 80 feet north of Manchester, while the MS&R width is 100 feet. The MS&R includes future widening to a four-lane divided roadway with a median and bicycle lanes. Due to the fact that Country Club has been previously widened to the east of the traditional roadway centerline (section line) near the intersection of Broadway Boulevard, a simple widening of right-of-way along the section line will not be possible for future dedication and roadway needs.

Since there are no future widening projects pending or planned for Country Club Road, a conceptual alignment was developed as part of this project to illustrate the shift in centerline and right-of-way (see *Exhibit III.F.1.c: Country Club Road Conceptual Alignment*, page 49 and *Exhibit III.F.1.b: Rights-of-Way Cross-Sections*, page 48). While this conceptual alignment was generally accepted by the Department of Transportation, a more detailed plan will need to be determined at time of development plan for right-of-way purposes. The future improvements will provide 11-foot travel lanes, 5-foot bicycle lanes, and 5-foot sidewalks.

Discussions with SunTran related to the existing bus shelter at the intersection of Country Club Road and Manchester Street have resulted in a combined right-turn lane and bus pullout. While not typical, this configuration was the preferred condition by SunTran. A permanent bus shelter will be provided behind the sidewalk and may be incorporated into the proposed building. SunTran requires a



9-foot deep landing area measured from the curb near the bus door to allow for ADA access. As stated earlier, a more detailed roadway plan will be determined at time of development plan. The Owner of Broadway Village will be responsible for the construction of the bus pull-out/right-turn lane along Country Club Road.



Exhibit III.F.1.a: Circulation Plan

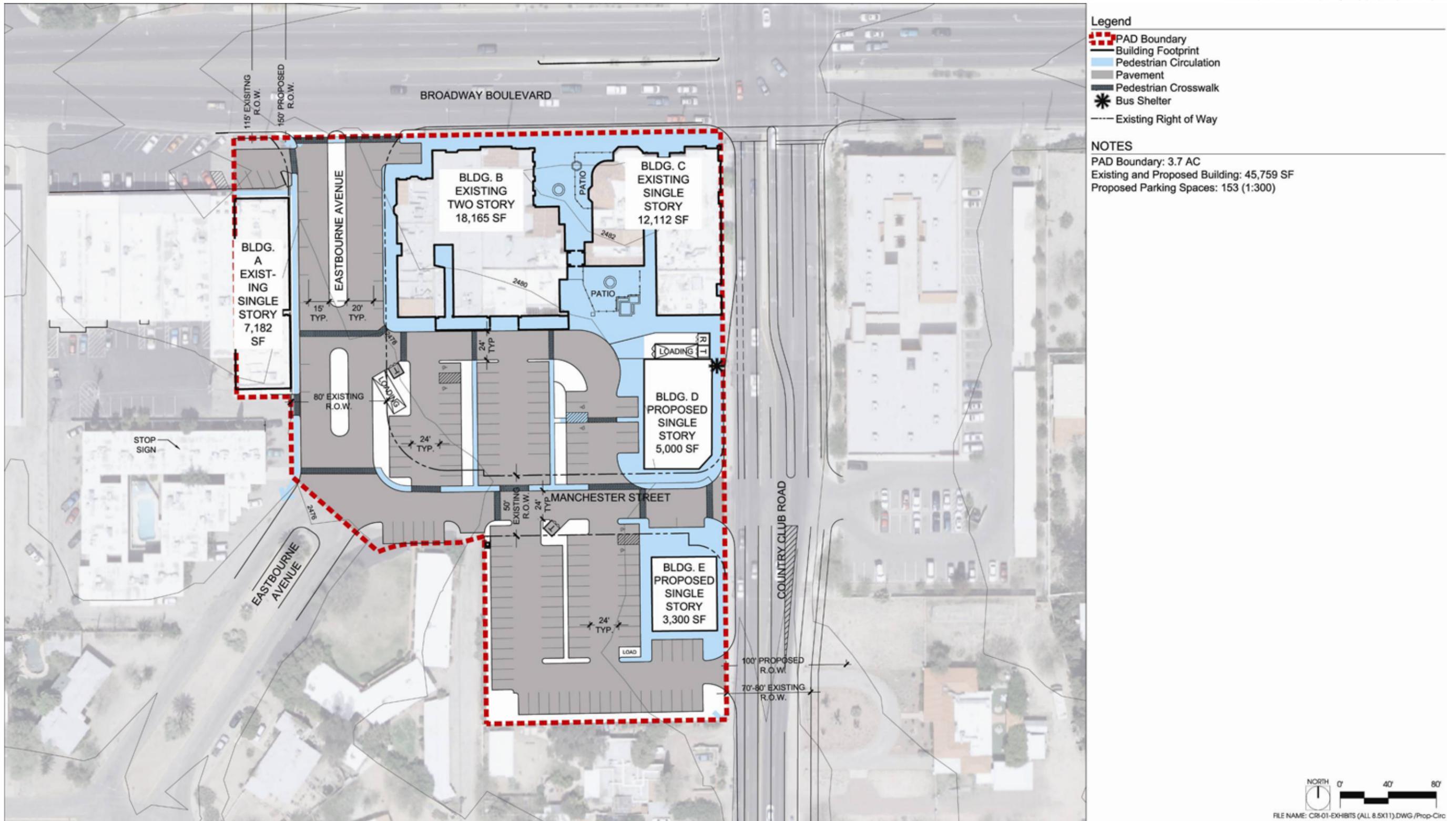
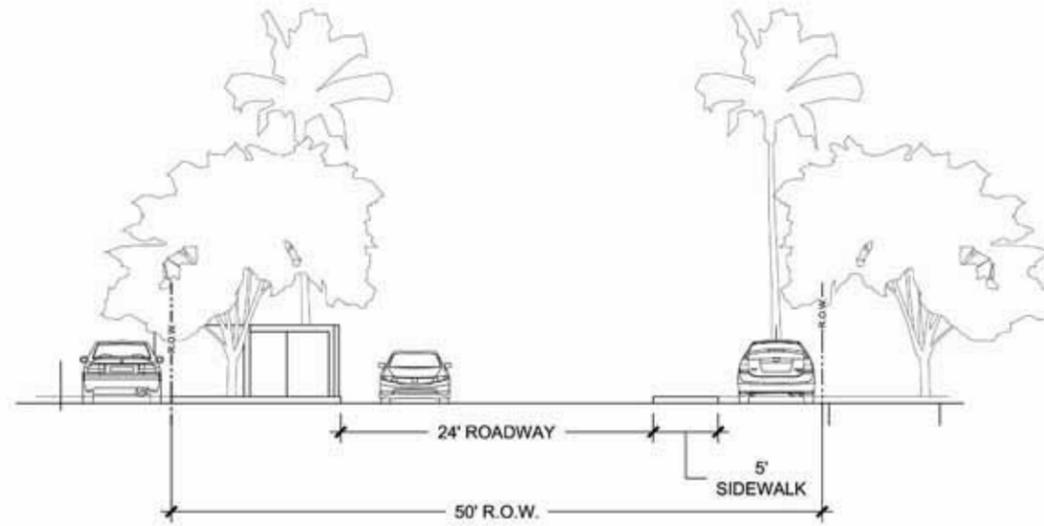
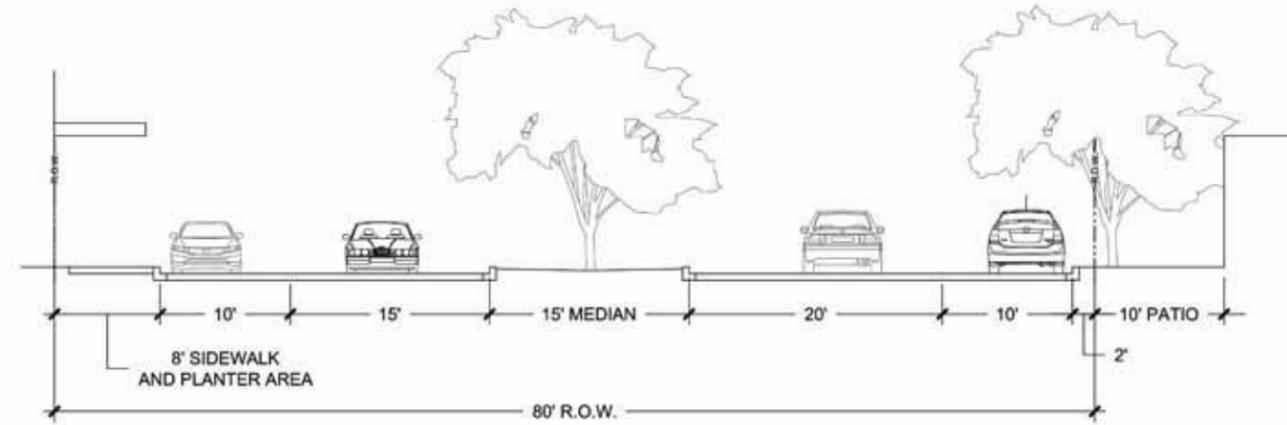


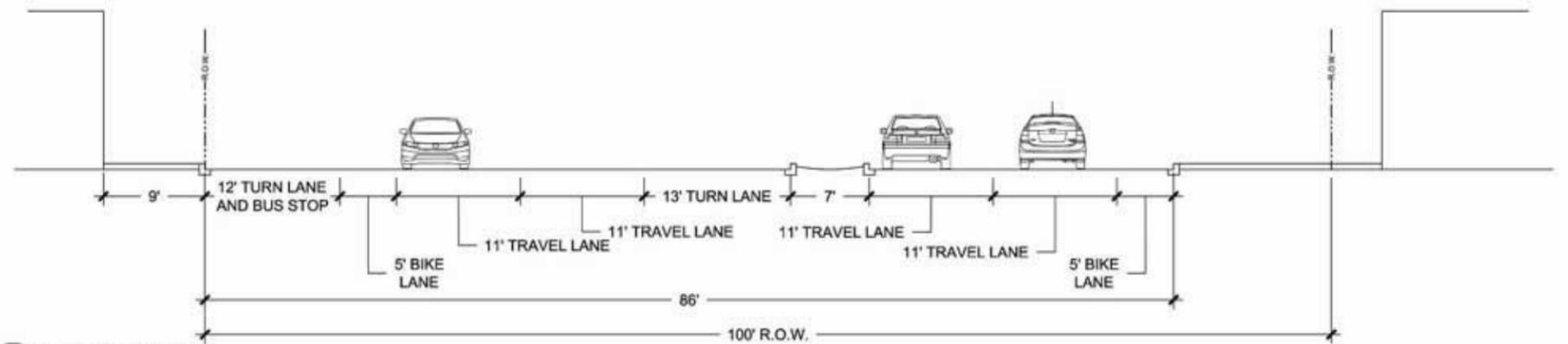
Exhibit III.F.1.b: Rights-of-Way Cross-Sections



**A** MANCHESTER STREET CROSS SECTION



**B** EASTBOURNE AVENUE - EXISTING CROSS SECTION - LOOKING NORTH



**C** COUNTRY CLUB ROAD CROSS SECTION



**Exhibit III.F.1.c: Country Club Road Conceptual Alignment**



## 2. Neighborhood Impacts

Potential neighborhood traffic impacts resulting from the redevelopment of the Broadway Village were evaluated to determine whether the increase in land uses would increase traffic within the adjacent Broadmoor neighborhood. Typical neighborhood concerns are centered around cut-through traffic and congestion.

With respect to cut-through traffic, it is not anticipated that the future additional buildings (or existing buildings) will generate and/or attract traffic from west of the Broadmoor neighborhood through the neighborhood streets. Patrons west and southwest of the Broadmoor neighborhood will likely use Tucson Boulevard to travel north to Broadway Boulevard where an existing traffic signal can facilitate their route to Broadway Village. On the return trip, these patrons will likely travel south on Country Club Road and eventually turn right at the appropriate east-west street. The lack of continuous east-west streets through the Broadmoor neighborhood reduces the potential for cut-through traffic and keeps drivers on the collector roadways of Tucson Boulevard and Country Club Road.

In terms of congestion, the proposed improvements to the internal parking and circulation layout will reduce localized congestion. The parking lots will be reconfigured to provide more discernable aisles and entrances to those aisles will be controlled by raised curb, with cut-outs or pervious paving to accommodate water harvesting in the landscaped areas. Manchester Street will be reconstructed to provide a typical cross-section more representative of a local street than a private drive. The intersection of Eastbourne and Manchester will also be reconstructed to provide a safer intersection with logical traffic control. Overall, the improvements to the public travel ways will significantly improve wayfinding and improve safety for drivers, bicyclists and pedestrians.

The developer will enter into a maintenance agreement with the City of Tucson regarding improvements in the public right-of-way.

## 3. Pedestrian Circulation

Pedestrian circulation will be provided between buildings and parking areas. Connections between Broadway Boulevard and Country Club Road through the PAD and adjacent neighborhoods will be provided as indicated in *Exhibit III.F.1.b: Circulation Plan*, page 50.

Sidewalks shall be constructed out of concrete, pervious concrete, permeable pavers, compacted DG, porous asphalt, reclaimed asphalt pavement, asphalt or other material, with approval from the City Engineer. Pedestrian crossings within the Broadway Village PAD shall be colored or textured as a safety precaution.

Sidewalks and pedestrian connectivity shall be a minimum of 5 feet wide within the parking area of the PAD and within the Manchester Road right-of-way. Plaza area circulation and sidewalks shall be a minimum of 5 feet wide. Sidewalks



along Broadway Boulevard and Country Club Road shall be a minimum of 5 feet wide. A minimum sidewalk width of 7 feet is required where perpendicular parking spaces are adjacent to a building to allow for sufficient vehicle overhang; however, a minimum separation of zero (0) feet between a sidewalk and a building is permitted (see *Exhibit III.F.1.b: Circulation Plan*, page 50).

All development within the PAD District will comply with ADA by adhering to the 2006 IDC, Chapter 11 and ICC/ANSI 1117.1, 2003 Edition. Historical structures will not be required to meet ADA compliance as long as there are no safety hazards, as per section 3407.1, "Historic Buildings" of the International Building Code (2006). The improvements to the PAD District will be privately funded and will comply with the ADA standards for curb ramps, sidewalks, detectable warnings and marked crosswalks.

The entire pedestrian circulation system will meet these requirements by the issuance of the last Certificate of Occupancy for the last new building to be built on-site.



## G. Project Design

Seaver-Franks Architects, Inc. has produced the *Architectural Design Guidelines* for the Broadway Village PAD and has been included as Appendix B in this document. The guidelines provide the framework for the design and character of Broadway Village and address the following goals:

- Guidelines should be clear, concise and enforceable.
- Recognize the particular character of the Center and provide instruction for transition zones between new and existing architecture.
- Protect the present attributes of the Center, particularly the history and physical character that give it a strong sense of place; provide a framework for design but allow for creativity and innovation.
- Address adjacent neighborhoods and provide for transitions at the edges of the property.
- Provide consistency with the PAD and the intentions of the document.
- Encourage the design and construction of buildings that are physically durable and visually enduring in a method that is consistent with the existing structures of Broadway Village.
- Ensure new development does not adversely impact existing neighborhood character by complying with the Compatible Design policies of the Broadmoor-Broadway Village Neighborhood Plan.
- Design new buildings to be compatible but differentiated from the existing historic buildings.

The property owners, in collaboration with the project architect, will review and approve all details of project design through a self-certification process. A copy of the self certification will be provided to the City of Tucson at the time of plan submittal advising whether the architectural design conforms to the Broadway Village Architectural Design Guidelines.



## **H. Interpretations and Amendments**

### **1. Interpretation**

The regulations and guidelines provided within this PAD supersede existing regulations within the City of Tucson Land Use Code and Development Standards. If an issue arises regarding definitions, conditions, standards and/or situations not addressed in this PAD, those in the LUC, Development Standards or other COT regulations shall prevail, as interpreted by the COT Zoning Administrator.

### **2. Amendments**

Amendments to the Broadway Village PAD may be necessary over time in order to respond to the changing market demands, or financial conditions, or to respond to the unanticipated needs of new users. Non-substantial changes to the PAD shall be approved pursuant to LUC Section 2.6.3.11.B.5.

Substantial changes (as defined in LUC Section 2.6.3.11.B.3), are subject to the amendment process outlined in LUC Section 2.6.3.11.B.4.



## **Bibliography**

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Balanced and Critical Basin Map, Pima County.

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City of Tucson Development Standard No. 1-07.0 Rezoning Procedures.

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Article III, Division 2. Development Criteria

Article III, Division 3. Motor Vehicle and Bicycle Parking Requirements

Article III, Division 7. Landscaping and Screening Regulations

FEMA Flood Insurance Rate Map, Pima County, Arizona.

Institute of Transportation Engineers, Trip Generation Manuals, 7<sup>th</sup> Edition, Volumes 1 & 3, 2003

Joesler & Murphey: An Architectural Legacy for Tucson. R. Brooks Jeffery, Coordinator of Preservation Studies at the College of Architecture and Landscape Architecture, University of Arizona, 1994.

Low Water Use/Drought Tolerant Plant List, Arizona Department of Water Resources – Tucson Active Management Area, 2007.

MapGuide, Pima County Department of Transportation, 2010.

Tucson City Code, Part 2, Chapter 29, Article VIII (WASH).





# Appendix A



## SUGGESTED PLANT MATERIAL

Theme Trees in particular = Pinus Halensis...Aleppo Pine

### LATIN NAME

Edcalyptus Leucoxydon Mac Rosea  
 Pinus Brutia Eldarica  
 Palo Brea  
 Parkincidium sp  
 Prosopis Chilensis  
 Olea Europaea Swan Hill Oblonga  
 Acacia Smalli

### COMMON NAME

Red Flowering Euculptus  
 Mondel Pine  
 Sonoran Palo Verde  
 Hybrid Palo Verde  
 Chilean Mesquite  
 Pollen Free Olive  
 Sweet Acacia

### Accent Trees:

Ulmus Parvifolia  
 Cordia Boissieri  
 Phoenix Dactylifera  
 Phoenix Canariensis  
 Washingtonia Filifera  
 Washingtonia Robusta  
 Cupressus Sempervirons  
 Citrus sp  
 Chaemorops Humilus  
 Pithocellobium Flexicaule  
 Quercus Virginiana Heritage  
 Trachy Carpus Fortunei

Evergreen Elm  
 Texas Olive  
 Date Palm  
 Canary Island Palm  
 California Fan Palm  
 Mexican Fan Palm  
 Italian Cypress  
 Citrus  
 Mediter. Fan Palm  
 Texas Ebony  
 Heritage Oak  
 Windmill Fan

### Shrubs:

Caesalpina Pulcherrima  
 Nerium Oleander Petite Pink  
     Semi-Dwarf DBL  
 Rosmarinus Officinalis  
 Jasminum Mesnyi  
 Rhus Ovata  
 Cistus sp.  
 Raphiolepis Indica  
 Euonymus Japonica

Red Bird of Paradise  
 Oleander  
  
 Rosemary  
 Primrose Jasmine  
 Sugar Bush  
 Rockrose  
 India Hawthorne  
 Evergreen Euonymus

### Accents:

Dasyilirion Wheeleri  
 Agave sp.  
 Opuntia Ficus Indica  
 Yucca Species  
 Cortaderia Selloana

Desert Spoon  
 Agave  
 Indian Fig  
 Yucca  
 Pampas Grass

Ground Covers:

Juniperis Horizontalis 'Wiltonii'

Verbena sp.

Lantana Montevidensis

Santolina Chamaecyparis

Santolina Virens

Blue Carpet Juniper

Verbena

Trailing Lantana

Lavendar Collot



# Appendix B



PROPOSED MARCH 1, 2010

# BROADWAY VILLAGE

## ARCHITECTURAL DESIGN GUIDELINES



## Design Review Process

Goals for Guidelines

Southwest / Existing Architecture

Building Forms

Human Scale

Building Materials

Architectural Detail

Signage

Lighting

Materials

Forms

Building / Building Relationships

Building / Street Relationships

The process for design review is dynamic and interactive and spans the project. The project architect and the owner's representative group will engage in a dialog very early on so that each aspect of the designed project is initiated in a manner supported by a self certified process and developed in the spirit of the guidelines set forth in the Secretary of Interior's Standards for Rehabilitation. A copy of the self certification will be provided to the City of Tucson at the time of plan submittal. The self certification will demonstrate compliance with the Broadway Village Architectural Design Guidelines. The various phases of the conventional design process normally listed in the agreement between owner and architect will be followed. The meetings of the owner's representative group are project specific and will be arranged according to the individual project schedule.

First, a conceptual design review takes place at the earliest stage when a new project is to be presented to the owner's group. Conceptual design is defined as the vision for the project; its architectural character, its physical dimensions and its proposed massing will be of primary concern at this phase. The proposed overall project schedule will also be discussed.

The owner's group will issue a letter following this meeting stating its recommendations, if any, for conformance to these guidelines. This phase will be followed by the schematic design phase.

Second, a review of the project will occur at a more advanced stage of design, roughly 50 percent of design development, to consider building materials, details and overall development of the design. Also, the owner's group will particularly consider whether the vision for the project presented at the first meeting has been maintained. Project schedule will also be reviewed.

The owner's group will then issue a second letter following this meeting that states its recommendations, if any, for conformance with these guidelines.

Third, a final review of the project will be held at roughly 90 percent of the Construction Documents phase to ensure that the approved design intentions and vision are preserved and confirm its scheduled completion date.

Following this third and final review, the owner's group will issue a third letter that states its final recommendation, based upon the project as it has been presented.

Design Review Process

**Goals for Guidelines**

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Building / Street Relationships

- Guidelines should be clear, concise and enforceable.
- They should recognize the particular character of Broadway Village and provide instruction for transition zones between new and existing architecture.
- They should protect the present attributes of Broadway Village, particularly the history and physical character that give it a strong sense of place in midtown Tucson.
- Guidelines should provide a framework for design but allow for creativity and innovation.
- They should address adjacent neighborhoods and provide for transitions at the edges of the property.
- Guidelines should be consistent with the PAD for Broadway Village and the intentions of that document.
- They should embrace the ethic for sustainable design.
- They should encourage the design and construction of buildings that are physically durable and visually enduring in a method that is consistent with the existing structures of Broadway Village.

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Tucson's architectural heritage is a composition of multiple traditions. A vernacular developed prior to statehood in 1912 including row houses constructed of adobe and rough-hewn lumber, Victorian railroad houses made of fired clay bricks, milled lumber and cast iron details and various expressions of the Arts and Crafts movement. Trends in the early 1900's such as Spanish Colonial Revival style and Craftsman and Mission Revival styles eventually made their way to Tucson but a strong Hispanic influence continued to influence design as well. It is during this time period the architect Josias Joesler arrived in Tucson.

The "Joesler style" borrows extensively from historic and contemporary styles of the time and blended them with local building materials and traditions. The majority of his work could be classified as Spanish Colonial with elements such as smooth stucco walls, arches, elaborate wrought iron ornamentation, low-pitched clay tile roofs, overhanging eaves and brick capped parapets often punctuated by canales to drain water from the roof.

For existing Buildings B and C Broadway Village, Joesler blended traditional forms such as arches, tile roofs and courtyards. Designed with an intimate scale of a Spanish Colonial village center, the buildings are a blend of stucco and mortar washed bricks to give the overall sense of an older period. Decorative items such as a tile on lower sections of exterior walls, metal framed "French" windows, wrought iron, a weather vane and statuettes give Broadway Village a strong sense of place.

For more information on Josias Joesler see Joesler & Murphey: An Architectural Legacy for Tucson by R. Brooks Jeffery.

Existing Building A in the western portion of Broadway Village is a mid century addition designed by Mexican architect Juan Warner Baz. While a good example of a period reinterpretation of the older portions of Broadway Village, this will not be used as a standard for future development. These characteristics do not so much identify a style as they do prescribe an attitude and feeling for forms of buildings and the whole they create. Individual characteristics may or may not be important toward establishing whether a new and particular design proposal manifests its overall quality as being appropriately similar to the architectural style of Broadway Village. New building proposals should be taken as a whole – judgment can then be made about its particular appropriateness as a new addition to the center:

- The use of light, shade and shadow
- Textural qualities that take advantage of the sunlight
- Thick wall/wide overhangs
- Building forms that harmonize with the surrounding scale of the existing structures and neighborhoods
- Materials and colors complimentary to the existing structures
- Planned outdoor spaces including shaded courtyards and pedestrian walkways

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While building forms in the Southwest vary, common elements have developed in response to the hot, dry Sonoran Desert climate and with the influence of the variety of cultures which settled here. These traditional forms, such as flat-roofed vernacular row houses with parapets, the redefined 'mission revival style' and the low-sloped clay tile roof structure come together to create an architecture with a strong regional character. Broadway Village is a striking example of historical Spanish Colonial architecture. Characteristic of most mission style architecture, Broadway Village predominately uses the limited palette of building materials which were available to the Spanish missions of the Southwest. Installations utilize massive walls with limited fenestration, wide projecting eaves and low-pitched clay tile roofs. Other features include long arcaded corridors, pierced arches and courtyards.

Such Elements shall serve as design standards in helping to define the architecture of Broadway Village:

- Roofs and parapets
- Roofs with low slopes in keeping with regional and local traditions.
- Architecturally detailed parapets with special consideration given to the appearance when seen against the sky and the patterns created by sun and shadow.
- Extended roofs covering arcades and large pillars or columns to create shade and areas of respite for pedestrians.
- Encourage the use of roof-installed solar energy devices that are architecturally integrated into the building.
- Use of materials consistent with the existing structures of Broadway Village.
- Windows and Orientation
- Windows to the North and shaded windows to the South.
- Fewer windows on East and West walls.
- Window shapes that are primarily rectangular.
- Exterior windows utilize natural light providing for views, an indoor/outdoor ambiance and where applicable address the interplay between retail establishment and street traffic while being mindful of solar energy impact.

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As Tucson's first suburban shopping center Broadway Village enjoys an intimacy of human scale not common to buildings with similar programs throughout the community.

#### Building Massing

- Articulation of the vertical massing with architectural detail, material and textural variation and terracing.

#### Solar Protection

- Solar Protection for openings with the use of overhangs, trellises or other devices consistent with the existing methods in use at Broadway Village.
- High performance glass, particularly on East and West exposures.

#### Doors and Entries

- Shaded and architecturally detailed to be friendly and inviting, protecting users from the weather while clearly demarcating their purpose.
- Safe and secure.

#### Accessibility

- Pedestrian entries at grade.
- Minimum usage of steps and ramps.

#### Wall Openings

- Window and Door openings detailed and enriched to enhance human scale.

#### Pedestrian Detail

- Provide architectural detail that visually relates to and guides pedestrians.
- New structures shall maintain the human scale, pedestrian quality and character of the existing portions of Broadway Village.

#### Expression of Structure

- Visually, the elements of the building such as beams, columns and walls shall appear to be properly supported and balanced to create clarity of structural expression in a manner consistent with the original sections of Broadway Village.

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As previously mentioned, historical Mission Revival architecture draws upon the construction of Spanish Missions in the Southwest. It is as well a part of a larger movement – the Arts and Crafts movement—and as such employs signature details such as well crafted inglenook-like corners, beamed ceilings, handmade metal details and tile details.

Readily available and indigenous materials

- Select materials with the idea of localizing the architectural effect and ambiance in a method coherent with the original structures.

Tactile Expression

- Choose building materials for their tactile effects, to be used in a contrasting manner; rough surfaces against smooth, vertical patterns against horizontal and so on.

Integral Color

- Choose materials for their integral colors and their visual and physical permanence in the Sonoran Desert.
- Colors shall be selected from the Broadway Village Colorization Plan or approved by the owner's representative group.

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Building / Street Relationships

Broadway Village has a unique set of architectural details which add visual interest and give the entire center a strong sense of craftsmanship.

#### Shade and Shadow

- Detail that responds to the brilliance of the sunlight creates opportunities for shade and shadow effects. Consider the amount of relief when proposing detail to achieve acceptable and desirable effects of shade and shadow similar to that of the existing parapet treatments of Broadway Village.

#### Articulation

- Architectural articulation such as clear definition between building masses or the design of items like lintels above windows and doors and how a column supports a beam.

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1. Blade signage to be retained as a model for future signage.
2. Acceptable stand-off signage.
3. Box signage to be phased out.
4. Acceptable with limited use.
5. Acceptable reverse channel neon signage.
6. Box signage to be phased out.
7. Blade signage to be retained as a model for future signage.
8. Relief signage to be phased out.
9. Acceptable reverse channel neon signage.
10. Plaque signage to be retained as a model for future signage.
11. Surface applied signage to be phased out.
12. Existing Monument Sign.
13. Temporary window banner.
14. Temporary wall banner.
15. Temporary door banner.

Existing graphics provide an excellent starting point for developing a colorful and elegant way finding and tenant identification system. The goal is to replace existing undesirable signage with acceptable signage that is historically unique and varied.

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1. Acceptable small scale historic accent lamp in South parking lot.
2. South parking lot lighting.
3. South parking lot lighting.
4. Acceptable fixture South West corner of Building C.
5. Existing fixture to be phased out.
6. Acceptable fixture East elevation of Building C.
7. Existing fixtures at soffit entryway.
8. Existing fixture to be phased out.
9. Acceptable fixture South walkway Building B.
10. Acceptable fixture North elevation of Building B.
11. Acceptable fixture West elevation of Building C.
12. Acceptable small scale capped lamp on Broadway pedestrian path.
13. Acceptable fixture courtyard stairway of Building B.



Existing fixtures to remain will be painted satin black enamel or oiled bronze / rusted or faux / antique bronze. Undesirable existing light fixtures will be removed and replaced with approved fixtures.

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1. South parking lot lighting.
2. South walkway Building B.
3. Fixture on South East corner Building B and arch joining Buildings B and C beyond to be removed.
4. East elevation Building C.
5. North elevation Building C.
6. North elevation Building C.
7. Courtyard stairway of Building B.
8. Courtyard between Buildings B and C.
9. North elevation Building B.
10. East elevation of Building A.
11. Proposed standard parking lot light pole / fixture.



Design Review Process  
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**Architectural Detail**

Signage

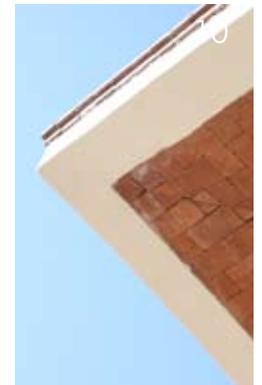
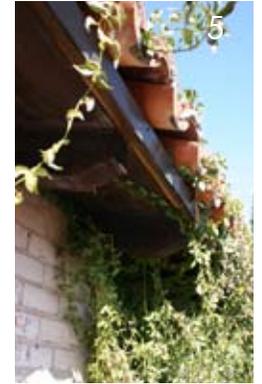
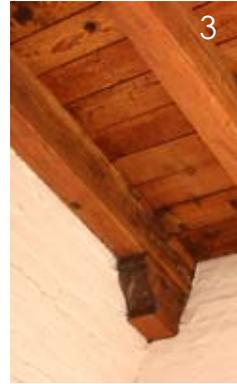
Lighting

**Materials**

Forms

Building / Building Relationships

Building / Street Relationships



1. Architecturally detailed parapet.
2. Texture expressed through irregular masonry surface.
3. Carved wood corbels used in areas with minimal exposure to sunlight.
4. Decorative tile.
5. Low pitched clay tile roof with wood corbels. Painted masonry wall.
6. Architecturally detailed parapet and stucco wall.
7. Architecturally detailed parapet and mortar washed brick.
8. Decorative tile.
9. Thick stucco wall with inset wood doorway.
10. Contrasting adjacent surfaces.
11. Original brick pavers.
12. Metal framed "French" windows.
13. Adobe parapet cap.
14. Decorative wrought iron.
15. Hand painted Mexican tile.

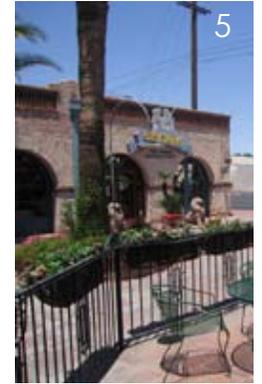
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**Architectural Detail**

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**Forms**

Building / Building Relationships  
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1. Brick arch connecting Buildings B and C.
2. Shaded walkway on South side Building B.
3. Courtyard stairway Building B.
4. Narrow rectangular windows South face Building C.
5. Wrought iron railing and arches in courtyard.
6. Pedestrian scale and detail West elevation Building B.
7. Courtyard stairway and terracing Building B.
8. Archway with deep shadowing over walkway South side Building B.
9. Thoughtful detailing where building meets the sky.
10. Building A North elevation.
11. Thoughtful detailing where building meets the sky.
12. Thick walls with deep openings.
13. Archway South side Building B looking in from parking lot.

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The importance of visual harmony is an important element of Broadway Village. This is achieved with visual interaction and dependency between adjacent structures.

#### Areas between buildings

- Provide pedestrian shade and landscaping between structures.
- Ensure that the space has functional purposes such as passive recreation, pedestrian circulation and that the space has a clearly intentional visual focal point.

#### Pedestrian connections

- Ensure pedestrian connections are convenient, obvious and accessible.

#### Architectural compatibility

- Consider material choices, architectural scale and detail.
- New buildings must visually co-exist with older structures and be compatible but differentiated in form and pallet.

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Building-to-Street relationship and pedestrian paths are especially important in establishing a cohesive visual harmony.

Prevailing scale at the street

- Maintain prevailing neighborhood scale and/or articulate massing of new structures to maintain that scale.

Street frontage/setback

- Maintain existing street building alignments or setbacks as a means of providing visual uniformity.



# Appendix C



**MEMORANDUM OF UNDERSTANDING**

This Memorandum of Understanding pertains to the Broadway Village Shopping Center Planned Area Development No. \_\_\_\_\_ (the "PAD").

The undersigned Owner/Developer will assume maintenance responsibility for all public and private infrastructure improvements that are constructed or integrated into the PAD site by the Owner/Developer. This will include maintenance of existing roadways and parking surfaces, sidewalks, striping, surface texturing or coloring, and landscape improvements located on the City rights-of-way for E. Manchester St. and S. Eastborne Ave. that are within the boundaries of the PAD, but will not include any other pre-existing subsurface public or private infrastructure improvements such as underground utility lines.

The parties agree to negotiate in good faith the terms of a formal Maintenance Agreement and Covenant (enabling the agreement to run with the land to successors and/or assigns of the respective properties) in recordable form, including the foregoing terms, providing for Owner/Developer's non-exclusive, non-commercial use of the infrastructure improvements, providing for insurance, hold harmless and indemnity for injuries and damages attributable to the Owner/Developer's acts and omissions, and providing for the term and termination of the Agreement and Covenant.

The approved PAD will contain a note referencing the Agreement and Covenant, with recording information to be provided as to recording date, docket and page.

CRI Broadway Village Partners, LLC

By: \_\_\_\_\_  
Authorized Agent, FRED J. HOWARD  
Date: 12-22-18

City Engineer

\_\_\_\_\_  
Andy Dinauer  
Date: 12/23/10



# Appendix D



# TRAFFIC IMPACT ANALYSIS

for

## **Broadway Village**

February 2010

Prepared for:

### **CRI Broadway Village Partners, LLC**

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Tucson, AZ 85711

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## I. INTRODUCTION

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### ***INTRODUCTION***

This report documents the traffic impact analysis prepared for the redevelopment of the Broadway Village shopping center located on the southwest corner of Broadway Boulevard and Country Club Road in the City of Tucson, Arizona. The development will result in the addition of approximately 10,000 square feet of retail/office, the reconstruction of the associated parking fields, and improvements to the public roadways that bisect the existing center.

This Traffic Impact Analysis was prepared based on the current City of Tucson *Transportation Access Management Guidelines for the City of Tucson Arizona (March 2003)*. The specific objectives of this study are to:

- (1) Evaluate existing (2009) conditions for the intersections of Broadway Boulevard and Country Club Road, Broadway Boulevard and Eastbourne Avenue, Country Club Road and Manchester Street, and Eastbourne Avenue and Manchester Street;
- (2) Evaluate analysis year (2010) level of service for the above study area intersections and a new driveway along Country Club Road;
- (3) Recommend exclusive turn lane storage, traffic control, and laneage for the adjacent intersections, as necessary, to maintain acceptable levels of service during the 2010 peak hours.



## II. PLANNED DEVELOPMENT

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### **SITE LOCATION**

The Broadway Village shopping center is located on the southwest corner of Broadway Boulevard and Country Club Road in the City of Tucson. The project location is shown in **Exhibit 1**.

### **SITE PLAN**

The development is anticipated to add approximately 10,000 square feet of retail/office which will result in the reconstruction of the adjacent parking fields and improvements to Eastbourne Avenue and Manchester Street. The planned site layout is illustrated in **Exhibit 2**. As shown, the existing access points along Broadway Boulevard and Country Club Road will remain. In addition, a new right-in, right-out driveway is proposed on Country Club Road south of Manchester Street.

### **ADJACENT LAND USE**

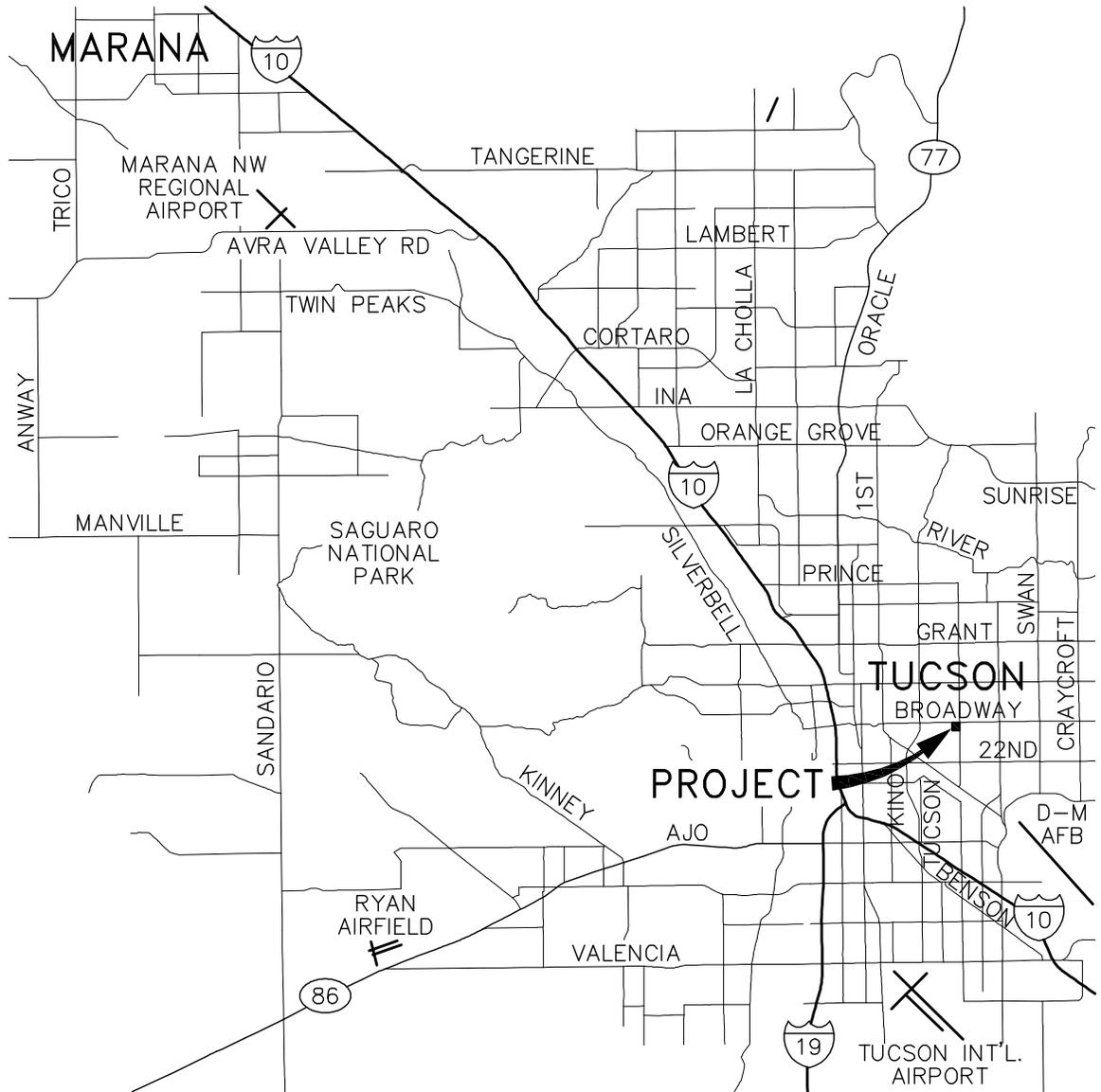
The majority of the land use in the surrounding area is residential in nature with the exception of several retail centers along Broadway Boulevard. The residential subdivisions that immediately adjoin the project site include Broadway Village subdivision and Broadmoor subdivision. The following subdivisions are located on the other quadrants of the Broadway/Country Club intersection: Colonia Solana, El Encanto Estates, Terra de Concini, and Terra Annex.

### **PLANNED ROADWAY WIDENING**

The City of Tucson has an ongoing project, Broadway Boulevard Corridor, that will result in the widening of Broadway Boulevard from Euclid Avenue to Country Club Road. The project is funded through the Regional Transportation Plan and Pima County Bonds at approximately \$71 million. While the design is still underway, preliminary information indicates that widening will result in an 8-lane roadway with one lane in each direction dedicated to transit. Right-of-way plans indicate that the alignment will shift to the north and the curb location along the Broadway Village property will not be significantly affected. Construction is not anticipated until 2014 or later.



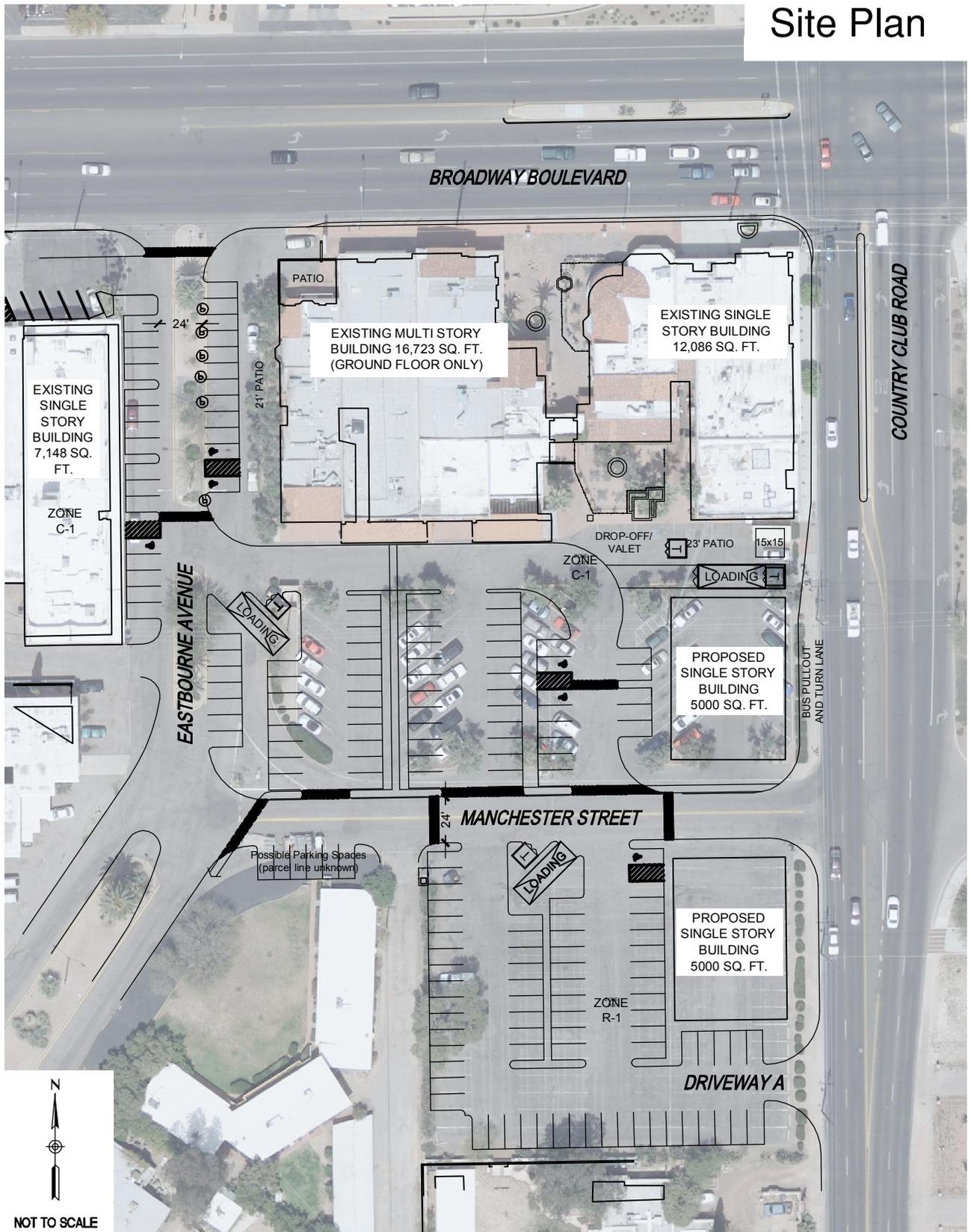
# Project Location



NOT TO SCALE



# Site Plan





### III. EXISTING CONDITIONS

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#### **PHYSICAL CHARACTERISTICS**

The existing roadways bordering the Broadway Village shopping center include Broadway Boulevard and Country Club Road. Broadway Boulevard is classified as an Urban Principal Arterial and Country Club Road is classified as an Urban Minor Arterial. All classifications are based on information provided by the Pima County Geographic Information System (GIS) and the Federal Highway Administration's Functional Classification Map for Tucson Arizona.

A graphical illustration of existing intersection laneage, traffic control, and posted speed limits in the vicinity of the development is shown in **Exhibit 3**. The following are summaries of the roadways within the vicinity of the Broadway Village shopping center:

**Broadway Boulevard**, west of Country Club Road provides two lanes in each direction with a continuous left-turn lane. East of Country Club Road, Broadway Boulevard provides three lanes in each direction with a raised center median. The roadway provides curb, bike lanes, and sidewalks. The posted speed limit is 35 mph in the westbound direction and 40 mph in the eastbound direction.

**Country Club Road**, in the vicinity of the site, provides two travel lanes in each direction. Curb and sidewalks are provided. Bike lanes are not provided. The posted speed limit is 35 mph.

**Eastbourne Avenue**, in the vicinity of the site, provides one lane in each direction with a raised center median and provides no sidewalks or bike lanes. The posted speed limit near the site is 25 mph.

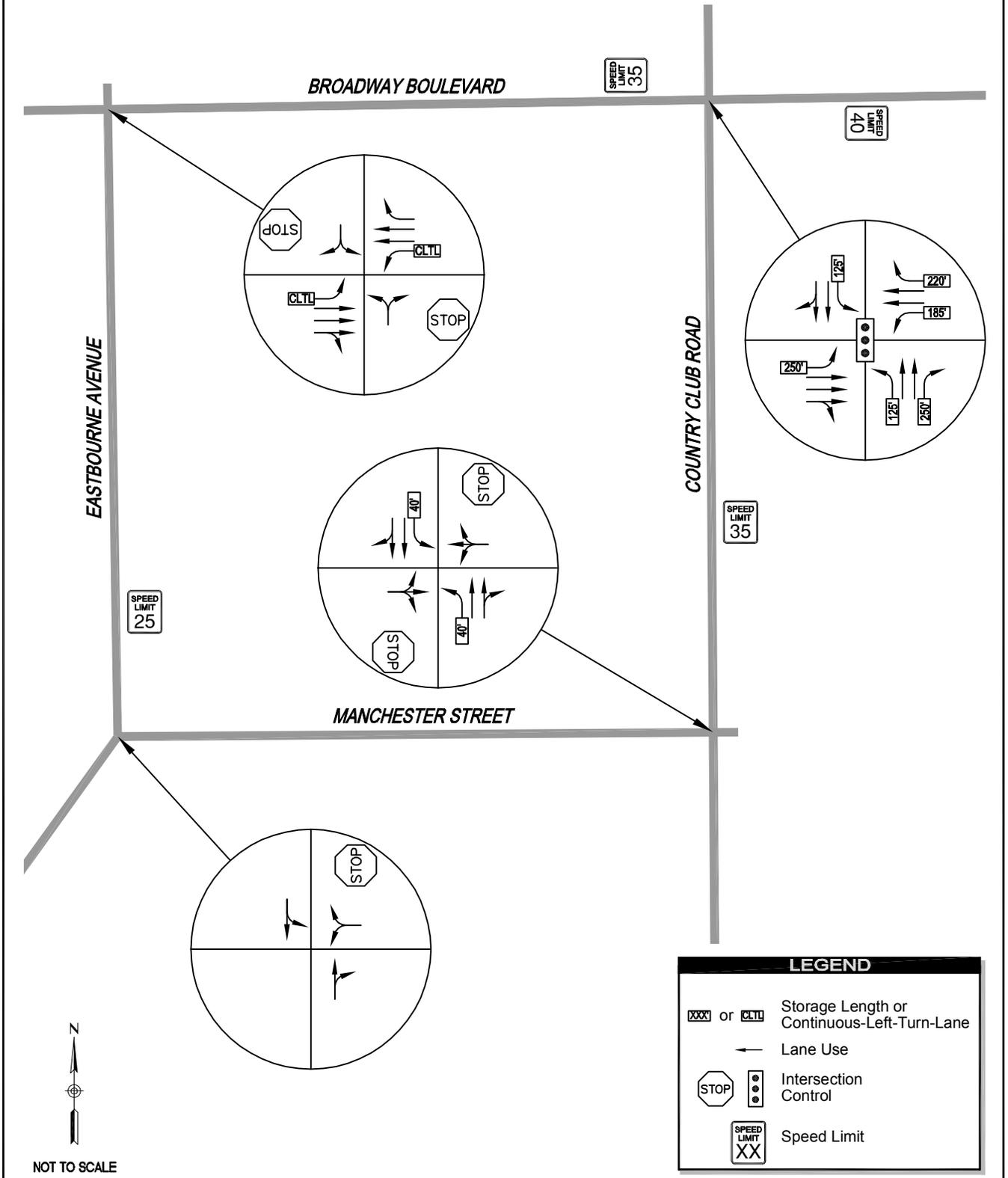
**Manchester Street**, in the vicinity of the site, provides one lane in each direction and does not provide sidewalks or bike lanes. The posted speed limit near the site is 25 mph.

#### **TRAFFIC VOLUMES**

Field Data Services of Arizona collected peak period turn movement counts on December 15, 2009 at the intersections of Broadway Boulevard and Country Club Road, Broadway Boulevard and Eastbourne Avenue, Country Club Road and Manchester Street, and Eastbourne Avenue and Manchester Street. Traffic count data can be found in the **Appendix**. Count data was reviewed and determined to be representative of existing conditions. Existing AM and PM peak hour traffic volumes are shown in **Exhibit 4**.



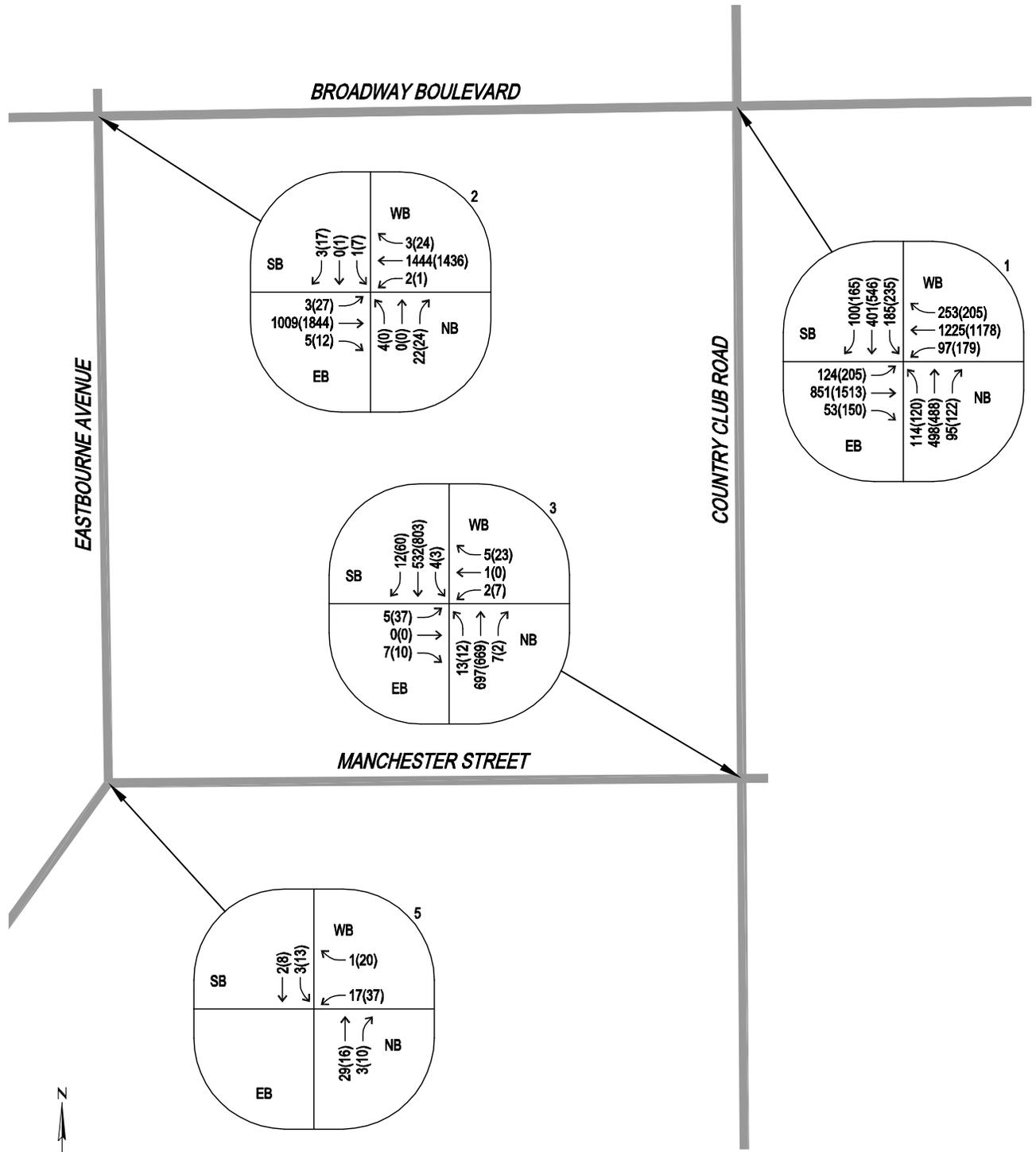
# Existing Conditions (2009)



NOT TO SCALE



# Existing Peak Hour Traffic (2009)



NOT TO SCALE

**LEGEND**

← XX(XX) AM(PM) Peak Hour Traffic Volumes



**EXISTING LEVEL OF SERVICE**

Traffic operations under existing conditions were evaluated for the AM and PM peak hours. The intersection was analyzed using Synchro 7.0, which utilizes the methodologies outlined in the *2000 Highway Capacity Manual*. Results of the analysis are shown in **Exhibit 5**. Output sheets for the analysis are provided in the Appendix.

**EXHIBIT 5 – EXISTING LOS (2009)**

Local Intersections	EB			WB			NB			SB			Intersection LOS *	Traffic Control
	L	T	R	L	T	R	L	T	R	L	T	R		
Broadway Boulevard / Country Club Road														
AM Peak Hour	D		B	B	C	A	C	D	A	D		C	C	Signalized
PM Peak Hour	E		C	E	D	A	D	C	A	E		D	D	
Broadway Boulevard / Eastbourne Avenue														
AM Peak Hour	B			B				B				B	B	Unsignalized
PM Peak Hour	B			C				B				C	C	
Country Club Road / Manchester Street														
AM Peak Hour			B			C						A		Unsignalized
PM Peak Hour			D			B						A		
Eastbourne Avenue / Manchester Street														
AM Peak Hour						A						A		Unsignalized
PM Peak Hour						A						A		

\* Intersection LOS for unsignalized intersections is reported as "Worst-Movement LOS"

As shown, all adjacent intersections operate at acceptable levels of service during the peak hours. It should be noted that several left-turn movements at the intersection of Broadway Boulevard and Country Club Road operate at unacceptable levels of service during the PM peak hour. This is typical of a major east/west arterial and reflects the large volume of regional traffic that utilizes the corridor. The planned City of Tucson widening of Broadway Boulevard will improve signal operations at this location when construction is complete.



## IV. PROJECTED TRAFFIC

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### ***SITE TRAFFIC FORECASTS***

#### Trip Generation

The Broadway Village site is largely an existing development that will be improved so much of the future trip generation is based on the two new buildings which will consist of 10,000 square feet of retail and office uses. While the existing building is already utilized, the occupancy is not 100 percent. A trip generation spreadsheet showing the existing individual uses is provided in **Exhibit 6A** for reference. The Institute of Transportation Engineers' *Trip Generation, 8<sup>th</sup> Edition* was used to obtain daily and peak hour trip generation rates and inbound-outbound percentages.

The future trip generation calculations were based on the use of "Specialty Retail" for the existing buildings and one of the new 5,000 square-foot buildings, and assumes 100 percent occupancy. The use of "Specialty Retail" covers many tenants including – apparel shops, professional offices, florists, small restaurants, and other typical users of smaller strip shopping centers. Future trip generation is provided in **Exhibit 6B** which also indicates the overall change in trip generation from existing to future uses.

#### **EXHIBIT 6A – TRIP GENERATION (EXISTING)**



Land Use	Intensity	Units	Daily Total	AM Peak			PM Peak		
				In	Out	Total	In	Out	Total
Specialty Retail Center	6	1000 SF	271	4	2	6	7	10	17
Apparel Store	1	1000 SF	74	1	0	1	2	2	4
Furniture Store	6	1000 SF	30	1	0	1	1	2	3
Hair Salon	5	1000 SF	221	6	0	6	1	6	7
Quality Restaurant	6	1000 SF	495	2	2	4	27	14	41
<b>TOTAL</b>			<b>1,091</b>	<b>13</b>	<b>5</b>	<b>18</b>	<b>40</b>	<b>32</b>	<b>72</b>

**Specialty Retail Center**

ITE 8th Edition: 814

Daily T = 44.32\*(1000's of SF) 50% In 50% Out  
 AM Peak Hour T = 1.00\*(1000's of SF) (ITE 820) 61% In 39% Out  
 PM Peak Hour T = 2.71\*(1000's of SF) 44% In 56% Out

**Apparel Store**

ITE 8th Edition: 876 0

Daily T = 66.40\*(1000's of SF) 50% In 50% Out  
 AM Peak Hour T = 1.00\*(1000's of SF) (ITE 820) 61% In 39% Out  
 PM Peak Hour T = 3.83\*(1000's of SF) 50% In 50% Out

**Furniture Store**

ITE 8th Edition: 890

Daily T = 5.06\*(1000's of SF) 50% In 50% Out  
 AM Peak Hour T = 0.17\*(1000's of SF) 69% In 31% Out  
 PM Peak Hour T = 0.45\*(1000's of SF) 48% In 52% Out

**Hair Salon**

ITE 8th Edition: 918

Daily T = 44.32\*(1000's of SF) (ITE 814) 50% In 50% Out  
 AM Peak Hour T = 1.21\*(1000's of SF) 100% In 0% Out  
 PM Peak Hour T = 1.45\*(1000's of SF) 17% In 83% Out

**Quality Restaurant**

ITE 8th Edition: 931

Daily T = 89.95\*(1000's of SF) 50% In 50% Out  
 AM Peak Hour T = 0.81\*(1000's of SF) 50% In 50% Out  
 PM Peak Hour T = 7.49\*(1000's of SF) 67% In 33% Out

**EXHIBIT 6B – TRIP GENERATION (PROPOSED)**

Land Use	Intensity	Units	Daily Total	AM Peak			PM Peak		
				In	Out	Total	In	Out	Total
General Office Building	5	1000 SF	55	7	1	8	1	6	7
Specialty Retail Center	42	1000 SF	1,870	26	16	42	50	64	114
<b>TOTAL</b>			<b>1,925</b>	<b>33</b>	<b>17</b>	<b>50</b>	<b>51</b>	<b>70</b>	<b>121</b>
<b>Change from Existing</b>				<b>20</b>	<b>12</b>	<b>32</b>	<b>11</b>	<b>38</b>	<b>49</b>

**General Office Building**

ITE 8th Edition: 710

Daily T = 11.01\*(1000's of SF) 50% In 50% Out  
 AM Peak Hour T = 1.55\*(1000's of SF) 88% In 12% Out  
 PM Peak Hour T = 1.49\*(1000's of SF) 17% In 83% Out

**Specialty Retail Center**

ITE 8th Edition: 814

Daily T = 44.32\*(1000's of SF) 50% In 50% Out  
 AM Peak Hour T = 1.00\*(1000's of SF) (ITE 820) 61% In 39% Out  
 PM Peak Hour T = 2.71\*(1000's of SF) 44% In 56% Out

As shown, trip generation for the redevelopment of the Broadway Village shopping center results in 32 new trips in the AM peak hour and 49 new trips in the PM peak hour.



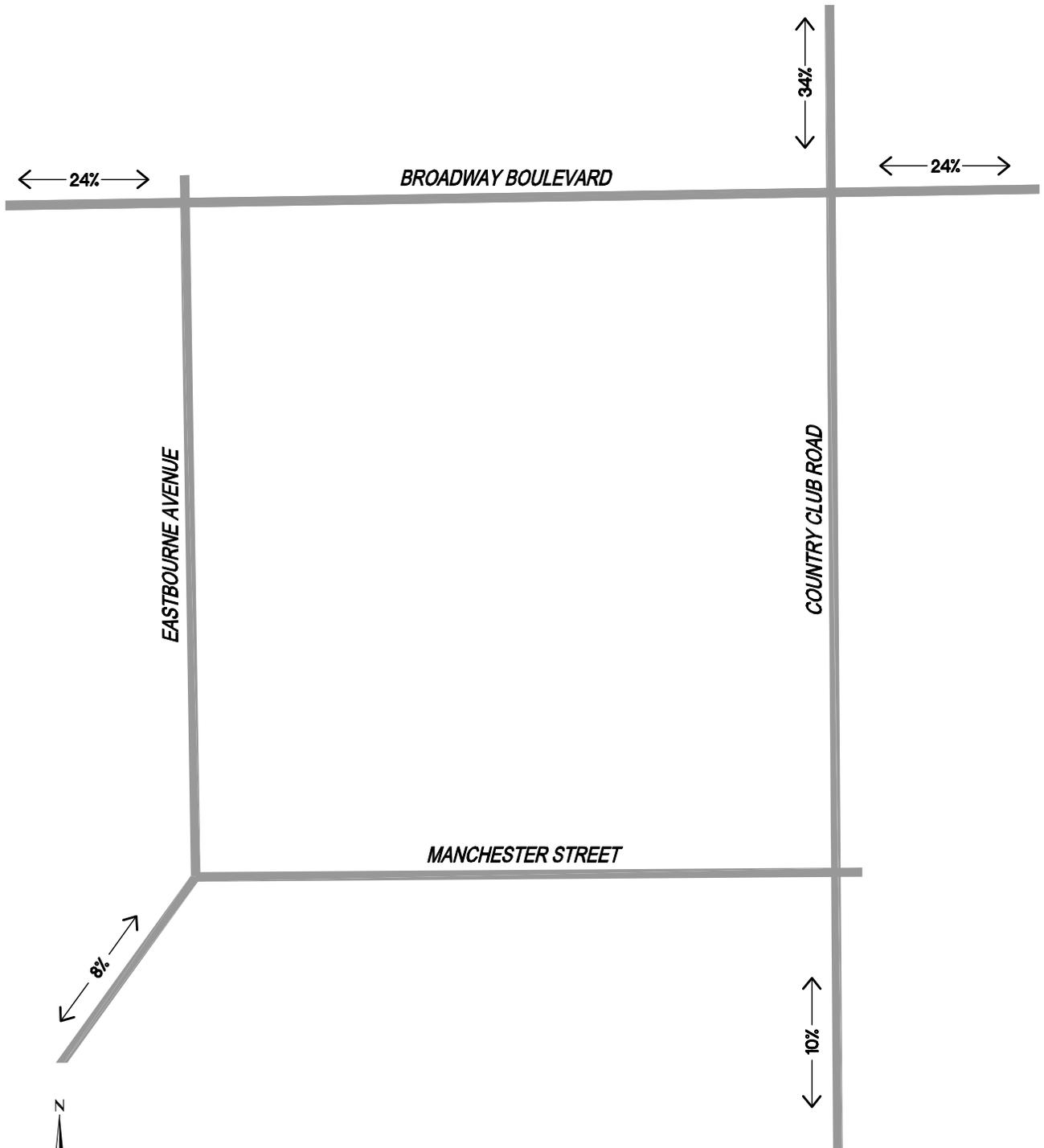
Kimley-Horn  
and Associates, Inc.

### Directional Distribution

Daily and peak hour site generated trips were distributed based upon residential housing data within a 3 mile radius. The distribution was further refined by considering the access points and roadway network near the site. The directional distribution of project traffic is illustrated in **Exhibit 7**. **Exhibit 8** shows the peak hour site generated traffic volumes for analysis year (2010).



# Directional Distribution Analysis Year (2010)



NOT TO SCALE

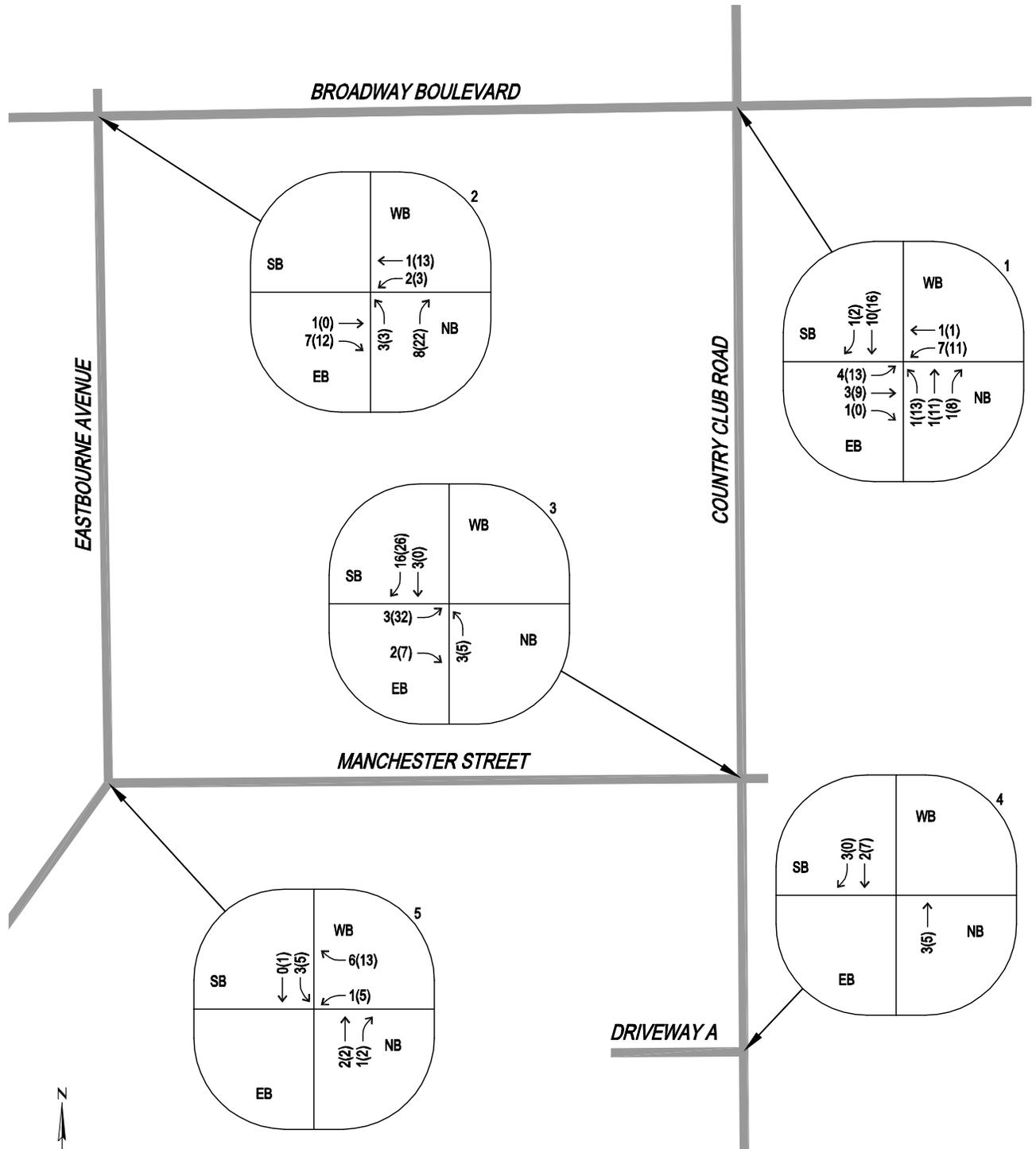
**LEGEND**

← X% →

Directional Distribution Percentage



# Site Generated Traffic Volumes (2010)



NOT TO SCALE

**LEGEND**

← XX(X) AM(PM) Peak Hour Traffic Volumes



### **BACKGROUND TRAFFIC FORECASTING**

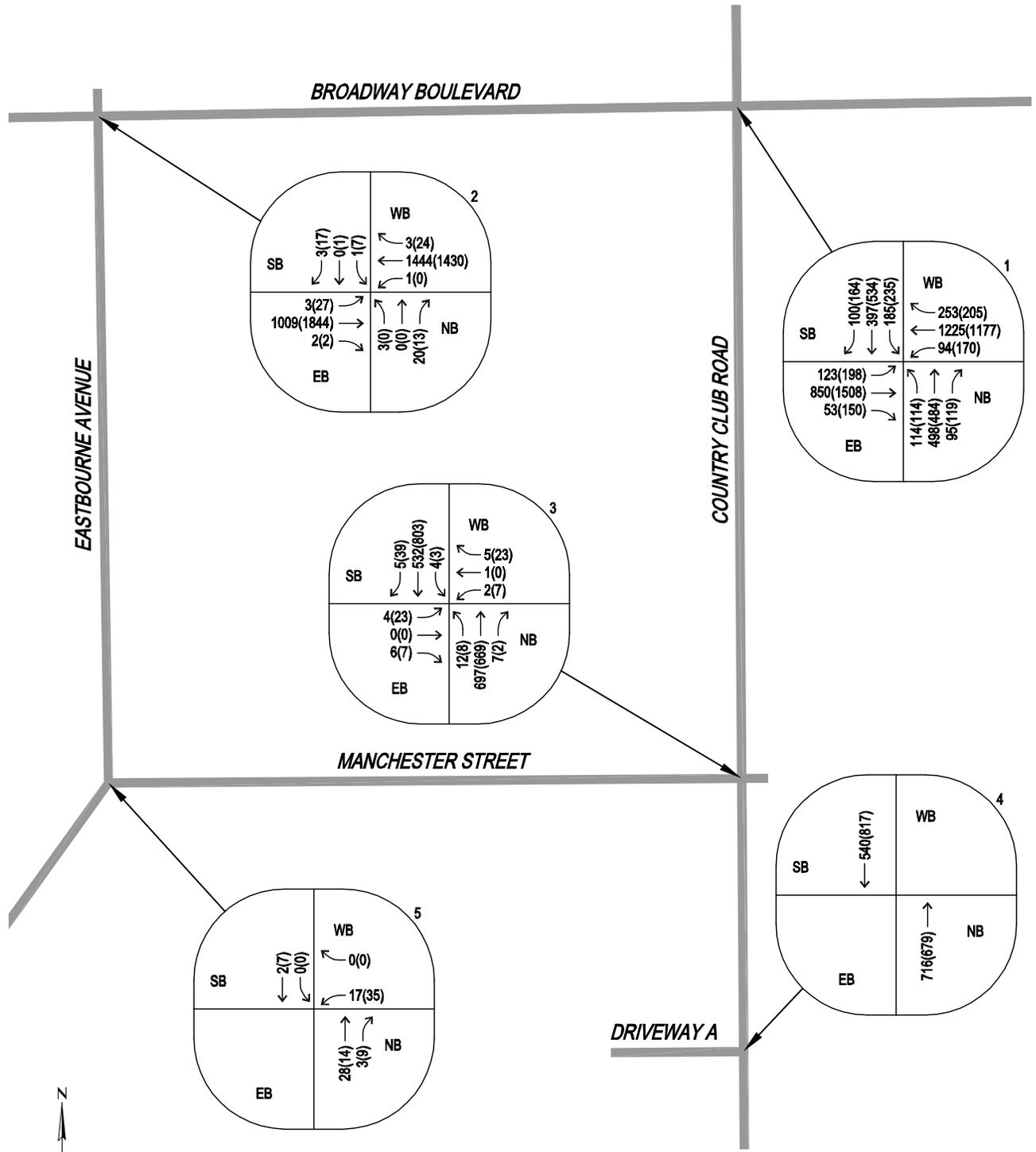
For analysis of future conditions, background traffic volumes for the analysis year (2010) had to be developed. The existing trip generation estimate for the current occupancy of Broadway Village shopping center, as defined in Exhibit 6A, was utilized to reduce the existing traffic counts so that the impact of full occupancy could be evaluated in addition to the new buildings. These retail-related trips were subtracted from the actual traffic counts which resulted in background volumes onto which the entire trip generation of the shopping center could be added. **Exhibit 9** shows the peak hour background traffic volumes for analysis year (2010).

### **TOTAL TRAFFIC**

The total traffic volumes at study area intersections were determined by adding site-generated traffic to the background volumes. **Exhibits 10** show the total traffic volumes expected in the analysis year (2010).



# Background Traffic Volumes (2010)



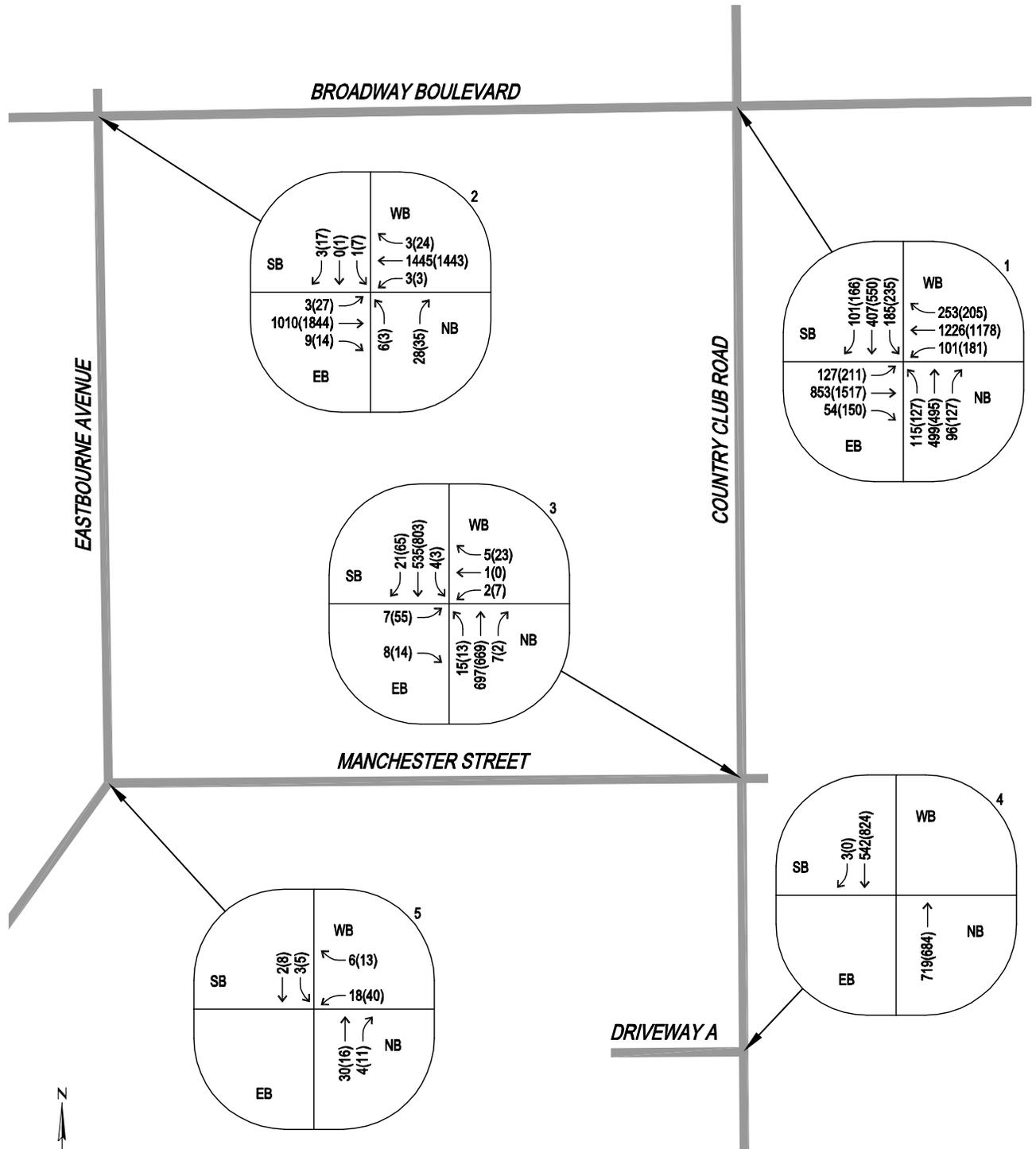
NOT TO SCALE

**LEGEND**

← XX(X) AM(PM) Peak Hour Traffic Volumes



# Total Traffic Volumes (2010)



NOT TO SCALE

**LEGEND**

← XX(X) AM(PM) Peak Hour Traffic Volumes



## V. TRAFFIC AND IMPROVEMENT ANALYSIS

### LEVEL OF SERVICE

The study area intersections were evaluated on the basis of future traffic projections shown in Exhibit 10. All intersections were analyzed using Synchro 7.0 which utilizes the methodologies outlined in the *Highway Capacity Manual 2000*. The results of the traffic analysis are shown in Exhibit 11 for the analysis year (2010). Exhibit 12 shows the intersection lane use assumptions used in the analysis of the site driveways. Software output sheets for the analyses are located in the Appendix.

EXHIBIT 11 – LEVEL OF SERVICE ANALYSIS (2010)

Local Intersections	EB			WB			NB			SB			Intersection LOS *	Traffic Control
	L	T	R	L	T	R	L	T	R	L	T	R		
Broadway Boulevard / Country Club Road														
AM Peak Hour	D	B		B	C	A	C	D	A	D		C	C	Signalized
PM Peak Hour	F		C	E	D	A	D	C	A	E		D	D	
Broadway Boulevard / Eastbourne Avenue														
AM Peak Hour	B			B				B				B	B	Unsignalized
PM Peak Hour	B			C				C				C	C	
Country Club Road / Manchester Street														
AM Peak Hour		B			C		A			A			C	Unsignalized
PM Peak Hour		E			B		A			A			E	
Eastbourne Avenue / Manchester Street														
AM Peak Hour					A					A			A	Unsignalized
PM Peak Hour					A					A			A	
Country Club Road / Driveway A														
AM Peak Hour				A									A	Unsignalized
PM Peak Hour				A									A	

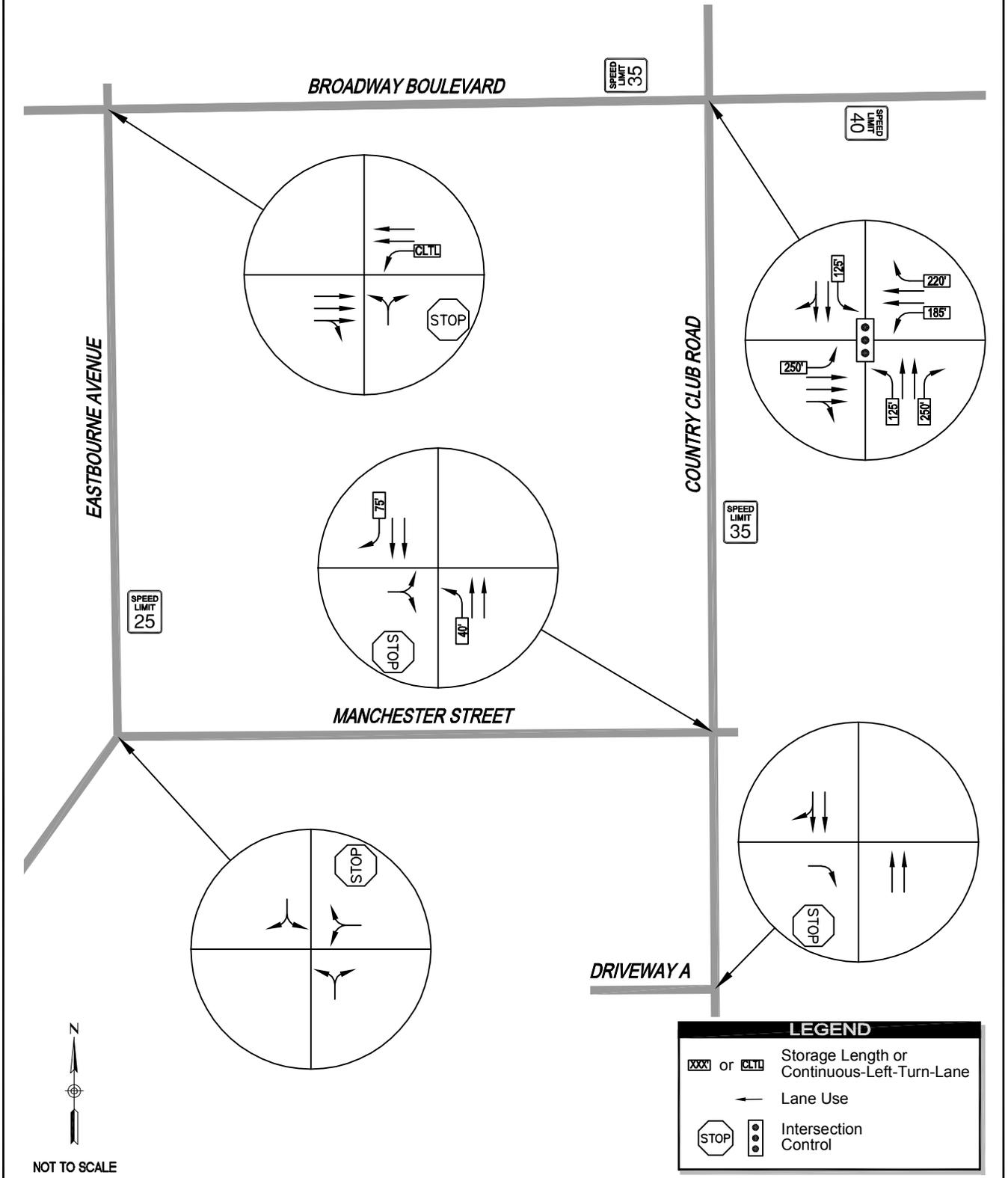
\* Intersection LOS for unsignalized intersections is reported as "Worst-Movement LOS"

As shown, it is anticipated that all study area intersections and driveways will operate at acceptable levels of service with the exception of the Country Club Road / Manchester Street intersection. The eastbound left-turn movement at Country Club Road / Manchester Street is anticipated to operate at LOS E which is typical of unsignalized intersections that allow left-turn movements onto arterials. The projected volume of left-turns during the peak hour is 55 vehicles which is not significant and drivers have the option of utilizing the Broadway Boulevard frontage via Eastbourne Avenue if delays increase.

Several left-turn movements at the intersection of Broadway Boulevard and Country Club Road will operate at unacceptable levels of service during the PM peak hour. This reflects the large volume of regional traffic that utilizes the intersection and the fact that the major east/west through movements have volume-to-capacity ratios above 0.90. The addition of a third through lane in each direction along Broadway Boulevard, as planned by the City, would improve the level of service for all movements to LOS E or better.



# Analysis Conditions (2010)



NOT TO SCALE

LEGEND	
	Storage Length or Continuous-Left-Turn-Lane
	Lane Use
	Intersection Control



### **TURN LANE ANALYSIS**

The need for left- and right-turn lanes at the site driveways was determined using the *Transportation Access Management Guidelines for the City of Tucson, March 2003*.

The existing left-turn lanes into the site are as follows: a westbound left-turn lane at Broadway Boulevard / Eastbourne Avenue and a northbound left-turn lane at Country Club Road / Manchester Street. The left-turn lane along Broadway Boulevard is a short section of continuous left-turn lane and Synchro analysis does not indicate a need for additional storage length. The left-turn lane along Country Club Road is limited to 40 feet which is less than the standard distance of 75 feet however Synchro analysis does not indicate a need for additional storage length. Driveway A will be right-in/right-out therefore a left-turn lane will not be provided.

The right-turn lane warrant compares the volumes of vehicles making right-turns to through volumes in the same direction. Based on projected future volumes, a dedicated southbound right-turn lane is warranted at Country Club Road / Manchester Street. It is recommended that a dedicated right-turn lane be provided at this location with a minimum length of 75 feet. A dedicated right-turn lane is not warranted along Country Club Road for Driveway A based on the total traffic volumes shown in Exhibit 10. The intersection of Broadway Boulevard / Eastbourne Avenue, while exceeding the City warrants, should not provide a dedicated right-turn lane as Broadway Boulevard transitions from two through lanes to three through lanes just prior to that intersection. The provision of a dedicated right-turn lane at this location should be evaluated as part of the larger scale roadway widening project.

### **DRIVEWAY SPACING**

Per City of Tucson standards, access points are limited to two per 300 feet of frontage with the nearest pavement edges spaced at least 80 feet apart. The spacing between the Manchester Street and Driveway A meets the minimum driveway spacing as there will be approximately 120 feet distance between the nearest pavement edges.

### **SIGHT TRIANGLES**

It is recommended that sight triangles consistent with the requirements described in *City of Tucson Development Standards* be provided at all site driveways.



## VI. CONCLUSIONS AND RECOMMENDATIONS

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This analysis has provided an overview of the traffic operations and the recommended improvements for the redevelopment of the Broadway Village shopping center located on the southwest corner of Broadway Boulevard and Country Club Road in the City of Tucson, Arizona. Following are the major conclusions of this analysis:

- The redevelopment of the site will result in the addition of approximately 10,000 square feet of retail/office, the reconstruction of the associated parking fields, and improvements to the public roadways that bisect the existing center.
- The existing access points along Broadway Boulevard and Country Club Road will remain. In addition, a new right-in, right-out driveway is proposed on Country Club Road south of Manchester Street. The driveway spacing between Manchester Street and Driveway A meets the City of Tucson driveway spacing requirements.
- All study area intersections currently operate at acceptable levels of service during the peak hours. It should be noted that several left-turn movements at the intersection of Broadway Boulevard and Country Club Road operate at unacceptable levels of service during the PM peak hour.
- Trip generation for the redevelopment of the Broadway Village shopping center results in 32 new trips in the AM peak hour and 49 new trips in the PM peak hour.
- It is anticipated that all study area intersections and driveways will operate at acceptable levels of service with the exception of the Country Club Road / Manchester Street intersection. The eastbound left-turn movement at Country Club Road / Manchester Street is anticipated to operate at LOS E which is typical of unsignalized intersections that allow left-turn movements onto arterials.
- The City of Tucson has an ongoing project, Broadway Boulevard Corridor, that will result in the widening of Broadway Boulevard from Euclid Avenue to Country Club Road. Preliminary information indicates that widening will result in an 8-lane roadway with one lane in each direction dedicated to transit.
- It is recommended that a dedicated southbound right-turn lane be provided at Country Club Road / Manchester Street with a minimum length of 75 feet. A dedicated right-turn lane is not warranted along Country Club Road for Driveway A.
- It is recommended that sight triangles consistent with the requirements described in *City of Tucson Development Standards* be provided at all site driveways.