

## HOUGHTON TOWN CENTER 2 PLANNED AREA DEVELOPMENT

SWC HOUGHTON AND OLD VAIL ROADS  
TUCSON, ARIZONA

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and  
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# SECTION 1 - INTRODUCTION

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## HOUGHTON TOWN CENTER 2 Planned Area Development

## A. Project Overview

The Houghton Town Center 2 Planned Area Development (“PAD”) encompasses approximately 68 acres in an infill area of the City of Tucson (“City”) at 9688 to 10080 East Old Vail Road near the corner of Houghton and Old Vail Roads (the “Property”). The Property is located approximately 1.5 miles north of the interchange at Interstate 10 and Houghton Road. The PAD provides employment, commercial, and retail opportunities to support its strategic location on the Houghton corridor, southeast Arizona, and to the surrounding neighborhoods in Ward 4. The Property is within the Esmond Station Area Plan and the Rita Ranch Neighborhood Association. No other Neighborhood Associations are located within a mile of the Property. The majority of the Property is owned by Houghton Commerce, LLC and Houghton Developers, LLC, (“HCHD”). Houghton Town Center Homeowners Association owns common space within Houghton Town Center (“HTC”) and will own common space within the Property when developed. The Property was annexed into the City by Ordinance No. 6142 in 1985 and has been zoned I-2 (Heavy Industrial) since original annexation. A portion of the Property is zoned C-1 (Commercial). A change in zoning to Planned Area Development is requested for development of the Property to appropriately acknowledge its location contiguous to HTC community development to the east and the ability to provide services and jobs to the surrounding community. The Property will provide for a mix of commercial uses and employment including, but not limited to, retail, food service, commercial, hospitality, industrial, healthcare, recreational, and office uses (the “Project”).

## B. Rationale and Benefits for Use of a PAD

A Planned Area Development zoning designation is designed to allow for comprehensively planned development with appropriate design standards. The primary purpose of this PAD is to ensure a cohesive development with design standards balanced with flexibility in developing the Property based on market demands. The development of the Property will be an asset to the immediate area and to the City by providing for diverse uses that serve a variety of needs in the community. The development is an infill project that is perfectly situated to serve the existing suburban Rita Ranch neighborhood, Pima County residents and businesses, jurisdictions further east including Vail and Benson, and residents and businesses south of the I-10 freeway. Along with the diversity in permitted uses, the PAD also encourages diversity in building massing and accessibility that adds architectural and contextual interest and excitement to the development. The development is the result of a master planning effort that appropriately utilizes a Planned Area Development zoning and development standards.

## C. Conformance with Plan Tucson

The PAD conforms to the policies of Plan Tucson, the City’s General & Sustainability Plan (“PT”). The redevelopment will provide for significant economic improvements to a highly visible and underutilized parcel of land that can serve both the surrounding community and those traveling to and through the region. The Property is an infill project surrounded by large tracts of both undeveloped and developed commercial and industrial sites and is identified in PT as a business center. The Rita Ranch residential neighborhood lies north of the property. The PAD will support a variety of commercial uses, and residents will appreciate the opportunities for employment as well as access to new and diverse commercial products and services. The Property is well located to capture a regional customer base.

The variety of land uses imagined for this PAD will encourage use of the Property by residents and businesses, a concept eagerly supported by PT. With easy access to the freeway and, of course, the businesses on the Property and along Houghton and Old Vail Roads, the PAD can serve as a primary destination for people from the City, County, and region.

The Property, located a short distance north of the I-10 Freeway, anchors the residential neighborhood without encroaching into it and revitalizes and stabilizes the region with an attractive, built environment that also improves tax revenues and property values.

Following are a few of the focus areas and policies that are supported with this PAD:

**Redevelopment & Revitalization:**

- **Redevelop and revitalize in areas with the greatest potential for long-term economic development**

The Property is at an ideal location in for the type of development proposed and is identified in Exhibit LT-7 for Business Centers. The largely vacant Property will be developed to provide a tax base for the City, jobs for the community, and a new shopping, hospitality, office, and general business destination for the region. Nearby developments provide a synergy of commercial uses that are compatible with the development of the Property. This development will be an asset for the neighbors and the region and will provide a major commercial core for this area in the City.

**Land Use, Transportation, & Urban Design:**

- **Locate retail and services to allow easy access between uses and reduce dependence on the car**
- **Increase the use of transit, walking and bicycles**
- **Create continuous pedestrian and bicycle networks**

The Property creates a commercial environment on otherwise vacant parcels with the potential for retail, hospitality, medical, office, industrial and service uses in a cohesively-designed development that is easily accessible. The Property is conveniently situated for access by vehicles using Houghton and Old Vail Roads and Interstate 10. The uses on the Property will be linked by lighted sidewalks to promote more pedestrian use of the development and a more urban character. This “three dimensional” planning approach is supported by PT as a way of creating a community’s “sense of place.” Both Houghton and Old Vail Roads provide bike routes, and bicycle parking will be provided to meet the needs of the biking community and satisfy the bicycle parking requirements as described in Section 3 of the PAD. The overall design of the Property will be appropriate for the southwest and for the surrounding built environment. The Property is not served by a bus route; however, it is within a Sun Van and RTA Sun Shuttle area. A Park and Ride Lot is located north of the Property on Old Vail Road.

**Economic Environment:**

- Benefits to residents and attraction to visitors
- Safe, distinctive, well-maintained, and attractive community contributing to Tucson’s quality of life and economic success.

The Property will provide commercial goods and services that benefit residents, business, and visitors alike, jobs that will boost the local economy, and increased tax revenues that benefit the City as a whole. The redevelopment will enhance the attractiveness of the immediate area by revitalizing underutilized, mostly vacant parcels of land.

**Public Safety:**

- **Encourage design for private spaces that incorporates principles of defensible space.**

The Property will be designed to allow for views from all businesses to pedestrian areas by use of adequate storefronts, carefully located landscaping, lighting that reduces shadows and dark areas, highly visible pedestrian and bicycle parking facilities, and security measures such as cameras and personnel. The backside of the buildings in the Property will also have adequate security lighting and will be designed to preclude access to the neighborhood to the north from the Property.

## Business Climate

- **Enhance Tucson’s business climate**

This PAD will allow for development of commercial businesses and services on the Property that will attract residents and travelers along the nearby I-10 Freeway. The Property will provide economic stability, serve as a destination for residents and tourists alike, and increase tax revenues.

## Water Resources:

- **Integrate land use and water resources planning**

Passive water harvesting will be provided on the Property, and landscaping throughout the parking lots will help reduce the carbon footprint while offering an attractive environment. The drainage way at the south of the HTC property provides a riparian corridor while meeting the drainage needs of the site.

## D. Esmond Station Area Plan

The Property is located in the Esmond Station Area Plan (“ESAP”) adopted by Mayor and Council in 1986. The ESAP’s goal is “to guide the creation of livable communities in the Esmond Station area where existing and future land uses can harmonize without conflict.” Commercial and industrial development policies that would be relative to this Property include:

- Promote commercial developments at appropriate locations and in appropriate amounts within the area.
- Ensure commercial development is compatible with surrounding uses.
- Ensure the availability of adequate services and the proper setting for industrial developments.
- Ensure that industrial development is compatible with surrounding uses.

The Property is currently zoned I-2 (Heavy Industrial) which is supported by the ESAP. Existing commercial and industrial uses are located adjacent to and near the Property which is approximately ¼ mile from the nearest residential development to the north.

Rezoning to a PAD allows for a greater variety of commercial and industrial uses that will improve developability of the sites to meet market needs. Fully improved arterials Houghton Road and Old Vail Road provide adequate access for the type of vehicles accessing the Property. Houghton Road leads directly to the I-10 freeway approximately ½ mile south of the Property. Old Vail Road leads to Rita Road further west and to another interchange with the freeway.

The Property is in the Davis-Monthan Air Force Base (“DM”) flight path which, according to the ESAP, renders it appropriate for commercial and industrial uses. The Property is in the ADC-3 zone and Noise Control Zone A, 65-70 Ldn zone. Besides industrial uses, the Unified Development Code (“UDC”) allows for a wide variety of commercial, retail, office, and service uses in airport environ zones subject to density and height limitations. The first phase of HTC has been largely realized with retail, service, restaurant, and office uses, and there is interest in expanding the Center with similar uses. There may also be industrial development which will benefit from the goods and services in HTC.

There are no specific recommendations in the ESAP relative to this Property.

## E. Compatibility with Adjoining Land Uses

The Property is bordered on the east by the developed HTC community center, on the west by industrial properties, on the north across Old Vail Road by industrial and commercial properties, and on the south by



undeveloped County- and State-owned land. The Union Pacific railroad runs parallel to the Property line at the south. The Rita Ranch residential neighborhood is situated approximately a quarter mile north of the closest corner of the Property. Much of the area within a mile around the Property is undeveloped except for the dense residential neighborhood further north and commercial and industrial uses. The PAD design will complement the existing commercial and residential developments and serve the community with commercial services, office space, and potential employment opportunities. The Property is separated from residential uses by a large area of industrial zoning to the north of Old Vail Road. The Property will have good visibility from Old Vail Road and from on-Property drives to facilitate safety and security.

## **F. Physical and Economic Suitability and Feasibility of the PAD**

The Property is largely undeveloped land. It is located with utilities surrounding but not yet fully extended to the Property. The Property is located along an east-west arterial street and within 1,000 feet of a north-south arterial and is buffered from residences by industrial-zoned land, making this a suitable location for a cohesively planned and developed commercial Property. Combined with other successful commercial developments and a large residential population on three sides, this is an ideal location for a major development offering new and expanded commercial possibilities.

# SECTION 2 - SITE ANALYSIS

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## HOUGHTON TOWN CENTER 2 Planned Area Development

**A. Significant Natural and Built Property Constraints**

The vast majority of the Property is relatively flat and a small portion of the Property has been cleared and re-graded in connection with development of HTC 1.

**B. Major Transportation and Circulation Elements Serving the PAD**

The Property will be accessed from Old Vail Road, a 90-foot wide MS&R route, which runs along the northern border of the Property and Houghton Road, a 200-foot wide scenic corridor MS&R route. Old Vail Road is a three-lane roadway with a center turn lane, and Houghton Road is a 4-lane divided roadway with left turn access into HTC. Access to and from Houghton Road will occur with a major private spine road that runs east-west through the development. Access points along Old Vail will be provided as required for the development. These will align with proposed and existing drives north of Old Vail Road. One signalized intersection may be provided as illustrated and discussed herein.

A revised traffic study has been completed by Rick Engineering Company and is included herein. Work included establishing current traffic counts along Houghton Road, Old Vail Road, and at the five existing driveways into the project, shown in the following table. Rick also performed Site Traffic Generation/Distribution//Capacity analyses. As discussed in the report, traffic operations require mitigation to improve traffic flow at intersections regardless of this PAD development. Recommendations are provided in the report.

Road Name	Old Vail Road	Houghton Road
City Classification	Arterial	Arterial
Existing ROW	90 feet	200 feet
Future ROW	90 feet	200 feet
Divided	No	Yes
Paved	Yes	Yes
Continuous	Yes	Yes
Travel Lanes	2	4
Speed Limit	45	55
Bike Route	Yes	Yes



<b>Average Daily Trip (Existing)</b>	2,820 (westbound) 2,778 (eastbound)	6,402 (northbound) 6,272 (southbound)
<b>Ownership</b>	City	City

**C. Existing and Nearby Zoning (within 150 feet of Property)**

Reference the Section 2 Exhibits, Exhibit 2. The existing zoning on the Property is I-2 (Heavy Industrial). The shopping center to the east of Property is zoned I-2 (Heavy Industrial) and C-1 (Commercial). The land north of the Property across Old Vail Road is zoned I-1 (Industrial) and C-1 (Commercial), and the industrial developments to the west are zoned I-2 (Heavy Industrial). To the south, the State-owned land is zoned I-2 (Heavy Industrial) and RX-1 (Residential). The Rita Ranch Neighborhood Association is mapped in the Section 2 Exhibits, Page 27

**D. Adjacent Parcels and Structures (within 150 feet of Property)**

To the north of the Property across Old Vail Road are large undeveloped industrially-zoned parcels and another tract of land being developed for a retail shopping center. Immediately west of the Property is an industrial site with an approximately 15,000 square foot building and other undeveloped land, and east is the Phase 1

development of HTC with over 250,000 square feet of constructed commercial buildings. South of the Property lies the Union Pacific railroad lines and large undeveloped State-owned parcels.

**E. Off-Property Open Space, Recreational Facilities, Parks, and Trails**

Reference Section 2 Exhibits, Page 25, for a mapping of open spaces/parks within a mile of the Property. There are no parks on or adjacent to the property. Purple Heart Park is located off Rita Road approximately .6 mile north. The Julian Wash Greenway is located approximately a mile to the west along the Union Pacific Railway. Esmond Station Park is located approximately a half mile to the northeast of the Property, and Southeast Regional Park is approximately two miles to the south across the freeway. A regional park is located across Houghton to the Northeast.

**F. Public, Educational, Community, and Cultural Facilities**

Reference Section 2 Exhibits, Page 25 for a mapping of facilities. The Property is in the Vail School District. One school is within one-mile driving distance of the Property: Empire High School. Immediately east of this school are the Vail Innovation Center and Vail Digital Learning Program. A new public library is located adjacent to the schools. Other schools within two to three miles driving distance include Cottonwood Elementary School, Desert Sky Middle School, and Mesquite Elementary School. There are no hospitals within a mile of the Property; however Northwest Emergency Center is located in the adjacent Houghton Town Center 1 property. The nearest fire station, Tucson Fire Department, Station 19, is 2.2 miles driving distance away on Esmond Loop northwest of the Property. A post office is located at the Rita Road and Houghton intersection approximately 1.4 miles north.

**G. Existing Drainage**

Based on the existing topography, the site drains in a southwesterly direction with an average slope of 1.5%. The on-site flows generally convey through shallow natural channels or are characterized by sheet flow. The project is



impacted by concentrated storm water discharge from Phase 1, east of the site and street flows from the southern half of Old Vail Road. Along the south property line a tiered retention/detention basin exists to meet the retention and detention requirements for Phases 1 and portions of future Phase 2. A series of outflow structures link the tiered basins (Basins 1, 3, and 4) with the final outlet located at the west side of Phase 2. Drainage Reports evaluating off-Property and on-Property existing drainage conditions will be required as part of the future development under the provisions of this PAD.

## **H. Overlay Zone Ordinances and MS&R Ordinance**

Old Vail Road is a 90-foot wide arterial street that borders the Property on the north running east-west. Houghton Road, a 200-foot wide scenic arterial, travels north-south approximately 564-feet to the east of the Property from the closest location on the Property. The scenic corridor for Houghton Road is not impacted by the Property. The existing private access road from Houghton Road that crosses HTC will continue through the Property and lead to Old Vail Road. The Property is also located in the Davis Monthan Air Force Base (“DMAFB”) Approach-Departure Corridor (“ADC-3”) and is subject to the Comprehensive Special Plan Area Policy. The maximum FAR for industrial, wholesaling, and storage land uses is .40 of the total project area including the Property and the existing HTC. For all other land uses, the maximum FAR is .20 of the total area. The Property is within the DMAFB NCD-A (65 LDN) Noise Control District. UDC Section 5.6.9 applies to this PAD.

## **I. Inventory of Existing Structures, Roads, etc.**

The Property is vacant land. No public or private paved roadways exist on the Property. Access will be provided by means of a proposed private drive connecting to an existing drive from Houghton Road and driveways leading directly to Old Vail Road.

## **J. Storm Water Drainage and Utilities**

Under the existing conditions, the site slopes southwesterly with an averaged slope of approximately 1.7%. According to the Tucson Stormwater Management Study, Balanced and Critical Basin Map, dated August 1999, the project lies within a drainage basin defined as balanced. Therefore, 5-year threshold retention and storm water detention apply to this project. Runoff from the site disperses westerly on surface and in shallow drainage ditch. With the proposed development, the runoff will continue to drain westerly via a combination of surface flow, underground storm-drain systems, drainage channels and basins, and leaves the site through spillways.

Utilities onsite consists of a private sewer system, a Tucson Water public water system, Tucson Electric Power primary electric, Southwest Gas public gas mains, plus public cable and telephone systems. In addition, there are two existing 10” high pressure gas mains, El Paso Natural Gas, crossing the northern half of the site.

## **K. Wastewater**

An onsite private sewer system has been constructed, and will be extended in phases, which consists of several thousand linear feet of 8-inch PVC pipe, with a minimum slope of 0.50%. 4-foot diameter manholes, per Pima County Regional Wastewater Reclamation (PCRWRD) standards, are been placed at each grade and alignment change. This private system outlets into manhole #8762-09 of the existing 12” public sewer, PN G-87-37, located at the southwest corner of the site. PCRWRD assures that there is downstream capacity for this project’s sewer system, as required by AAC R18-9-E301.C.1. The building plumbing is served by Building Connection Sewer (BCS), varies in size, connecting from this 8-inch main to cleanouts located approximately 5-feet outside the building. No effluent will be used for the benefit of the project.

## **L. Existing Infrastructure and Public Services**

Other than public utilities within the project, there are no public roadway, public drainage way or infrastructures within the project site.

## M. Hydrology

In the future condition, the existing drainage patterns on the site will be maintained. Per City of Tucson/Pima County Detention/Retention Manual, a commercial development greater than one-acre in size, must provide 5-year threshold retention. Per the Tucson Stormwater Management Study Balanced and Critical Basin Map, the project site is located within a Balanced Basin. As a result, detention requirements are warranted in order to insure the post-development 2-, 10- and 100-year peak discharges are less than or equal to the pre-development discharges. Both retention and detention requirements will be met with the future extension of the existing tiered Basin (Basin 1, 3 and 4) along the south side of the property. For additional hydrology discussion, reference the Preliminary Drainage Statement prepared for this PAD document (Appendix A).

## N. Topography and Slope

The elevation on the site ranges from 3,010 feet (at the east property) to 2,975 feet (at the southwest property corner) above Mean Sea Level. Under existing conditions, the site slopes approximately 1.72% in a westerly direction as sheet flow. In general, the topography on the site slopes in a southwesterly direction, from the intersection of Old Vail Road and Houghton Road to the southwest corner of the project. The project site consists of 118.2 acres of land. The undeveloped portion of the site has relatively low vegetation. The subsurface soils consist of sandy lean clay, lean clay with sand, clayey sand, poorly graded gravel with sand, and sandy silt. The nearest receiving water, an unnamed tributary to the Julian Wash, is approximately 0.1 miles to the southwest from the site. This project is not located within ¼ mile of an Impaired (as listed under section 303(d) of the Clean Water Act) or Unique Water (as listed under AAC R18-11-112). To the best of our knowledge, there are no wetlands located on or near the site.

## O. Wildlife

A restored riparian area was created under agreement with the City in a new drainage area on the south side of the development. This drainage area will remain and be extended and enhanced with development of HTC 2 and will support wildlife habitat.

## P. Geology and Soils

A Geotechnical Report by ConformaTECH was completed on October 27, 2014 for the Property. The study anticipated typical buildings found in malls and shopping centers and some multi-story construction, large areas of asphalt concrete for parking and driveways and concrete paving for loading and some driving areas, a bridge over the tributary at the southwest corner of the Property, and detention basins. Fifteen borings were taken around the Property in widely spaced locations. Considerations that were identified include:

- Deposits of soils that classify as clays, silts, and sands;
- The clays are generally low or medium in plasticity;
- Some lime cementation varying from weak or moderate were found at the shallow Property soils;
- Deep sands encountered were non-plastic and were un-cemented;
- No free groundwater was encountered.

According to the report, the clays have low to moderate swell potential under anticipated floor slab and foundation loading conditions. Surface soils in the eastern side of the Property have a moderate settlement potential under anticipated foundation loading conditions and moderate to high settlement potential under heavier foundation loading conditions. The clay soils have poor pavement support capability due to the plasticity and fines content of the soils, and clayey sands have poor pavement support capacity due to low density and settlement potential when wetted. Nearly all the soils have a collapse potential varying from low to moderate under a sustained wetted condition. Engineered fill will be required to minimize the potential for excessive settlements. Spread footing foundations and concrete slabs on structural fill are recommended for all buildings. A copy of the report is included in Appendix 1.

**Q. View Sheds and Visual Analysis**

There are long distant views to the Catalina Mountains to the north, Tucson Mountains to the west, Rincon Mountains to the northeast, and Santa Rita Mountains to the south from the Property. The views are of little significance due to their distance and obstruction by other buildings in the area. There are no other important views from or across this Property. Reference the Exhibit 2 Exhibits, pages 22-24, for view sheds.

**R. Cultural (Archaeological and Historical) Sites**

An on-foot archaeological survey of the Property and adjacent Houghton Town Center 1 site was conducted in 1999 by Professional Archaeological Services and Technologies. Two sites were found. One was limited to two rock features and two chipped stone artifacts. One feature appeared to be a roasting pit less than a meter in diameter, and the other could not be positively identified. It was determined that no meaningful information could be derived from the site. The other site appeared to be associated with construction of the railroad and included some debris, solder top cans, railroad construction materials, and purple glass. It was suggested that ground-disturbing activities could reveal undiscovered archaeological resources that would signal a stop to construction activities until consultation with the State Historic Preservation Office is completed.

A Class III Cultural Resources Survey was produced by Harris Environmental Group, Inc, in 2005 for the a large portion of the Property. Before field work was conducted for this work, site records within a mile of the Property were investigated that indicated 17 Class III Cultural Resources Surveys were conducted. A total of nine archaeological site were identified within a mile of the Property including Hohokam roasting pits and artifact scatters, historic foundations and artifacts, the historic Esmond Railroad Station, and the southern alignment of the Southern Pacific Railroad. Seven isolated occurrences (artifacts and features that do not qualify as archaeological sites) were identified near but not on the Property. The isolated occurrences include old food, beverage, and tobacco type cans. One archaeological site that was recommended ineligible to the National Register of Historic Places, a dumping area dating to the early 1900's, is identified on the Property consisting of sixty old, beverage and food cans with solder-dots and crimped seams. No additional archaeological investigations were recommended for the project area.

**S. Existing Vegetation**

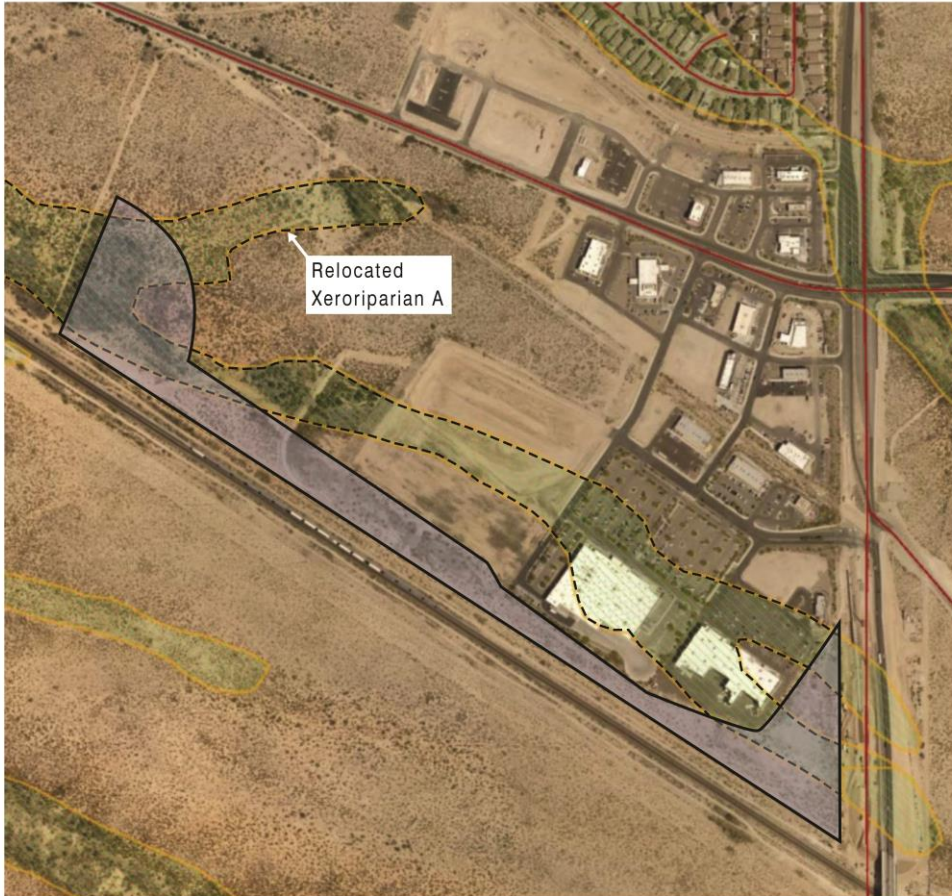
The vegetation within the HTC 2 PAD is representative of the Arizona Upland Subdivision of Sonoran Desert scrub. Specifically, this site is characterized by concentration of creosote bush and isolated occurrences of Mesquite, Acacia and Palo Verde species located at runoff collection points. The site runoff points have changed or been altered over time due to the perimeter changes that occurred along Old Vail Road, Houghton road and Rail line. These changes to the drainage patterns have impacted site drainage flows affecting the existing vegetation.

Predominant Native Species with PAD:

Parkinsonia florida	Blue Paloverde	Celtis ehrenbergiana	Desert Hackberry
Prosopis velutina	Velvet Mesquite	Baccharis sarothroides	Desert Broom (weed)
Acacia constricta	Whitethorn Acacia	Hilaria mutica	Tobosa grass
Acacia greggii	Catclaw Acacia	Ambrosia deltoidea	Triangle leaf bursage
Larrea tridentata	Creosote Bush	Ericamerica laricifolia	Turpentine bush
Opuntia sp.	Cholla Cacti	Ziziphus obtusifolia	Greythorn
Ferocactus wislizenii	Barrel Cacti		
Fouquieria splendens	Ocotillo		







Existing and Relocated Xeroriparian Areas



No special status including federally listed threatened, endangered species or highly safeguarded species of plant life were identified on the site.

A riparian area can be found along the southern portion of the site with a tributary from Old Vail Road. This riparian area can be linked to the improvements from Old Vail Road, El Paso Gas Line, Railroad, and Houghton Road Bridge Crossing over railroad. Grade buildup for railroad and concentration of flows from exterior influences have impacted site flow from southeast to northwest and throughout the site. This grading in turn, aided in water retention along south and west property lines of the HTC 1 PAD

The riparian area parcel delineation was developed within HTC 1 improvements. Preservation and restoration of riparian will remain within parcel delineation for development of HTC 2 PAD. No additional riparian mitigation is required.

Pima County Mapguide indicates a 'xeroriparian habitat c' zone exists on the site as shown in exhibits (1 & 2). 'Xeroriparian c' is typified as vegetation habitats that occur along minor watercourses with smaller plants and



lower overall vegetative volumes. The majority of the area designated 'xeroriparian habitat c' consists vegetation described previously. Pockets of mesquite/acacia groupings exist within the 'xeroriparian habitat c' in depressed areas that are fed by concentrated flows. The vegetation in these washes consists of predominantly Acacia, Mesquite, and Palo Verde in poor to fair condition due to variation in rain events. Native and invasive grasses can be found where water ponds. The 'xeroriparian habitat c' vegetation is isolated and truncated from any meaningful critical riparian habitat as defined in the COT UDC 5.7.2.

During the initial development of HTC 1 and through discussions with City of Tucson staff, it was agreed to adjust and restore the riparian area along southern property line abutting the railroad with staggered retention as flows progress to the west property line to provide retention and restore the riparian area HTC 1 and HTC 2 PAD.

A native plant preservation plan was completed on the entire 118-acre project in 2005. Viable vegetation within the graded area of HTC 1 was salvaged and relocated within the redefined riparian area. Area not graded contained few remaining viable trees. Only Barrels and Ocotillo cacti should be salvaged from the remaining areas to be cleared. Mitigation of Acacia, Palo Verde and Mesquite shall occur through plantings of new specimens at minimum of 24" box size. No further salvaging of trees shall be required.



Location Map of Condition Photos

Photographs are indicative of general conditions, vegetative densities and quality.

Reference Exhibit 2 for photo locations.



1: Restored basin & riparian area



2: Disturbance & west nature vegetation



3: Disturbed slope & natural vegetation



4: Restored channel & riparian area



5: Northwest



6: Riparian area

## Summation

Recent activities of HTC 1 relocated the riparian area along south property line while addressing Native Plant Preservation requirements for both HTC 1 and HTC 2 PAD. Viable plant material was salvaged and relocated as required by code to HTC 1 retention, basin and restoration of riparian area. This delineation and adjustment to riparian area was and remains critical to HTC 1 and II PAD.

The El Paso Gas line crosses the development of HTC 1 and HTC 2 PAD. Easement, high pressure lines, service road and linear trail were coordinated through HTC 1 and will continue to remain and be improved as a passive recreational trail per agreements in place.

Stormwater is directed to the restored riparian area. Mitigation by instrument of approved native plant preservation plan includes salvage of viable barrel and ocotillo cacti within proposed site improvement areas. Native mesquite trees and other native and drought tolerant vegetation shall be planted within restored riparian and site development areas to maintain a harmonious natural built environment.

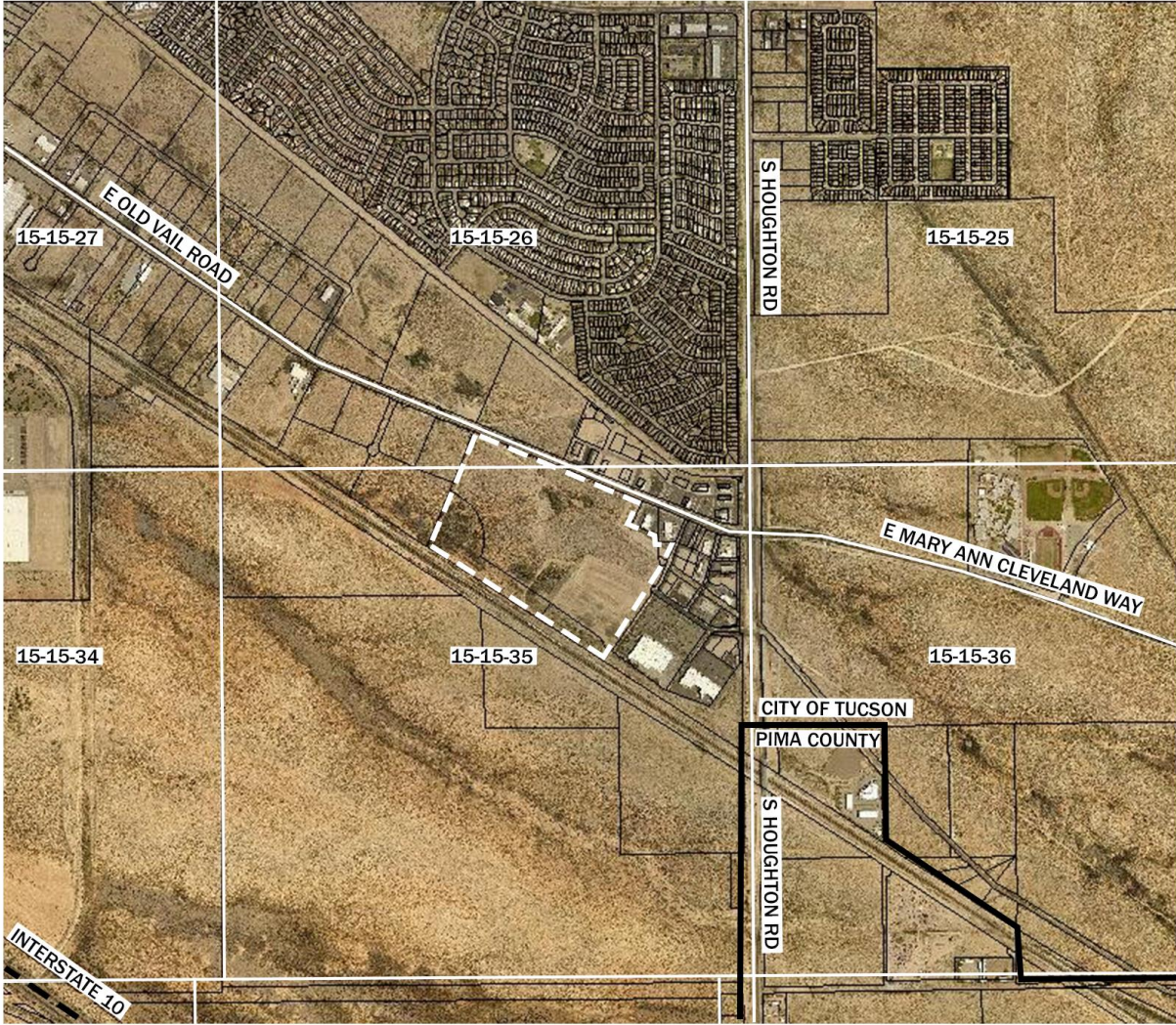
# SECTION 2 - EXHIBITS

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## HOUGHTON TOWN CENTER 2 Planned Area Development



EXHIBIT 1: LOCATION MAP



LEGEND




-  JURISDICTION BOUNDARY
-  INTERSTATE
-  SITE BOUNDARY





EXHIBIT 2: EXISTING ZONING

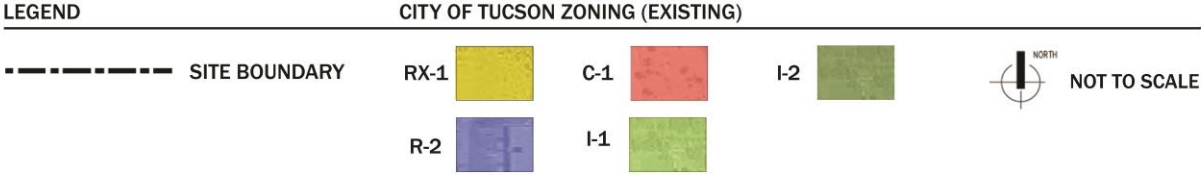
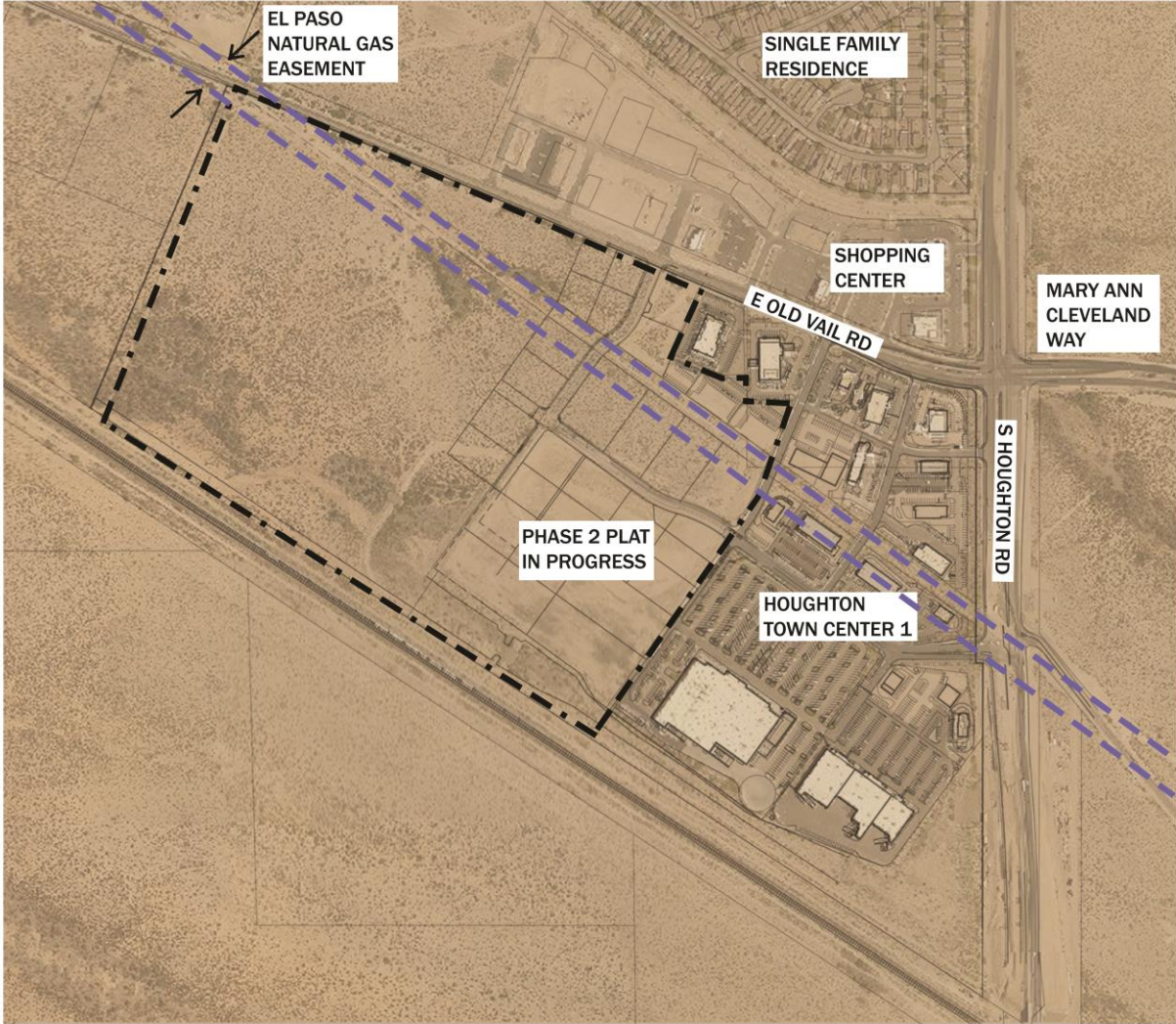




EXHIBIT 3: EXISTING ON-PROPERTY LAND USES



LEGEND

- UTILITY EASEMENT
- SITE BOUNDARY

NORTH  
NOT TO SCALE



EXHIBIT 4 : PHOTOS



 NORTH  
NOT TO SCALE



EXHIBIT 4: SITE PHOTOS



PHOTO 1: WEST VIEW



PHOTO 2: EAST VIEW



PHOTO 3: NORTH VIEW



PHOTO 4: SOUTH VIEW



PHOTO 5: NORTH VIEW



PHOTO 6: SOUTHEAST VIEW

EXHIBIT 4: SITE PHOTOS



PHOTO 7: WEST VIEW



PHOTO 8: WEST VIEW



PHOTO 9: NORTHWEST VIEW



PHOTO 10: EAST VIEW



PHOTO 11: EAST VIEW



PHOTO 12: WEST VIEW



EXHIBIT 4: SITE PHOTOS



PHOTO 13: SOUTH VIEW



PHOTO 14: WEST VIEW

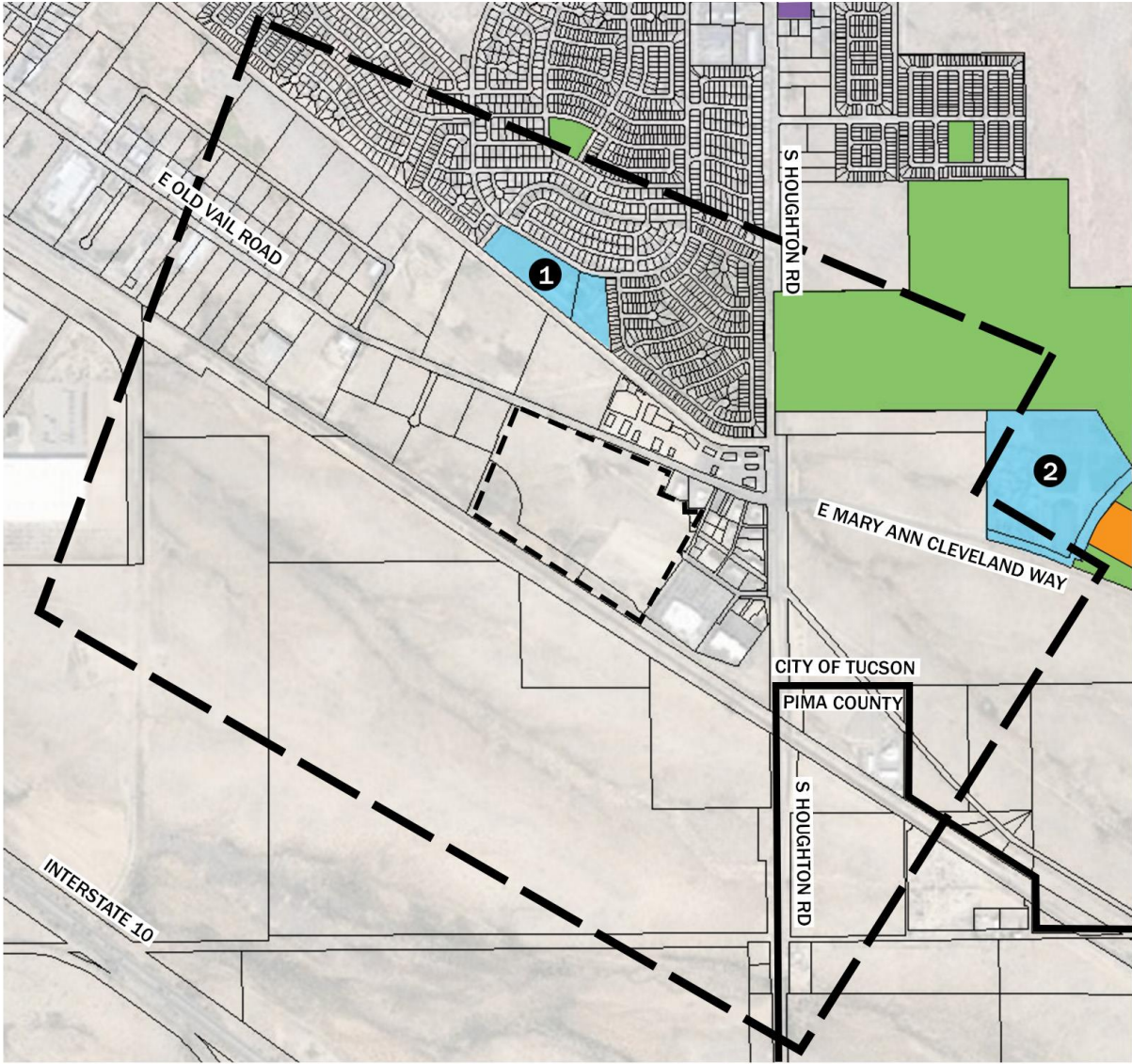


PHOTO 15: NORTH VIEW



PHOTO 16: EAST VIEW

EXHIBIT : COMMUNITY FACILITIES



LEGEND

	JURISDICTION BOUNDARY		PARKS		POST OFFICE
	INTERSTATE		SCHOOLS		LIBRARY
	SITE BOUNDARY				
	1-MILE RADIUS				

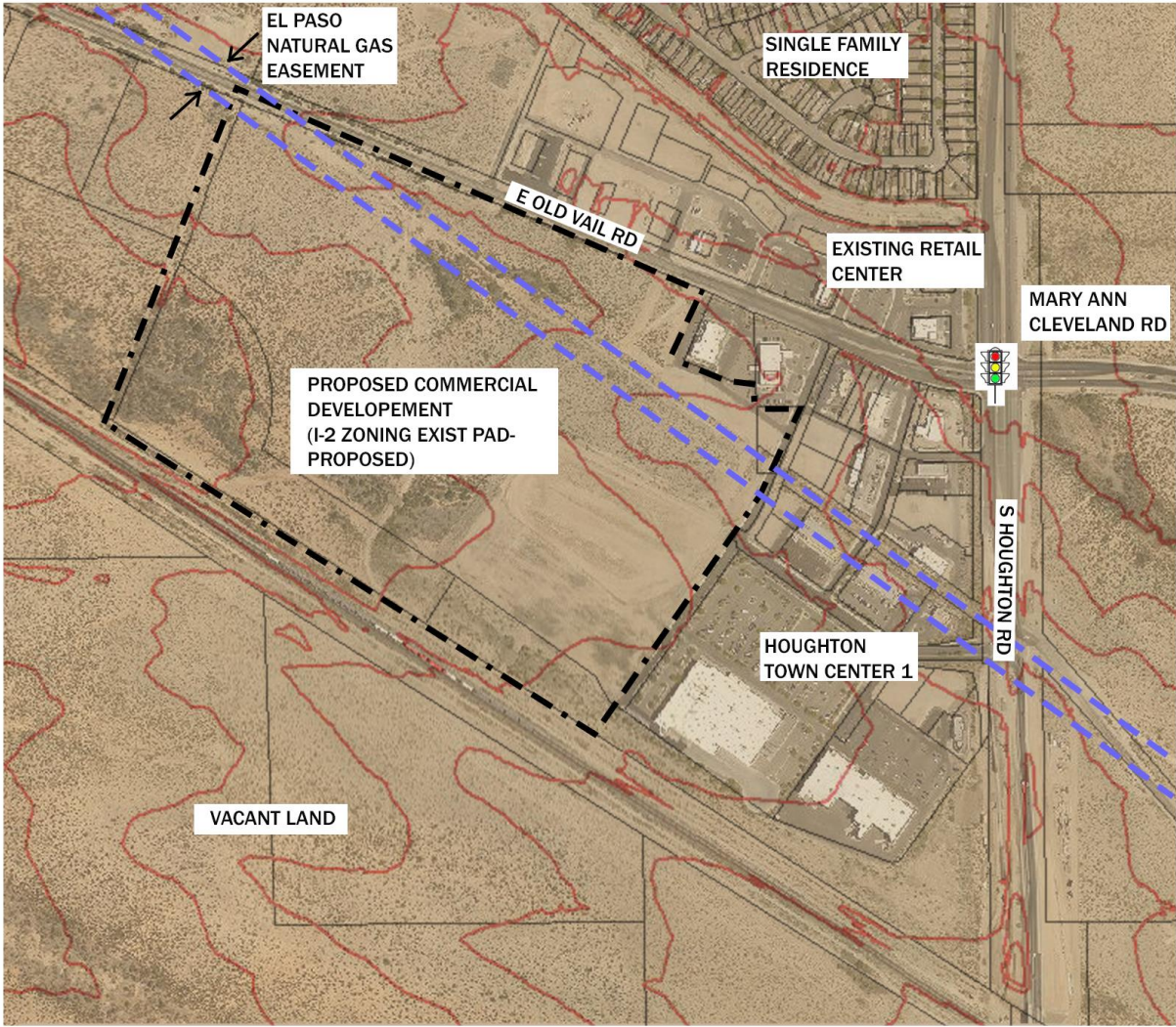


1. COTTONWOOD ELEMENTARY SCHOOL    2. EMPIRE HIGHSCHOOL





EXHIBIT 6 : COMPOSITE PLAN



LEGEND







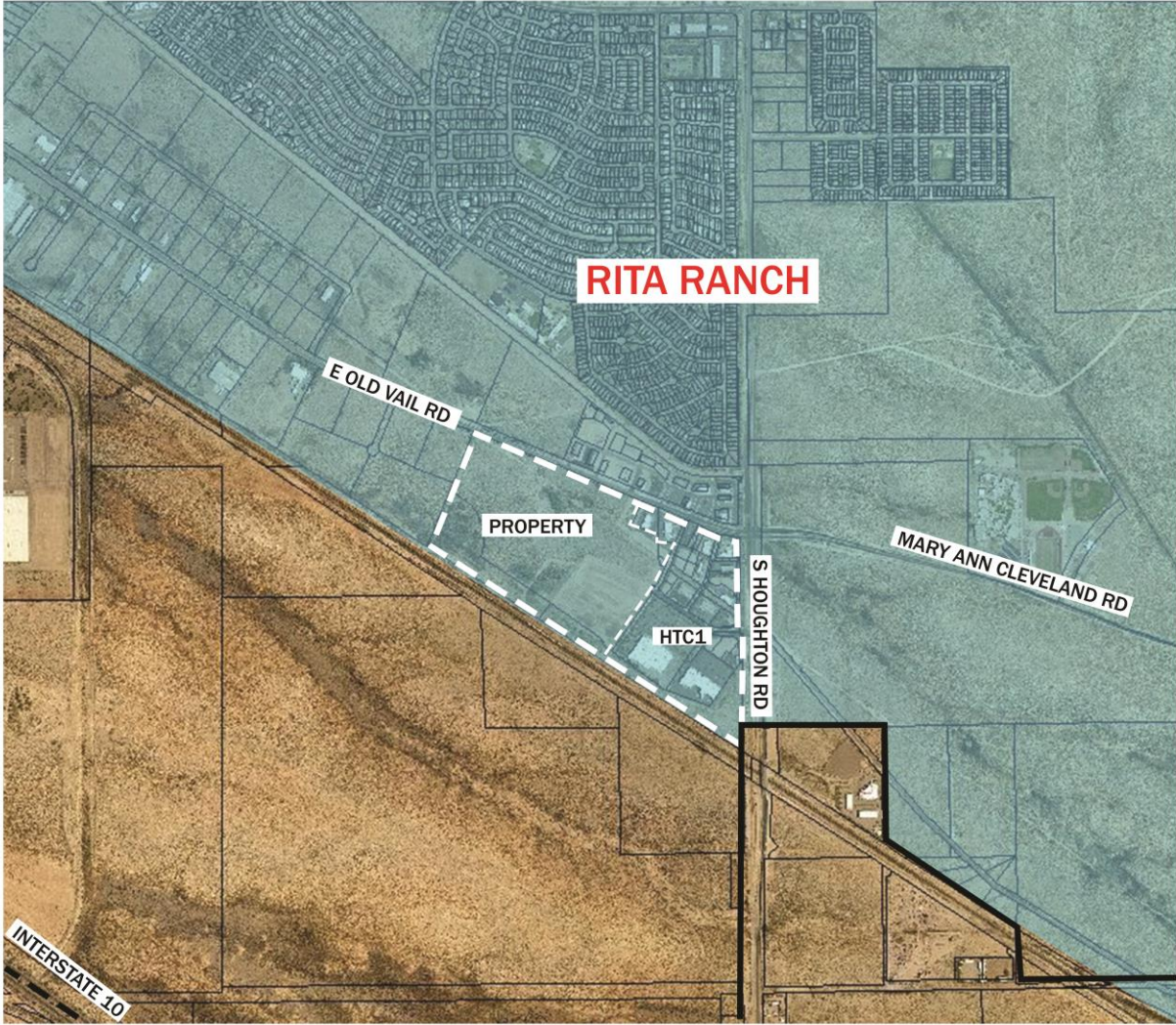
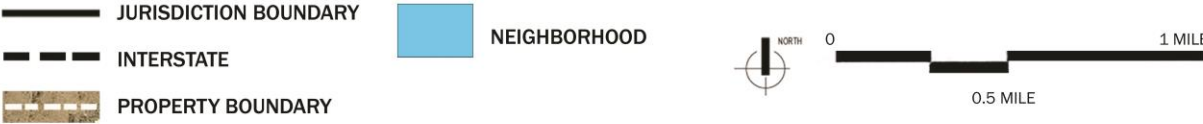
-  SIGNALIZED INTERSECTION
-  TOPOGRAPHY
-  SITE BOUNDARY
-  UTILITY EASEMENT
-  NORTH
-  NOT TO SCALE



EXHIBIT 7: NEIGHBORHOOD MAP



LEGEND



# SECTION 3 - PAD PROPOSAL

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## HOUGHTON TOWN CENTER 2 Planned Area Development



## A. PAD Overview

The Houghton Town Center 2 PAD will provide a vibrant commercial development to serve the immediate neighborhood, visitors, the southeast Arizona region, and the people driving along Interstate 10. This PAD provides a conceptual Property plan that suggests development in two conceptual Development Areas. This PAD was developed in part based on known Property constraints.

Development Areas are referred to as 1 and 2 herein and on the Exhibit 8, Page 41. The actual order of development and boundaries of Development Areas may vary due to market demands and responses to those market conditions. A plat is underway for Development Area 1. The following descriptions are conceptual and subject to change.

All utility improvements required for each Development Area will be designed and extended during the development package process for that area.

- Development Area 1:** Approximately 24 acres that will be developed with a variety of pad sizes potentially including anchor retail uses that will help attract more customers into the Property. Primary access will be from Old Vail Road and the private east-west Roadway serving HTC. Vehicular and pedestrian circulation routes will tie this Development Area together with HTC on the east. On-Property work to be completed with Development Area 1 includes extension of the existing HTC Roadway and additional driveway access points onto Old Vail Road. The existing drainage way along the south of HTC will be extended through the entire Development Area along the south property line and north of the Union Pacific Railroad as previously approved by the City.
- Development Area 2:** Approximately 43 acres located west of Area 1, this area will be developed with similar uses as Area 1 and the original HTC and may include employment industrial uses as well. This portion of the Property will be tied to HTC and Area 1 by way of vehicular and pedestrian circulation routes. The east-west private drive will be completed during development of this Area.

## B. Permitted and Excluded Uses

### 1. Permitted Uses

Permitted uses for the entire Property shall include all uses that are listed as permitted or as special exception uses in the I-1 zone of the City Unified Development Code ("UDC"), and as modified in this PAD.

Notwithstanding any specific provisions of UDC Section 4.9.9.D or elsewhere in the UDC to the contrary, the following provisions of this PAD shall govern Large Retail Establishments ("LRE"):

LRE, as defined in the UDC, shall be permitted anywhere on the property.

Because the PAD District is established through the Zoning Examiner Legislative Procedure, no separate or additional approval (including but not limited to Special Exception Land Use, Design Review Board, Zoning Examiner Legislative Procedure, or otherwise) will be required for any proposed LRE proposed for the Property. All proposed LREs shall be reviewed and approved in accordance with the Development Package Review process.

Additional permitted uses shall include:

- Any use listed as permitted or special exception in O-3, C-1, C-2, C-3, and I-1 Zones.
- Food Service with Alcoholic Beverage Service as an accessory use
- Food Service with a Microbrewery as an accessory use
- Travelers Accommodation with Alcoholic Beverage Service as an accessory use

- Drive-In Theater
- Outdoor Recreational Uses

2. **Accessory Uses**

Land uses accessory to the Permitted Uses are allowed within the PAD subject to UDC Section 6.6.

3. **Excluded Uses**

The following uses shall be excluded from the PAD:

- All uses within the Restricted Adult Activities Use Group
- Residential Uses
- Educational Use – Elementary and Secondary
- Day Care
- Extended Care Medical Services
- Hazardous material manufacturing
- Hazardous Material Wholesaling

**C. Development Standards**

For the purpose of setback requirements and other development standards, the PAD shall be considered as a single parcel regardless of the ultimate parcel configuration within the PAD area. All new development within the PAD shall conform to applicable building, fire and other life safety standards.

For the purposes of administration or interpretation of this PAD, the UDC, or any other provision, any reference to distances from a residential zone shall only apply if the residential zone is occupied by residential units. Any measurement of setback distances as described above shall be measured from the residentially-used property line only. The UDC and/or use-specific standard regulation for residential zones shall not apply if the residentially-zoned property is used or intended for commercial, industrial, open space, or other non-residential use. Nor shall the UDC and/or use-specific standard regulation for residential zones apply where the residential zone is crossed by a railroad.

**TABLE C.1.0: Development Standards**

	Permitted Uses
Minimum Property Area	None
Minimum Lot Width	None
Maximum Floor Area Ratio (HTC 1 and HTC 2 combined)	20%
Maximum Building Height	62 feet from finished grade

<b>Minimum Building Separation</b>	<b>Per Building Code (lot lines internal to the PAD boundary shall not be considered lot lines for purposes of building and fire codes)</b>
<b>Minimum Setback from East Property Line</b>	<b>0</b>
<b>Minimum Setback from Old Vail Road</b>	<b>21 feet from back of future curb or Building Height (whichever is greater)</b>
<b>Minimum Setback from West Property Line</b>	<b>0</b>
<b>Minimum Setback from South Property Line</b>	<b>0</b>
<b>Setback from Drainage Basins</b>	<b>20'</b>

- Vehicular Parking:** Parking shall be provided at a minimum of 1 space per 300 square feet for all permitted use. Except as modified herein, all other provisions of UDC Section 7.4 shall apply. The PAD shall meet 100 percent of the required parking by the City UDC and PAD at the time of issuance of the last Certificate of Occupancy for the last new building to be built on-Property.

A Zoning Documentation Table showing vehicular and bicycle parking and loading tabulations shall be provided with each Development Package submittal. The same table, updated for all changes made with each phase of development, shall be shown in each Development Package submittal using the following or similar format:

**Zoning Documentation Table**

LOT	TENANT	CASE NO.	USE	PROPERTY AREA	BLDG. AREA	VEHICLE PARKING		ACCESS PARKING		LOADING		SHORT TERM BICYCLE		LONG TERM BICYCLE		NOTES
						REQ	PROV	REQ	PROV	REQ	PROV	REQ	PROV	REQ	PROV	
<b>TOTALS</b>																

- Bicycle Parking:** The PAD shall comply with the Bicycle Parking requirement of UDC Section 7.4, Motor Vehicle and Bicycle Parking with the following exceptions:
  - Short-Term Bicycle Parking**
    - Short-term bicycle parking shall be located within seventy-five (75) feet of a public entrance to a building as measured along the most direct pedestrian access route.
    - Where buildings have more than one public entrance or a Development Area has more than one building, short-term bicycle parking shall be distributed so that at least one short-term bicycle parking space is within seventy-five (75) feet of each public entrance.



- Short-term bicycle parking may be located across the PAAL from a public entrance as long as it meets all other criteria established herein.
  - **Long-Term Bicycle Parking**
    - Long-term bicycle parking for the PAD may be reduced up to 10 percent.
    - Bicycles may be kept inside buildings at the discretion of tenants with no special design requirements.
    - Spaces provided outside buildings may be grouped to serve more than one business with no proximity limitations.
3. **Parking Area Access Lanes (PAAL):** PAAL's shall be a minimum of twenty (24) feet wide for a two-way PAAL and twenty (20) feet wide for a one-way access aisle.
  4. **Drive Through Lanes:** Drive thru lanes shall be a minimum of nine (9) feet wide. The bottom of roof or canopy structures over a drive through shall be a minimum of ten (10) feet above the drive lane.
  5. **Paving Materials:** Vehicle use areas shall be constructed utilizing materials and construction techniques in accordance with the recommendations of the geotechnical engineer and concurrence from City Engineering staff.
  6. **Accessibility:** Development within the PAD, including vehicle parking spaces, pedestrian walkways, building entrances, curb ramps, sidewalks, detectable warnings, and marked crosswalks within the PAD shall comply with accessibility requirements of the 2018 IBC and ICC/ANSI 117.1, 2009 Edition. A clearly defined accessible pedestrian pathway shall be provided to Old Vail Road and to each building on the Property. See Section 3.C.13.e for additional standards.
  7. **Loading Zones:** The PAD shall comply with the Loading Requirements of UDC Section 7.5 except as noted below:
    - A shared loading space may be located within 250 feet of the use it serves.
  8. **Solid Waste and Recycling Material Collection and Storage:** All required Solid Waste and Recycling material collection and storage shall be screened from sidewalks, PAAL's, and public and private roadways by use of six (6) foot high constructed enclosures. Buildings may also be used to block views to collection and storage areas. Collection yards used to hold multiple collection containers for more than one use shall be enclosed by a six (6) foot high constructed enclosure and/or building. Collection and storage locations shall allow for safe access and maneuverability within the development. Pedestrian paths may cross directly in front of gates to constructed enclosures. Enclosures for the storage of containers shall be designed in accordance with the Environmental Services development standards per UDC Section 8, Solid Waste and Recycle Disposal Collection and Storage.
  9. **Wastewater:** Prior to submittal of the future Development Package ("DP") to the City, the owner/developer shall obtain a Type I Capacity Letter from RWRD verifying that capacity exists in the downstream public sewer system. The following items shall constitute applicable RWRD rezoning conditions:
    - 1) The owner/developer shall not construe any action by Pima County as a commitment to provide sewer service to any new development within the rezoning area until Pima County executes an agreement with the owner/developer to that effect.
    - 2) The owner/developer shall obtain written documentation from the RWRD that treatment and conveyance capacity is available for any new development within the rezoning area no more than 90 days before submitting any tentative plat, development plan, preliminary sewer layout, sewer improvement plan, or request for building permit for review. Should treatment

and/or conveyance capacity not be available at that time, the owner/developer shall enter into a written agreement addressing the option of funding, designing, and constructing the necessary improvements to the Pima County's public sewerage system at his or her sole expense or cooperatively with other affected parties. All such improvements shall be designed and constructed as directed by the RWRD.

- 3) The owner/developer shall time all new development within the rezoning area to coincide with the availability of treatment and conveyance capacity in the downstream public sewerage system.
  - 4) The owner/developer shall connect all development within the rezoning area to Pima County's public sewer system at the location and in the manner specified by the RWRD in its capacity response letter and as specified by RWRD at the time of review of the tentative plat, development plan, preliminary sewer layout, sewer construction plan, or request for building permit.
  - 5) The owner/developer shall fund, design, and construct all off-Property and on-Property sewers necessary to serve the rezoning area in the manner specified at the time of review of the tentative plat, development plan, preliminary sewer layout, sewer construction plan, or request for building permit.
  - 6) The owner/developer shall complete the construction of all necessary public and/or private sewerage facilities as required by all applicable agreements with Pima County and all applicable regulations including the Clean Water Act and those promulgated by ADEQ before treatment and conveyance capacity in the downstream public sewage system will be permanently committed for any new development within the rezoning area.
10. **Traffic:** Other than Old Vail Road, no other public streets directly serve the Property. Full turns will be provided at Old Vail Road at all driveway entrances. Future traffic lights may be provided near the west end of the development.

**11. Drainage Requirements:**

Note: Updated drainage statements, in conformance with the master drainage report, shall be submitted for each development plan associated with this PAD.

Note: Updated Drainage Statements, in conformance with the Master Drainage Report, shall be submitted for each development plan associated with this PAD.

A. Local Floodplain Areas:

1. Building Erosion Hazard Setbacks (BEHS's) will be established per the specific Drainage Statements required for the future development within the provisions of this PAD. BEHS's evaluation shall follow current standards for review and approval at the time this PAD is approved.
  2. Drainage Maintenance Requirements within the Property shall be the responsibility of the Property Association and as reviewed and approved as a part of the master Drainage Report submittal
12. **Setbacks from basins:** The setbacks will be determined by a soils report for setbacks between any areas subject to storm water infiltration and pavement structures and building structures. All future stockpiling, if utilized, will be subject to City criteria. The proper functioning of the retention/detention and drainage systems onsite is dependent on the Property Association providing annual and continuous maintenance of these basins.

13. **Design Guidelines:** The Property will be designed in a style compatible with surrounding developments and the southwest environment with inviting, contemporary architectural styles to form a cohesive whole. The following guidelines apply to all permitted uses on the Property:
- a. **Vehicular Access:** The Property shall provide motor vehicle access points from Old Vail Road as required.
  - b. **Landscape Buffers:** A ten (10) foot landscape buffer meeting UDC requirements will be provided along Old Vail Road. No buffer shall be required between the Property and any adjacent property.
  - c. **Screen Walls:** When otherwise required in this PAD, screen walls shall be constructed of masonry, concrete materials, metal, or similar materials and shall incorporate any combination of one or more of the following: various textures, materials, patterns, or colors. Screen walls for other than truck docks and more than seventy (70) feet long shall have offsets of at least 8 inches at no more than twenty (20) foot intervals. Perimeter walls shall be provided if required by the UDC. The terms “screen wall”, “fence”, “perimeter wall”, or “screen” may be considered the same for purposes of this PAD.
  - d. **Outdoor Storage Areas:** A six (6) foot high screen wall is required to screen Outdoor Storage Areas adjacent to a public roadway or sidewalk, unless the Outdoor Storage Area is otherwise screened by buildings, landscaping, or freeway sound walls.
  - e. **Pedestrian Flow:** The Property shall provide pedestrian accessibility, safety, and convenience to reduce traffic impacts and enable the development to project a friendly, inviting image. Pedestrian paths providing access to main building entrances shall have an unobstructed width of at least four (4) feet. A pedestrian pathway along the east-west Roadway linking the entire Property and Development Areas to Houghton and Old Vail Roads shall be a minimum eight (8) feet wide clear. This walkway shall be shaded by use of canopy trees, building overhangs, freestanding shade structures, or other means.
  - f. **Customer Drop-Off Areas:** Any customer drop-off/pick-up provided should be integrated into the design and shall not conflict with traffic lanes or pedestrian paths. Drop-off/pick-up areas parallel to a PAAL shall be minimum nine (9) feet wide. Drop-off/pick-up areas perpendicular to a PAAL shall be a minimum twenty (20) feet deep and nine (9) feet wide. Drop-off/pick-up areas shall have signs clearly identifying their intentions.
  - g. **Entryways:** The building design shall provide design elements that orient customers and that add aesthetically pleasing character to buildings by providing clearly-defined, highly-visible customer entrances.
  - h. **Delivery and Loading Spaces:** Depressed delivery and loading zones used by semi-trailer trucks shall be screened from Old Vail Road and public sidewalks by use of a screen wall as described in this PAD. The screen shall be at least eight (8) feet high to screen loading activity, measured from the floor of the loading dock. An adjacent building can also serve as the screen. No screen is required from PAAL’s or the east-west Roadway.
  - i. **Traffic Impacts:** A comprehensive Traffic Impact Analysis was submitted as part of this PAD process. Should any substantial changes be made as to uses or other assumptions in that Analysis, the City may require an updated traffic impact statement at the time of Development Package review.
  - j. **Outdoor Lighting:** All outdoor lighting shall comply with the City Outdoor Lighting Code.
  - k. **Outdoor Sales Display/Ancillary Uses:** Outdoor activities (including but not limited to outdoor merchandise display and sales, outdoor storage, and outdoor snack bar and eating areas) located on sidewalks or in front of buildings shall provide a minimum four (4) foot clear pedestrian path to major public entrances into the buildings.
  - l. **Hazardous Materials:** Any hazardous material users shall provide a Hazardous Materials Management Plan and Hazardous Materials Inventory Statement as provided in the Tucson Fire Code.
  - m. **Building Form and Colors:** Buildings shall provide architectural features that contribute to the visual interest at the pedestrian scale. These features shall reduce the massive aesthetic



effect by breaking up the building wall (front, side, or rear) with color, texture change, wall offsets, reveals, or projecting ribs. Varying building forms and masonry, synthetic stone, concrete masonry, painted/textured concrete tilt panels, stucco, EIFS, stone, tile, painted or rusted metal, and glass materials may be used to provide interest and variation. A variety of regional colors are recommended with an emphasis on subtle earth tones with accents. Bright or deep accent colors are encouraged and should be used appropriately.

**n. Screening of Mechanical Equipment:**

- 1) Roof or ground-mounted mechanical equipment shall be screened to mitigate noise and views from public roadways, PAAL's, and sidewalks within thirty (30) feet measured horizontally from the equipment at the ground level.
  - 2) Ground-mounted screens shall be of sufficient height to screen the view and block the noise of the equipment.
- o. Roof Materials:** Acceptable materials for exposed roofs include standing seam metal roofing and roof tiles. Asphalt shingle tiles, wood shakes, and rolled membrane roofing that is visible to the public are not acceptable. Cool roof technologies shall be employed on all roofs.
- p. Sustainability:** In an effort to be more sustainable and energy efficient, the following tenant interior considerations are suggested but not required:
- 1) Paint products containing low volatile organic compounds not exceeding 50 g/L (flats) or 150 g/L (non-flats).
  - 2) Carpeting and padding meeting the testing and product requirements of the Carpet and Rug Institute's Green Label Plus Program, carpet adhesive having a VOC limit of 50 g/L.
  - 3) Cabinetry adhesives containing no added urea-formaldehyde resins
  - 4) Partition framing of metal studs with recycled content.
  - 5) FSC-certified wood framing members, flooring, sub-flooring, wood doors and finishes.
  - 6) HVAC units that meet LEED-equivalent energy and refrigerant specifications.
  - 7) Skylights and exterior windows to improve interior light quality and reduce electrical consumption.
  - 8) Low flow water fixtures.
  - 9) Construction waste management.
  - 10) Use of recycled building materials.

**14. Architectural Review Committee:**

Subsequent to PAD approval and at the time Declarations are recorded for the Property, the Property owner shall establish an Architectural Review Committee ("ARC") to review and approve architectural design within the Property for compliance with the Development Standards in Section 3 of this PAD and any additional design criteria for the Property. Materials used in the building and Property features will be compatible with the desert environment, complement existing development in the area, and contain architectural details that provide interest and character to the development. The design guidelines will be approved by the ARC.

The ARC shall review all proposed architectural plans.

The ARC shall remain in place through 100 percent completion of the initial build out of the development. Beyond that point, the ARC's function will be established in the Declarations.

**15. Administration and Interpretation:**

Section 3 of this PAD has been structured to allow the City clear guidance in applying and interpreting the specialized land use and zoning framework for the PAD. The PAD will not modify or change any existing City adopted building codes or other ordinances, except for those portions of the UDC, Administrative Manual, and Zoning Map specifically modified in this PAD. The PAD shall be generally administered under the authority of the Director of the Planning and Development Services Department (PDSD).

If future supplemental PAD changes or interpretations become necessary, they shall occur as follows:

- Whenever a conflict arises between the PAD and the UDC, the PAD shall control.
- When the PAD does not specifically address a particular topic, the UDC and Administrative Manual shall control within the overall intent of the PAD.

**16. Amendments to the PAD:**

The PAD may be amended, and an amendment shall be categorized as either a minor amendment or a major amendment.

*Minor Amendments: Criteria & Process*

The PDSD Director may administratively approve minor changes to the PAD, as defined below, provided such changes do not conflict with the overall intent, goals and objectives of the PAD.

The following shall be considered minor changes subject to administrative approval by the PDSD Director:

- Addition of new information to the PAD, maps, or text that does not change the effect of any regulation, development standard, or guideline;
- Modification to the proposed development provided that the Development Standards in Section 3(C) are maintained;
- The addition of permitted or accessory uses not specifically listed in Section 3(B) that are determined to be sufficiently similar in type and nature to those listed as permitted;
- Adjustments to the Development Standards in Section 3(C) of this document that are not harmful to the interests of the larger community or adjacent neighborhoods, or which are not explicitly stated in the PAD but are consistent with the guiding goals and objectives of the development and do not create any public health or safety issues;
- Changes to the currently described public or private infrastructure that is necessary to properly serve the intended development, and that is not primarily intended to increase the development capacity, nor alter the guiding goals and objectives of same;
- Adjustments to any aspect of Section 3 of this PAD that is required in order to comply with changes in local, state or federal safety and/or health codes; or
- Any other items not expressly defined as major change to the PAD, as described in the UDC Section 3.5.5(J)(2)(c).

*Major Amendments: Criteria and process*

Major amendments to the PAD shall be those changes or modifications that materially alter the guiding goals and objectives as presented in the PAD.

The PDSD Director will determine if a proposed amendment would result in a major change per the criteria established in this PAD and UDC Section 3.5.5(J)(2)(c). Major amendments to the PAD shall be processed in accordance with UDC Section 3.5.3, Zoning Examiner Legislative Procedure.



## D. Landscape

The HTC 2 PAD shall comply with the Landscape and Screening requirements of the UDC 7.6 with the following exceptions/ additions.

- Loading and Delivery docks, outdoor storage areas, garbage and recycling areas and other similar exterior improvements facing residential uses or residentially zoned properties outside the PAD boundaries and adjacent public ROW's will be screened (unless substantially screened by buildings or other structures or landscaping) with walls a minimum of six feet high or dense landscaping with plant material that can be kept at six feet high or greater.
- Street landscape borders along Old Vail Road shall have a ten-foot landscape border per City of Tucson UDC 7.6.
- Southern Landscape borders shall be minimum of 30' natural restored riparian habitat landscape with one tree and five shrubs every twenty-five linear feet and hydro-seed. No additional screening structure shall be required.
- The Property is subject to the Commercial Rainwater Harvesting Ordinance. Infrastructure associated with the Commercial Rainwater Harvesting requirements is already constructed. Passive water harvesting shall be provided at all development where feasible with overflow directed to the stormwater retention basin and riparian restoration area.
- A native plant preservation plan shall not be required. Plans was completed, and majority of vegetation was salvaged with HTC 1. In lieu of inventory, all viable barrel, ocotillo and saguaro cacti in disturbed areas shall be salvaged. Prior to site clearing owner shall have Landscape Architect and Salvage contractor review, tag and salvage any viable Mesquite, Palo Verde or Acacia sp. within proposed grading area and relocated within the site to be developed.
- All landscaping within public rights-of-way (ROW) will require approval from Tucson Department of Transportation (TDOT)
- All landscape shall be irrigated by underground irrigation system with start control systems. All pipe shall be Sch. 40 - PVC due to soil conditions and rodent activities.

### Landscape Plant Palette Trees

Prosopis x 'Phoenix' (Hybrid Mesquite)  
 Prosopis velutina (Velvet Mesquite)  
 Parkinsonia x 'Desert Museum' (Desert Museum Palo Verde)  
 Parkinsonia florida (Blue Palo Verde)  
 Fraxinus velutina (Arizona Ash)  
 Chilopsis linearis (Desert Willow)  
 Sophora secundiflora (Texas Mountain Laurel)  
 Pistacia chinensis (Chinese Pistache)  
 Dalbergia sissoo (Indian Rosewood)

## Landscape Plant Palette Shrubs, Accents, Vines and Groundcovers

Leucophyllum levigatum (Chihuahuan Sage)	Dodonea viscosa (Hopseed)
Hesperaloe parvifolia (Red Yucca)	Lantana camara (Bush Lantana)
Cordia parvifolia (Little leaf Cordia)	Caesalpinia pulcherrima (Red bird of paradise)
Calliandra eriophylla (Fairy duster)	Calliandra californica (Baja fairy duster)
Mimosa dysocarpa (Mimosa)	Larrea tridentate (Creosote bush)
Callistemon citrinus (Bottlebrush)	Acacia (Senegalia) greggii (Catclaw acacia)
Acacia (Vachellia) constricta (White thorn acacia)	Ruellia brittoniana (Purple Ruellia)
Ruellia peninsularis (Baja Ruellia)	Fouquieria splendens (Ocotillo)
Dasyliirion wheeleri (Desert spoon)	Dasyliirion quadrangulatum (Toothless spoon)
Viguiera parishii (Goldeneye)	Yucca elata (Soaptree Yucca)
Ferocactus wislizeni (Fishhook barrel)	Simmondsia chinensis (Jojoba)
Berlandiera lyrata (Chocolate flower)	Muhlenbergia rigens (Deergrass)
Pachycereus schottii (Totem pole)	Hesperaloe funifera (Giant Hesperaloe)
Chrysactinia Mexicana (Damianita)	Agave sp.
Xerophyllum tenax (Beargrass)	Caesalpinia Mexicana (Mexican bird of paradise)
Justicia californica (Chuparosa)	Cassia sturtii (Cassia)
Tecoma alata (Orange jubilee)	Opuntia ficus indica (Indian fig)
Asclepias angustifolia (Milkweed)	Parthenocissus sp. 'Hacienda' (Hacienda creeper)
Euphorbia rigida (Gopher plant)	Euphorbia macrocarpus (Slipper plant)
Cylindropuntia bigelovii (Teddybear Cholla)	Muhlenbergia capillaris (Regal mist)
Salvia clevelandii (Cleveland sage)	Carnegiea gigantea (Saguaro)
Tecoma stans (Yellow bells)	Celtis ehrenbergiana (Desert Hackberry)

## E. Traffic Circulation

A Traffic Impact Analysis (TIA), dated March 2006, was prepared by Rick Engineering Company Inc., for the Development of the entire Houghton Town Center. An addendum to the aforementioned TIA, dated October 17, 2018 is recently added for the proposed Phase II and III development (The PAD). The Addendum is a stand-alone companion documents to this application. The Addendum contains recommendations for improving the traffic circulation around the Property in correlation with development. Its recommendations acknowledge that the developer's mitigation obligations are limited only to traffic impacts created by the development of the Property, and do not extend to those traffic conditions that currently exist.

The Addendum provides the following conclusions and recommendations:

- The Old Vail Road/Mary Ann Cleveland Way and Houghton Road intersection is already full-built. Eastbound and westbound approaches have two through lanes along with exclusive single left- and single right-turn lanes. The northbound and southbound approaches have two through lanes along with dual left- and single right-turn lanes. The intersection is currently operating at **LOS C or better** at both AM and PM peak hours without the site traffic.
- The proposed PAD is expected to build a total of 500,286 SF of building area which consists of three major uses: fitness center, shopping center, and home improvement store. Using the appropriate land uses and ITE Trip Generation Manual 10th edition, it is estimated that the site will generate approximately 14,640 trips on a typical weekday with a net total of **578 trips in AM peak hour** (343 - inbound/ 235 - outbound), and **1,168 trips in the PM peak hour** (563 – inbound/605- outbound).

- The Old Vail Road/Mary Ann Cleveland Way and Houghton Road intersection is expected to operate at **LOS D or better** at both AM and PM peak hours with the site traffic. Field review at a PM peak hour did not observe any excessive delays or queuing issues on any approaches, which supports the synchro analysis.
- There are two proposed full access driveways onto Old Vail Road at the west side of the project (one for Phase II and one for Phase III). According to the 2018 traffic data counts on Old Vail Road, and the Transportation Access Management Guidelines for the City of Tucson, these two driveways do not meet the right-turn deceleration lane warrants. STOP-signs are recommended to be installed at these driveways. Although right-turn lane warrants are not met at either of these driveways, right-turn-lanes will be added by the owners or developers as each access point to Old Vail Road is developed.

## F. Signage

Signage currently lies outside of the scope of the PAD regulations, so the signage that will be located within the PAD will be effectuate pursuant to the 2008 Sign Code Variance and 2018 Master Sign Program.

## G. Historical

Prior to any incidental site ground disturbance and development package submittal, including grading and stockpiling permits, the applicant will provide documentation and approvals from City of Tucson Historic Preservation Office that archaeological clearance has been completed for the site area and provide archaeological clearance documentation and approvals for any additional acreage acquired from the City for development. Archaeological clearance has already been obtained for the rest of the Property.

## H. Definitions

- **Architectural Review Committee:** A committee as defined in the Property's declaration documents whose function is to review and approve all details of design to ensure the Property is developed in accordance with the approved design standards.
- **Development Areas:** Development Areas 1 – 5 as shown on Exhibit 9, Development Area Plan. The areas are not phases. Development in these areas may occur as dictated by the market and will not necessarily occur according to the designated development area number.
- **Drainage Facilities / Storm Water Drainage Facilities:** Drainage facilities that include, but are not limited to, engineered swales and channels, water harvesting areas, detention/retention ponds, and underground pipe systems. The purpose of drainage facilities is to provide for the protection of abutting and off-Property properties that would be adversely affected by any increase in runoff attributed to the development.
- **Open Space:** An area not occupied by buildings and non-pervious materials.
- **Outdoor Storage:** The location of goods, wares, merchandise, commodities, debris, or any other item outside a completely enclosed building for a continuous period typically longer than 24 hours

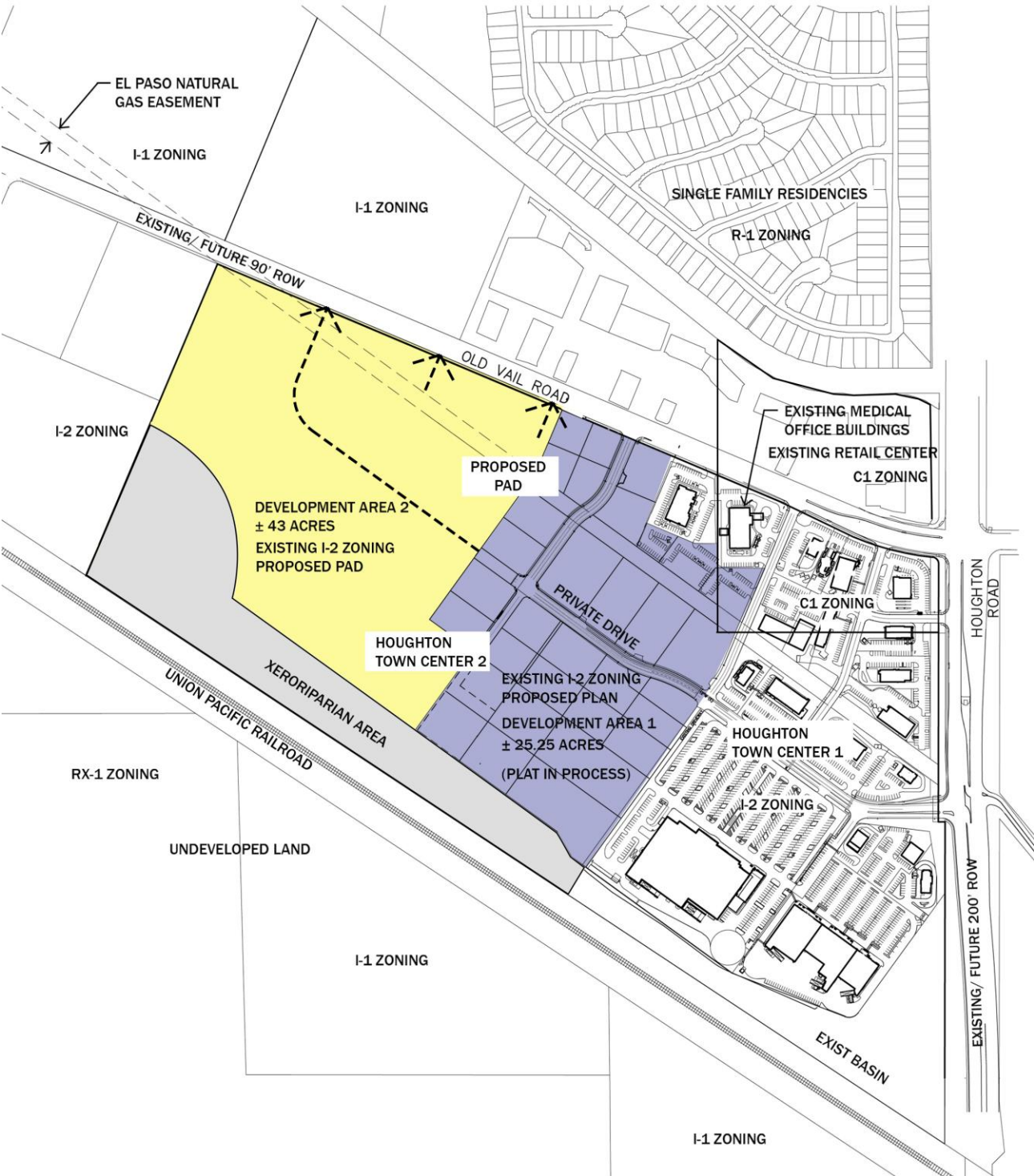
# SECTION 3 - EXHIBITS

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## HOUGHTON TOWN CENTER 2 Planned Area Development



EXHIBIT : DEVELOPMENT AREA BOUNDARIES



NOT TO SCALE

