

# PRINCE 10 PLANNED AREA DEVELOPMENT

City of Tucson

August 2021

Amendment 1: Revised August 10, 2023  
Adopted by Mayor and Council on August 22,  
2023, Ordinance No. 12027





# PRINCE 10

## Planned Area Development

North Business Center Drive and La Cholla Boulevard  
Tucson, Arizona

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## TABLE OF CONTENTS

<b>Part I. Introduction &amp; Policy .....</b>	<b>1</b>
A. Project Overview .....	2
B. Rationale & Benefits for Use of a PAD .....	4
C. Benefits to the Community .....	4
D. Conformance with Adopted Plans .....	4
1. Plan Tucson .....	4
2. Santa Cruz Area Plan .....	8
E. Compatibility with Adjoining Land Uses .....	9
F. Suitability with Existing Infrastructure .....	9
<b>Part II. PAD Proposal.....</b>	<b>10</b>
A. Plan Overview .....	11
B. Land Use Plan .....	11
1. Conceptual Architecture .....	14
2. Plazas and Gathering Areas .....	15
3. Landscape.....	15
4. Artwork.....	15
C. Circulation Plan .....	18
D. Drainage Plan .....	21
1. Drainage Constraints.....	22
2. Drainage Design .....	25
E. Phasing Plan .....	27
F. Infrastructure Phasing & Construction .....	28
1. Sanitary Sewer Disposal .....	28
2. Water.....	29
3. Solid Waste Disposal .....	30
4. Other Utilities .....	30
5. La Cholla Boulevard / Fort Lowell Road .....	31
<b>Part III. Development Regulations .....</b>	<b>32</b>
A. Development Regulations .....	33
1. Interstate Commerce (IC) .....	33
a. Permitted Uses .....	33
b. Special Exception Land Uses.....	35
c. Prohibited Uses.....	35
d. Accessory Uses and Structures .....	37
e. PAD Development Standards .....	37
B. Additional Development Standards .....	38
2. Circulation Standards .....	38
a. Vehicular Access .....	38
b. Pedestrian Access – Sidewalks .....	38
3. Parking (Motor Vehicles and Bicycles) and Loading Standards.....	38
a. Motor Vehicle Parking .....	38
b. Bicycle Facilities .....	39
c. Parking Structures .....	40
d. Accessible Parking Facilities.....	40

- e. Loading ..... 40
  - 4. Landscape and Screening Standards ..... 41
    - a. Theming and Materials ..... 41
    - b. Street Landscape Border ..... 41
    - c. Perimeter Landscape Borders ..... 41
    - d. Parking Area Landscape – Surface Parking ..... 42
  - 5. Water Conservation Standards ..... 42
    - a. Low Water Use Plants ..... 42
    - b. Irrigation ..... 42
    - c. Rainwater Harvesting ..... 42
  - 6. Signage and Monumentation ..... 43
  - 7. Solid Waste Standards ..... 46
  - 8. Lighting ..... 46
  - 9. Public Art ..... 46
- Part IV. Implementation & Administration ..... 47**
  - A. Design Standards and Guidelines ..... 48
  - B. Interpretations and Amendments ..... 48
    - 1. Interpretation ..... 48
    - 2. Amendments ..... 48
- Part V. Site Analysis ..... 50**
  - A. Significant Natural & Built Constraints ..... 51
  - B. Transportation & Circulation ..... 51
  - C. Existing Zoning ..... 53
  - D. Adjacent Parcels & Structures ..... 54
  - E. Open Space, Recreational Facilities, Parks, & Trails ..... 56
  - F. Public, Educational, Community, & Cultural Facilities ..... 56
  - G. Existing Drainage ..... 56
  - H. Overlay Zones ..... 58
  - I. Inventory of Existing Structures, Roads, & Other Development ..... 58
    - 1. Structures ..... 58
    - 2. Roads ..... 59
    - 3. Development ..... 59
  - J. Sewer & Wastewater ..... 59
  - K. Inventory of Existing Public Services ..... 59
    - 1. Police ..... 59
    - 2. Fire ..... 59
    - 3. Library ..... 59
  - L. Hydrology ..... 59
  - M. Topography and Slope ..... 60
  - N. Vegetation & Wildlife ..... 62
  - O. Geology & Soils ..... 62
  - P. Viewsheds ..... 62
  - Q. Archaeological & Historical Sites ..... 65
- Appendix I. Modification of Submittal Requirements ..... 68**



## List of Exhibits

Exhibit I.D.1: Plan Tucson Future Growth Scenario Map .....	5
Exhibit I.D.2: Santa Cruz Area Plan Map.....	8
Exhibit II.B.1: Land Use Plan .....	12
Exhibit II.B.2: Illustrative Site Plan .....	13
Exhibit II.C.1: City of Tucson Bicycle Park Concept.....	19
Exhibit II.D: Developed Conditions Watershed Map .....	21
Exhibit II.D.1.a: Drainage Constraints .....	23
Exhibit II.D.1.b: Santa Cruz River Area Ownership .....	24
Exhibit II.D.2: Drainage Design.....	26
Exhibit V.C: Existing Zoning.....	53
Exhibit V.D.1: Surrounding Development .....	54
Exhibit V.D.2: Surrounding Development .....	55
Exhibit V.G: Existing Conditions Watershed Map .....	57
Exhibit V.M: Existing Topography and Slope .....	61
Exhibit V.P: Viewsheds .....	63
Exhibit V.P: Viewsheds (continued) .....	64

## List of Tables

Table III.A.1.e: Development Standards.....	37
Table V.C.1: Surrounding Zoning .....	53
Table V.D: Existing Land Uses for Adjacent Parcels .....	54
Table V.D: Existing Development Plans & Subdivisions within 150 feet .....	54

# PART I Introduction & Policy



## A. Project Overview

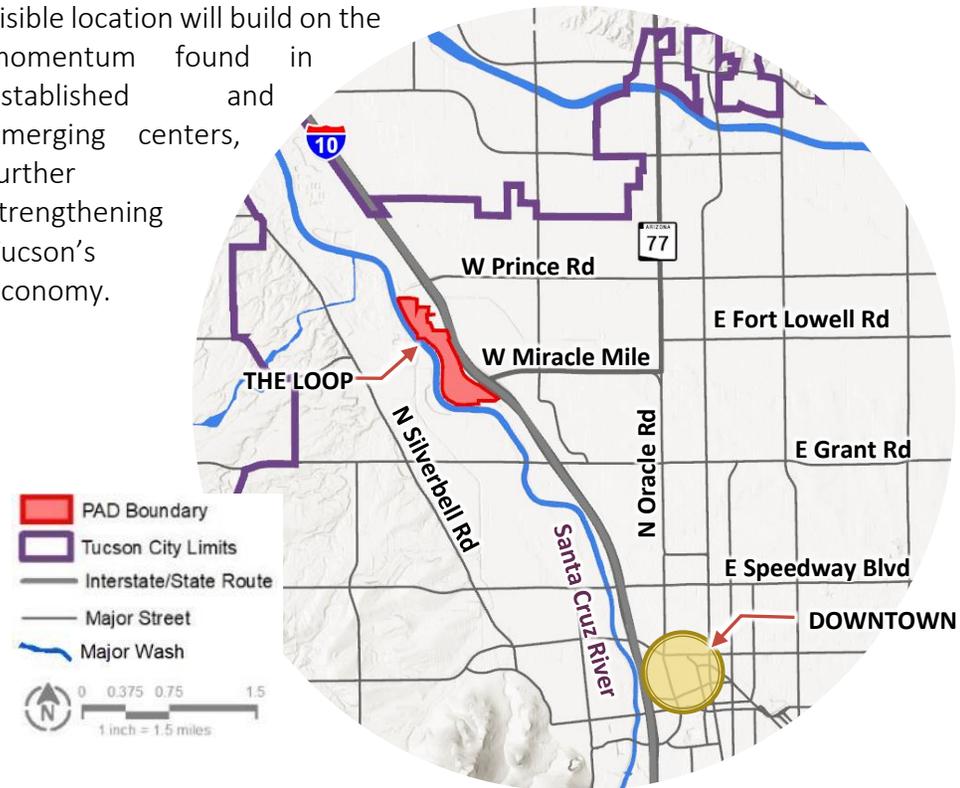
PRINCE 10 is envisioned as a progressive, mixed-use, interstate commerce campus. The PAD comprises approximately 114 acres and features nine development areas with the potential to accommodate high-tech industry and 10 to 12-story office buildings. Supplemental land uses may include hospitality, restaurants, other commercial uses, and multifamily housing. The Site Selector’s Guild, whose *“members provide location strategy to corporations across the globe”*, have three criteria for prospective properties in Metro Tucson, and PRINCE 10 meets all of them: 1) shovel-ready land, 2) direct freeway access, and 3) location north of downtown. Grading, drainage, and utility improvements are already underway to transform the entire PAD into shovel-ready land, and PRINCE 10 is ideally situated between Interstate 10 and the Santa Cruz River, approximately 3 miles north of downtown Tucson. Amendment 1 incorporates the former Tra-Tel RV Park parcel into the PAD. Including this 3.3-acre property removes a pinch point at the center of the PAD, allowing for a more cohesive development with a better north/south connection. The amendment also adjusts the southern boundary to incorporate a parcel remnant.

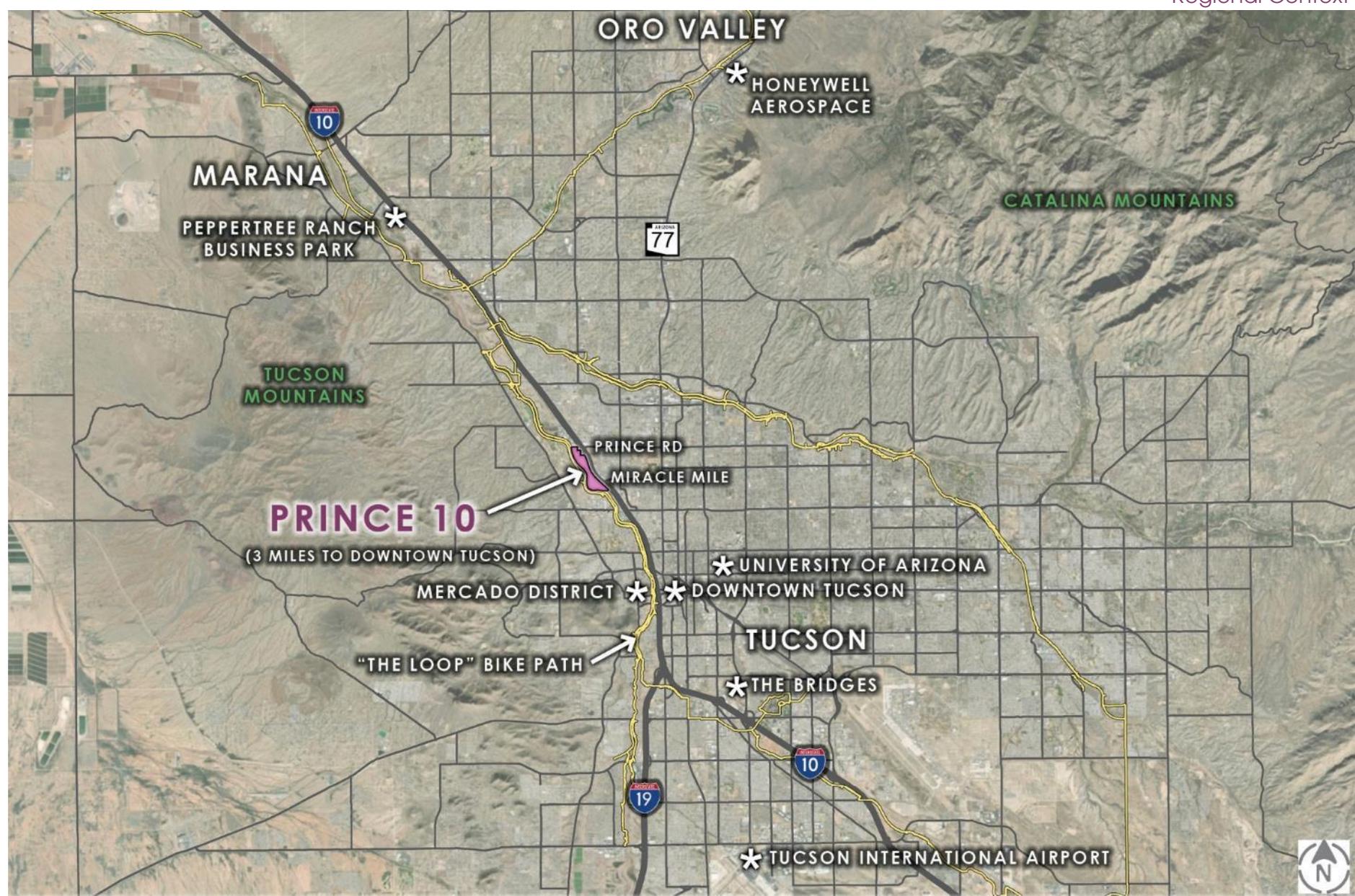
Pedestrian access and vehicular access are abundant. PRINCE 10 is next to approximately 1.25 miles of the Chuck Huckelberry Loop shared-use path (“the Loop”) along the Santa Cruz River, and features direct freeway access via two interchanges at Prince Road and Miracle Mile. New public art installations bordering the west side of the PAD will help make PRINCE 10 a destination point along the Loop.

Views of the adjacent mountains are spectacular, and the land is located within a designated Opportunity Zone (OZ), which offers potential tax savings for qualified investments, but only through 2026. Until then, if an Opportunity Zone Fund is established for any

of the PRINCE 10 property, the fund then has 30 months to invest in improvements to that property in order to qualify for the program.

Strategically located along Interstate 10, PRINCE 10 offers a unique opportunity to expand the City’s economic base to underdeveloped freeway frontage. Development along the segment of Interstate 10 (I-10) between downtown Tucson and Marana is comprised of construction suppliers, wholesalers, aggregate mining, and wastewater treatment. Buildings in this area are typically 1 or 2 stories, with limited architectural variety. The Regional Context map on the following page depicts the PAD site in relation to other prominent commercial activity centers in the metro area. Areas such as downtown and the University of Arizona are already thriving. Emerging areas such as the Bridges and the Mercado District have been planned out and are under construction. Developing a quality, multistory employment campus in a highly visible location will build on the momentum found in established and emerging centers, further strengthening Tucson’s economy.





## B. Rationale & Benefits for Use of a PAD

Creating a PAD is most suitable for PRINCE 10 due to its economic development potential, location, and mix of proposed uses. Applying traditional zoning categories to this irregularly-shaped property would require several variances and special exceptions, without which PRINCE 10 would not be attractive to site selectors seeking shovel-ready land to bring major employers to Tucson. The creation of a PAD provides the flexibility to develop site-specific solutions to transform PRINCE 10 into an economic driver in the community. Additionally, the PAD prescribes a vision for the property, ensuring a high-quality finished product with building heights not currently seen outside of downtown Tucson or around the University of Arizona.



## C. Benefits to the Community

The PRINCE 10 PAD generates considerable benefits to the community by:

- Increasing high-paying, high-skilled jobs in Tucson,
- Attracting new investment from major employers,
- Adding economic vibrancy in the area,
- Encouraging pedestrian and bike travel, thereby reducing traffic on local roads,
- Providing public art installations along the Loop to celebrate its importance to the community,
- Adding product variety to existing housing stock,
- Capitalizing on existing infrastructure and expanding it to maximize current capacity, and

- Providing a complementary mix of uses that creates a major employment hub complete with housing and support services in proximity to I-10 and recreational amenities.

## D. Conformance with Adopted Plans

PRINCE 10 is compatible with surrounding land uses, and is consistent with *Plan Tucson*, the City's General & Sustainability Plan adopted in 2013, as well as the *Santa Cruz Area Plan*.

### 1. Plan Tucson

As shown in *Exhibit I.D.1: Plan Tucson Future Growth Scenario Map*, PRINCE 10 is in the Existing Neighborhoods Growth Scenario Building Block as well as the Industrial Areas (adjacent to I-10) and Business Centers (Arizona Board of Regents-owned land) Building Blocks. According to *Plan Tucson*, Existing Neighborhoods are,

*“primarily developed and largely built-out residential neighborhoods and commercial districts in which minimal new development and redevelopment is expected in the next several decades. The goal is to maintain the character of these neighborhoods while accommodating some new development and redevelopment and encouraging reinvestment and new services and amenities that contribute further to neighborhood stability.”*

Industrial Areas are,

*“strategically located for efficient handling of intermodal freight movements. These areas support national and international freight movement through Tucson by connecting existing major regional commercial transportation routes, including railway, major highways, and the airports.”*



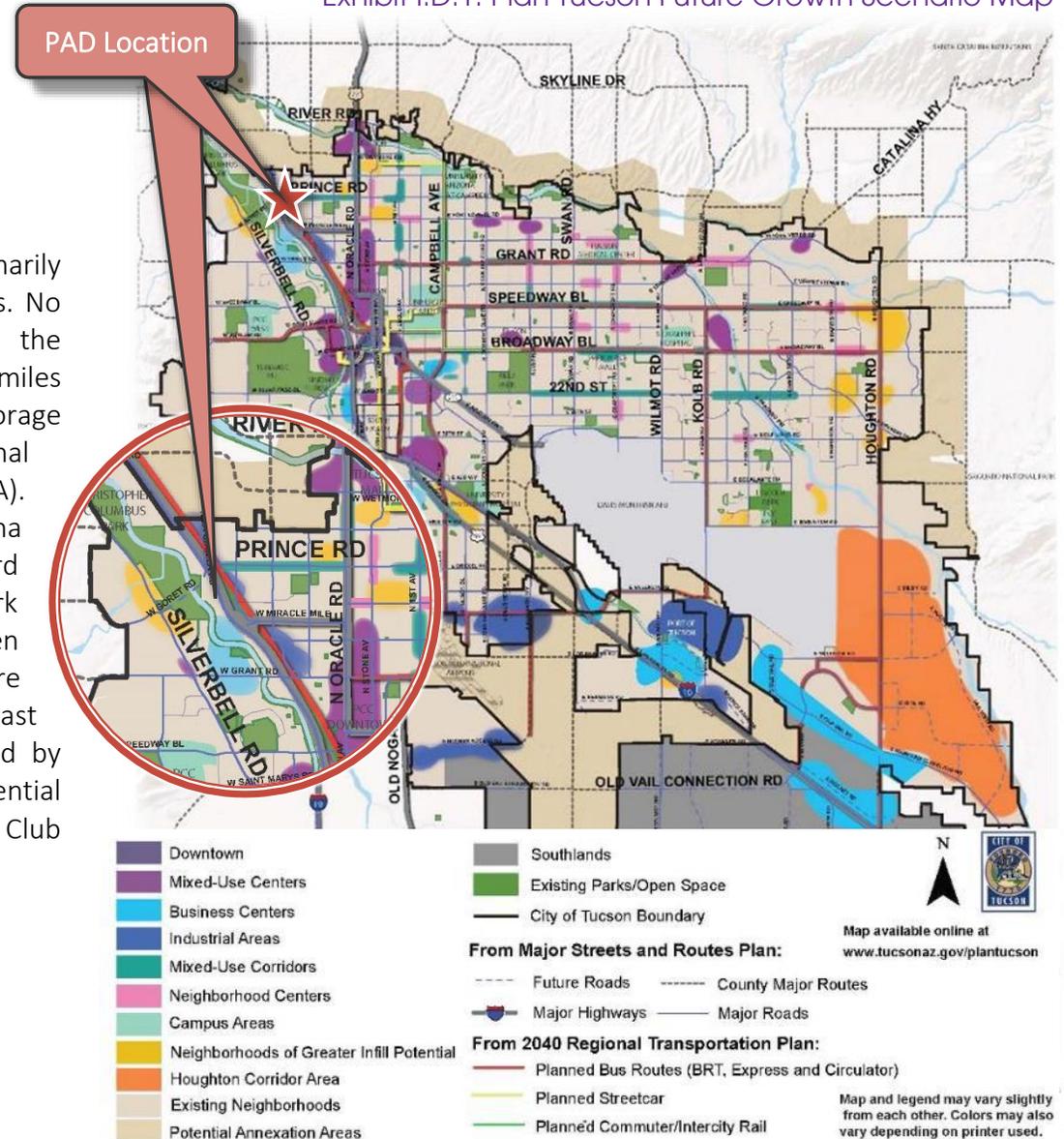
and Business Centers are,

*“major commercial or employment districts that act as major drivers of Tucson’s economy. These centers generally contain corporate or multiple-use office, industrial, or retail uses. Existing examples in Tucson include the University of Arizona Science and Technology Park, Tucson Mall, and the Tucson International Airport area.”*

The area between I-10 and the Santa Cruz River is primarily developed with intensive commercial and industrial uses. No housing or residential neighborhoods exist between the interstate and river until south of West 18<sup>th</sup> Street, 3½ miles south of the PAD site. The PAD consists of vacant land, a storage yard for construction materials, and agricultural/ educational facilities associated with the University of Arizona (UA). Heading south from the UA facilities is an Arizona Department of Transportation (ADOT) construction yard followed by various commercial uses and a business park south of Grant Road. I-10 and the Santa Cruz River are then directly adjacent to each other before giving way to more commercial uses down to West Mission Lane. North and east of PRINCE 10 along the interstate is also characterized by commercial and industrial use without any existing residential until you reach homes surrounding the Quarry Pines Golf Club in Marana.



Exhibit I.D.1: Plan Tucson Future Growth Scenario Map



PRINCE 10 will bring quality reinvestment and redevelopment to this area that also happens to be centrally located off Interstate 10 in a semi-industrial, largely vacant Opportunity Zone area. The project is envisioned as a future employment hub to attract site selectors seeking shovel-ready land north of downtown and close to the interstate. This PAD will allow the entire area to be marketed, designed, and built as a cohesive project with common access and sensitivity to Interstate 10 as a State-designated scenic route. Successfully integrating a mix of uses and restricting access across the Santa Cruz River toward existing residential neighborhoods will help make PRINCE 10 a model for economic development in Tucson.



*Glimpse of existing homes on the opposite (west) side of the Santa Cruz River*

The PAD meets *Plan Tucson's* goals and policies by spurring economic development for Tucson through the provision of shovel-ready land for major employers. It provides for a mix of high-quality light industrial, commercial, and multifamily housing along a major interstate corridor and urban trail system. Connecting the PAD to the Loop further enhances Tucson's appeal to site selectors as a healthy, active, outdoor

community connected to the natural environment. Upon PAD approval, infrastructure will already be in place to promote sustainable, higher intensity development with opportunities to introduce low impact development techniques and other best practices. Urban design elements within PRINCE 10 connect public, semi-public, and private spaces and allow for a range of mobility options. Other related goals and policies include:

#### *The Social Environment*

- **Goal 2:** A stabilized local economy with opportunities for diversified economic growth supported by high-level, high-quality public infrastructure, facilities, and services.
- **Goal 4:** A community whose economic stability and sense of place reflects its commitment to arts and culture and its care for the natural environment.
- **Goal 5:** A community that is healthy physically, mentally, economically, and environmentally
- **Policy PR9:** Develop an urban multipurpose path system that provides mobility options, with recreational and health benefits, to access parks, residential areas, places of employment, shopping, schools, recreational facilities, transportation hubs, natural resources, and watercourses for people of all abilities.
- **Policy PH1:** Pursue land use patterns; alternate mode transportation systems, including multipurpose paths; and public open space development and programming that encourage physical activity, promote healthy living, and reduce chronic illness.

#### *The Natural Environment*

- **Policy GI3:** Create and maintain a connected urban greenway system for non-motorized mobility and to provide human and environmental health benefits.



- **Policy GI5:** Create, preserve, and manage biologically rich, connected open space; wildlife and plant habitat; and wildlife corridors, including natural washes and pockets of native vegetation, while working to eradicate invasive species.

### *The Economic Environment*

- **Goal 9:** An economy that supports existing businesses and attracts new businesses to increase employment opportunities, raise income levels, expand the tax base, and generate public and private investment leading to a high quality of life for the community.
- **Goal 12:** A sustainable and diversified economy that maximizes Tucson's strategic location and balances traditional import and export of resources with locally supplied goods and services to meet local demand.
- **Policy JW2:** Recruit, retain, and expand businesses and industries within Tucson's key economic sectors, including but not limited to aerospace and defense, bio-sciences, renewable energy, astronomy, and optics to increase high-quality, high-paying job opportunities.
- **Policy BC5:** Foster the success of commercial areas, including downtown; major corridors; and arts, entertainment, and business districts through targeted investment, incentives, and other revitalization strategies.
- **Policy RG2:** Capitalize on Tucson's strategic location by maintaining and enhancing Tucson as an international port and center for commerce and logistics.

### *The Built Environment*

- **Goal 25:** An urban form that conserves natural resources, improves and builds on existing public infrastructure and facilities, and provides an interconnected multi-modal

transportation system to enhance the mobility of people and goods.

- **Policy RR1:** Redevelop and revitalize in areas with the greatest potential for long-term economic development by focusing public resources, tools, and incentives to catalyze private investment
- **Policy LT3:** Support development opportunities where:
  - a. residential, commercial, employment, and recreational uses are located or could be located and integrated.
  - c. multi-modal transportation choice exists or can be accommodated.
  - d. there is potential to develop moderate to higher density development.
- **Policy LT4:** Ensure urban design that:
  - b. integrates alternative transportation choices, creates safe gathering places, and fosters social interaction.
  - e. takes into account prominent viewsheds.
- **Policy LT9:** Locate housing, employment, retail, and services in proximity to each other to allow easy access between uses and reduce dependence on the car.
- **Policy LT14:** Create pedestrian and bicycle networks that are continuous and provide safe and convenient alternatives within neighborhoods and for getting to school, work, parks, shopping, services, and other destinations on a regular basis.

*New industry and greater  
freeway presence*



## 2. Santa Cruz Area Plan

Within the *Santa Cruz Area Plan*, the PRINCE 10 PAD crosses four Key Parcels: 12-15. Each key parcel calls for more intense private development in the form of park industrial and commercial uses with a mix of residential. As noted in the Plan excerpts below, the potential for River Park (a.k.a. the Loop) expansion on the UA land owned by the Arizona Board of Regents (ABOR) is also encouraged. However, land has already been dedicated and the Loop is complete along both banks of the Santa Cruz River. The Loop offers a tremendous amenity to attract interest and improve quality of life in the area, and it will be showcased accordingly in the development of PRINCE 10. Per the *Santa Cruz Area Plan*, the land that comprises PRINCE 10:

*“...provides a potential for Riverpark expansion and more intense private development...”* (Key Parcel 12, Existing Use)

(could have) *“A mix of industrial and residential uses...integrated throughout most of the area. This would reflect existing zoning and approved rezoning patterns. Park industrial uses should be located close to the eastern portion of the area, and access to these uses should be oriented toward...the Prince Road alignment...”* (Key Parcel 12, Proposed Use)

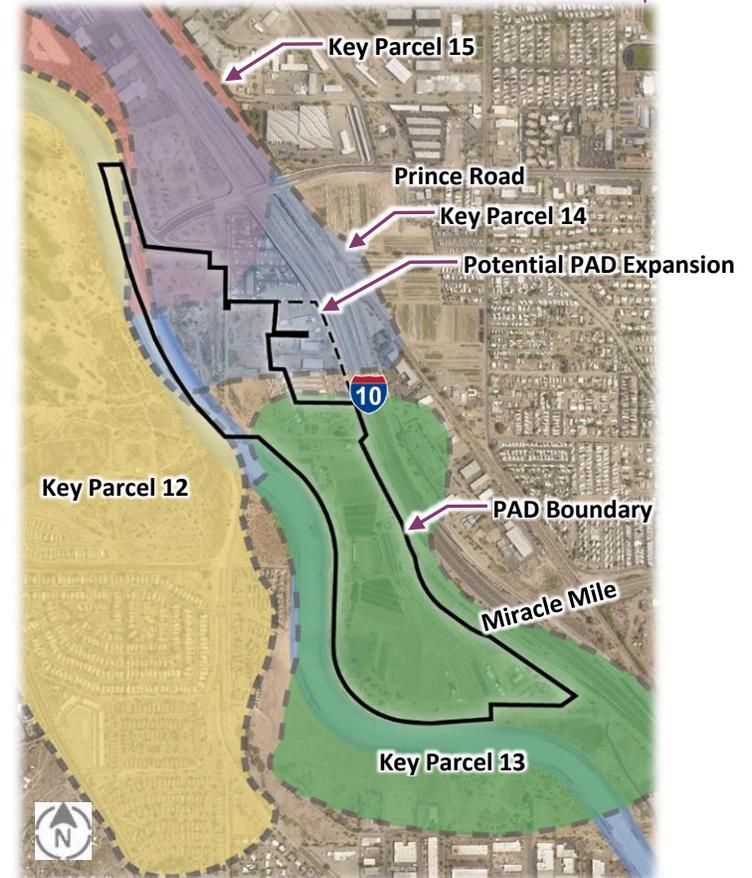
*“...is most suitable for industrial or commercial facilities for motorists in conjunction with the Interstate 10 interchange. Development should be designed to provide an attractive approach to Tucson along the Interstate consistent with State designation of Interstate-10 as a scenic route. The University should be encouraged to dedicate a portion of the parcel or an easement for Riverpark purposes. Any subsequent sale and rezoning of the property should also require this*

*consideration.”* (Key Parcel 13 (UA experimental ag. facility), Proposed Use)

*“...would be suitable for development as a business and industrial park.”* (Key Parcel 14, Proposed Use)

*“...development should be done in conjunction with the park industrial zoning in order to provide an attractive approach to Tucson along Interstate-10...”* (Key Parcel 15, Proposed Use)

Exhibit I.D.2: Santa Cruz Area Plan Map



## E. Compatibility with Adjoining Land Uses

The creation of an interstate commerce campus with a mix of uses is appropriate for the area given that the PAD is located off a major interstate thoroughfare and a prominent regional trail system. Access to the regional trail system can be facilitated anywhere along the western PAD boundary and most of the south boundary. The Loop forms the western limits of future development in PRINCE 10 as it sits at the top of the Santa Cruz River's eastern bank. The Santa Cruz River physically separates PRINCE 10 from the nearest residential properties located nearly five-hundred feet (500') away on the west bank of the river.

*Existing Residential Development*



**Santa Cruz River**

## F. Suitability with Existing Infrastructure

Existing infrastructure is in place to support the proposed development. The property is located off Interstate 10 between Prince Road and Miracle Mile, both arterial streets, with water, sewer, and electric infrastructure in place. The Loop shared-use path runs along the entire western boundary and provides convenient access to an over 100-mile regional trail system.

**Interstate 10**



**The Loop**

**Prince Road**



# PART II PAD Proposal



## A. Plan Overview

As shown in *Exhibit II.B.1: Land Use Plan*, the PRINCE 10 Planned Area Development (PAD) envisions the development of a mixed-use employment center located directly west of I-10 between Prince Road and Miracle Mile. The 113-acre site is well-positioned to support a variety of users in a central campus connected to a variety of transportation networks. Interstate 10 to the east connects the property to an international transportation system. The Loop shared-use path along the western PAD border connects the site to the regional trail system. The Santa Cruz River and Tucson Mountains to the west provide a stunning backdrop, reminding users of our unique desert setting. Development standards and regulations provide flexibility for a range of industry, commercial development, and housing. PRINCE 10 combines flexibility and connectivity in a desirable location to attract businesses from multiple sectors while showcasing the best Tucson has to offer.

## B. Land Use Plan

PRINCE 10 PAD consists of a single land use designation of Interstate Commerce (IC) covering the entire site. The IC designation blends commercial, office, light industrial, and residential uses into a cohesive campus, promoting the integration of ideas and opportunities. With a primary focus on commerce and employment, IC is meant to attract corporate offices, research and development, precision manufacturing, logistics, and other major employment. Commercial uses are intended to support the major employers within the campus by providing convenience and accessibility for all users. Supporting commercial uses may take the form of professional services, restaurants, or coffee shops. Residential uses are limited to high-density multifamily residences like apartments and rental casitas.

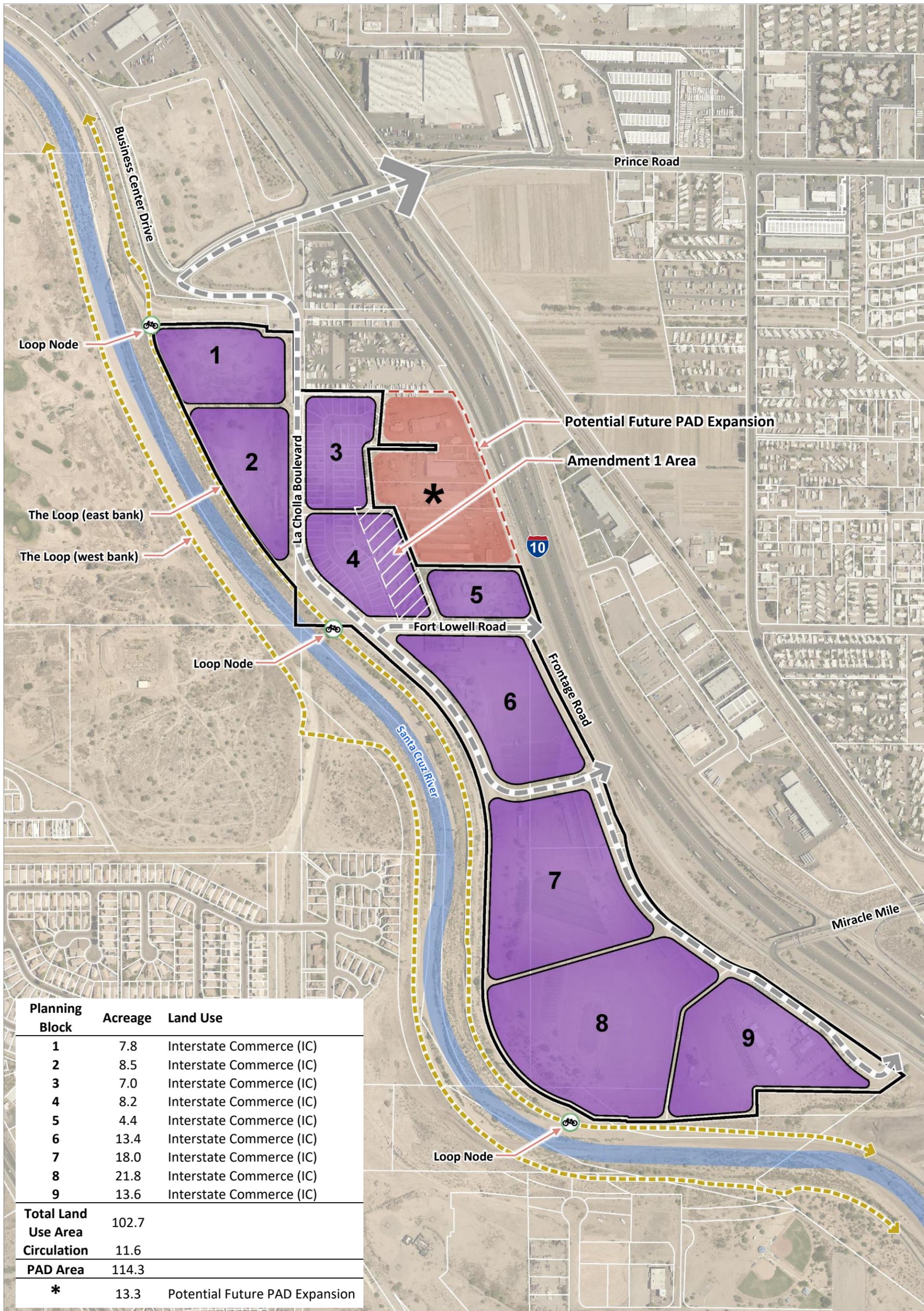
The property is divided into nine planning blocks ranging between approximately 4 to 20 acres in size to accommodate multiple types of development. Detailed engineering and site work has already begun, including utility installation.

Building heights of up to 10-12 stories promote a vertical mix of uses within a single planning block, as shown in *Exhibit II.B.2: Illustrative Site Plan*. Land uses are connected internally via streets and the Loop path running along the entire western boundary.



Properties within the approximately 13-acre *Potential Future PAD Expansion* area are not part of the proposed land use plan and are not subject to the PAD regulations. These properties are included in this document to facilitate future expansion of the PAD. They may be added to the PRINCE 10 PAD in the future via a major PAD amendment. Any property added to PRINCE 10 will have an IC designation.

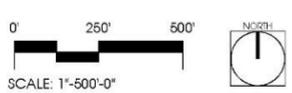


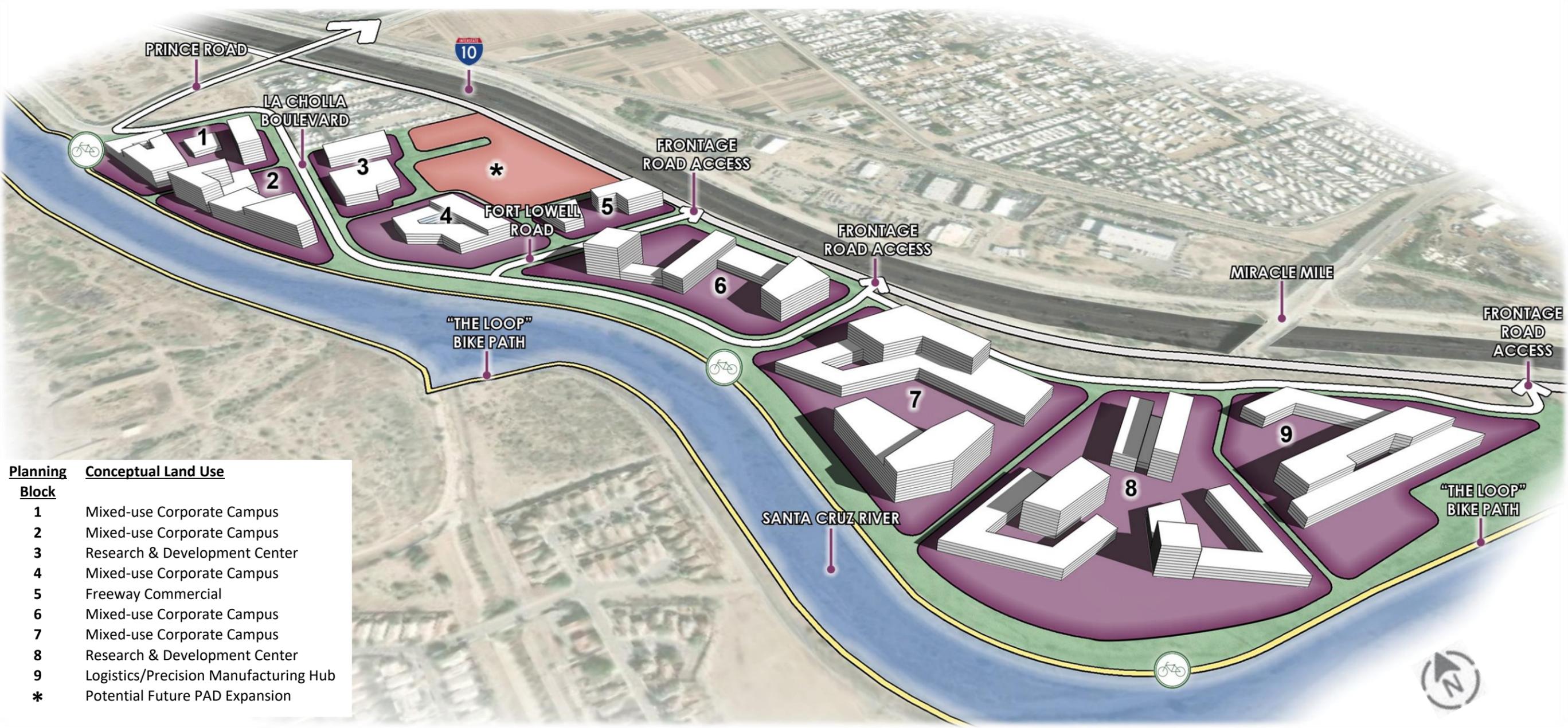


Planning Block	Acreage	Land Use
1	7.8	Interstate Commerce (IC)
2	8.5	Interstate Commerce (IC)
3	7.0	Interstate Commerce (IC)
4	8.2	Interstate Commerce (IC)
5	4.4	Interstate Commerce (IC)
6	13.4	Interstate Commerce (IC)
7	18.0	Interstate Commerce (IC)
8	21.8	Interstate Commerce (IC)
9	13.6	Interstate Commerce (IC)
<b>Total Land Use Area</b>	<b>102.7</b>	
<b>Circulation</b>	<b>11.6</b>	
<b>PAD Area</b>	<b>114.3</b>	
*	13.3	Potential Future PAD Expansion

# PRINCE 10

PROJECT: BFL-02 DATE: 2/17/23  
 FILE NAME: BFL-03\_LAND USE PLAN.DWG





## 1. Conceptual Architecture

Architecture within PRINCE 10 PAD reflects the progressive nature of the mixed-use commerce campus. A contemporary high-quality, and high-tech aesthetic energizes PAD users while connecting them to the local setting. Concrete, glass, stone, and steel are configured in a sleek, modern design, compatible with the desert climate. The use of kinetic architecture is encouraged to maximize visual interest from the freeway and the Loop.



*Kinetic Building Façade*



*Example of Desert Contemporary Architecture*



*Conceptual Architectural Rendering*



## 2. Plazas and Gathering Areas

Outdoor areas allow opportunities to gather and enjoy Tucson's magnificent climate. Plazas create entry experiences to buildings while gathering areas between buildings accommodate a variety of activities.



Seating further defines outdoor spaces while providing comfort for groups and individuals. Restaurants or coffee shops may also offer additional outdoor seating, activating exterior spaces. Shade may be introduced into plazas by incorporating canopy trees, awnings, or shade sails.

Loop Nodes, as shown in *Exhibit II.B.1: Land Use Plan*, are another opportunity to create a sense of place for PRINCE 10 and connect people with the outdoors. These nodes connect the PRINCE 10 campus to the Loop and Santa Cruz River along the western boundary. Connections to the Loop may include seating, shade and artwork, or enhanced landscaping.



## 3. Landscape

Landscape further expresses sense of place by weaving a cohesive theme throughout the PAD. PRINCE 10's landscape complements the contemporary architecture while connecting the property to the local setting. Native and desert-adapted plants add color, form, and structure to plazas and gathering areas. Trees provide shade and help cool walkways, outdoor seating, and parking areas. Plant material and other landscape elements define spaces and articulate a human-scale environment within the multistory campus.



## 4. Artwork



Artwork plays a vital role in PRINCE 10 by creating a sense of place and reflecting Tucson's local character. Outdoor artwork in entry plazas, gathering areas, and important connection points is encouraged throughout the entire PAD. Monumentation along the I-10 frontage may incorporate art to draw users to the campus. Art installations along the Loop offer opportunities to increase connectivity and interest in PRINCE 10. They also help tell the story of Tucson's rich history and culture, a history that began on the land that has been continuously occupied by different peoples along the Santa Cruz River at the base of Sentinel Peak for over 4,000 years.



There is a strong tradition of incorporating artwork throughout the entire Loop network. Pima County has even created a map for self-guided tours of *Art on The Loop*. PRINCE 10 occupies a portion of a 5-mile section of the Loop between Saint Mary's Road and Ruthrauff Road with no artwork at all. With nearly 1.25 miles of Loop frontage, artwork associated with the PRINCE 10 development will fill the void along this stretch of the Santa Cruz.



Art pieces of various sizes and styles will be placed at intervals along the PRINCE 10 section of the Loop to enliven the pathway experience and contribute to the Loop's 131-mile open space art gallery. One art element that evokes a strong sense of place is kinetic art. This type of outdoor artwork relies on movement, either natural or mechanical, to create an everchanging visual experience.



# Art on The Loop

Stroll, ride or run along The Chuck Huckelberry Loop and you're likely to spot more than desert vegetation, wildlife and fellow users. A whimsical steel sculpture of bats on bikes shares space with mosaic tiled bridges, historical homages and sculptural arches. Users are able to enjoy these — and more than four dozen other pieces of art along The Loop — thanks to the Pima County Board of Supervisors' longtime support of a Pima County Public Art Program.

This guide will help you enjoy all of the sculptures, tilework and more that you'll find along The Loop.

For dates and more detailed information can be found at [pima.gov/LoopArt](http://pima.gov/LoopArt)



**Puddles the Great Plains Toad, 2016**  
Paseo de las Iglesias  
Artist: City High School students



**Entry Ramada, 2015**  
Paseo de las Iglesias  
Artist: Andy Dufford, Chevo Studios



**Entry Monument, 2015**  
Paseo de las Iglesias  
Artist: Andy Dufford, Chevo Studios



**Anza and the Kino Tile Mural, 2015**  
Paseo de las Iglesias  
Artist: Las Artes Arts and Education Center



**Tumamoc and the River of Life, 1993**  
West bank of Santa Cruz at Silverlake Road  
Artist: Linda Haworth, John Lovegrove and Les Wallach



**Be Kind**  
East bank of Santa Cruz, south of Cushing Street  
Artist: Various



**Luis G. Gutierrez Bridge, 2012**  
Cushing Street  
Artist: Brenda Sermanick and Structural Grace Inc.  
Architect: Dave Dabler



**Sentinel Plaza, 2004**  
West bank of Santa Cruz at Congress  
Artist: Joy Fox, Andrew Rush, Chuck Sternberg, Judith Stewart, Bob Vint



**Garden of Gethsemane, 1945**  
West bank of Santa Cruz at Congress  
Artist: Felix Lucero



**Mesquite Garden Archway, 1990**  
West bank of Santa Cruz at Congress near Garden of Gethsemane  
Artist: Barbara Grygulis



**Santa Cruz Gateway, 1992**  
West side of Santa Cruz at St. Mary's  
Artist: Susan Gamble  
Artist: Joseph Lupiani



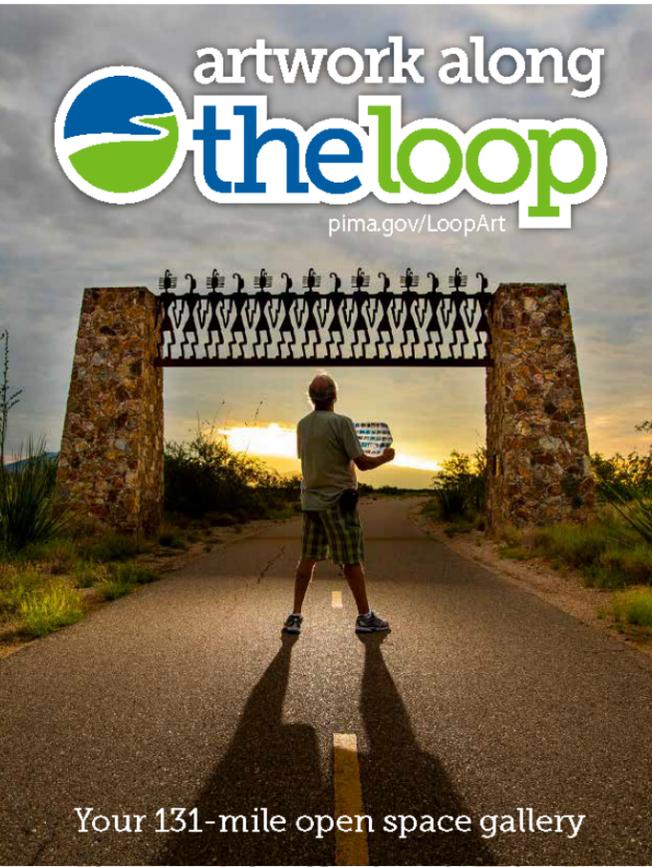
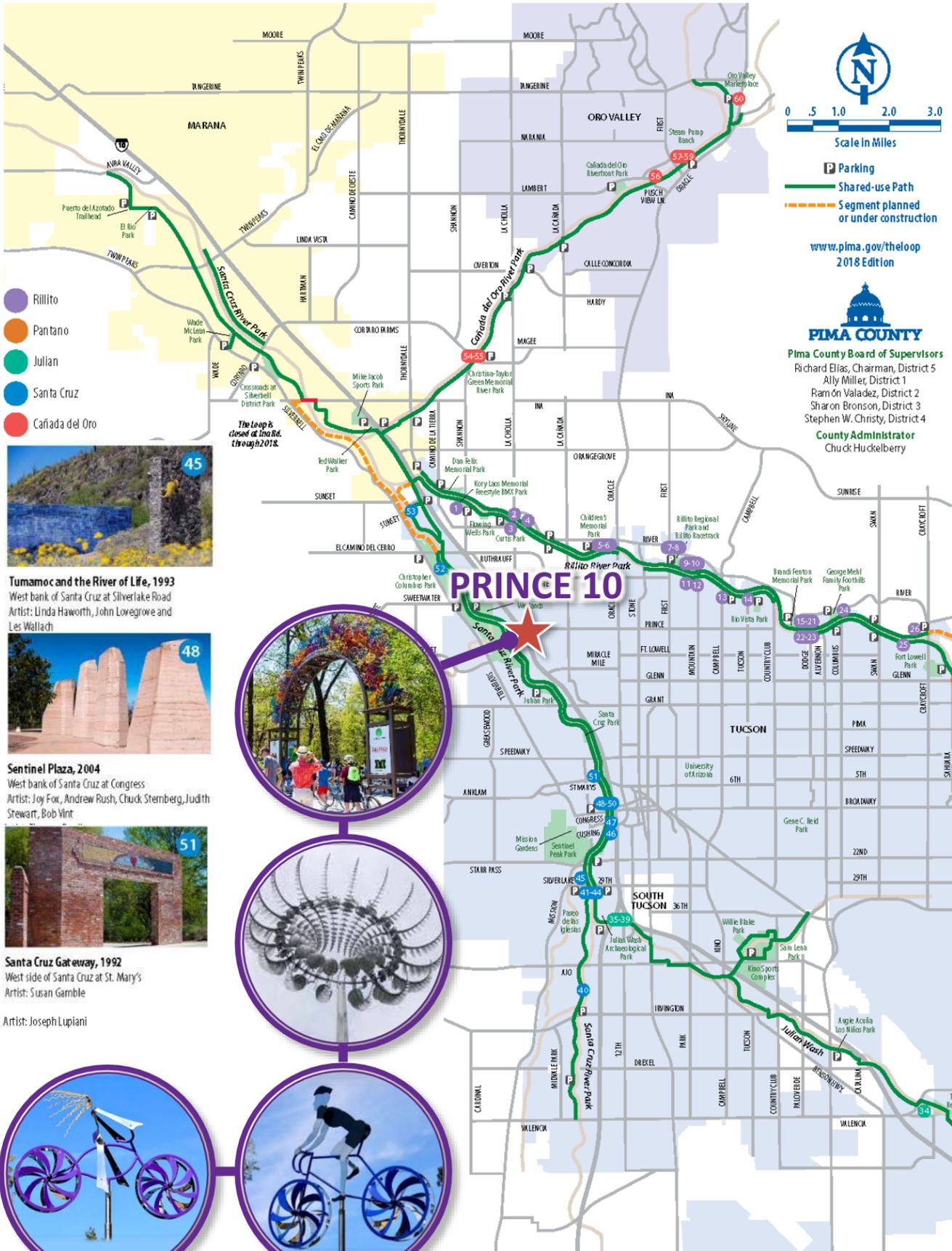
**Columbus Park Entryway, 2011**  
Entry to Columbus Park, 4600 N. Silverbell  
Artist: Gregory D. Schoon



**Desert Bloom, 2017**  
Santa Cruz at Sunset Road Bridge  
Artist: Daniel Martin Diaz  
Artist: Chris Tanz



- Rillito
- Pantano
- Julian
- Santa Cruz
- Cañada del Oro



## C. Circulation Plan

The goal of the Circulation Plan is to promote regional connectivity to PRINCE 10's walkable, campus-like atmosphere. Three modes of circulation create interconnectivity within and around PRINCE 10: the street network, the Loop, and pedestrian sidewalks/crosswalks. The proposed street network interfaces with the existing freeway infrastructure, connecting the site to an international transportation network. The Prince Road interchange brings travelers from the interstate to the north end of the PAD via a left-turn onto Business Center Drive and a right-turn onto La Cholla Boulevard.



La Cholla Boulevard is the main circulator through PRINCE 10. It extends the length of the site, providing two-way access throughout the PAD. It transitions to Fort Lowell Road between Planning Blocks 5 and 6 and then turns southeast running parallel to the I-10 Frontage Road before intersecting it at the southeast corner of the PAD. La Cholla will be a two to three lane public street with capacity for approximately 18,000 vehicles per day (vpd). The roadway cross-section will not include curbs or sidewalks to accommodate sheet flow drainage from east to west across the PAD. However, sidewalks around buildings and crosswalks on driveways/streets within and between planning blocks will be provided to ensure safe passage for pedestrians. Connections to the Loop will also comply with the Americans with Disabilities Act (ADA).

The southbound I-10 Frontage Road along the eastern boundary is one-way only and provides visibility and access to adjacent planning blocks. Several connector streets run east-west between La Cholla Boulevard and the I-10 Frontage Road, further tying the PAD to Interstate 10.



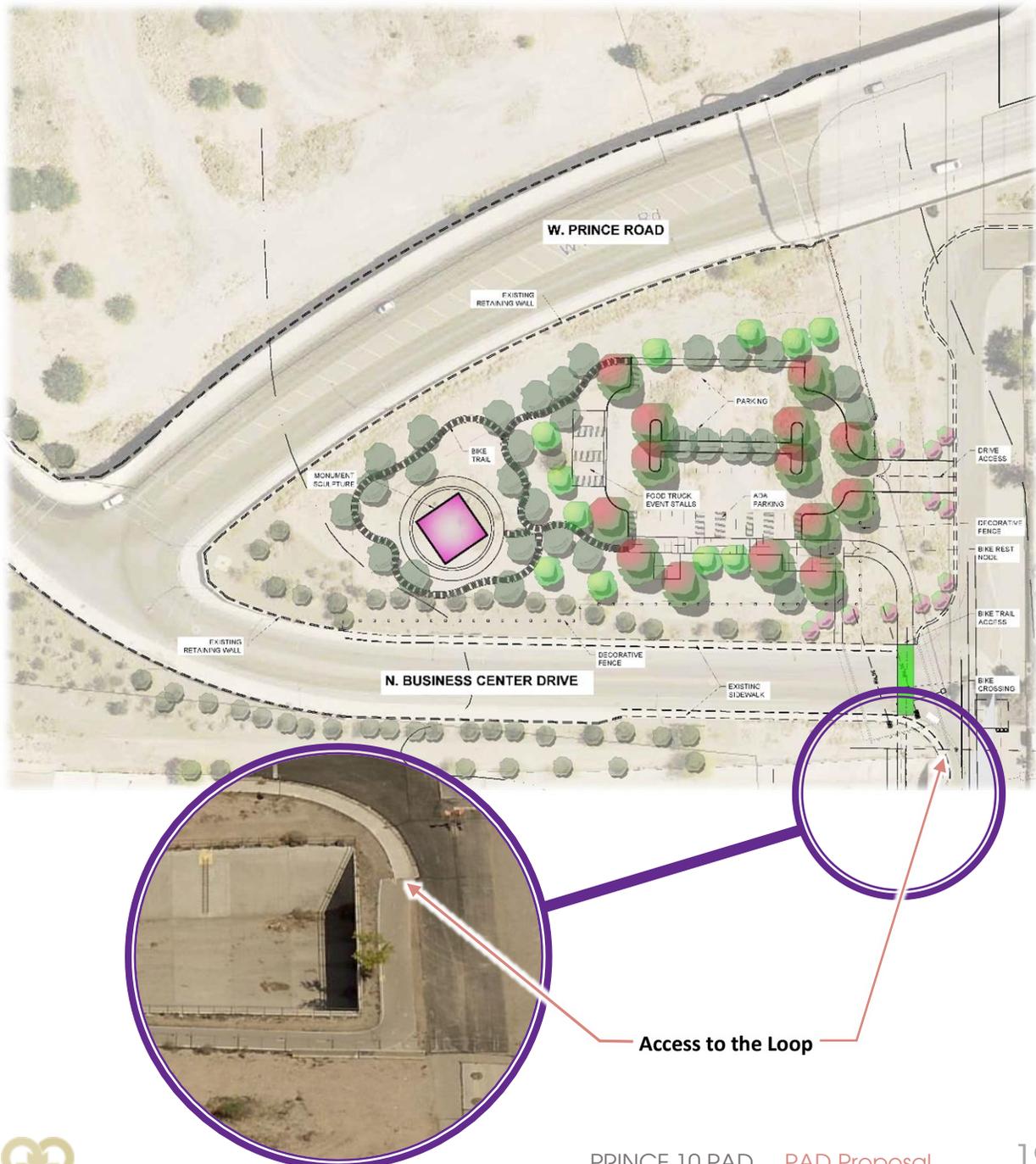
Non-motorized circulation is designed to maximize pedestrian and bike connections to the Loop regional trail system to promote alternatives to vehicular travel. With over 1.25 miles of Loop frontage along the western and southern PAD boundaries, pedestrians and cyclists can traverse the entire length of the property completely separated from vehicles. Meandering sidewalks and paths with pocket groupings



of massed mixed vegetation will be featured at the Loop Nodes, intersections, and points of interest within the PAD.

One major circulation component is the potential for a new bicycle park adjacent to PRINCE 10. As shown in *Exhibit II.C.1*, the City of Tucson is considering construction of a new park for Loop users on the remnant triangle of vacant land between Prince Road and Business Center Drive. The park could offer Loop users a place to park their cars and access the section of the Loop running along the southern bank of the Fort Lowell Drainageway south of Business Center Drive. With the Loop's already extensive bicycle and pedestrian network, PRINCE 10 would complement and benefit from a recreation amenity of this caliber.

Exhibit II.C.1: City of Tucson Bicycle Park Concept



During the Development Package process, the developer will need to submit a Traffic Impact Analysis (TIA) based on ADOT's Traffic Engineering Guidelines and Processes (TGP 240) to show, provide, and analyze the potential impacts to the traffic operations of the Local Street Network, the Frontage Road Access Points, and the Interstate system.

Each individual development may then be required on a cooperative basis with the City of Tucson and ADOT to submit additional studies if found to be necessary during the Development Agreement process. Any off-site improvements shown to be required mitigations as a part of the TIA will be the responsibility of the Owner/Developer and handled through ADOT's Encroachment Permit process through the Southcentral District office.

The Pima Association of Government's 2045 Travel Demand Model will need to be updated once this PAD and development is determined.

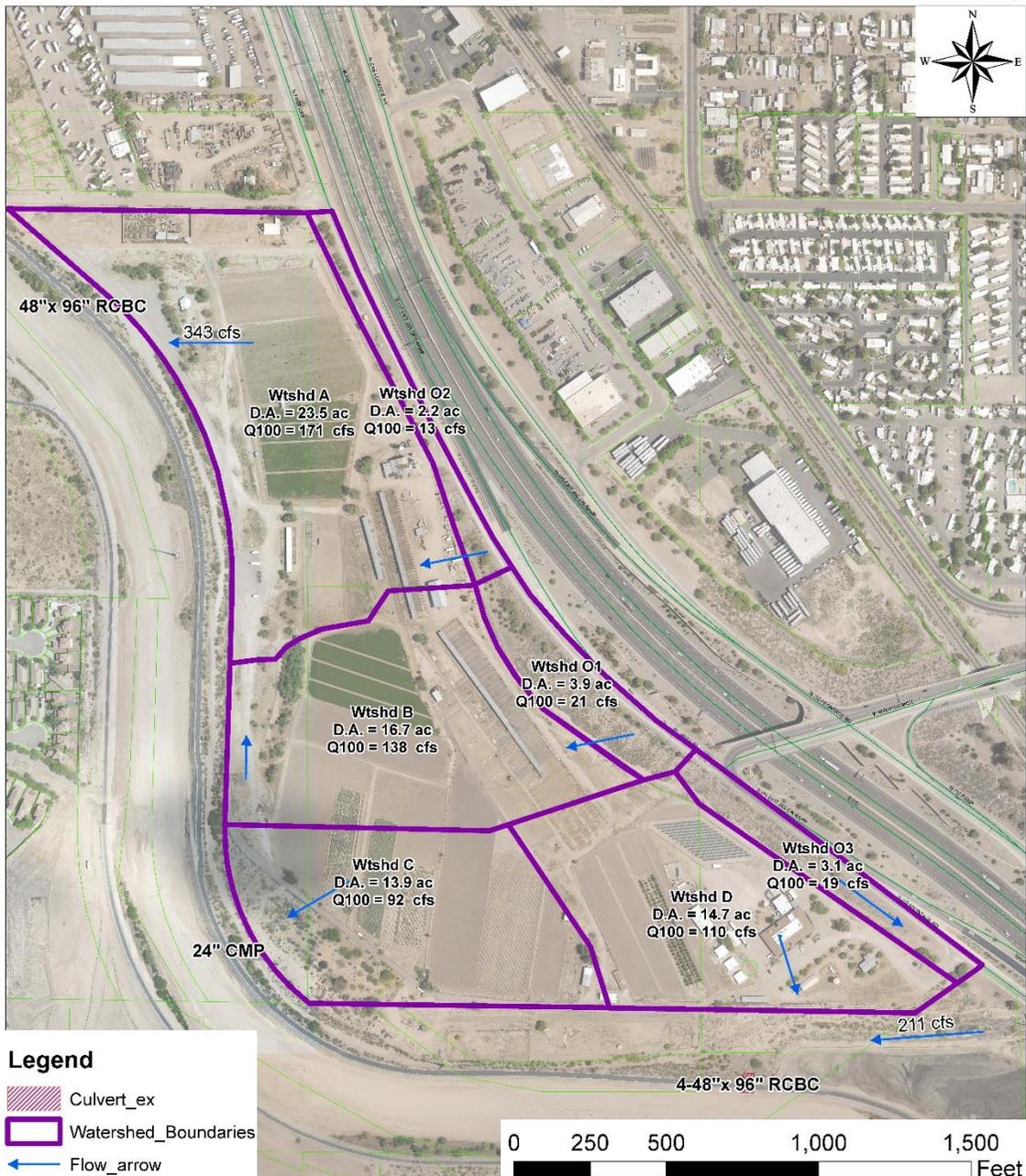
If needed, land, not to exceed 100' by 100', shall be dedicated in fee to the Pima County Regional Flood Control District to allow for an abutment/landing of a future Loop bridge over the Santa Cruz River.



## D. Drainage Plan

In similar fashion to what is anticipated with Phase 1, Phase 2 will consist of high-intensity uses. To account for this eventuality, the impervious cover, runoff coefficient, and basin factor were revised. As expected, the 100-year peak discharges increased accordingly such that Watershed A was anticipated to generate 171 cfs, Watershed B 138 cfs, Watershed C 93 cfs, and Watershed D 110 cfs. A revised watershed map has been provided which reflects the increased discharges. This increase in discharge will result in 343 cfs being conveyed to the RCBC located in Watershed A. It is anticipated that once constructed, the flow will be conveyed in the parking lots as shallow sheet flow. *Exhibit II.D: Developed Conditions Watershed Map* depicts the discharges calculated under developed conditions.

Exhibit II.D: Developed Conditions Watershed Map



## 1. Drainage Constraints

### *FEMA Floodplain*

In accordance with the latest effective FIRM, Phase 2 contains several special flood hazard zones. As a result of flow in the Santa Cruz River surging into the existing 48" x 96" RCBC, a Zone A Floodplain has been delineated at the northern portion of Phase 2. In addition, because the bank protection along the Santa Cruz River is elevated above the terrain of Phase 2, portions of Phase 2 are placed in a Zone X Protected - By Levee Floodplain. Finally, due to the presence of the levee, portions of Phase 2 are subjected to shallow sheet flooding. The Special Flood Hazard Zones are shown in *Exhibit II.D.1.a: Drainage Constraints*. Note that Krueger Wash contains a Zone A Special Flood Hazard Zone, while the Santa Cruz Wash contains both a Zone AE Special Flood Hazard Zone and a Floodway.

### *Santa Cruz River Basin Management Study*

In addition to the flood hazard zones, and water surface elevations considered to be effective by FEMA, Pima County Regional Flood Control District is in the process of adopting new floodplain mapping information as part of the Santa Cruz River Basin Management Study. The results of this study will be used in combination with the floodplain information associated with the effective FIRM to identify areas of potential flood inundation within the project.

### *ERZ/WASH Watercourses*

Based upon a review of information provided by the City of Tucson, there are no ERZ or WASH Watercourses located within the PAD limits.

### *Erosion Hazard Setback*

While the constructed bank protection will abate the lateral migration of the Santa Cruz River towards Phase 2, a 50-foot area measured from the top of the bank protection and levee shall be dedicated to Pima County Regional Flood Control District in fee. This area is shown in *Exhibit II.D.1.a: Drainage Constraints* and *Exhibit II.D.1.b: Santa Cruz River Area Ownership*. If dedication occurs at each phase, dedication is required prior to starting construction.

Conveying runoff in excess of 100 cfs and not having bank protection, Krueger Wash will have an associated erosion hazard setback. It is anticipated that this setback will also be 50 feet, which coincides with both the City of Tucson requirement and the setback determined by Pima County Flood Control District. The setback is shown in *Exhibit II.D.1.a: Drainage Constraints*.



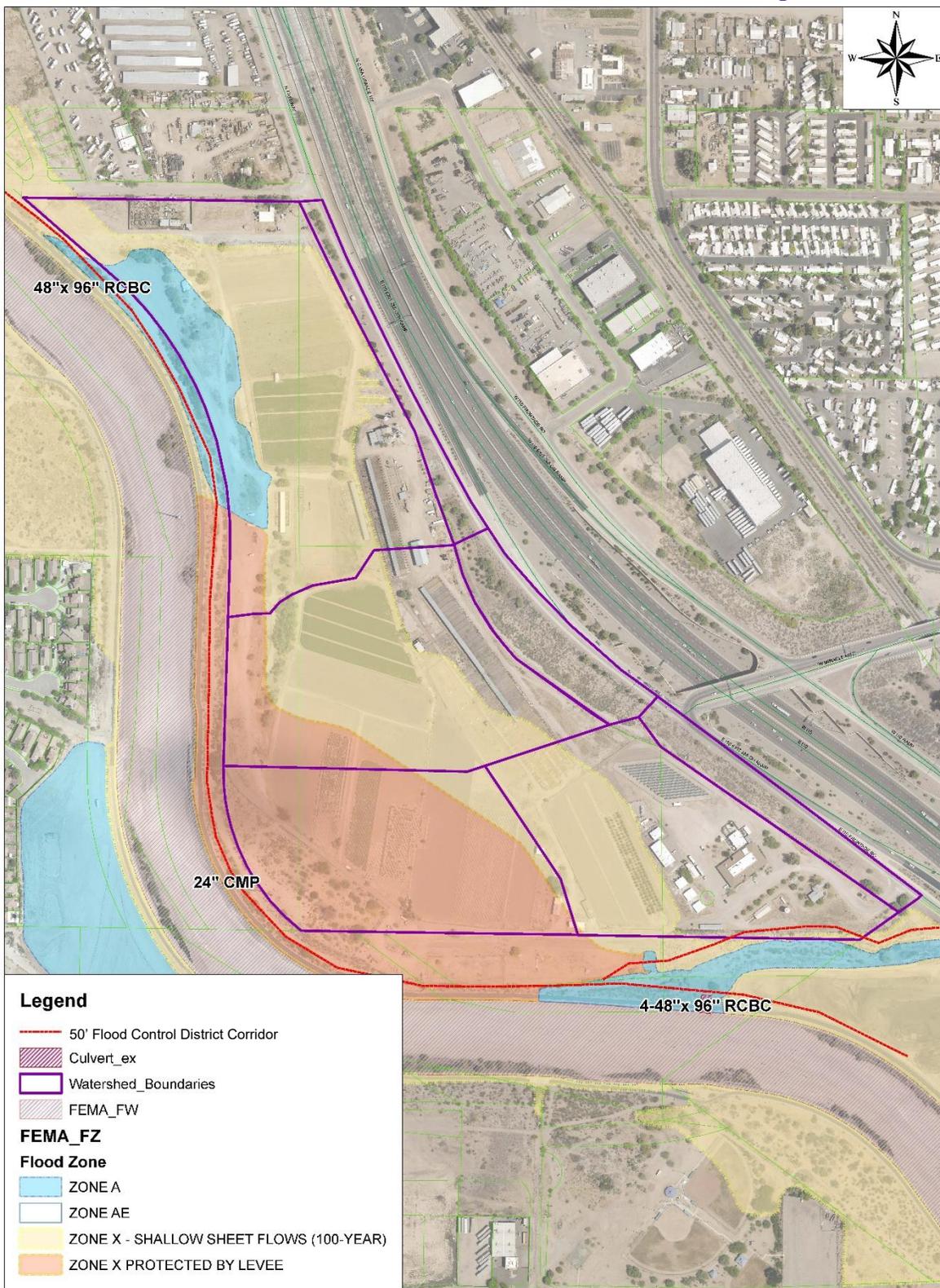
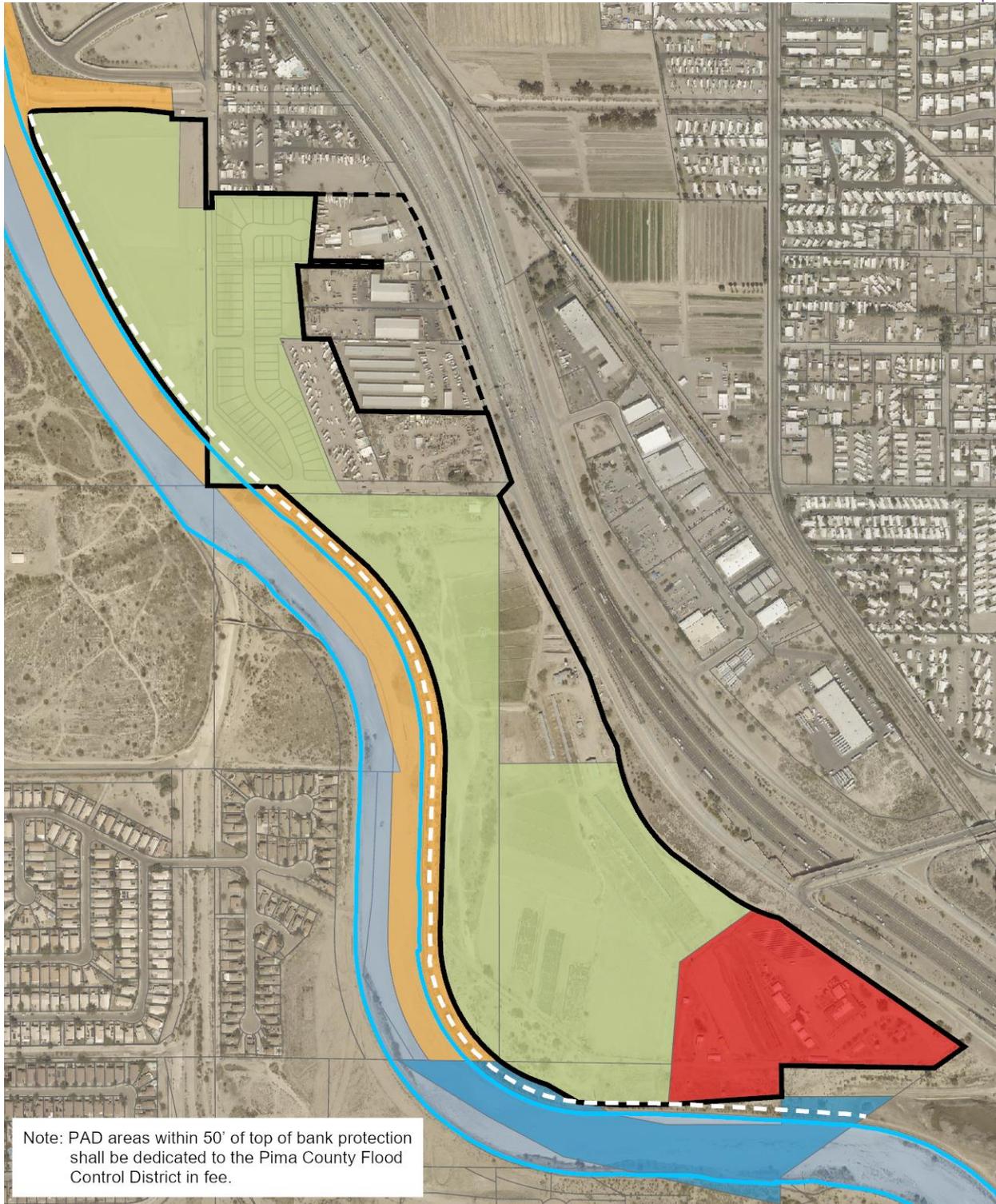


Exhibit II.D.1.b: Santa Cruz River Area Ownership

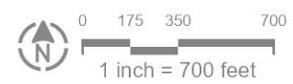


Note: PAD areas within 50' of top of bank protection shall be dedicated to the Pima County Flood Control District in fee.

**LEGEND**

- PAD Boundary
- Potential PAD Expansion
- Santa Cruz River
- Top of Bank Protection
- 50' Top of Bank Offset

- Ownership**
- Arizona Board of Regents
  - City of Tucson
  - PRINCE 10 COT SPE LLC
  - Pima County Flood Control District



File Name: BLF-03  
SOURCE: Pima County GIS, 2022



## 2. Drainage Design

While the Phase 2 layout has not been finalized, it is anticipated that the drainage design will be similar to what was designed for Phase 1. Runoff will be conveyed as sheet flow through the parking lots at depths less than 1 foot. Sufficient first flush retention shall be provided on-site to pass flows through the levee using existing or approved future conveyance infrastructure. Any proposed connection from PRINCE 10 to the existing outlets on District property requires a Facility Impact Permit. All costs associated with connecting circulation and drainage to the Rillito Creek and Chuck Huckelberry Loop, both on- and off-site shall be at no cost to Pima County.

The flow from Watershed A and Watershed B will be conveyed within a drainage corridor to the RCBC located in the northern portion of Phase 2. Watershed C will be conveyed to the existing 24" RCP. It is recognized that because the RCP will have a limited capacity and because the Pima County Regional Flood Control District will prohibit any additional penetrations through the bank protection, basins will be constructed ahead of the culvert to provide sufficient storage to augment the capacity shortfall. The possible location for the drainage corridor and the retention basins are depicted in *Exhibit II.D.2: Drainage Design*.

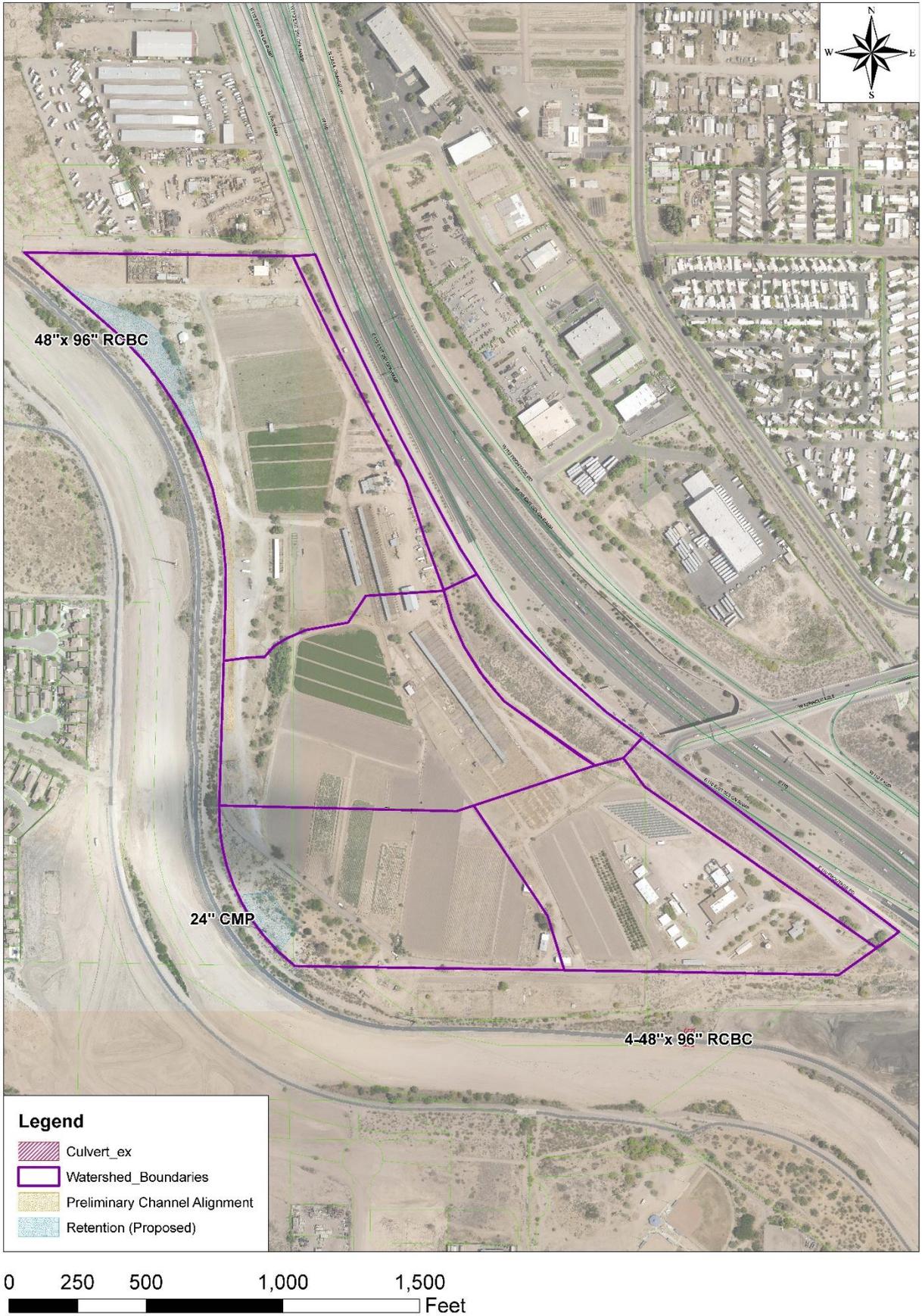
It is anticipated that parts of the private transportation infrastructure will bisect the development pads and as such, flow will need to be conveyed either under the pavement should the roadway be elevated, or over the top of the roadway surface should the road be at-grade. For flow under the road, concrete arches similar to those used in Phase 1 will be employed to convey runoff. Where the flow is allowed to cross over the roadway surface, it will do so at depths less than 8 inches.

As with Phase 1, aside from the basins used to capture runoff ahead of the culverts, retention/detention basins will not be constructed for the purpose of attenuating or storing runoff outside of what is necessary to allow for proper conveyance of flow through the existing outfalls through the bank protection. Additional water harvesting and first flush retention facilities will be incorporated into the landscape and infrastructure design. These facilities would be similar to those presented in the City of Tucson Water Harvesting Guidance Manual. The location and size will be determined as part of the future development design.

Pad elevations for the future development will be based on the more conservative of the base flood elevations (BFE) from the Santa Cruz Basin Management Plan or the effective FIRM. The finished floor elevations will be based on the regulatory flood elevation (RFE), which is set 1' above the governing BFE. Given the length of the project along the Santa Cruz River, the governing elevation will vary and as such both the BFE and RFE will be determined on a pad-by-pad basis using the relevant cross-section. It is not anticipated that the entire site will be raised but like Phase 1, fill will be used where necessary to create the building pad. If elevated pads are located within a mapped FEMA Special Flood Hazard Area, Letters of Map Revision will be prepared and submitted for review and approval to remove those areas from the federal floodplain through the LOMR-F process. If the entire site is to be raised, the requirement is to raise the area to the BFE, without impacts to the existing levee.



Exhibit II.D.2: Drainage Design



## E. Phasing Plan

As previously mentioned, work is already underway to transform PRINCE 10 into shovel-ready land. Development within the PAD will likely proceed north to south, commencing with office/light industrial construction. This generalized phasing is entirely dependent upon market demand and may differ at the time of development.

Presidio Engineering, Inc. prepared and processed a Super Pad Mass Grading Development Package (DP20-0135) for the area within Planning Blocks 1-4. JE Fuller Hydrology & Geomorphology prepared the supporting Drainage Report for DP20-0135. The intent of the development package plan is to create “super pads” to support future site development. The super pad grades are 1-foot above the adjacent Santa Cruz Base Flood Elevation (BFE). As users are identified, detailed DP’s will need to be prepared and processed to support detailed on and off-site grading, drainage, paving, utilities, and landscaping. The grading and drainage work contained within DP20-0135 is currently in progress. The mass grading development package will be revised as necessary in the future to include the expanded size of Block 4 from Amendment 1.

For Planning Blocks 5-9, Presidio has prepared preliminary due diligence items with the intent to ultimately create a Super Pad Mass Grading Plan similar to that of Planning Blocks 1-4. The original intent was to fill this portion of the PAD site and construct an internal access road and collector channel along the west side parallel to the Santa Cruz bank protection. However, Presidio’s initial findings indicated that the site improvements would most likely be minimized if the internal access road was shifted to the east side of the PAD parallel to the frontage road, thereby eliminating the need for a collector channel and reducing fill requirements. JE Fuller also provided the initial supporting drainage due diligence work. Again, the intent of the Super Pad Mass Grading Plan is to create pads to support future site development. The super pad grades are to be 1-foot above the adjacent Santa Cruz BFE, as they are in Planning Blocks 1-4. As users are identified, detailed DP’s will need to be prepared and processed to support detailed on and off-site grading, drainage, paving, utilities, and landscaping.

A direct pedestrian connection to the Santa Cruz River Park will be provided in the first phase to provide access to the Loop. At least one kinetic sculpture will be installed when the first user develops land within the PAD.

According to the Federal Aviation Administration (FAA) Notice Criteria Tool, this project area is located in proximity to a navigation facility and could impact navigation signal reception. As the project site develops every project applicant shall file FAA Form 7460 with the FAA at least 45 days before construction activities begin for every proposed project unless FAA staff, with the Obstruction Evaluation / Airport Airspace Analysis (OE/AAA), provides the project applicant with written communication that filing FAA Form 7460 is not required. It is highly recommended that the applicant file earlier than 45 days to provide the applicant with sufficient time to respond to any concerns which are identified by the FAA. Any cranes which are used must also be identified with Form 7460. Please file Form 7460 at <https://oeaaa.faa.gov/oeaaa/external/portal.jsp>.



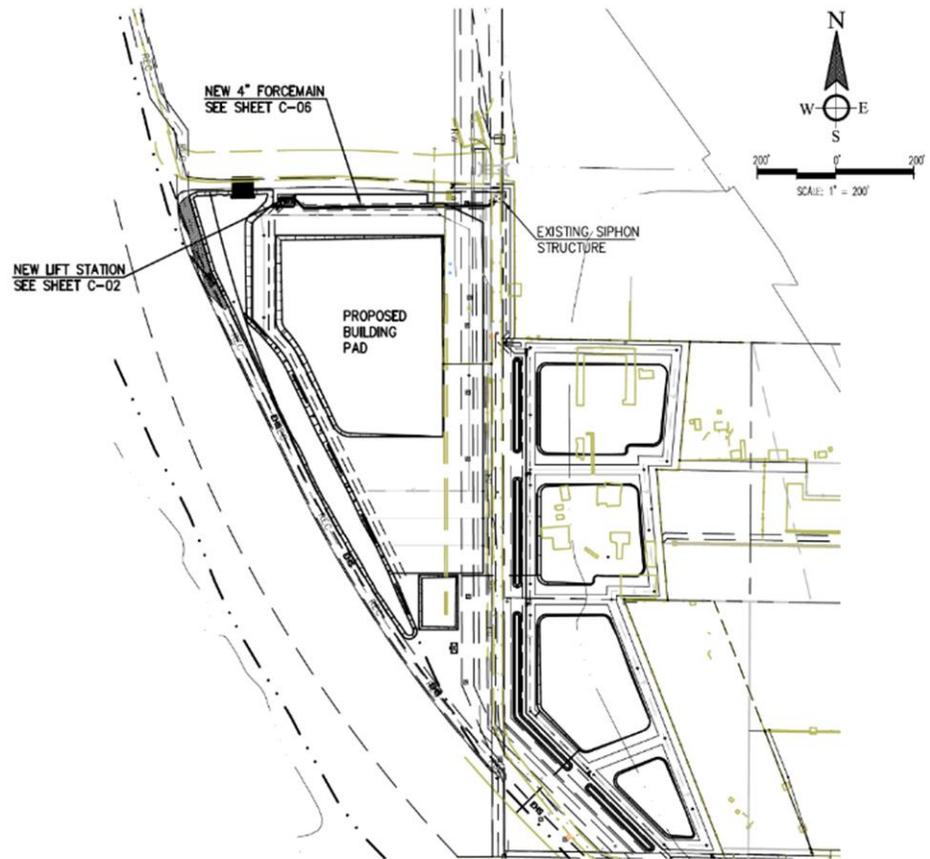
## F. Infrastructure Phasing & Construction

Infrastructure phasing for the PAD will generally proceed in two phases. Phase 1 consists of Planning Blocks 1-4. Phase 2 includes Planning Blocks 5-9 south of Fort Lowell Road.

### 1. Sanitary Sewer Disposal

#### *Phase 1 (Planning Blocks 1-4)*

Pima County Wastewater Reclamation Department serves the PAD, and capacity is expected to be adequate for the proposed development within PRINCE 10. Existing public sewers on and adjacent to Phase 1 include the 8-inch (G-2001-087 & G-64-014) and 72-inch (G-2001-087) public sewers running within La Cholla Boulevard. There is also an 8-inch (G-64-014) sewer line that runs between Planning Blocks 3 and 4 (super pads 4 and 5) and north of Planning Block 3 (super pad 3). A 42-inch (G-2009-068) sewer line also runs along the northeast side of the project, north of Planning Block 3. Sewer service to Planning Blocks 1 and 2 (super pads 1 and 2) will be provided via a private lift station/force main that will discharge into the adjacent 72-inch main. WestLand Resources, Inc. prepared the lift station and force main plans (P20SD00048). These plans were approved through the Pima County Department of Environmental Quality (PDEQ), and a Construction Authorization was obtained. The developer will be constructing the lift station and force main upon completion of the Super Pad Mass Grading. Gravity sewer service to Planning Blocks 3 and 4 (super pads 3 thru 6) is intended to be provided via connection to the existing adjacent 8-inch sewers at a later date.



#### *Phase 2 (Planning Blocks 5-9)*

The only existing on-site public sewer for Planning Blocks 5-9 consists of the 72-inch (G-2001-087) sewer mostly running under the Loop shared-use path adjacent to the Santa Cruz bank protection. Additionally, the 8-inch (G-2001-087) public system that runs through Phase 1 terminates just north of Phase 2. There also appears to be an existing 8-inch private sewer at



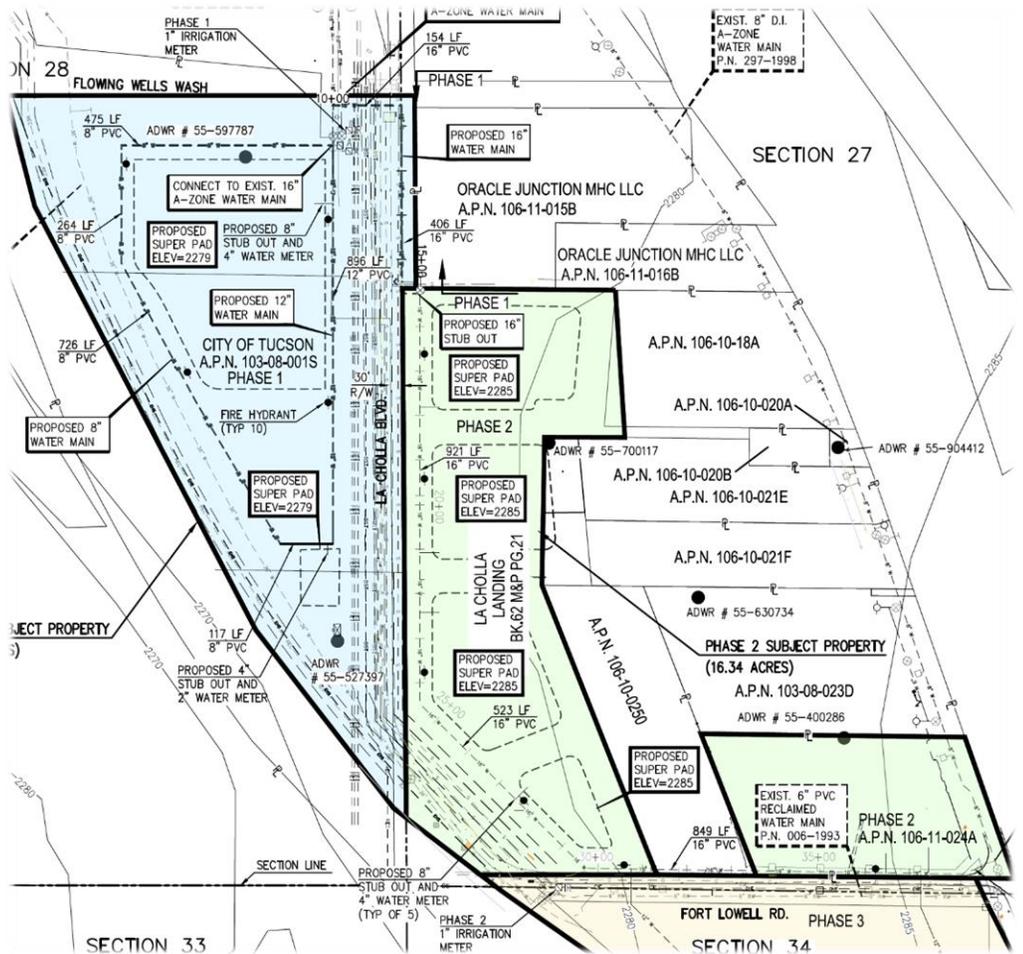
the south end of Phase 2 that may not be usable for PRINCE 10. Sewer service to Phase 2 is anticipated to utilize a new private on-site gravity system and a private lift station/force main that will discharge into an 8-inch extension of G-2001-087 mentioned above.

## 2. Water

### Phase 1 (Planning Blocks 1-4)

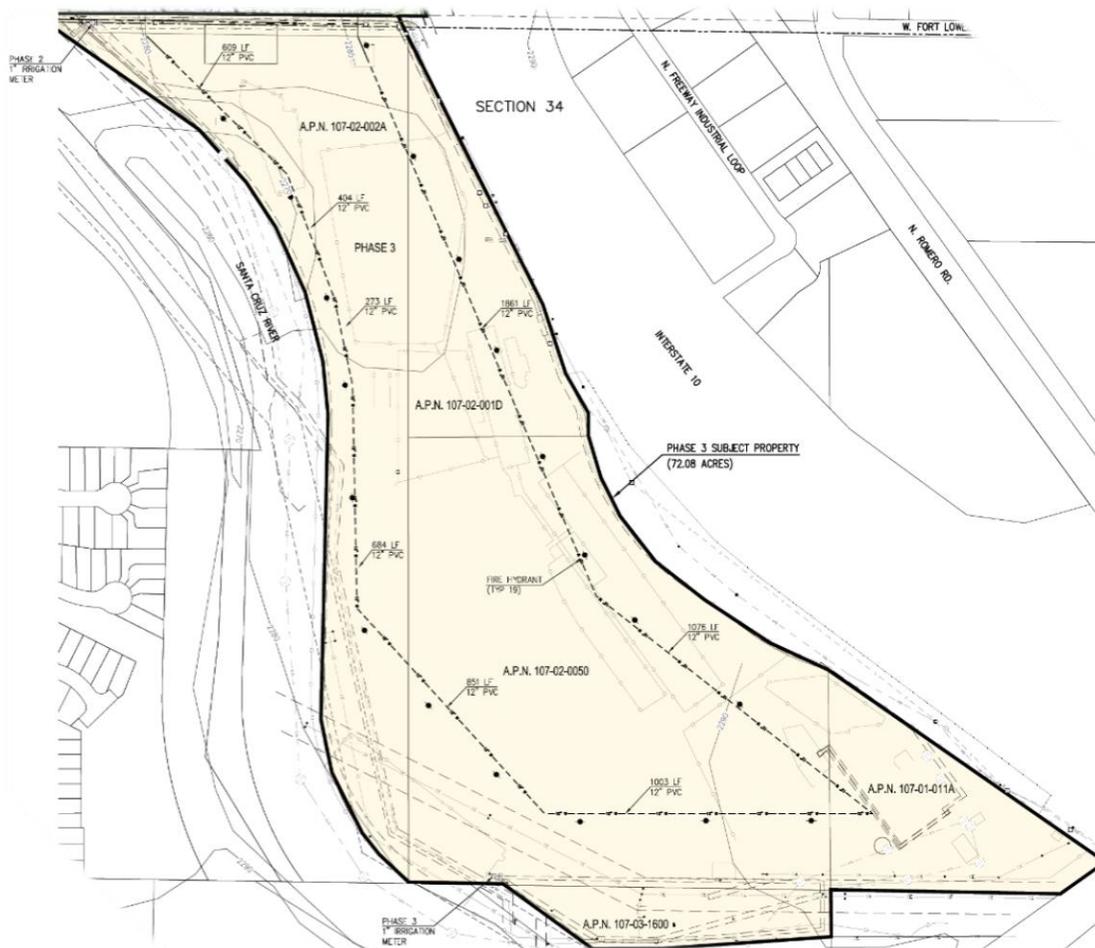
Existing water facilities include a 36-inch Tucson Water (PN 146-1985) reclaimed water line running parallel to the back of the Santa Cruz River bank protection within Phase 1. There is an existing 16-inch Tucson Water (PN 073-2010) water main and an 8-inch Tucson Water (PN 073-2010) reclaimed main within La Cholla Boulevard at the north end of the PAD. An 8-inch Tucson Water main (PN 297-1998) exists at the intersection of Fort Lowell Road and the frontage road.

WestLand Resources is processing a Water Master Plan in support of both Phase 1 and Phase 2. The Water Master Plan is currently under review by Tucson Water. It contemplates domestic and fire flow service to be provided to the pads via a main extension from the 16-inch main at the north end of the PAD to the 8-inch at the frontage road. It is currently anticipated that an internal 8-inch looped system will be used to serve Planning Blocks 1 and 2 (super pads 1 and 2). WestLand has also prepared a Water Plan under review at Tucson Water which shows the connection to the existing 16-inch main and the installation of a 16-inch extension south through Phase 1 and then to the east across La Cholla Boulevard. The developer intends to construct this short 16-inch extension upon the completion of the mass grading activities. The Water Master Plan will be revised as necessary in the future to include the expanded size of Block 4 from Amendment 1.



### Phase 2 (Planning Blocks 5-9)

Existing water facilities include a 36-inch Tucson Water (PN 146-1985) reclaimed water line running parallel to the back of the Santa Cruz River bank protection within Phase 1. There also appears to be a series of private wells and a private on-site storage/booster and distribution system owned by the University of Arizona. As mentioned in Phase 1, WestLand Resources is processing a Water Master Plan (WMP) to support both phases of PRINCE 10. For Phase 2, the WMP contemplates providing domestic and fire flow service via the construction of an internal 12-inch looped system with two points of connection to a proposed 16-inch main to be installed within Fort Lowell Road.



### 3. Solid Waste Disposal

Solid waste disposal will be provided either by the City of Tucson or a private contractor.

### 4. Other Utilities



#### Phase 1 (Planning Blocks 1-4)

The developer is working with Tucson Electric Power (TEP) to extend its facilities through the site. Kinder Morgan has facilities that also run parallel to La Cholla Boulevard within and adjacent to super pads 1 and 2. There are also Century Link



fiber-optic lines and Southwest Gas lines running within and adjacent to La Cholla Boulevard.

*Phase 2 (Planning Blocks 5-9)*

TEP has extensive underground facilities along the frontage road and overhead facilities adjacent to the Santa Cruz bank protection. Additionally, a Western Area Power Administration (WAPA) line traverses the PAD's southwestern corner.

## 5. La Cholla Boulevard / Fort Lowell Road

*Phase 1 (Planning Blocks 1-4)*

Currently, La Cholla Boulevard is a strip-paved, two-lane (one travel lane in each direction) roadway. Additionally, the existing 72-inch sewer lies under the road, forcing the drainage of the east side of the PAD to be at-grade across the road to the west. This at-grade requirement will impact future roadway improvements with respect to curbing and sidewalks.

*Phase 2 (Planning Blocks 5-9)*

The intent is to pull a public access road through Phase 2 to the southern limits of PRINCE 10, as shown in this PAD. The public roadway will essentially be an extension of La Cholla Boulevard, and some reconfiguration of Fort Lowell Road may be needed. A final roadway cross-section has not been defined yet. If the developer acquires La Cholla Boulevard from the City of Tucson, the roadway may become private.



PART III

Development Regulations



## A. Development Regulations

This section provides development regulations for the PRINCE 10 PAD. All new development within the PAD shall conform to applicable building, fire, and other applicable safety standards.

### 1. Interstate Commerce (IC)

Land uses within the PRINCE 10 PAD are modeled after those found in the City of Tucson *Unified Development Code* (UDC) Tables 4.8-4 and 4.8-5 for the C-3 and I-1 zones. Due to PRINCE 10's economic development potential and limited constraints, uses are permitted per those two zones, whose development standards are slightly modified per this PAD. With a goal of providing high-tech, high-rise development that serves the community and region along a major transportation corridor, a mix of development types is encouraged, including office, commercial, industrial, and high-density residential uses. This framework guides the PAD, and land uses are tailored to a range of potential users.

#### a. Permitted Uses

The following land uses are permitted within the PRINCE 10 PAD. The C-3 and I-1 Use-Specific Standards in the City of Tucson *Unified Development Code* (UDC) shall apply for each use unless otherwise modified herein.

#### From the Agricultural Land Use Group:

- Community Gardens
- Urban Farms



#### From the Civic Land Use Group:

- Civic Assembly uses such as convention centers
- Cultural uses such as museums
- Educational uses such as K-12 schools, instructional schools, trade schools, community colleges, and universities
- Membership Organizations like the boys and girls club, fraternal and veteran's organizations
- Postal Services such as USPS and FedEx
- Protective Services such as police and fire stations
- Religious uses such as churches, synagogues and temples

#### From the Commercial Services Land Use Group:

- Administrative and professional offices such as real estate firms, legal services and commodity brokers
- Alcoholic Beverage Service such as bars, taverns, and microbreweries (does not require food service): use-specific standard 4.9.4.C.3 shall not apply, but 4.9.4.C.2 applies to a Large Bar
- Animal Service uses such as vet clinics, kennels and boarding, and grooming services: only the I-1 use-specific standards (4.9.13.Q) shall apply
- Artisan Residence where artists can live and work in the same place



- Automotive uses such as oil and tire facilities, repair shops, and dealer service departments
- Buildings and Ground Maintenance uses such as landscape companies, and janitorial companies
- Commercial Recreation uses such as athletic clubs, gyms, and indoor shooting ranges
- Communication uses such as radio and tv broadcasting, wireless communication facilities, and antennas: only the I-1 use-specific standards (4.9.13.Q and 4.9.4.I.2, .3, & .4.a or .4.b) shall apply
- Day Care facilities including both adult and childcare
- Entertainment uses such as movie theaters, concert venues and theaters for live performances: use-specific standard 4.9.4.C.3 shall not apply, but 4.9.4.C.2 applies to a Large Dance Hall
- Financial services such as banks and credit unions
- Food Service uses such as coffee shops, fast food establishments, and restaurants with or without bar/microbrewery: use-specific standard 4.9.4.C.3 shall not apply
- Medical Services such as hospitals, quick-med facilities, extended healthcare, and in/outpatient care
- Parking of all kinds
- Personal Services such as nail salons, tailors, and beauty and barber shops
- Research and Product Development services such as laboratory services for research, commercial testing and engineering labs
- Technical Services such as recording studios and computer access centers: use-specific standard 4.9.4.W.2 shall not apply
- Trade Service and Repair uses such as automotive body work and paint services, welding shops, appliance repair, reupholstery shops and locksmiths
- Transportation Services (Air and Land) uses such as heliports, glider facilities, ultralight fields, trucking companies, bus stations and rail facilities, and taxicab services
- Travelers Accommodation, Lodging such as hotels and motels: use-specific standard 4.9.4.C.3 shall not apply

#### From the Industrial Land Use Group:

- Craftwork uses such as glass blowing and pottery making
- General Manufacturing uses such as furniture assembly, production of cloth and plastic goods, and auto assembly plants
- Motion Picture uses such as soundstages, outdoor sets, and incidental storage and service facilities
- Perishable Goods Manufacturing uses such as breweries and wineries and bottling plants
- Precision Manufacturing uses such as manufacturing of scientific and medical equipment, audio and visual equipment, and watches



#### From the Recreation Land Use Group:



- Parks and Recreation uses such as pools, ball courts, playground equipment, and general park facilities

#### From the Residential Land Use Group:

- Multifamily Development such as apartments and rental casitas

#### From the Retail Trade Use Group:

- Food and Beverage Sales uses such as bakeries, butcher shops, and grocery stores
- Farmers Markets and Food Truck Parks
- Gem Show: as a temporary use
- General Merchandise Sales uses such as florists, furniture stores, automotive accessories and new parts stores, home improvement centers, and shopping center facilities
- Marijuana uses such as cultivation, dispensaries, manufacturing, and lab testing: use-specific standards 4.9.9.E.2.a, .e, .f, and .g shall not apply
- Vehicle Rental and Sales uses such as auto and truck dealers and rental agencies



#### From the Storage Use Group:

- Commercial storage such as warehouses, data storage facilities, and utility storage yards

#### From the Utilities Use Group:

- Distribution System uses such as electric substations and water wells
- Renewable Energy Generation uses such as solar and wind power



#### From the Wholesaling Use Group:

- Business Equipment Supply and Wholesaling uses such as medical and restaurant equipment and supplies
- Food and Beverage Wholesaling uses such as beverage distributors

#### Mixed Uses:

- Mixed Use and Residential Mixed Use are permitted in the PRINCE 10 PAD.

#### *b. Special Exception Land Uses*

Uses that are similar in nature and intensity to the uses expressly permitted in the PRINCE 10 PAD may be permitted as Special Exception Land Uses if approved through a Planning and Development Services Department (PDSD) Director Special Exception Procedure (UDC Section 3.4.2).

#### *c. Prohibited Uses*

The following uses are specifically excluded from the PRINCE 10 District and may not be approved even as Special Exception Land Uses.

#### From the Civic Land Use Group:

- Correctional uses such as jails, detention facilities, and halfway houses



#### From the Commercial Services Land Use Group:

- Billboards
- Construction Services such as carpentry services and plumbing services
- Funeral Services such as funeral homes and crematories
- RV parks, resorts and guest ranches

#### From the Industrial Land Use Group:

- Extraction of metallic or nonmetallic minerals and energy resources through surface and subsurface mining
- General Manufacturing of explosives and fireworks
- Processing and Cleaning uses such as dry-cleaning plants and linen supply services
- Refining uses such as the smelting and converting of ores and petroleum
- Salvaging and Recycling uses such as auto salvage yards, junkyards and household donation centers
- Primary Manufacturing uses such as foundries and textile mills

#### From the Recreation Land Use Group:

- Golf Courses

#### From the Residential Land Use Group:

- Single family attached and detached, manufactured homes and trailer parks (Family Dwelling uses)
- Group Dwelling Facilities such as student housing, fraternity and sorority housing
- Residential Care Services such as adult care facilities, group homes for developmentally disabled, foster homes, rehabilitation centers for adjudicated delinquents and shelters

#### From the Restricted Adult Activities Land Use Group:

- Adult Commercial Services such as adult model studios, adult massage parlors, and adult theaters
- Adult Industrial uses such as the production of adult motion pictures
- Adult Recreation Services such as adult arcades and body painting studios
- Adult Retail Trade Services such as adult bookstores

#### From the Retail Trade Land Use Group:

- Construction Material Sales uses such as fence companies and lumberyards
- Heavy Equipment Sales uses such as the sale of commercial vehicles and construction equipment
- Swap meets and Auction uses such as flea markets and auction houses

#### From the Storage Land Use Group:

- Utility storage yards (Commercial storage)
- Hazardous Material Storage such as storage for distribution of petroleum products



- Personal Storage such as mini-warehouses and multiple unit storage facilities unless accessory to a permitted use

**From the Utilities Land Use Group:**

- Sanitation System uses such as hazardous material treatment facilities

**From the Wholesaling Land Use Group:**

- Construction/Heavy Equipment Wholesaling and Supply uses such as construction and farm equipment distributors

*d. Accessory Uses and Structures*

Accessory uses are permitted for the use groups in the manner provided in the UDC.

*e. PAD Development Standards*

Table III.A.1.e: Development Standards

<u>Development Category</u>	<u>Development Standard</u>
Minimum Lot Area	None
Minimum Lot Width	None
Minimum Building Setbacks from the PAD Boundary (1)(2)	North (adjacent to vacant industrial land): 0 feet South (adjacent to Santa Cruz River and vacant industrial land): 0 feet East (adjacent to existing commercial development and I-10 Frontage Road): 0 feet West (adjacent to Santa Cruz River): 0 feet
Minimum Separation Between Buildings	Per International Building Code
Maximum Lot Coverage	None
Maximum Density	None
Maximum Building/Structure Height (3)(4)	150 feet

**Notes:**

- (1) Must be outside of all Flood Control dedications/easements.
- (2) Canopies, awnings, colonnades, architectural shading features for pedestrian areas may be located in the right-of-way (ROW) subject to (i) the approval of the Department of Transportation and (ii) the owner/operator obtaining and maintaining the requisite license or easement from the ROW owner.
- (3) UDC Section 7.3 (Solar Considerations) shall not apply to the PRINCE 10 PAD.



- (4) Building height shall be measured in accordance with UDC Section 6.4.4.A (Measurement of Building Height) with the structure height measurement exceptions contained in Section 6.4.4.C (Height Exceptions).

## B. Additional Development Standards

### 2. Circulation Standards

#### a. Vehicular Access

- La Cholla Boulevard / Fort Lowell Road shall be 2-3 lanes without curbs or sidewalks to accommodate sheet flow across the site and meet drainage requirements.
- A minimum 24-foot parking area access lane (PAAL) shall be used as shown on sheet 16 of the PRINCE 10 Super Pad Mass Grading Development Package (DP20-0315).



#### b. Pedestrian Access – Sidewalks

- Pedestrian circulation paths per Technical Standard 7-01.3.0 are not required along La Cholla Boulevard, Fort Lowell Road, or any other main roadway through the PAD except where needed to meet accessibility requirements.
- Crosswalks on driveways/streets within and between planning blocks are required where needed to meet accessibility requirements.
- The minimum width for sidewalks shall be 4 feet.
- Outdoor seating, dining areas, public art and landscaping may be located (i) in the sidewalk area and (ii) in the right-of-way (subject to the owner/ operator obtaining and maintaining the requisite license or easement from the ROW owner). In all cases, a 4-foot minimum clear sidewalk area shall be maintained.
- Sidewalks are not required inside parking structures.
- Sidewalks and pedestrian circulation areas are not required in alleys and are not required to connect the front yard to the rear yard of a property. If more than one building is located on one property, sidewalks and pedestrian circulation areas shall be provided to connect and provide safe circulation between the buildings.



### 3. Parking (Motor Vehicles and Bicycles) and Loading Standards

The goal of the PRINCE 10 PAD to create shovel-ready land for employment generation is reflected in parking requirements that are established by the end user to fit their unique parking needs. Bicycle use is also highly encouraged given the Loop adjacency. This PAD section supersedes parking provisions in the UDC, including UDC Sections 7.4.4 (Required Number of Motor Vehicle Parking Spaces), 7.4.6 (Motor Vehicle Use Area Design Criteria), and 7.4.8 (Required Number of Bicycle Parking Spaces).

#### a. Motor Vehicle Parking

To be determined by end-user based on a Parking Justification Report (PJR) reviewed and approved by the Director. The PJR shall include:



- An analysis of the parking demand from the existing and proposed on-site buildings or uses, including hours of operation and peak use time and demand for each proposed building or use.
- The number and location of proposed on-site parking spaces (including accessible parking spaces).
- The data source used to establish the number of proposed on-site parking spaces.

Parking may be provided either solely by one of the following options or a combination of the following options: on-site; off-site within one-quarter (1/4) of a mile of the PAD, and either owned by the property owner or provided through a shared parking agreement with the City or a third party.

Requests for reductions of the minimum parking requirements of an approved Parking Justification Report shall be supported by a parking statement or, if required by the Director, a Parking Impact Analysis demonstrating no overflow parking onto adjacent properties. Minimum parking requirements for multifamily residential uses may be reduced to 1 space per dwelling unit with an approved PJR.

Installation of electric vehicle (EV) charging infrastructure is encouraged in parking areas, as is the use of solar atop covered parking.



#### *b. Bicycle Facilities*

##### Short Term Bicycle Parking Facilities

- Residential: Minimum of the greater of (a) 0.5 spaces per Dwelling Unit or (b) 3 spaces
- Non-Residential (Office): Minimum of 2 spaces or 1 per 25,000 square feet of GFA, whichever is greater.
- Non-Residential (Retail): Minimum of 2 spaces or 1 per 12,000 square feet of GFA, whichever is greater.
- Non-Residential (Food Service): Minimum of 2 spaces or 1 per 6,000 square feet of GFA, whichever is greater.
- Non-Residential (Light Industrial): Minimum of 2 spaces or 1 per 20,000 square feet of GFA, whichever is greater. Maximum required is 10 spaces.
- Short-term bicycle parking area or the entrance to an interior short-term bicycle parking area shall be located within 100 feet of a building entrance.
- Bike parking facilities may be shared by multiple users within ¼-mile of one another.
- Departures from the Bicycle Parking Design Criteria (UDC Sec. 7.4.9) concerning bicycle storage racks and spacing may be submitted and reviewed in the Development Package submittal and approved by the Director if consistent with urban design best practices.



### Long Term Bicycle Parking Facilities

- Residential: Minimum of the greater of (a) 1 space per Dwelling Unit or (b) 3 spaces
- Non-Residential: Minimum of 2 spaces or 1 per 15,000 square feet of GFA, whichever is greater.
- The number of long-term bicycle parking spaces may be reduced by a Bicycle Share program; for each bicycle in the Bicycle Share program, the number of long-term bicycle spaces may be reduced by 2 spaces, provided that the required number of spaces may not be reduced in the aggregate by more than two-thirds.
- Long term bicycle storage must be secure and accessible to building occupants.
- Long term bicycle parking requirements may be satisfied with interior long-term parking spaces.

#### *c. Parking Structures*

Parking structures shall be designed so that parked vehicles are screened from view at street level, through incorporation of design elements including, but not limited to, landscaping, pedestrian arcades, occupied space, or display space.

Lighting within the parking structure shall provide for safety and security and shall be integrated into the architectural character of the building design. No light bulbs shall be directly visible from outside the parking structure, and light spillage out of the parking structure shall be controlled according to urban design best practices.

The internal circulation to and within the garage shall be convenient, safe, and clearly identified for both pedestrian and vehicular traffic, including appropriate signage for and placement of elevator and stairs.

The minimum height clearance in parking structures shall be as required by the building code.

The minimum width and length for compact parking spaces are 7' 6" and 16' 0", respectively. Compact parking spaces are allowed in parking structures and surface parking lots. The number of compact spaces shall not exceed 50% of the total number of spaces.

#### *d. Accessible Parking Facilities*

The number of accessible parking spaces required by the City of Tucson's adopted Building Code shall not be reduced or eliminated and shall be based on the number of motor vehicle parking spaces required prior to any modification.

#### *e. Loading*

Required loading will be provided in accordance with UDC Section 7.5.



## 4. Landscape and Screening Standards

### a. Theming and Materials

Development within the PAD shall utilize material palette references and embellish PRINCE 10's material lists, when necessary, to maintain continuity while allowing for individual expression. Incorporating a mix of small flowering trees and large shrubs, accents, and ground cover plantings with monumentation features is encouraged. Swaths of artificial turf and a backdrop created by perimeter screening or other background material will help anchor these points while maintaining a cohesive palette for the overall PAD development. The streetscape, Loop Nodes, wash, and bike trail vegetation shall embrace a blend of non-native drought-tolerant species and native riparian vegetative species to promote biodiversity, pollination, and habitat creation.

### b. Street Landscape Border

La Cholla Boulevard street enhancements shall incorporate the City of Tucson design section as approved by the Tucson Department of Transportation (TDOT) while considering the design features and amenities discussed in the previous PAD section to enhance the streetscape experience within PRINCE 10. Large drought tolerant shade trees spaced at approximately 40 feet on center to create a tree-lined streetscape shall be required.

### c. Perimeter Landscape Borders

No landscape borders are required for the north, south, and west property boundaries. Landscape applications in these areas shall be left to the discretion of the developer. If industrial uses are proposed adjacent to existing commercial uses to the east, a landscape border and screening shall be required. An example of this may be found east of the North Business Center Drive/North La Cholla Boulevard intersection. In this location, the PRINCE 10 developer has already installed an 8-12' hedge and vehicle gate to screen the adjacent RV property, Prince of Tucson RV Park (see before and after photos below). Installation of the hedge included an automatic irrigation system using reclaimed water to ensure vegetation viability. Other screening techniques may be more appropriate depending on the adjacent use. Border size and screening type shall be at the developer's discretion.



Otherwise,  
no  
landscaping  
or screening  
is required.

*La Cholla & Business  
Center Drive – Before*



*La Cholla & Business  
Center Drive – After  
(Artistic Rendering)*



#### *d. Parking Area Landscape – Surface Parking*

Canopy trees shall comply with UDC Section 7.6 Landscaping and Screening requirements with all parking spaces located within 40 feet of a tree. In addition to providing shade, canopy trees help mitigate the urban heat island effect.

### 5. Water Conservation Standards

The developer is already using reclaimed water for landscaping. However, conservation standards will be further enhanced by using drought-tolerant plants, efficient irrigation, and rainwater harvesting.

#### *a. Low Water Use Plants*

The plant palette will consist of predominately low water use, native, and regionally-adapted plants except for the existing 8 to 12-foot tall privet hedge. The plants will be located relative to their functionality and the uses associated with the planning blocks within which they are planted. The use of low water use plants in locations appropriate with their species characteristics provides for the conservation of potable water while assuring the survivability and long-term health of such plant material.



#### *b. Irrigation*

Plants requiring irrigation shall be irrigated by means of an efficient underground drip irrigation system and may utilize reclaimed water where available. Underground drip systems reduce water evaporation and waste, thereby conserving water. The irrigation system will be controlled by a programmable controller which can be used to adjust irrigation schedules. The use of different seasonal irrigation schedules reduces the amount of water applied during cooler and wetter periods. Irrigation systems shall be fitted with irrigation controllers and shall be capable of monitoring and responding to plant water needs through the use of weather stations and/or evapotranspiration data.



The technology chosen should be capable of preventing the irrigation system from running if sufficient moisture is present to support the vegetation.

#### *c. Rainwater Harvesting*

In compliance with City of Tucson Development Standards, a water harvesting plan illustrating 50 percent of the estimated landscape water budget is met by water harvesting techniques will be prepared for commercial uses at the time of development package submittal. The water harvesting plan will utilize passive water harvesting techniques to collect rainwater and direct it to planting areas, thereby reducing potable water consumption, if any, for irrigation purposes. Several passive rainwater harvesting techniques may be employed to direct surface water and capture rainfall for the benefit of the landscape, including but not limited to: curb cuts, flush curbs, recessed planting areas, gravel sumps, trenches, collection swales, minimized compaction of planting areas, and semi-pervious pavers. First flush retention shall be provided in Low Impact Development practices distributed throughout the site and shall provide depressed areas (up to 9") for stormwater harvesting to supplement irrigation in the landscape buffers.



### 6. Signage and Monumentation

Signage and monummentation within the PAD are intended to create a sense of place for PRINCE 10 and an arrival statement along I-10 and the Loop. It is also meant to make wayfinding easier throughout such a large area. Signage may be freestanding (horizontal or vertical detached sign structures), wall-mounted, or incorporated into the building architecture. Monolithic sign structures with a high-quality design aesthetic are permitted.

All signage, including freeway signs if proposed, shall meet the UDC design criteria for signs. A Master Sign Program will be sought for any innovative signs that do not meet the UDC design criteria.

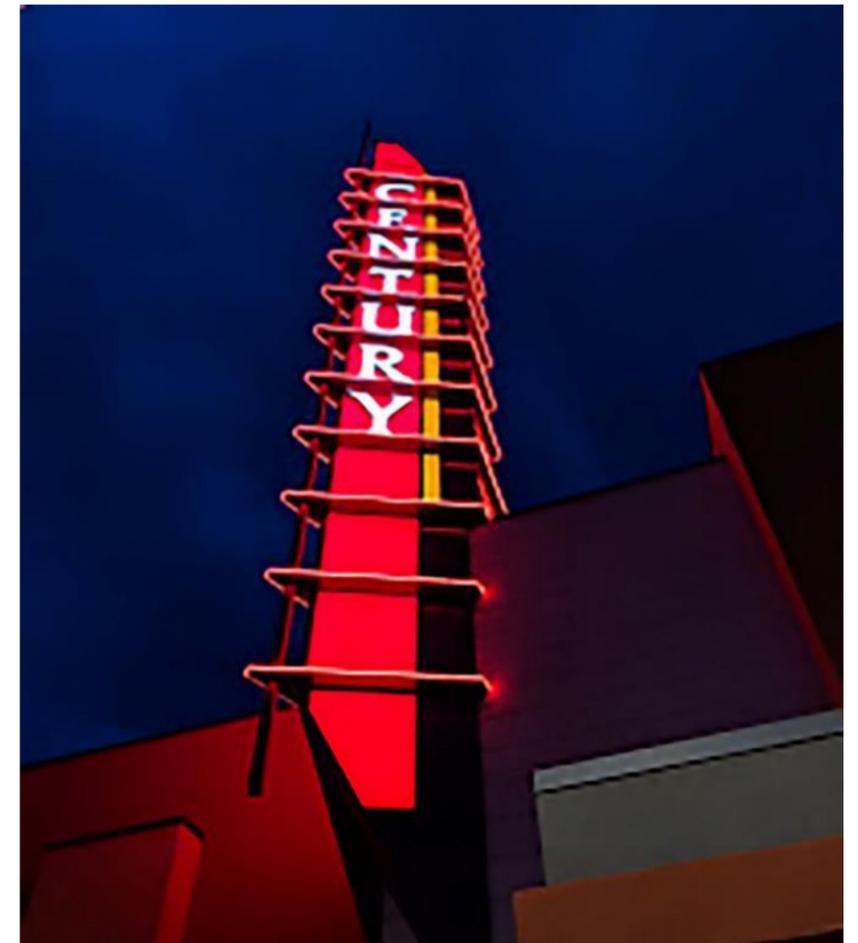
Monuments will primarily use artistic form and structure with minimal text to signify important access locations throughout PRINCE 10. Downlighting and internal lighting may be integrated into monuments in accordance with City of Tucson outdoor lighting standards.



Example Wayfinding Monument



Examples of Vertical Monumentation Elements



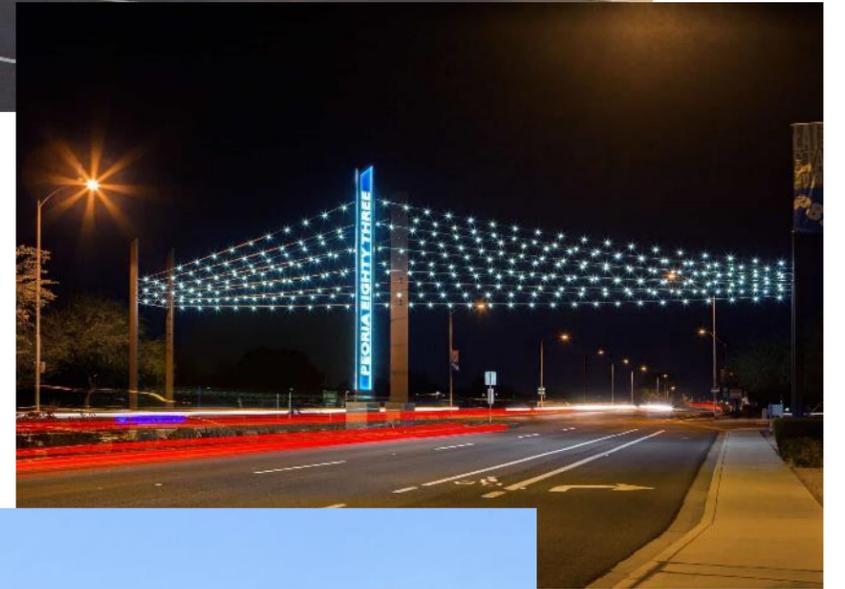
Precedent Image: Century Theatres spire at The Bridges (Kino Blvd & I-10)

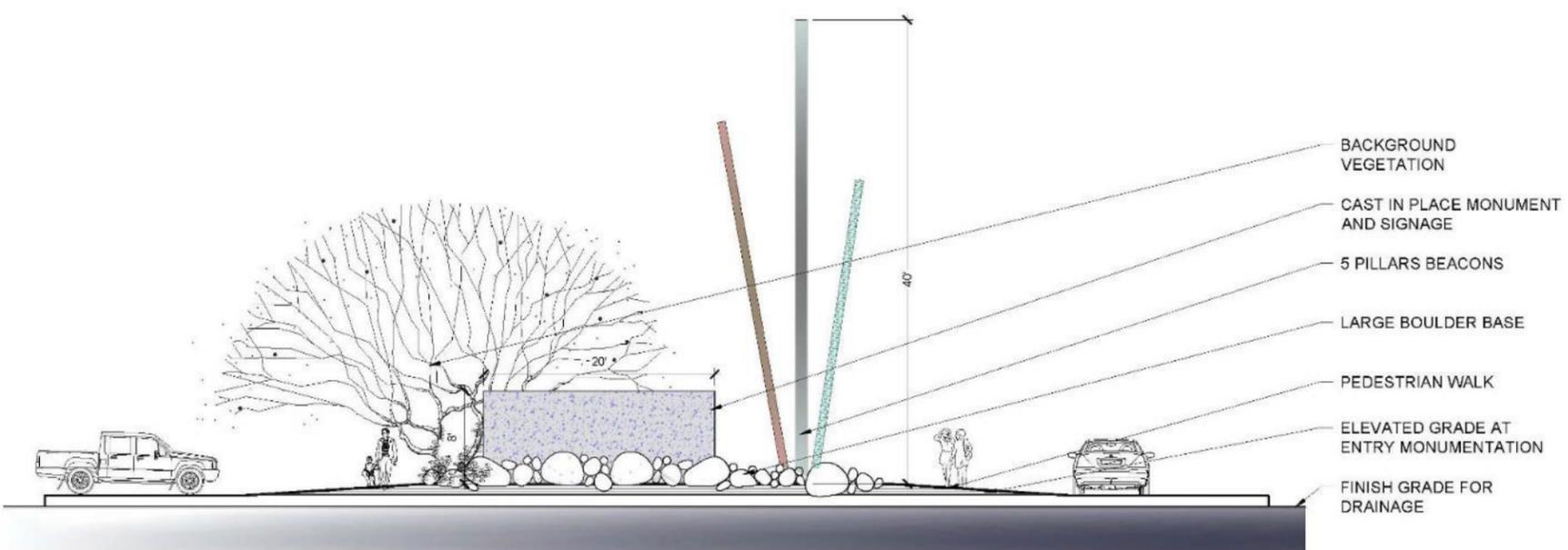
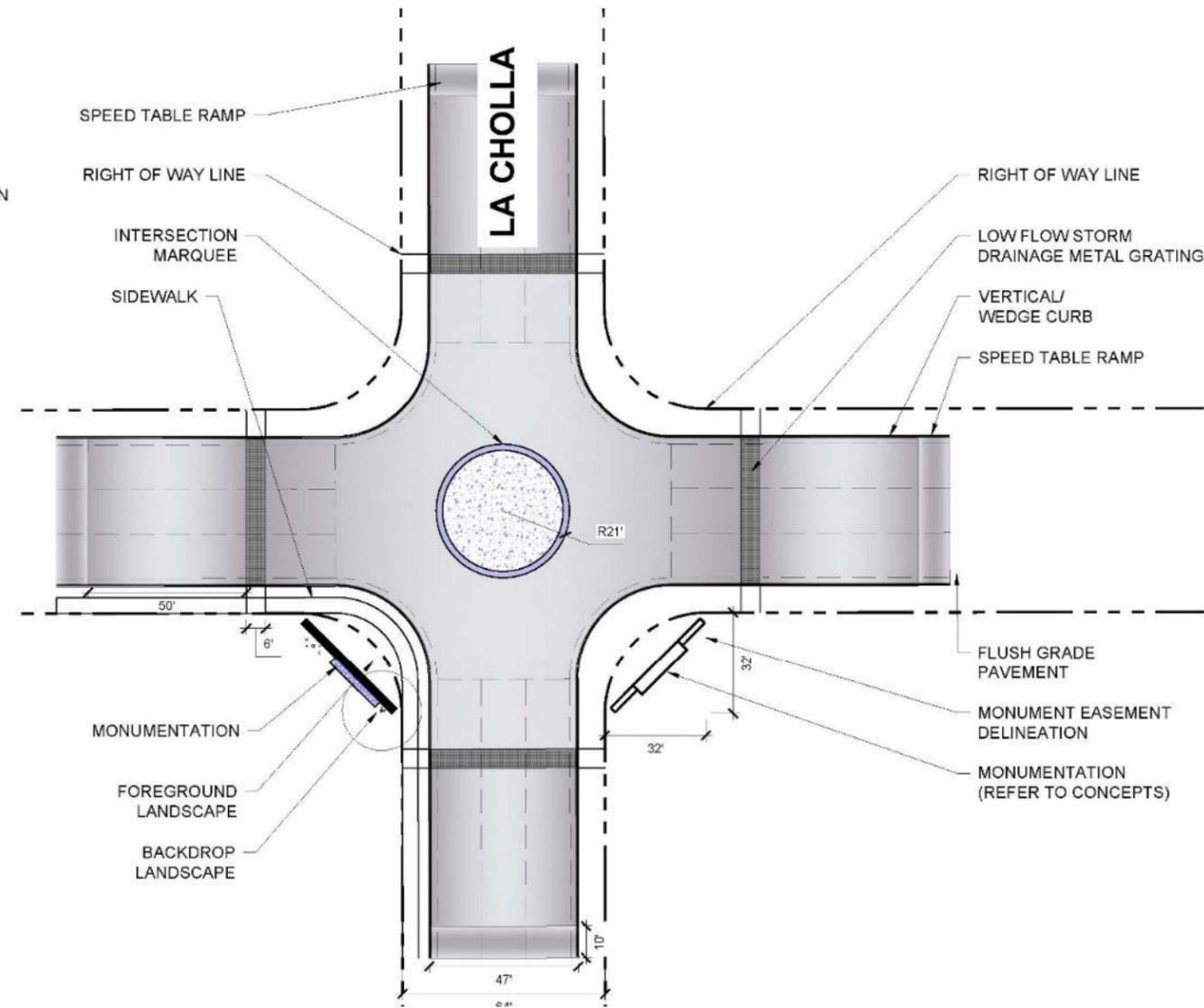
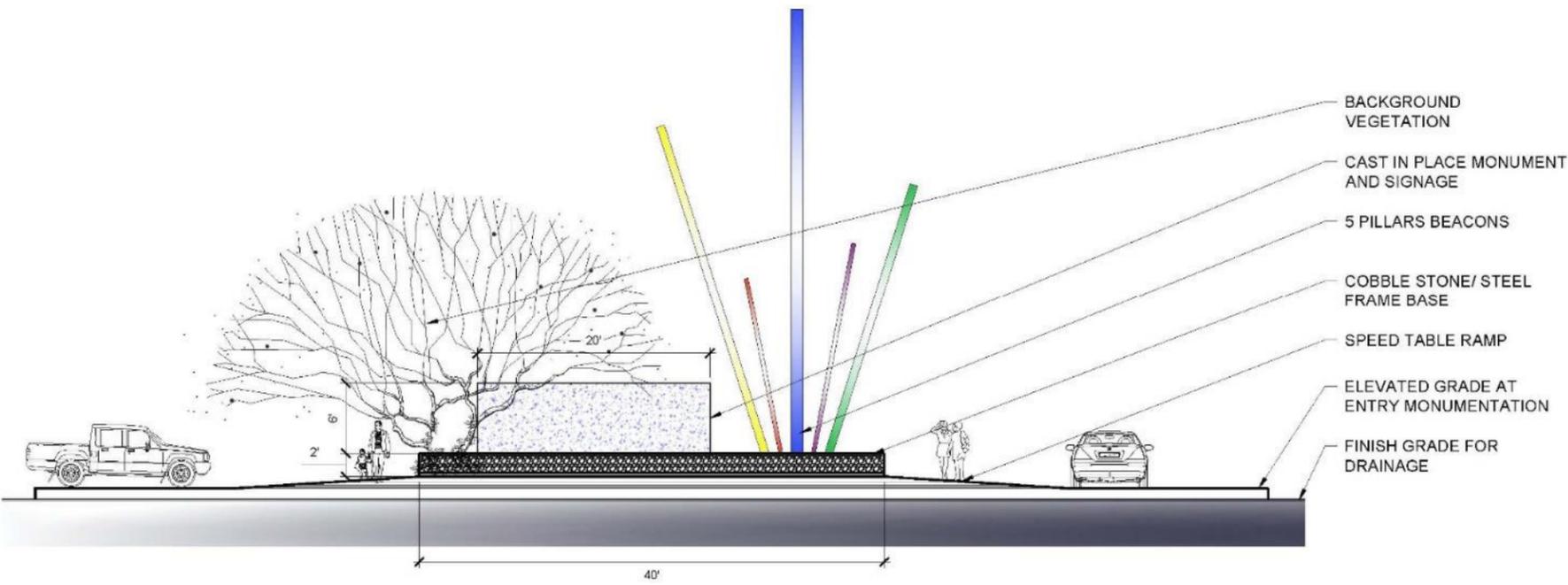


Precedent Image: Century Theatres spire at The Bridges (Kino Blvd & I-10)  
100-foot tall, illuminated spire incorporated into building architecture



Monumentation and Lighting Inspiration from P83 in Peoria, AZ



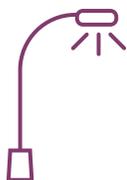


## 7. Solid Waste Standards

All required solid waste and recycling materials collection and storage shall be designed according to the City of Tucson Technical Standards Manual, Section 8: Solid Waste and Recycling Disposal, Collection, and Storage Standards. Dumpsters may be recessed 4 feet into the ground and screened with landscaping instead of an enclosure, as shown in the example photos below. Solid waste and recycling collection and storage containers associated with standalone non-residential uses shall be setback a minimum of 25 feet from any residential use.



## 8. Lighting



All outdoor lighting shall comply with the City of Tucson Outdoor Lighting Code. Street lighting is not required for public or private streets. Lighting may be integrated at the discretion of the future developer(s). In addition, lighting may be provided to illuminate common areas, residential lots, and mixed-use commercial sites using full cut off lights and landscape accent lighting in accordance with the Outdoor Lighting Code.

## 9. Public Art

Public artwork along the Loop shall be located at an interval of at least one art installation every ¼-mile. A minimum of one public art installation shall be installed along the Loop in conjunction with Phase 1. Loop nodes shall contain at least one major art piece that may be counted towards the ¼-mile interval requirement. Public art may consist of sculpture, mosaics, decorative paving, or other media as deemed appropriate by the developer. Art pieces shall be constructed with durable materials suitable for exposure to outdoor elements. Local artists should be commissioned whenever possible to showcase local creativity and talent. Signifying the importance of artwork in PRINCE 10, the developer has already been in contact with the UA Art Department on works of art for the PAD. Additional artwork may be installed throughout PRINCE 10 and along the Loop at the developer's discretion.



# PART IV Implementation & Administration



## A. Design Standards and Guidelines

Future design guidelines and standards prepared by the Developer of PRINCE 10 prior to submittal of a Development Package will be representative of the overall intent of the PRINCE 10 PAD and will strive to ensure the creation of a cohesive mixed-use development. The PAD design guidelines must be submitted to PDSO for its review and approval before the guidelines are finalized and implemented. The standards and guidelines will provide the framework for the design and character of the PRINCE 10 Planned Area Development and address the following goals:

- a. Establishing a common theme and design elements that will be used throughout the PAD. Theming and design will cover unifying site design elements, including streetscape design, monumentation, signage, materials, colors, and architectural styles, and will be reviewed and approved by the Planning Director. Guidelines should be clear, concise and enforceable.
- b. Consideration of adjacent development and providing for appropriate edge treatment between existing and future uses within and adjacent to PRINCE 10.
- c. Ensuring compatibility with Contemporary architectural styles while utilizing building materials and techniques consistent with Tucson's arid climate.
- d. Providing consistency with the PAD and the intentions of the document.

The PAD design guidelines will be applied to the IC planning blocks where appropriate, and will require review by the PRINCE 10 Developer and City of Tucson staff to ensure compatibility with surrounding land uses and the City's planning guidance. Due to the potential integration of uses promoted in the PAD, compatibility design for mixing multifamily residential and commercial land uses with light industrial uses within the Interstate Commerce designation will be addressed. Design compatibility must be addressed in the Development Package if deemed appropriate by the PDSO Director.

The Developer will be responsible for establishing the PRINCE 10 design guidelines. The Developer will have approval authority on these matters until such time that the property is entirely transferred from the Developer. Once the project is completed, control will be transferred to a future Owner's Association or management company.

## B. Interpretations and Amendments

### 1. Interpretation

The regulations and guidelines provided within this PAD supersede existing regulations within the City of Tucson (COT) Unified Development Code. If an issue arises regarding definitions, conditions, standards and/or situations not addressed in this PAD, those in the UDC or other COT regulations shall prevail, as interpreted by the COT Zoning Administrator.

### 2. Amendments

Amendments to this PAD may be necessary over time to respond to the changing market demands, or financial conditions, or to respond to the unanticipated needs of new users. Non-substantial changes to the PAD shall be approved pursuant to UDC Section 3.5.5.I and include the following:



- a. Modifications to the permitted uses that do not change the overall intent of the PAD.
- b. Modifications to tax code parcel boundaries, including changes to interior boundaries, right-of-way acquisition, or combining parcels.
- c. Modifications to the proposed PDP provided the Development Standards set forth in the PAD are maintained.
- d. Any other items not expressly defined as substantial based on UDC Section 3.5.5.J.

Substantial changes (as defined in UDC Section 3.5.5.J.2.c), are subject to the amendment application process outlined in UDC Section 3.5.5.J.2.



# PART V Site Analysis



## A. Significant Natural & Built Constraints

No significant natural constraints exist within the PAD boundaries. The property is comprised of either fully graded vacant or semi-developed land, including the ABOR agricultural land, 4H High Ropes Course, Arizona Veterinary Diagnostics Laboratory, and a former RV park. The Santa Cruz River, with bank and levee protection, abuts the western edge of the Loop shared-use path. The Fort Lowell Drainageway, a concrete channel owned by the City of Tucson, sits between the PAD and Business Center Drive. The southbound I-10 Frontage Road borders the eastern PAD boundary, and West Miracle Mile dead-ends at the elevated freeway ramp.

## B. Transportation & Circulation

The property is located east of the Santa Cruz River and west of Interstate 10 (I-10) between West Prince Road and just south of West Miracle Mile. Both banks of the Santa Cruz include the paved Chuck Huckelberry Loop (“the Loop”) shared-use path, which connects PRINCE 10 to the rest of Tucson via the Loop’s car-free regional trail system. Adjacent to the PAD, I-10 is an eight-lane freeway connecting PRINCE 10 to local, regional, and national transportation networks. Existing sound walls and right-of-way landscaping buffer neighboring properties from freeway traffic. The southbound I-10 Frontage Road runs along the east side of PRINCE 10. The frontage road consists of two one-way lanes and provides southbound access to the various commercial businesses west of I-10 and the land owned by the Arizona Board of Regents.



West Prince Road provides primary access from I-10. It is a four-lane road with curbs, sidewalks, and bike lanes. It crosses over I-10 before tapering down to two lanes and terminating at North Business Center Drive north of the property. The elevated interchange at I-10 provides full access to the interstate and frontage roads. This interchange also separates Prince Road from the Southern Pacific Railroad line running along the east side of I-10. Tucson's Major Streets and Route Plan classifies Prince Road as an arterial street.

West Prince Road ends at North Business Center Drive. North Business Center Drive is a two-lane street providing access to the PAD and industrially-zoned businesses to the north. From Prince Road it runs east for approximately 650 feet before terminating at North La Cholla Boulevard. The short segment between La Cholla and Prince provides a critical connection to PRINCE 10.

North La Cholla Boulevard in this location is a narrow, two-lane strip-paved road with no curb, or sidewalks. This street is the only transportation infrastructure present inside the PAD Boundary. It runs north to south, bisecting the northern portion of the property before turning east and becoming West Fort Lowell Road.



West Fort Lowell Road separates the northern and southern portions of the property. It transitions from the North La Cholla Boulevard alignment and runs east before terminating at the southbound I-10 Frontage Road. Like La Cholla Boulevard, this is a two-lane paved road with a thirty-foot right-of-way and no curb, shoulders, or sidewalks. The La Cholla/Fort Lowell alignment creates an access road through the northern half of the PAD.

West Miracle Mile crosses I-10 and terminates at the southbound frontage road near the southeastern portion of the property. The Miracle Mile/I-10 interchange provides full access to I-10 and the northbound and southbound frontage roads. The interchange has sidewalks, turn lanes, a raised median, and bicycle lanes.

As previously mentioned, PRINCE 10 is adjacent to over 1.25 miles of the Loop, which connects to over 100 miles of shared-use paths and buffered bike lanes throughout Metropolitan Tucson. This segment is part of the Santa Cruz River Park and consists of a fourteen-foot paved asphalt path running along the Santa Cruz River's eastern and western banks.



## C. Existing Zoning

As shown in *Exhibit V.C: Existing Zoning*, the PAD boundary currently contains three different zones: R-1, O-3, and C-2. The R-1 Urban Residence Zone is the most extensive, covering half of the northern portion, and the entire southern portion, of the PAD. The other half of the northern portion is mostly zoned O-3 Office Zone, with C-2 Commercial Zone covering the remainder.

Zoning for the surrounding properties within 150 feet is listed below in *Table V.C.2: Surrounding Zoning*.

Table V.C.1: Surrounding Zoning

Direction	Zone
North	I-1 (Light Industrial)
South	R-1 (Urban Residence)
East	RV (Recreational Vehicle), O-3 (Office), C-2 (Commercial)
West	R-1 (Urban Residence)

Exhibit V.C: Existing Zoning

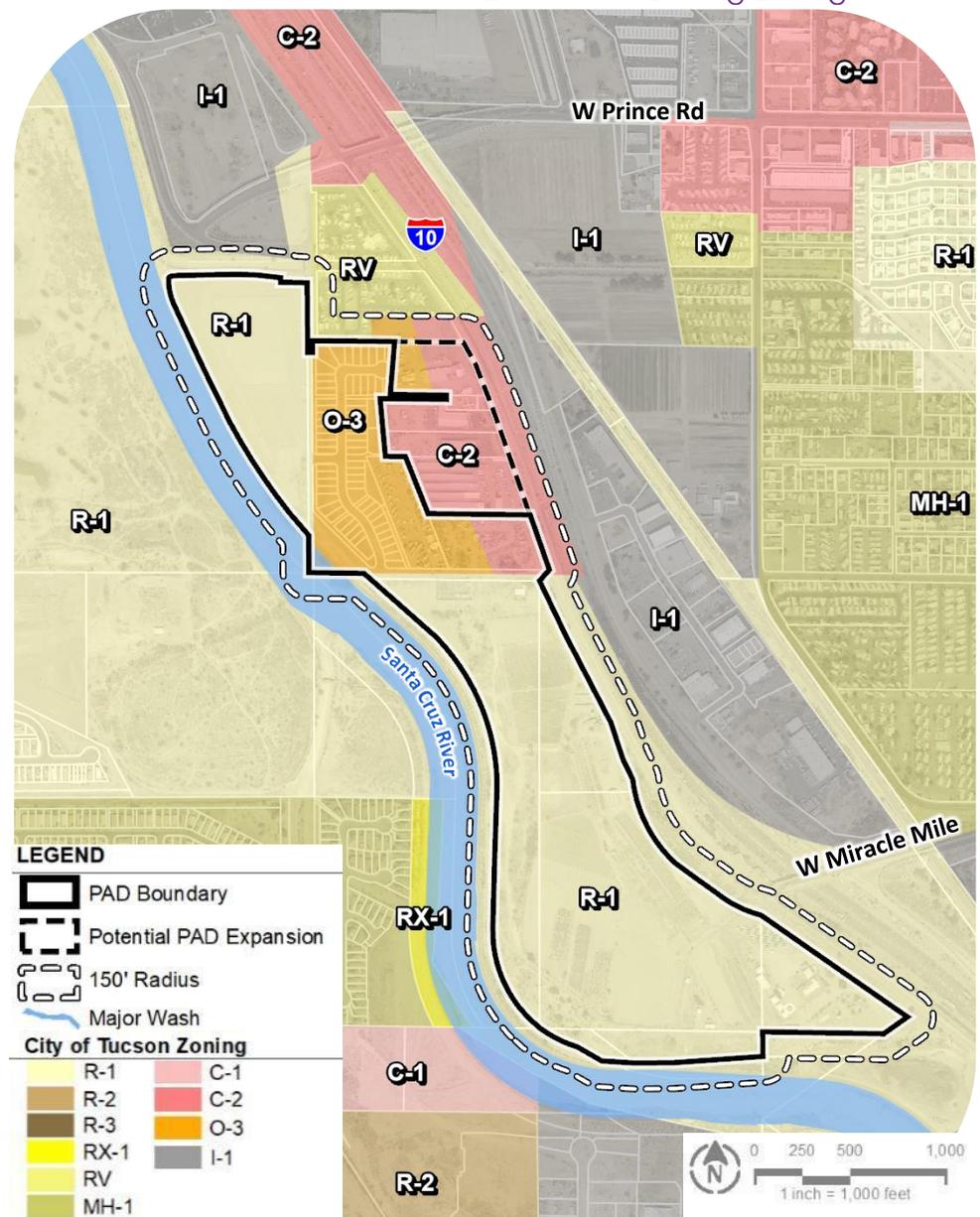
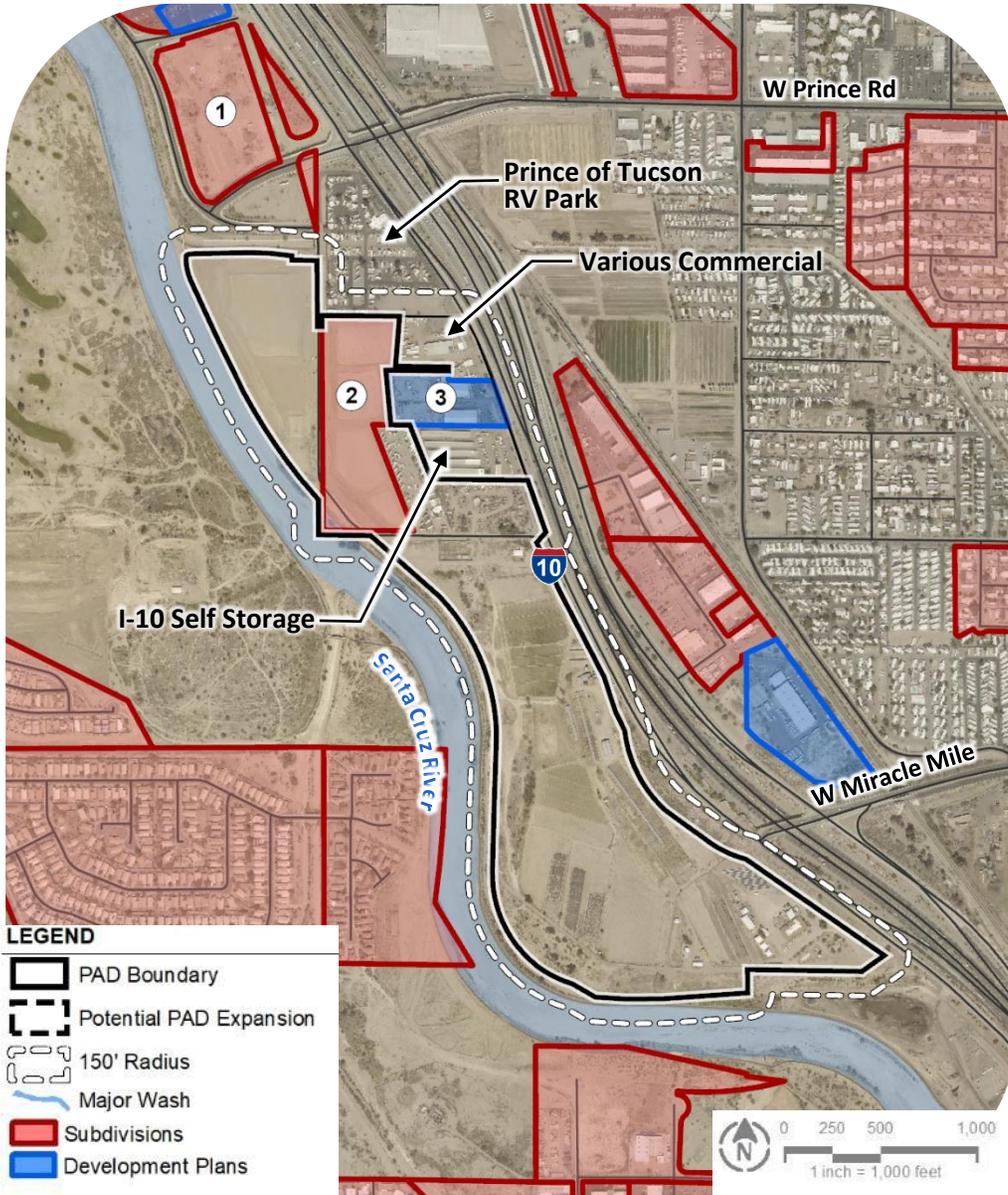


Exhibit V.D.1: Surrounding Development



D. Adjacent Parcels & Structures

Stretching over a mile from north to south, the PAD borders a variety of properties as shown in Exhibits V.D.1 and V.D.2. Uses to the east include ADOT right-of-way, RV parks, feed stores, storage facilities, and a tile and stone business. Existing land uses and approved development plans and subdivisions on adjacent parcels within 150 feet of the PAD boundary are listed below.

Table V.D: Existing Land Uses for Adjacent Parcels

Direction	Property Use
North	Vacant
South	Santa Cruz River, ADOT Construction Yard
East	Prince of Tucson RV Park, Mexican Tile & Stone Co., Vaquero Feed & Livestock Supply, Animal Health Express, Valley Oasis Pools & Spas and I-10 Self Storage
West	Santa Cruz River

Table V.D: Existing Development Plans & Subdivisions within 150 feet

Map ID	Name	Type
1	North Tucson Business Center Blocks 1 thru 4	Subdivision
2	La Cholla Landing (1-61) *	Subdivision
3	Tucson Development LLC	Development Plan

\*Located within the PAD boundaries. Subdivision plat is in the process of being abandoned per Sequence No. 20190300545, Page 5, Dated 1/30/2019



Exhibit V.D.2: Surrounding Development



Prince of Tucson RV Park



Commercial along Frontage Road



## E. Open Space, Recreational Facilities, Parks, & Trails

This section is omitted per the approved Modification of Submittal Requirements in Appendix 1.

## F. Public, Educational, Community, & Cultural Facilities

This section is omitted per the approved Modification of Submittal Requirements in Appendix 1.

## G. Existing Drainage

For the purpose of this hydrology discussion, PRINCE 10 PAD is separated into two phases. Phase 1 will consist of the parcels north of Fort Lowell Road. The development of Phase 1 has already commenced with the creation of graded super pads to accommodate future commercial uses. The pre-and post-construction drainage design for the super pads was approved by the City of Tucson and Pima County Flood Control District. This information is presented in the Drainage Report for PRINCE 10 Super Pad Grading, 3455 N. La Cholla Boulevard (JEF 2020). Hydrologic, hydraulic, and drainage information for Phase 1 can be referenced from this report.



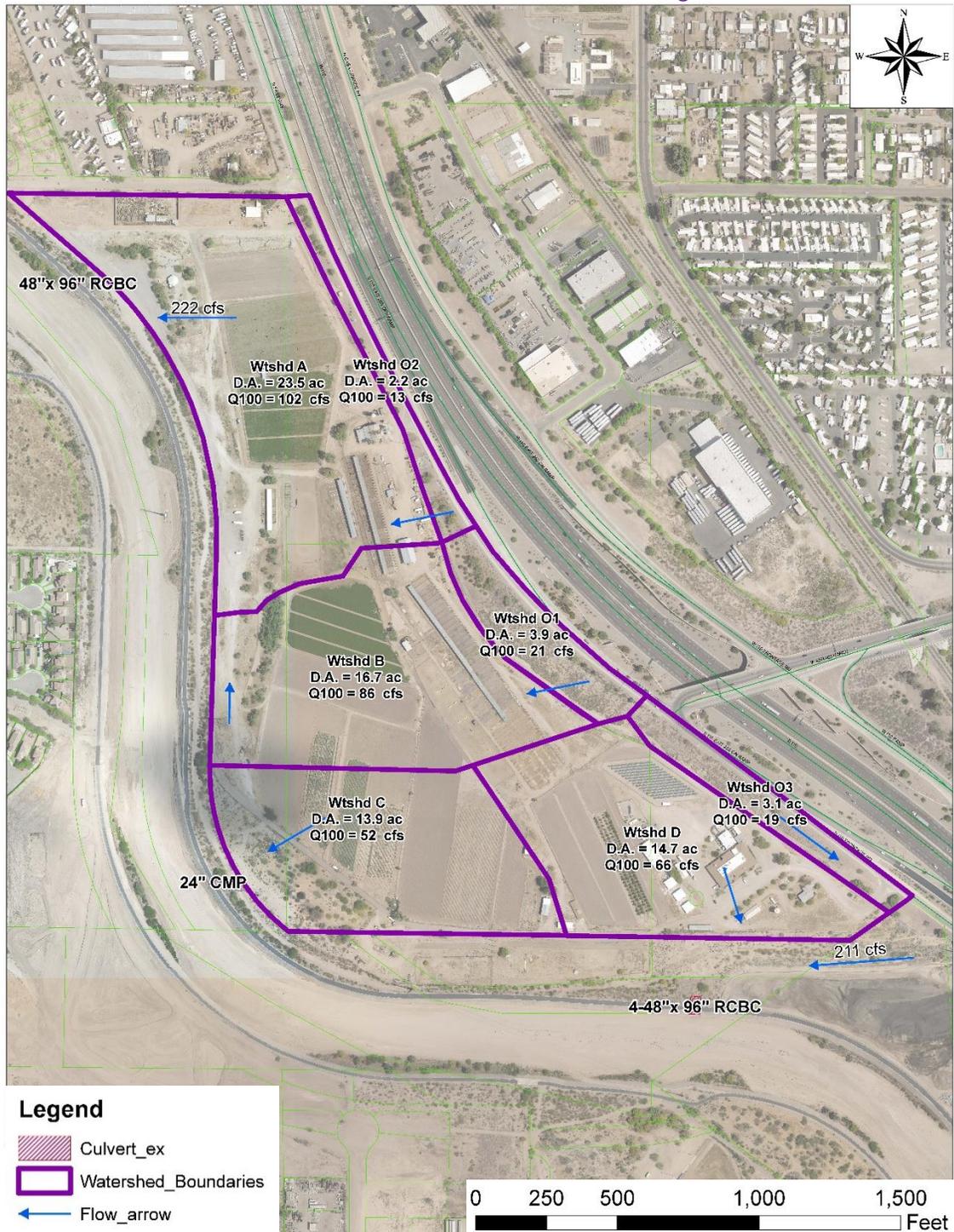
Phase 2 will consist of the parcels south of Fort Lowell Road and extending to the confluence of Krueger Wash and the Santa Cruz River. As a result of Interstate 10 and the associated frontage roads, Phase 2 is hydrologically isolated from contributing watersheds located to the east. Runoff generated by these off-site watersheds is captured in a stormdrain, which connects to a culvert that ultimately discharges runoff into Krueger Wash. Per the ADOT plans, the 50-year peak discharge into Krueger Wash is 128 cfs. Using the City of Tucson ratios, this extrapolates to an estimated 100-year peak discharge of 211 cfs. As a result of the interstate and the drainage infrastructure therein, only runoff generated by the right-of-way located west of the on-ramps impacts Phase 2 (Watersheds O1, Watershed O2 and Watershed O3). Runoff from Watershed O1 (Q100 = 21 cfs) and Watershed O2 (Q100 = 13 cfs) is combined with runoff generated by Phase 2. Runoff from Watershed O3 (Q100 = 19 cfs) is conveyed to the south and enters Krueger Wash.

In accounting for a potential future drainage design, Phase 2 was divided into four on-site watersheds (Watershed A – Watershed D). Watershed A, which comprises the northern third of Phase 2, generates 102 cfs under existing conditions. Watershed A also accepts runoff from Watershed O2 and, as such, the total runoff being conveyed towards the Santa Cruz River from these two watersheds is 115 cfs during the 100-year event. Under existing conditions, the runoff from this watershed is discharged into the Santa Cruz River via an existing 48" x 96" RCBC. Watershed B, located directly south of Watershed A, generates a 100-year peak discharge of 86 cfs under existing conditions. This watershed also accepts runoff from Watershed O2. The combined 100-year peak discharge (Q100 = 107 cfs) is partially emptied into the Santa Cruz River via a 24" CMP, with the majority being conveyed to the north to the aforementioned RCBC. Combining the runoff from Watershed A and Watershed B, Watershed O1 and Watershed O2, the peak 100-year discharge conveyed by the box culvert is estimated to be 222 cfs. Watershed C consists of a portion of the lower third of Phase 2. During the 100-year event, Watershed C generates 52 cfs. Runoff from this watershed is released into the Santa Cruz River via a 24" CMP. Because Phase 2 is currently used for agricultural activities and has been graded for this purpose, runoff crossing Phase 2 does so as shallow sheet flow. Watershed D consists of the southeast



corner of the site. Under existing conditions, this watershed generates a 100-year peak discharge of 66 cfs, which is conveyed to Krueger Wash, and discharges flow into the Santa Cruz via an existing four-cell 48" x 96" RCBC. As with the rest of Phase 2, the flow is conveyed as shallow sheet flow until it exits the site, at which point it is conveyed in a natural watercourse to Krueger Wash. The following map illustrates the delineated watersheds with the representative discharges and drainage areas.

Exhibit V.G: Existing Conditions Watershed



## H. Overlay Zones

PRINCE 10 PAD is not located within any City of Tucson overlay zones.

## I. Inventory of Existing Structures, Roads, & Other Development

### 1. Structures



The property consists of nine separate parcels and the approved La Cholla Landing subdivision. Most of the property's northern portion is undeveloped except for a 3.3-acre parcel north of Fort Lowell Road and a 4-acre parcel abutting the I-10 frontage road. The 3.3-acre parcel is the former home of the Tra-Tel RV Park, which contained approximately 66 RV spaces, a pool, clubhouse, and a manager's residence. The parcel abutting I-10 was once used as a construction storage yard. It has three one-story structures on it and is currently abandoned.

The University of Arizona's Veterinary Diagnostic Laboratory and associated facilities comprise the southern portion of PRINCE 10. Located in the southeast corner of the PAD, Arizona Veterinary Diagnostic Laboratory is the largest building on site. Various facilities and structures are spread throughout the remainder of



the land owned by the Arizona Board of Regents. Most of these buildings are single-story, used for storage, research, or housing livestock. Several silos surround a grain storage facility as pictured to the left near the beginning of the I-10 exit ramp for West Miracle Mile.



## 2. Roads

No vehicular access currently exists west of PRINCE 10. Interstate 10 and the southbound frontage road connect the PAD to the greater transportation network. Prince Road is the primary access to the property, with Miracle Mile providing potential future egress via the one-way southbound frontage road. The northern portion of the PAD is bisected by North La Cholla Boulevard. La Cholla Boulevard runs south through the property before curving east and turning into West Fort Lowell Road. Fort Lowell runs east before terminating at the southbound I-10 frontage road.

## 3. Development

La Cholla Landing, a sixty-four-lot subdivision, was recorded in January of 2007, but remains undeveloped. As previously mentioned, the subdivision plat is in the process of being abandoned per Sequence No. 20190300545. The former Tra-Tel RV park was developed with gravel access lanes and a pool, clubhouse, and manager's residential unit.

## J. Sewer & Wastewater

Existing public sewers on and adjacent to the site include:

- 72" (G-2001-087) public sewers running along the western bank of the Santa Cruz River before turning north and running within La Cholla Boulevard
- 8" (G-2001-087 & G-64-014) also within La Cholla Boulevard
- 8" (G-64-014) and 42" (G-2009-068) sewer lines running along the north PAD boundary east of La Cholla Boulevard

## K. Inventory of Existing Public Services

### 1. Police

The property is located within the Tucson Police Department's Operation Division West. The Westside Police Service Center is located approximately one-half mile to the east on West Miracle Mile and North Flowing Wells Road.



### 2. Fire

Tucson Fire Department Station #8 is located at 250 West King Road, near West Prince Road and North Oracle Road. The station is approximately three miles east of PRINCE 10.

### 3. Library

Pima County's Woods Memorial Library is the nearest public library. It is located approximately four miles to the east on 1st Avenue, south of Prince Road.



## L. Hydrology

Refer to PAD Section V.G: Existing Drainage.



## M. Topography and Slope

As shown in *Exhibit V.M: Existing Topography and Slope*, the property is relatively flat and generally slopes from the southeast to the northwest. The southeast corner of the property has an approximate elevation of 2,302 feet. The northwest corner of the property, at the intersection of Prince Road and Business Center Drive, has an approximate elevation of 2,274 feet. The entire property has been graded or otherwise disturbed by various activities over time. There are no naturally occurring slopes greater than 15% on-site. Slopes greater than 15% are minimal and are the result of human activity on the property. The ABOR parcels in the southern portion have been altered for agricultural research. The Miracle Mile interchange is situated approximately 20 feet higher in elevation than the southeast property corner. Drainage improvements and bank protection along the Santa Cruz River constitute the other sloping areas on the property. There are no rock outcroppings or other significant topographic features on the property.



The average cross slope of the parcel is 3.8%, as calculated by performing the following calculation:

$$\text{Average Cross Slope (ACS)} = \frac{I \times L \times 0.0023}{A}$$

$$\text{ACS} = \frac{2 \times 92,709 \times 0.0023}{113.3 \text{ acres}}$$

$$\text{Interval (I)} = 2'$$

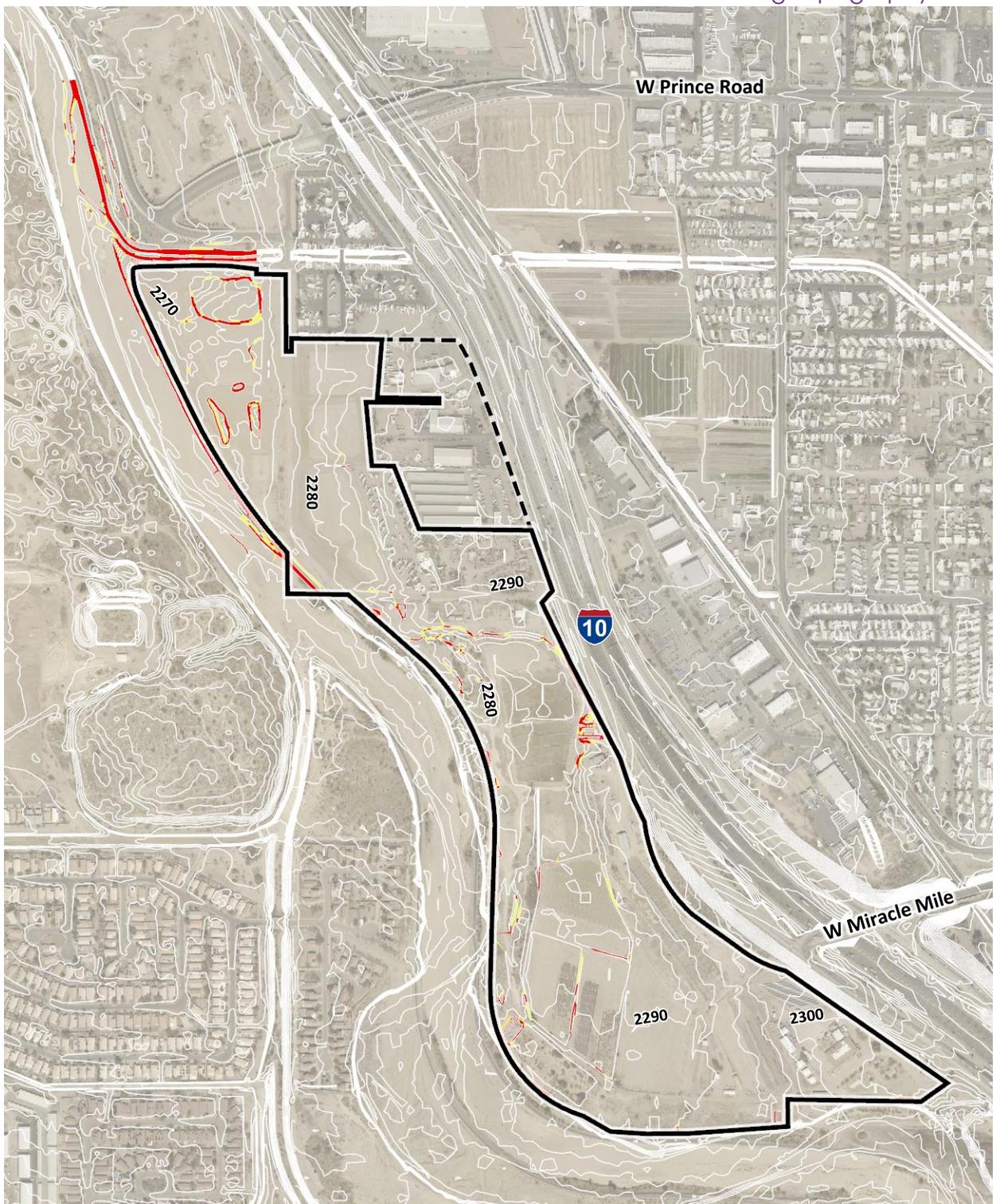
$$\text{Total length of contours (L)} = 92,709 \text{ feet}$$

$$\text{Area (A)} = 113.3 \text{ acres}$$

$$\text{ACS} = 3.8\%$$



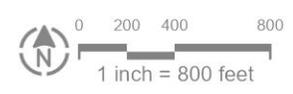
Exhibit V.M: Existing Topography and



LEGEND

- PAD Boundary
- Potential PAD Expansion
- 2-foot Contour
- Santa Cruz River

- Slope Category
- 15-25%
  - >25%



File Name: BLF-03  
 SOURCE: Pima County GIS, 2022



## N. Vegetation & Wildlife

This section is omitted per the approved Modification of Submittal Requirements in Appendix 1.

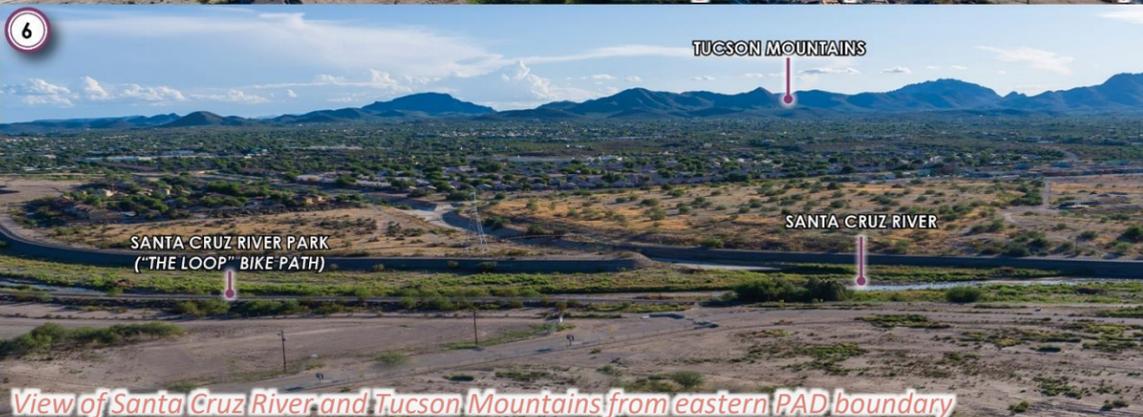
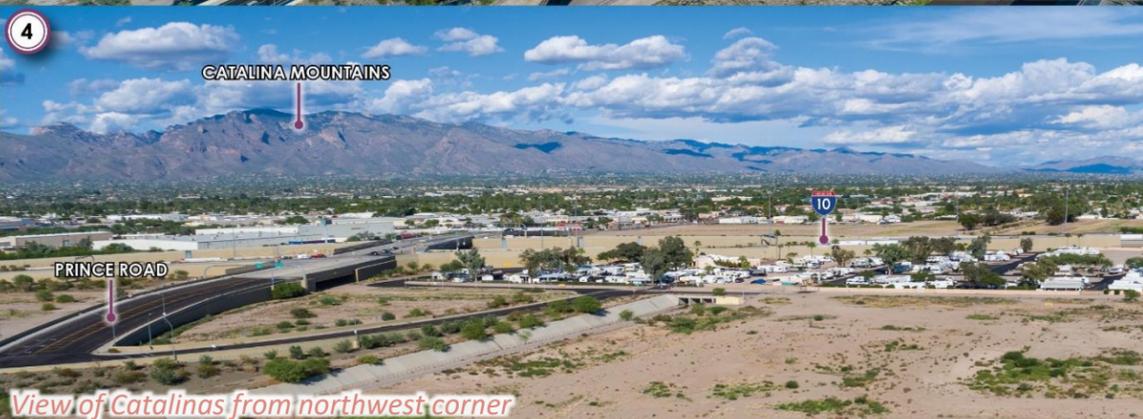
## O. Geology & Soils

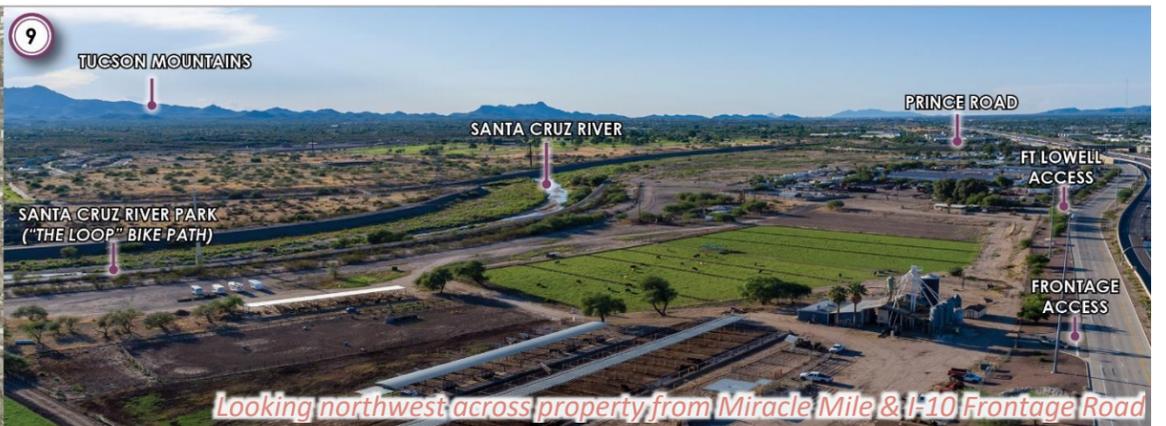
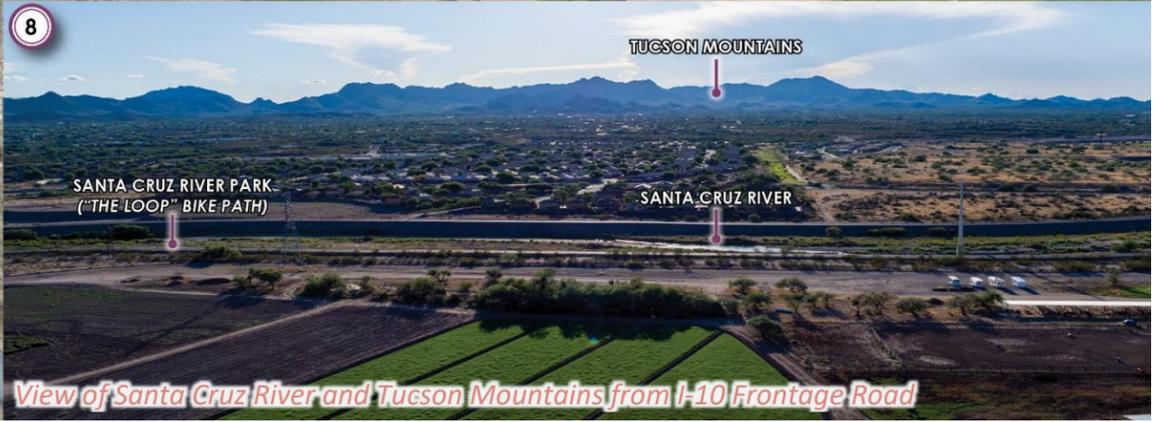
This section is omitted per the approved Modification of Submittal Requirements in Appendix 1.

## P. Viewsheds

Capturing viewsheds on such a large property required drone photography to fully realize the site in context with its surroundings. *Exhibit V.P: Viewsheds* identifies multiple aerial vantage points and significant surrounding features. As shown in the bird's eye view photos, PRINCE 10 is isolated from other uses except for those to the east along the frontage road.







## Q. Archaeological & Historical Sites

The entire PAD area has been archaeologically surveyed and/or assessed for the presence of buried resources. The only portion of the PAD that has not been archaeologically cleared is in the southern portion where a partially-excavated prehistoric village site (The Santa Cruz Bend site) is located.



### Northern Portion of PAD

Archaeological excavation was carried out in the northern end of the proposed PAD to investigate a prehistoric habitation area (AZ AA:12:85/AA:12:735[ASM]) that may have dated as early as 800 B.C. Testing and data recovery efforts indicate that the site is smaller than originally supposed. It is limited to areas east of La Cholla Boulevard. A small cluster of prehistoric features was found and excavated in the La Cholla Landing Subdivision in 2017 by Tierra Right of Way, effectively clearing that area, a recommendation that received concurrence from City of Tucson Historic Preservation Office staff. In 2021, archaeological testing conducted by SWCA indicated that the site does not extend into the property at the northwest corner of Fort Lowell Road and the I-10 frontage road. It is unknown whether any archaeological deposits extend east beyond the boundary of the PAD.

Archaeological testing was also conducted by SWCA just south of Fort Lowell Road within areas that had been thought to be part of AZ AA:12:85/AA:12:785 (ASM). No features or archaeological deposits associated with the site were identified in this area. However, a small segment of a prehistoric irrigation canal was encountered near the western terminus of Fort Lowell Road. This isolated feature is likely related to similar features that have been encountered outside of the PAD. The newly-discovered canal segment was fully documented, and no further archaeological work is warranted in the area.

All of the northern portion of the PAD has been archaeologically cleared.

### Central Portion of PAD

Archaeological testing carried out by SWCA in 2020 ruled out the presence of buried features in the central portion of the PAD, outside of known archaeological site boundaries. A previously recorded site along the I-10 Frontage Road (AZ AA:12:745 [ASM]) was thought to extend into this area, but archaeological test trenches indicate that deposits associated with the site extend no further than a natural terrace just east of the property line.

All portions of the central portion of the PAD have been archaeologically cleared.

### Southern Portion of the PAD

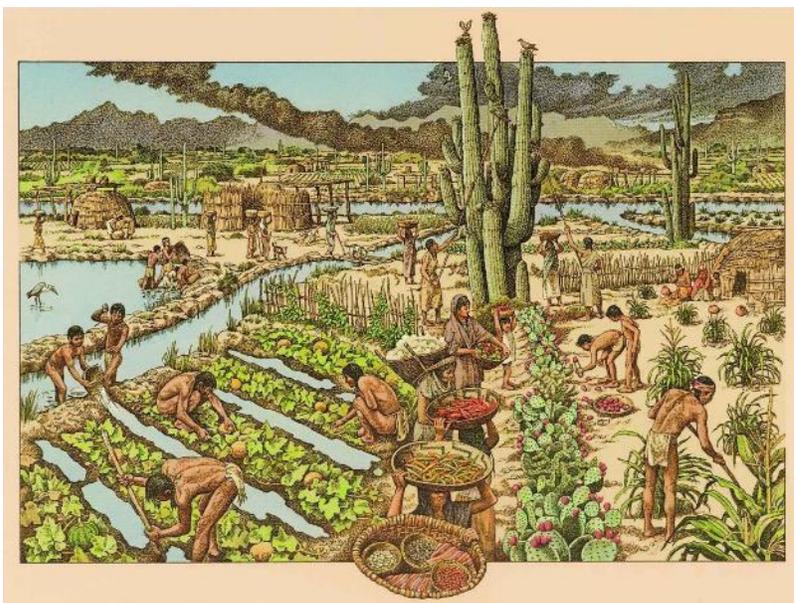
A large prehistoric farming settlement has been documented at the southern end of the PAD. AZ AA:12:746 (ASM), the Santa Cruz Bend site, was identified by Desert Archaeology during archaeological investigations preceding the widening of Interstate 10 in 1993. The Santa Cruz Bend site was first occupied during the Early Cienega Phase (800 – 400 B.C) of the Early Agricultural Period, but additional occupation took place as late as the Middle Rincon phase (A.D. 1000 – 1100) of the Hohokam Period. In the 1990s, intensive archaeological excavations took place within 1.2



hectares of the site in the eastbound frontage road area of the interstate and within a portion of the University of Arizona West Campus Agricultural Center.

In December 2020, SWCA placed several trenches in the central portion of the PAD, outside of known site boundaries, to determine whether the Santa Cruz Bend site extends further west or north. However, no cultural materials were encountered, indicating that the site is no larger than currently known.

An archaeological data recovery and monitoring plan is in preparation by SWCA for a portion of the Santa Cruz Bend site within the PAD. It will be reviewed by the Arizona State Museum prior to the issuance of an archaeological permit. Once the draft of the final report is prepared, 30 days shall be provided to review and finalize the report by the City of Tucson



*Artist's depiction of prehistoric irrigation canals*

Historic Preservation Office and the Arizona State Museum (ASM); the same review timeframe shall also apply to future work. This work will provide additional information about the extent and condition of the northern end of the site and will ensure that any archaeological materials encountered during utility installation around the margins of the southern-most parcel are properly documented.

Any remaining portions of the Santa Cruz Bend site not covered by previous and upcoming excavations will also require archaeological testing, data recovery and/or construction monitoring prior to development. Archaeological work must meet Arizona State Museum standards, and compliance with approved plans will be confirmed by City of Tucson Historic Preservation Office staff.



*Examples of prehistoric pottery (A.D. 850-A.D. 950)*

Source: Arizona State Museum



## References:

Aerial Photographs, Pictometry, 2020.

Balanced and Critical Basin Map, Pima County.

Plan Tucson, City of Tucson General & Sustainability Plan, ratified 2014.

City of Tucson GIS, TDOT Map Center, 2016.

City of Tucson Unified Development Code, 2016.

FEMA Flood Insurance Rate Map, Pima County, Arizona.

Institute of Transportation Engineers, Trip Generation Manuals, 7<sup>th</sup> Edition, Volumes 1 & 3, 2003.

MapGuide, Pima County Department of Transportation.

Stormwater Detention/Retention Manual, City of Tucson.

USGS Topographic Map, Jaynes Quadrangle.

<https://www.inmaricopa.com/hohokam-canals-maricopa/>

<https://statemuseum.arizona.edu/online-exhibit/culture-history-southern-arizona/hohokam>



# APPENDIX 1

# Modification of Submittal Requirements





October 18, 2019

Scott Clark  
 Director  
 City of Tucson Planning & Development Services Department (PDSD)  
 201 N. Stone Avenue  
 Tucson, AZ 85701

Subject: Modification of Application Submittal Requirements Request for PRINCE 10 – APNs: 103-08-001S, 106-11-024A, 106-11-0250, 107-01-011A, 107-02-001D, 107-02-002A, 107-02-0050, and final plat for La Cholla Landing Lots 1-61 & Common Areas A, B & C

Dear Mr. Clark:

PRINCE 10 is envisioned as a progressive, mixed-use, interstate commerce campus. The project site comprises approximately 125 acres and features 9 development areas with the potential to accommodate 10-story office buildings, or taller. Supplemental land uses may include multi-family housing, hospitality, and restaurants. The project site is ideally situated between Interstate 10 and the Santa Cruz River, approximately 3 miles from downtown Tucson. Pedestrian access and vehicular access are abundant. PRINCE 10 is next to approximately 1.25 miles of "The Loop" multi-use path along the Santa Cruz and features direct freeway access through two interchanges (Prince Road and Miracle Mile). Views of the adjacent mountains are spectacular, and the land is located within a designated Opportunity Zone (OZ), which offers potential tax savings for qualified investments, but only through 2026. Until then, if an Opportunity Zone Fund is established for any of the PRINCE 10 property, the fund then has 30 months to invest in improvements to that property in order to qualify for the program, which is why it is important for review times and submittal requirements to be streamlined to create a shovel-ready economic development opportunity for Tucson.

This letter is a request for modifications to the rezoning submittal requirements found in section 2-04.4.0 *Planned Area Development (PAD) Content Requirements of the Unified Development Code (UDC) Administrative Manual*. Specifically, this request includes exceptions to submittal requirements found in sections 4.2 *Site Analysis* and 4.3 *PAD Proposal*. The subject parcels are consistent with *Plan Tucson* and the *Santa Cruz Area Plan* and the development proposal is compatible with surrounding land uses. *Plan Tucson*, the City's General & Sustainability Plan adopted in 2013, designates this site as being in the Existing Neighborhoods Growth Scenario Building Block as well as partially within the Industrial Areas (adjacent to I-10) and Existing Parks/Open Space building blocks (adjacent to the Santa Cruz River). According to *Plan Tucson*, Existing Neighborhoods are,

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 o 520.623.6146  
 f 520.622.1950  
 w azplanningcenter.com



October 18, 2019  
 Mr. Scott Clark, City of Tucson PDSD Director  
 Modification to Submittal Requirements Request  
 Page 2  
 BFL-02

*“...primarily developed and largely built-out residential neighborhoods and commercial districts in which minimal new development and redevelopment is expected in the next several decades. The goal is to maintain the character of these neighborhoods while accommodating some new development and redevelopment and encouraging reinvestment and new services and amenities that contribute further to neighborhood stability.”*

and Industrial Areas are,

*“... strategically located for efficient handling of intermodal freight movements. These areas support national and international freight movement through Tucson by connecting existing major regional commercial transportation routes, including railway, major highways, and the airports”.*

PRINCE 10 will bring quality reinvestment and redevelopment to this area that also happens to be centrally located off Interstate 10 in a semi-industrial, largely vacant Opportunity Zone area. The goal of Opportunity Zones to see underserved areas benefit from sorely needed investment, is yet another project asset that must be maximized to spur timely new improvements along this stretch of interstate. Guidance from the Arizona Commerce Commission (ACC) stresses the importance of resolving any entitlement issues to facilitate the limited timeframe for Opportunity Zone developments allowed by the Treasury Department in accordance with the 2017 Tax Cuts and Jobs Act. Expediently rezoning and reviewing subsequent development plans/permits for the subject parcels to take advantage of the OZ incentives and allow this entire area to be marketed, designed and built as a cohesive project with common access and sensitivity to Interstate 10 as a scenic route and to neighboring uses will help make PRINCE 10 a successful model for economic development in Tucson.

Within the Santa Cruz Area Plan, the proposed PAD property crosses four Key Parcels: 12-15. Each key parcel calls for more intense private development in the form of park industrial and commercial uses with a mix of residential. The potential for Riverpark (aka The Loop) expansion is also encouraged, particularly on the land owned by the University of Arizona. The Loop offers a tremendous amenity to attract interest and improve quality of life in the area, and it will be showcased accordingly in the development of PRINCE 10. Per the Santa Cruz Area Plan, the land that comprises PRINCE 10:

*“...provides a potential for Riverpark expansion and more intense private development...” (Key Parcel 12, Existing Use)*

*(could have) “A mix of industrial and residential uses...integrated throughout most of the area. This would reflect existing zoning and approved rezoning patterns. Park industrial uses should be located close to the eastern portion of the area, and access*



October 18, 2019  
 Mr. Scott Clark, City of Tucson PDSD Director  
 Modification to Submittal Requirements Request  
 Page 3  
 BFL-02

*to these uses should be oriented toward... the Prince Road alignment to avoid impact on adjacent neighborhoods.” (Key Parcel 12, Proposed Use)*

*“...is most suitable for industrial or commercial facilities for motorists in conjunction with the Interstate 10 interchange. Development should be designed to provide an attractive approach to Tucson along the Interstate consistent with State designation of Interstate-10 as a scenic route. The University should be encouraged to dedicate a portion of the parcel or an easement for Riverpark purposes. Any subsequent sale and rezoning of the property should also require this consideration.” (Key Parcel 13 (UA experimental ag. facility), Proposed Use)*

*“...would be suitable for development as a business and industrial park.” (Key Parcel 14, Proposed Use)*

*“...development should be done in conjunction with the park industrial zoning in order to provide an attractive approach to Tucson along Interstate-10...” (Key Parcel 15, Proposed Use)*

PRINCE 10 is already underway with infrastructure improvement plans for water, sewer, electric and gas, as well as grading and drainage plans; the entire property has also been heavily disturbed by past, and in some cases current, use. Because of this, exceptions to the following from 4.2 *Site Analysis* and 4.3 *PAD Proposal* are requested:

#### 4.2 *Site Analysis*

- A. Significant natural and built constraints of the site and surroundings;
- E. Off-site open space, recreational facilities, parks, and trails within one mile of the PAD site *(although this section will not be included, The Loop will still be discussed extensively in the PAD document)*;
- F. Public, educational, community, and cultural facilities on site and within one mile off site;
- G. Existing drainage *(to include reference to approved drainage report only)*;
- H. PAD site affected by any overlay zone ordinances and the Major Streets and Routes (MS&R) Ordinance;
- J. Location and extent of existing provisions for sewage disposal, effluent use, stormwater drainage, and utilities *(to include reference to approved plans only)*;
- L. Hydrology and water resources *(to include reference to approved plans only)*;
- N. Vegetation and wildlife;



October 18, 2019  
 Mr. Scott Clark, City of Tucson PDSD Director  
 Modification to Submittal Requirements Request  
 Page 4  
 BFL-02

- O. Geology and soils;
- P. Viewsheds and visual analysis (*will substitute aerial drone imagery and massings*); and,
- Q. Paleontological and cultural (archaeological and historical) sites, structures, and districts (*letter from AZ State Museum only; rest deferred to development plan/permit stage*)

#### 4.3 PAD Proposal

- G. Standards for the conservation, development, or utilization of natural resources, including surface water, soils, vegetation, and wildlife;
- H. Where applicable, the methods of conservation for scenic natural and built features and viewsheds;
- L. A phasing schedule for the following, as applicable:
  1. The preservation of site features established by the PAD;
  2. The development of the PAD; and,
  3. The construction, dedication, and provision of public services.
- M. When financial assurances are required, a draft form of financial assurances to be recorded prior to ordinance adoption;
- P. Development design guidelines;
- Q. General landscape program;
- R. Drainage plan (*to include reference to approved drainage report only*);
- S. A traffic and transportation study which includes trip generation factors for various modes, estimated trips per day by land use, proposed vehicular access and circulation plan, and traffic impacts by mode on adjacent development (*deferred to development plan as individual parcels are developed*);
- T. Impacts on existing structures, roads, and other development;
- U. Impacts on existing infrastructure and public services; and,
- V. Location and extent of proposed provisions for sewage disposal, effluent use, stormwater drainage, and utilities.

Potential development of an attractive mixed-use, interstate commerce campus is envisioned for this area in accordance with *Plan Tucson* and the Santa Cruz Area Plan. With a concerted effort between the developer and the city to submit high-quality work and minimize review times, the development potential of this property can be maximized, and it can be designed and built quickly



October 18, 2019  
Mr. Scott Clark, City of Tucson PDSD Director  
Modification to Submittal Requirements Request  
Page 5  
BFL-02

to help spur additional business interest along the I-10 corridor and other parts of Tucson. And given the limited timeframe for investors to benefit from the Opportunity Zone program, it is appropriate to consider streamlined PAD, development plan and permitting review processes to achieve highest and best use of these 125 acres by the most effective and time-efficient means possible. To that end, we appreciate your consideration of this modification of submittal requirements request and look forward to working with PDSD, the City Manager and Economic Initiatives offices, and elected and appointed officials on a successful PAD rezoning that paves the way for new industry to come to Tucson.

Sincerely,  
THE PLANNING CENTER



Brian Underwood  
Director of Planning

cc: Michael Ortega, City Manager  
Barbra Coffee  
John Beall  
Garry Brav  
David Larson

