

6TH/5TH STREET
ROAD DIET TRAFFIC ANALYSIS
CAMPBELL AVENUE TO WILMOT ROAD

December 19, 2022



Inside front cover

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6th/5th Street Road Diet Traffic Analysis Campbell Avenue to Wilmot Road

Prepared for:



City of Tucson Department of Transportation & Mobility
201 N. Stone Ave
Tucson, AZ 85701

Prepared by:



Kittelsson & Associates, Inc.
2 East Congress Street, Suite 705
Tucson, AZ 85701
520.544.4067

Project Manager:
Bridger Helm
Engineering Associate

Project Principal:
Felipe Ladrón de Guevara
Associate Engineer

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Section 1 Introduction

INTRODUCTION

Kittelson & Associates, Inc. (Kittelson) has been retained by the City of Tucson to evaluate the traffic impacts of a road diet alternative on 6TH/5TH Street between Campbell Avenue and Wilmot Road. This report summarizes the results of traffic simulation conducted for 6TH/5TH Street Road Diet Assessment.

The City of Tucson Department of Transportation and Mobility is conducting a planning study for the 6th/5th Street corridor from Campbell Avenue to Wilmot Road. This corridor is being considered for street improvements and construction of new pedestrian and bicycle facilities. The project is funded through Proposition 101 – Tucson Delivers Better Streets¹ and Proposition 407 – Tucson Delivers Strong Connections². The current plan for the corridor includes two options, Option A (4-lane cross-section) and Option B (3-lane cross-section). Option A maintains the existing roadway lane configuration along the corridor. The roadway cross-section for Option A is shown in Figure 1³. Option B proposes a lane redesign (road diet) from four lanes, two vehicle travel lanes in either direction, to 3 lanes, one vehicle lane in either direction with a center two-way left turn lane (TWLTL). The lane redesign would also include a buffered bike lane in both directions. The roadway cross-section for Option B is shown in Figure 2³.



Figure 1. Option A Proposed Cross-Section



Figure 2. Option B Proposed Cross-Section

SCOPE OF REPORT

The 6th/5th Street Road Diet Traffic Analysis is intended to evaluate the viability from the traffic operations perspective of implementing a road diet on 6th Street from Campbell Avenue to Country Club Road and 5th Street from Country Club Road to Wilmot Road.

PROJECT LIMITS

The project is located in central Tucson, Arizona, abutted by the University of Arizona to the west and residential neighborhoods to the north, south and east. As shown in Figure 3, the project limits include 6th Street from Campbell Avenue to Country Club Road and 5th Street from Country Club Road to Wilmot Road. The study analyzed the following fifteen intersections along the corridor (listed west to east):

6TH Street Corridor:

1. Campbell Avenue (Signalized)
2. Wilson Avenue (HAWK⁴)
3. Tucson Boulevard (Signalized)
4. Country Club Road (Signalized)

¹ City of Tucson, "Proposition 101," adopted May 16, 2017

² City of Tucson, "Proposition 407," adopted November 6, 2018

³ City of Tucson. "5th Street and 6th Street Complete Streets." Tucson Delivers, <https://tucsondelivers.tucsonaz.gov/pages/5th6th>.

⁴ High-Intensity Activated Crosswalk Beacon

5TH Street Corridor:

5. Dodge Boulevard (Signalized)
6. Alvernon Way (Signalized)
7. Irving Circle (HAWK)
8. Columbus Boulevard (Signalized)
9. Swan Road (Signalized)
10. Arcadia Avenue (Bike HAWK)
11. Rosemont Boulevard (Signalized)
12. Craycroft Road (Signalized)
13. Chantilly Avenue (HAWK)
14. Sahuara Avenue (TWSC)
15. Wilmot Road (Signalized)



Figure 3

Project Vicinity Map
Tucson, AZ

- Study Intersections



ANALYSIS SCENARIOS

The following analysis scenarios were included in the report. Each combination of traffic count, study year, and time period was considered, resulting in a total of 8 scenarios.:

- Traffic Counts:
 - Scenario A – PAG Pre-COVID Counts (2017-2019)
 - Scenario B – Collected Post-COVID Counts (2022)
- Study Years:
 - Existing Traffic Conditions (Year 2022)
 - Future Year Traffic Conditions (Year 2045)
- Time Periods:
 - Weekday AM Peak Hour (7-9 AM)
 - Weekday PM Peak Hour (4-6 PM)

PERFORMANCE MEASURES

The performance measures utilized in this analysis to evaluate effects of the 6th/5th Street Road Diet project on traffic operations are:

- Intersection level of service (LOS)
- Corridor Travel Time and LOS
- Average and Maximum vehicular queue length
- Side street delay
- Total vehicles denied entry (i.e., traffic rerouted to adjacent streets)
- FHWA Road Diet Evaluation Criteria

Each performance measure is described in more detail below.

Level of Service

“Level of service” (LOS) describes the operating conditions experienced by users of a facility. Within the scope of this analysis, LOS is a quantitative stratification of a performance measure or measures representing quality of service for vehicular travel. The measures used to determine LOS for transportation system elements are called service measures. The Highway Capacity Manual (HCM) defines six levels of service for vehicular travel, ranging from A to F, for each service measure or combination of service measures. The service measures to define the LOS of intersections are control delay and volume-to-capacity (V/C) ratio. Control delay alone is used to characterize LOS for the entire intersection or an approach. Kittelson used Synchro and Simtraffic software’s to calculate control delay and intersection LOS.

Table 1 presents the relationship of control delay to level of service for signalized intersections.

Table 1. Signalized Intersection Level of Service Definitions⁵

Control Delay Per Vehicle (Seconds)	LOS	Description of Traffic Conditions
≤10.0	A	This level is typically assigned when the volume-to-capacity ratio is low and either progression is exceptionally favorable, or the cycle length is very short.
>10.0 and ≤20.0	B	This level is typically assigned when the volume-to-capacity ratio is low and either progression is highly favorable, or the cycle length is short. More vehicles stop than with LOS A.
>20.0 and ≤35.0	C	This level is typically assigned when progression is favorable, or the cycle length is moderate. Individual cycle failures (i.e., one or more queued vehicles are not able to depart as a result of insufficient capacity during the cycle) may begin to appear at this level. The number of vehicles stopping is significant, although many vehicles still pass through the intersection without stopping.
>35.0 and ≤55.0	D	This level is typically assigned when the volume-to-capacity ratio is high and either progression is ineffective, or the cycle length is long. Many vehicles stop and individual cycle failures are noticeable.
>55.0 and ≤80.0	E	This level is typically assigned when the volume-to-capacity ratio is high, progression is unfavorable, and the cycle length is long. Individual cycle failures are frequent.
>80.0	F	This level is typically assigned when the volume-to-capacity ratio is higher than one, progression is very bad, and cycle length is long. Most cycles are not able to clear the queue.

Corridor Travel Time

Kittelton used SimTraffic software to perform ten simulations of the AM and PM peak hour network operations. Each simulation recorded the travel time for the corridor segments. The average travel time across all ten simulations was used in this analysis as the “corridor travel time”.

Corridor travel time was calculated for both the eastbound and westbound directions for three roadway segments, Campbell Avenue to Country Club Road, Country Club Road to Alvernon Way, and Alvernon Way to Wilmot Road. The corresponding LOS for each segment was determined based on the travel times for each scenario.

Queue Lengths

Kittelton used SimTraffic software to perform ten simulations of the AM and PM peak hour network operations. Each simulation recorded maximum and average queue lengths at the study intersections for each lane group. The maximum queue is the largest queue observed across all simulations– it represents the longest queue that may be experienced by users of the corridor during the peak hour under a specific scenario. The average queue is the average of all queue lengths observed in the simulations. Average queue lengths are representative of a typical queue that may be observed during any specific cycle during the peak hour. Both the maximum and average queue lengths are utilized to determine the adequacy of existing vehicle storage approaching the intersection.

⁵ Transportation Research Board, Highway Capacity Manual 6th Edition, Washington, D.C., 2016.

Side Street Delay

The side street delay metric represents the projected intersection delay that would be experienced by vehicles making a left turn movement onto 6th/5th Street. Kittelson used Simtraffic software to perform ten simulations in which the average two-way stop control delay was calculated. To model a typical side street along the corridor Kittelson included 50 vehicles making both a northbound and southbound left onto 6th/5th Street. These volumes are consistent with traffic levels observed during data collection. This performance measure shows the possible impacts to unsignalized cross streets and driveways along the corridor.

Total Denied Entry/Rerouted Traffic

The total denied entry or rerouted traffic metric represents the amount of vehicular demand that exceeds the capacity of the corridor or study intersection. These trips would reroute to parallel corridors. During the peak hour, the vehicles denied entry are trips that would utilize the corridor under no-build conditions that would not be able to enter under the road diet conditions and thus would find an alternative route. This performance measure illustrates the changes in capacity across several scenarios.

FHWA Road Diet Feasibility Evaluation Criteria

To determine the feasibility of a road diet Kittelson used the Federal Highway Association (FHWA) *Road Diet Informational Guide*⁶. Within Section 3.6 of that guide, the City of Seattle Modeling Flow Chart for Road Diet Feasibility Determination is provided. The flowchart uses Average Daily Traffic (ADT), Vehicles per hour per direction (Vphpd), left-turning vehicles per hour (VphLT), Level of Service and Travel Time to determine the feasibility of a road diet. The flow chart is shown in **Error! Reference source not found.**

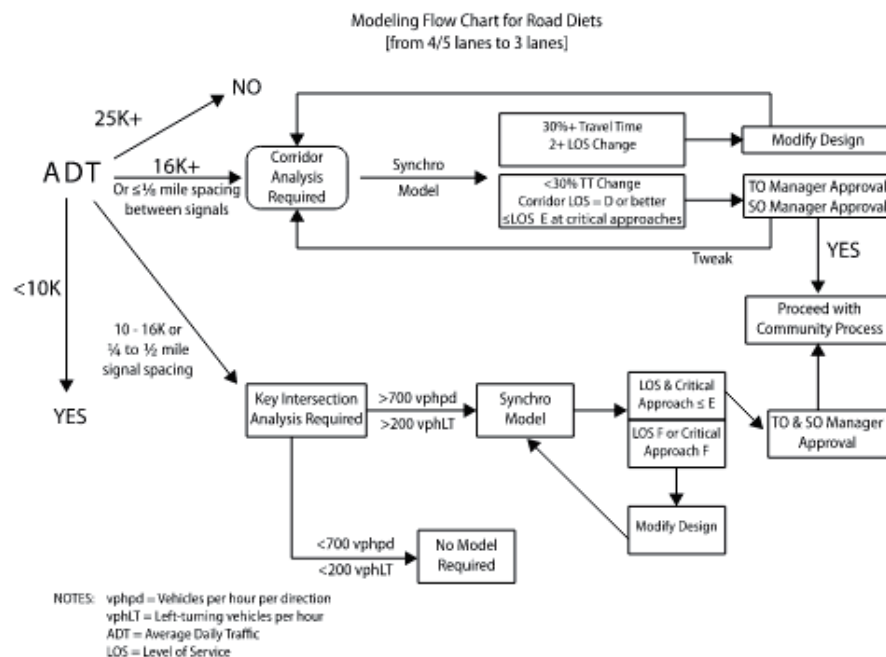


Figure 4. City of Seattle Modeling Flow Chart for Road Diet Feasibility Determination

⁶ Knapp, Keith K., et al. Road Diet Informational Guide. United States Department of Transportation, Federal Highway Administration, 2014.



Section 2 Data Collection

DATA COLLECTION

The existing roadway network within the project limits includes the following roadway types, lane configurations, and traffic controls.

STUDY CORRIDOR

6TH Street is a one-mile, two-way urban minor arterial between Campbell Avenue and Country Club Road. The undivided roadway consists of two eastbound lanes and two westbound lanes within the project limits. There are three signalized intersections along 6TH Street at Campbell Avenue, Tucson Boulevard, and Country Club Road, and nine two-way stop-controlled intersections that provide business and residential access to neighboring communities. The signal timing on 6TH Street is coordinated along the 6TH Street corridor. Left turn lanes are present at each of the signalized intersections along 6TH Street. The 6TH Street approach to Country Club Road has an exclusive right-turn lane. The speed limit along 6TH Street is 30 MPH.



Figure 5. Existing 6th Street Cross Section

5TH Street is a four-mile, two-way urban minor arterial between Country Club Road and Wilmot Road. The undivided roadway consists of two eastbound lanes and two westbound lanes from Country Club Road to Wilmot Road. 5TH Street has no on-street parking from Country Club Road to Wilmot Road. There are eight signalized intersections along 5TH Street at Country Club Road, Dodge Boulevard, Alvernon Way, Columbus Boulevard, Swan Road, Rosemont Boulevard, Craycroft Road, and Wilmot Road, and several two-way stop-controlled intersections that provide business and residential access to neighboring communities. Left turn lanes are present at each of the signalized intersections along 5TH Street. The speed limit along 5TH Street is 30 MPH.



Figure 6. Existing 5th Street Cross Section

CROSS STREETS

Campbell Avenue is a northbound/southbound two-way street at the west end of the corridor along 6TH Street. Campbell Avenue consists of two through lanes with single left turn lanes on the west and east approach of 6TH Street, and three through lanes with single left turn lanes on the north and south approaches. The intersection also features single right-turn lanes on the west and north approaches. The speed limit along Campbell Avenue is 35 MPH and the roadway is classified as an urban principal arterial.



Figure 7. Existing 6th Street / Campbell Avenue Intersection

Wilson Avenue is a northbound/southbound two-way street between Campbell Avenue and Tucson Boulevard along 6th Street. The unsignalized intersection with 6th Street features a two-way stop control at the north and south approaches. The north and south approaches do not include designated lanes onto 6th Street. The speed limit along Wilson Avenue is 25 MPH and the roadway is classified as an urban local street. A High Intensity Activated Crosswalk (HAWK) is featured on the east to cross along 6th Street.

Tucson Boulevard is a northbound/southbound two-way street between Campbell Avenue and Country Club Road along 6th Street. Tucson Boulevard consists of one through lane in each direction with a continuous two-way left turn lane that converts to single left turn lanes at the intersection with 6th Street. The speed limit along Tucson Boulevard is 30 MPH and the roadway is classified as an urban collector.

Country Club Road is a northbound/southbound two-way street between Tucson Boulevard and Dodge Boulevard. Country Club Road intersects the study roadway at an angle, with 6th Street on the west approach and 5th Street on the east approach. Country Club Road consists of two through lanes and single left turn lanes along each approach. The intersection also features a single right-turn lane at the north approach and single, channelized right-turn lanes at the east and west approaches. The speed limit along Country Club Road is 35 MPH and the roadway is classified as an urban minor arterial.



Figure 8. Existing 6th/5th Street / Country Club Road Intersection

Dodge Boulevard is a northbound/southbound two-way street between Country Club Road and Alvernon Way along 5TH Street. Dodge Boulevard is the south approach of a T-intersection with 5TH Street and includes single left-turn and right-turn lanes. The east approach includes two through lanes with a single left-turn lane and the west approach includes two through lanes. The speed limit along Dodge Boulevard is 25 MPH and the roadway is classified as an urban minor collector.

Alvernon Way is a northbound/southbound two-way street between Dodge Boulevard and Columbus Boulevard along 5TH Street. Alvernon Way consists of two through lanes with single left turn lanes at each approach to 5th Street. The intersection includes raised medians at each approach. The speed limit along Alvernon Way is 35 MPH and the roadway is classified as an urban principal arterial.

Irving Circle is a northbound/southbound two-way street between Alvernon Way and Columbus Boulevard along 5TH Street. The unsignalized intersection features a two-way stop control at the north and south approaches. The north and south approaches do not include designated lanes onto 5TH Street. The speed limit along Irving Circle is 25 MPH and the roadway is classified as an urban local street. A HAWK crossing is featured on the west to cross along 5TH Street.



Figure 9. Irving Circle HAWK Crossing on 5th Street

Columbus Boulevard is a northbound/southbound two-way street between Alvernon Way and Swan Road along 5th Street. Columbus Boulevard consists of two through lanes and single left turn lanes at the east and west approaches to 5th Street and one through lanes and single left-turn lanes at the north and south approaches. The speed limit along Columbus Boulevard is 35 MPH and the roadway is classified as an urban collector.

Swan Road is a northbound/southbound two-way street between Columbus Boulevard and Rosemont Boulevard along 5th Street. Swan Road consists of two through lanes and single left-turn lanes along each approach to 5th Street, with single right-turn lanes at the north and south approach. The north and south approaches feature raised medians at the intersection. The speed limit at Swan Road is 40 MPH and the roadway is classified as an urban minor arterial.

Arcadia Avenue is a northbound/southbound two-way street between Swan Road and Rosemont Boulevard along 5th Street. The unsignalized intersection features a two-way stop control at the north and south approaches. The north approach does not include designated lanes onto 5th Street, and the south approach includes a raised-median that separated the receiving lane and right-turn lane. The speed limit along Arcadia Avenue is 25 MPH and the roadway is classified as an urban local street. A Bike HAWK crossing is featured on the west to cross along 5th Street.

Rosemont Boulevard is a northbound/southbound two-way street between Swan Road and Craycroft Road along 5th Street. In the project limits, Rosemont Boulevard consists of two through lanes and single left-turn lanes at the east and west approaches with single through and left-turn lanes at the north and south approaches. The speed limit along Rosemont Boulevard is 25 MPH and the roadway is classified as an urban collector.

Craycroft Road is a northbound/southbound two-way street between Rosemont Boulevard and Wilmot Road along 5th Street. Craycroft Road consists of two through lanes and single left-turn lanes at each approach to 5th Street; single right-turn lanes are present at the north and south approaches. Each approach features a raised median. The speed limit along Craycroft Road is 40 MPH, and the roadway is classified as an urban minor arterial.

Chantilly Drive is a northbound/southbound two-way street between Craycroft Road and Sahuara Avenue along 5th Street. The T-unsignalized intersection features a two-way stop control at the south approach. The south approach does not include designated lanes onto 5th Street. The speed limit along Chantilly Drive is 25 MPH and the roadway is classified as an urban local street. A HAWK crossing is featured on the east approach to cross 5th Street.

Sahuara Avenue is a northbound/southbound two-way street between Craycroft Road and Wilmot Road along 5TH Street. The unsignalized intersection features a two-way stop control at the north and south approaches. The north and south approaches do not include designated lanes onto 5TH Street. The speed limit along Sahuara Avenue is 25 MPH and the roadway is classified as an urban local street.

Wilmot Road is a northbound/southbound two-way street at the east end of the corridor along 5TH Street. Wilmot Road consists of three through lanes with single, dedicated left-turn and right-turn lanes at both the north and south approaches to 5th Street. The west approach (5th Street) consists of one left-turn lane, one all-way-turn lane, and one right-turn lane; the east approach is the driveway to the St. Michael & All Angels Episcopal Church & School and features one all-way-turn lane. The north approach also features a buffered single right-turn lane. The speed limit along Wilmot Road is 40 MPH, and the roadway is classified as an urban minor arterial.



Figure 10. Existing 5th Street / Wilmot Road Intersection

PEDESTRIAN FACILITIES

Sidewalks are present along most of the 6TH / 5TH Street corridor in the project vicinity. However, a notable sidewalk gap exists on both sides of 5th Street between Country Club Road and Alvernon Way. Sidewalk gaps also exist on several commercial parking lot frontages – most of these locations do not provide a designated pedestrian path but are paved with asphalt.

Sidewalks vary in width from 4-ft to 6-ft along the corridor. Sidewalks are present at the signalized intersection approaches and sidewalks are included along a portion of the HAWK and Bike HAWK approaches. Crosswalks are present on all legs at all study intersections except the north leg of Sahuara Avenue. All signalized intersection crosswalks have pedestrian signal heads and curb ramps.

The following intersection feature HAWK crossings. Figure 11 shows the Wilson Avenue HAWK crossing.

- 6TH Street / Wilson Avenue
- 5TH Street / Irving Circle
- 5TH Street / Arcadia Avenue
- 5TH Street / Chantilly Drive



Figure 11. 6th Street / Wilson Avenue HAWK Crossing

BICYCLE FACILITIES

There are no bicycle facilities, such as a bike lane, in the project limits along the 6TH / 5TH Street corridor. The HAWK crossing at Arcadia Avenue features a bike crossing signal (BikeHAWK).

TRANSIT SERVICES

SunTran is a public transportation service provider providing services within Pima County, AZ. SunTran Route 3 services the immediate project vicinity. The route runs east-west along the entirety of the corridor. SunTran Route 3 runs from Pima Community College (PCC) West to PCC East starting at 5:00 AM and running until 12:00 AM on weekdays. The route has scheduled headways of 30-minutes from 5:00 AM to 6:00 PM and scheduled headways of 1-hour from 6:00 PM to 12:00 AM. Figure 12 shows the current bus stop locations for Route 3 within the project limits.



Figure 12. SunTran Route 3

There are bus pullouts along the 6TH / 5TH Street corridor at the following bus stop locations:

- Eastbound:
 - Campbell Avenue (Far Side)
 - Alvernon Way (Far Side)
 - Arcadia Avenue (Near Side)

- Westbound
 - Wilmot Road (Far Side)
 - Alvernon Way (Far Side)
 - Campbell Avenue (Far Side)

SOLID WASTE SERVICES

The City of Tucson waste management collects trash along the corridor. Trash collection is conducted on Mondays for areas along the corridor from Campbell Avenue to Craycroft Road. From Craycroft Road to Wilmot Road, trash is collected on Thursdays. An alleyway is located behind houses abutting 6th Street from Campbell Avenue to Country Club Road in which waste management collects trash, with no services on 6th Street along this portion of the corridor. Additionally, along 5th Street alleyways that are used for trash collection exist from Guapo Way to Dodge Blvd and Rosemont Blvd to Wilmot Road. The segments of the corridor not listed above (5th Street from Country Club Road to Guapo Way and Dodge Blvd to Rosemont Blvd) require trash collection along the 5th Street corridor for properties facing 5th Street.

MAIL SERVICES

The US Postal Service conducts mail services along the corridor. Carrier route C018 serves the corridor from Campbell Avenue to Country Club Road, C023 serves from Country Club Road to Rosemont Boulevard and C022 serves from Rosemont Boulevard to Wilmot Road. The mail services currently utilize 6th/5th Street as the primary access point for deliveries along the corridor.

EXISTING SIGNAL TIMING

City of Tucson staff provided existing signal timing plans for the signalized intersection along the corridor via a Synchro file. During both the AM and PM peak periods most of the signalized intersections operate with a 108 second cycle length except for 5th Street/Columbus Boulevard and 5th Street/Rosemont Boulevard, which operate with 54-second cycle lengths. All of the HAWKs along the corridor run free. Table 2 provides the existing intersections cycle lengths and left turn phasings. The intersection of 5th Street/Wilmot Road is the only intersection with a right turn overlap (eastbound right turn).

Table 2. 6th/5th Street Signal Timing

No.	Intersection	Control	Coordination	Cycle Length	Left Turn Phasing	
1	Campbell Ave	Traffic Signal	East-West	108	NBL/SBL	Permitted-Protected
					EBL/WBL	Permitted-Protected
2	Wilson Ave	HAWK	East-West	Free	-	-
3	Tucson Blvd	Traffic Signal	East-West	108	NBL/SBL	Permitted
					EBL/WBL	Permitted
4	Country Club Rd	Traffic Signal	East-West	108	NBL/SBL	Permitted-Protected
					EBL/WBL	Permitted-Protected
5	Dodge Blvd	Traffic Signal	East-West	108	NBL	Protected
					WBL	Permitted-Protected
6	Alvernon Way	Traffic Signal	North-South	108	NBL/SBL	Permitted-Protected
					EBL/WBL	Permitted-Protected
7	Irving Circle	HAWK	East-West	Free	-	-
8	Columbus Blvd	Traffic Signal	East-West	54	NBL/SBL	Permitted
					EBL/WBL	Permitted
9	Swan Rd	Traffic Signal	North-South	108	NBL/SBL	Permitted-Protected
					EBL/WBL	Permitted-Protected
10	Arcadia Ave	Bike HAWK	East-West	Free	-	-
11	Rosemont Blvd	Traffic Signal	East-West	54	NBL/SBL	Permitted
					EBL/WBL	Permitted
12	Craycroft Rd	Traffic Signal	North-South	108	NBL/SBL	Permitted-Protected
					EBL/WBL	Permitted-Protected
13	Chantilly Ave	HAWK	East-West	Free	-	-
14	Suhara Ave	TWSC	-	-	-	-
15	Wilmot Rd	Traffic Signal	North-South	108	NBL/SBL	Permitted-Protected
					EBL/WBL	Permitted

FLOODPLAIN DATA

Various floodplains and washes are located within the project limits. A description of each FEMA flood hazard zone and location along the corridor is as follows:

- Zone AE – area that presents a one percent-annual-chance of flooding
 - Between Arcadia Avenue and Rosemont Boulevard

City of Tucson washes lay within the project limits include the following:

- High School Wash (6th Street between Norton Ave and Country Club Rd)
- Christmas Wash (5th Street between Camino Miramonte and Palo Verde Ave)
- Alvernon Wash (crosses 5th Street)
- Midway Wash (crosses 5th Street)
- Arcadia Wash (crosses 5th Street)
- Van Buren Wash (crosses 5th Street)
- Sahuara Wash (crosses 5th Street)
- Alamo Wash (crosses 5th Street)

A map showing the locations and extents of each floodplain area and wash is provided in *Appendix A*.

TRAFFIC VOLUME DATA

Two sources of traffic volume data were used in this analysis. As of 2022, although most of the major and obvious impacts that the COVID-19 pandemic has had on traffic patterns have lessened, there is still uncertainty regarding lingering effects and whether traffic volumes will return to pre-COVID levels. To ensure that this analysis considers appropriate volumes to establish a conservative “existing conditions” scenario, both data collected in 2022 and historical data collected before COVID-19 were used. These sources are detailed below.

1. Pre-Covid Volumes (Scenario A) – The PAG Transportation Data Management System was used to obtain pre-covid Turning Movement Counts (TMC) for 6th/5th Street. Historical daily volumes were also gathered from the PAG MS2 website.
2. 2022 Collected Volumes (Scenario B) – Traffic volumes were collected in August 2022 at the study intersections. Daily volumes were collected for segments along the corridor.

PAG TRAFFIC VOLUMES – SCENARIO A

Weekday turning movement counts were obtained from the PAG MS2 website. The turning movement counts obtained include counts from 2017, 2018 and 2019 all taken in October or November. Historical counts for five of the fifteen study intersections were not available; for these intersections, 2022 TMCs were used for both Scenario A and Scenario B.

Table 3. Existing Weekday Peak Hour Turning Movement Counts

#	Intersection	Control	Historical PAG Data Available	Count Date
1	6 th Street / Campbell Avenue	Signal	Yes	10/19/2017
2	6 th Street / Wilson Avenue	HAWK	No	N/A
3	6 th Street / Tucson Boulevard	Signal	Yes	10/19/2017
4	6 th Street / 5 th Street / Country Club Road	Signal	Yes	10/31/2018
5	5 th Street / Dodge Boulevard	Signal	Yes	11/1/2018
6	5 th Street / Alvernon Way	Signal	Yes	11/1/2018
7	5 th Street / Irving Circle	HAWK	No	N/A
8	5 th Street / Columbus Boulevard	Signal	Yes	11/1/2018
9	5 th Street / Swan Road	Signal	Yes	11/1/2018
10	5 th Street / Arcadia Avenue	BikeHAWK	No	N/A
11	5 th Street / Rosemont Boulevard	Signal	Yes	10/2/2018
12	5 th Street / Craycroft Road	Signal	Yes	10/17/2019
13	5 th Street / Chantilly Avenue	HAWK	No	N/A
14	5 th Street / Sahuara Avenue	TWSC	No	N/A
15	5 th Street / Wilmot Rd	Signal	Yes	10/2/2018

2022 TRAFFIC COUNTS VOLUMES – SCENARIO B

Weekday turning movement counts were collected on Tuesday, August 23rd, 2022, at the study intersections. Counts were collected for vehicles, bicycles, and pedestrians during the AM and PM peak periods, 7:00 AM to 9:00 AM and 4:00 PM to 6:00 PM.

The intersections at Campbell Avenue, Alvernon Way, and Wilmot Road experienced the highest vehicular traffic volumes. Bicycle traffic was greatest at the Irving Circle HAWK crossing, and pedestrian volumes were the highest at the Arcadia Avenue HAWK crossing and Campbell Avenue intersection.

Roadway segment counts were also collected on Tuesday, August 23rd, 2022, at the following study segments:

6th Street Corridor:

1. Campbell Avenue to Tucson Boulevard

5th Street Corridor:

1. Country Club Road to Alvernon Way
2. Swan Road to Rosemont Boulevard
3. Sahuara Avenue to Wilmot Road

Daily volume profiles for each segment are shown in Figure 13.

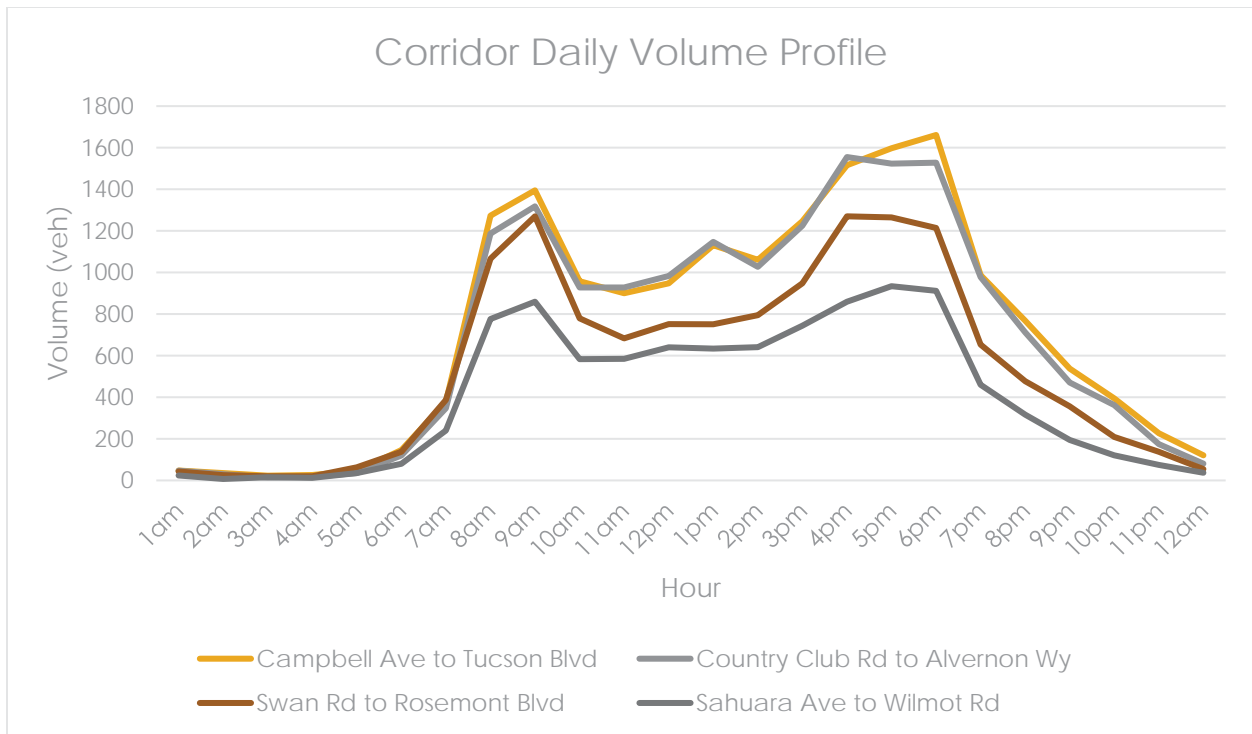


Figure 13. Corridor Daily Volume Profile

The daily volume profiles for each segment are relatively consistent. The highest volume segment is between Campbell Avenue and Tucson Boulevard. The lowest volume segment is between Sahuara Avenue and Wilmot Road. Daily volumes decrease moving east along the corridor with the highest volume on the western portion of the project limits and the lowest volumes on the eastern most portion. This is most likely due to trips associated with the University of Arizona on the western portion of the corridor. Some additional key details for volumes include:

- Daily Volumes
 - Campbell Avenue to Tucson Boulevard Segment = 17,424 vehicles.
 - Country Club Road to Alvernon Way Segment = 16,739 vehicles
 - Swan Road to Rosemont Boulevard Segment = 13,377 vehicles
 - Sahuara Avenue to Wilmot Road Segment = 9,784 vehicles

- Peak Period Peak Direction Volumes
 - AM Peak
 - Campbell Avenue to Tucson Boulevard Segment (7:30 AM)
 - Westbound = 950 vehicles
 - Country Club Road to Alvernon Way Segment (7:30 AM)
 - Westbound = 895 vehicles
 - Swan Road to Rosemont Boulevard Segment (8:00 AM)
 - Eastbound = 687 vehicles
 - Sahuara Avenue to Wilmot Road Segment (7:30 AM)
 - Westbound = 534 vehicles
 - PM Peak
 - Campbell Avenue to Tucson Boulevard Segment (4:45 PM)
 - Eastbound = 952 Vehicles
 - Country Club Road to Alvernon Way Segment (4:30 PM)
 - Eastbound = 933 vehicles

- Swan Road to Rosemont Boulevard Segment (4:30 PM)
 - Eastbound = 751 Vehicles
- Sahuara Avenue to Wilmot Road Segment (4:30 PM)
 - Eastbound = 580 vehicles

The count data (vehicular, pedestrian and bicycle counts) are provided in *Appendix B*.

SCENARIO A AND SCENARIO B VOLUME COMPARISON

An analysis of the PAG Pre-COVID and 2022 Post-COVID TMCs indicates that on average the Scenario A (PAG) volumes are higher than the counts collected with this project. Table 4 shows the TMC comparison for the locations where count data was available for both scenarios.

Table 4. PAG and Collected Counts TMC Comparison

#	Intersection	Percent of AM (TMC/PAG)	Percent of PM (TMC/PAG)
1	Campbell Ave	92%	81%
3	Tucson Blvd	102%	88%
4	Country Club Rd	83%	80%
5	Dodge Blvd	118%	103%
6	Alvernon Way	102%	91%
8	Columbus Blvd	107%	88%
9	Swan Rd	92%	83%
11	Rosemont Blvd	93%	83%
12	Craycroft Rd	90%	89%
15	Wilmot Rd	94%	89%
Average		97% (-3%)	87% (-13%)

As shown in Table 4, the collected 2022 TMC volumes are lower overall than the PAG volumes. On average, the AM peak period volumes collected in 2022 are 3% lower than the PAG volumes; the difference during the PM peak period is more significant, with 2022 volumes approximately 13% lower than PAG volumes. Both TMCs were utilized in this analysis, with the Scenario A volumes representing the more conservative analysis.

2045 PAG TRAVEL DEMAND MODEL

Pima Association of Governments (PAG) provided Kittelson with the 2045 PAG travel demand model run for the study area. PAG uses an activity-based model to predict travel behavior and the resulting travel demand for future time frames. The PAG travel demand model anticipates a modest growth rate of 0.5% per year between 2022 and 2045. The 2045 PAG data projections are provided in *Appendix C*.

CRASH DATA

The following crash analysis was performed as part of the 6th/5th Street Road Diet analysis for fifteen study intersections. The crash data, obtained from ADOT’s Safety Data Mart⁷, documents crashes that occurred

⁷ Arizona Department of Transportation “Safety Data Mart”

in the 5-year period from January 1, 2017, to December 31, 2021, and includes information such as crash type, severity, crossing streets, lighting conditions, etc.

Appendix D contains a copy of the raw crash data obtained from ADOT.

Table 5 illustrates the crash distribution between the study sites and segments along the corridor. There was a total of 370 recorded crashes along the study corridor with 311 crashes occurring at the study intersections and 59 crashes occurring at segments along the corridor.

Table 5. 6th/5th Street Crashes by Severity – 2017 to 2021

#	Corridor Location	Fatal	Incapacitating Injury	Non-Incapacitating Injury	Possible Injury	Property Damage Only	Total
1	Signalized Intersections	1	14	66	61	156	298
2	Unsignalized Intersections	0	0	0	0	0	0
3	HAWK Crossings	0	0	2	0	3	5
4	BikeHAWK Crossings	0	1	2	2	3	8
5	Corridor Segments	1	2	13	12	31	59
Total		2	17	83	75	193	370

Most crashes occurred at signalized intersections (81%) with the most severe crashes also occurring at signalized intersections. One fatal crash and 14 suspected serious injury crashes occurred at the signalized intersections. The fatal crash at the intersection occurred in April of 2020 and involved a pedestrian fatality in which according to the crash record, the pedestrian failed to yield to the traffic control. The pedestrian crash was recorded as “other” crash type during clear weather condition in early morning, lighted conditions, with dry pavement surface.

16% of crashes occurred on the corridor roadway segments with one crash resulting in a fatality and two crashes resulting in suspected serious injuries. The fatal crash occurred at Plumer Avenue along 6th Street. The fatal crash at the two-way stop control intersection occurred in May of 2020 and involved a motorist fatality in which the motor vehicle ran off the road to the right of the centerline. The crash was identified as an angle collision during clear weather conditions in early morning with lighted conditions and dry pavement surface.

CRASHES BY YEAR

Table 6 illustrates the number of crashes that occurred at each of the study intersections along the corridor during the most recent five-year period. There were 298 total crashes reported at ten signalized intersections and 13 crashes reported at the BikeHAWK and HAWK study intersections. Crashes remained relatively constant year-over-year since 2017.

Table 6. 6th/5th Street Crashes by Year – 2017 to 2021

#	Intersection	2017	2018	2019	2020	2021	Total
1	6 TH Street / Campbell Ave (Signalized)	7	7	7	7	5	33
2	6 TH Street / Wilson Ave (HAWK)	0	0	2	0	0	2
3	6 TH Street / Tucson Blvd (Signalized)	7	5	4	4	4	24
4	6 TH Street / Country Club Rd (Signalized)	10	6	12	5	7	40
5	5 TH Street / Dodge Blvd (Signalized)	4	2	1	4	1	12
6	5 TH Street / Alvernon Way (Signalized)	11	12	14	5	12	54
7	5 TH Street / Irving Cir (HAWK)	2	0	1	0	0	3
8	5 TH Street / Columbus Blvd (Signalized)	5	5	7	6	3	26
9	5 TH Street / Swan Rd (Signalized)	10	3	4	9	7	33
10	5 TH Street / Arcadia Ave (BikeHAWK)	1	1	4	1	1	8
11	5 TH Street / Rosemont Blvd (Signalized)	2	3	4	3	3	15
12	5 TH Street / Craycroft Rd (Signalized)	10	5	3	7	7	32
13	5 TH Street / Chantilly Dr (HAWK)	0	0	0	0	0	0
14	5 TH Street / Sahuara Ave (TWSC)	0	0	0	0	0	0
15	5 TH Street / Wilmot Rd (Signalized)	3	8	5	4	9	29
Total		72	57	68	55	59	311

CRASHES BY TYPE

Table 7 shows the number of crashes that occurred at each signalized intersection along the corridor by crash type. Left turn crashes were the most common crash type recorded at the intersections on this corridor, specifically at Country Club Road, Alvernon Way, and Campbell Avenue. The second most common crash type were angle crashes, followed by rear end crashes. Rear end crashes tend to be the most common crash type at signalized intersections, and signal re-optimization has the potential to reduce this type of crash.

Table 7. 6th/5th Street Crashes by Type – 2017 to 2021

#	Intersection	Single Vehicle	Head On	Angle	Left Turn	Rear End	Side Swipe	Other	Total
1	6 TH Street / Campbell Ave (Signalized)	1	1	9	12	9	0	1	33
2	6 TH Street / Wilson Ave (HAWK)	0	0	1	0	1	0	0	2
3	6 TH Street / Tucson Blvd (Signalized)	0	0	9	8	4	3	0	24
4	6 TH Street / Country Club Rd (Signalized)	2	0	13	20	1	3	1	40
5	5 TH Street / Dodge Blvd (Signalized)	1	0	3	2	5	0	1	12
6	5 TH Street / Alvernon Way (Signalized)	8	0	13	14	16	1	2	54
7	5 TH Street / Irving Cir (HAWK)	0	0	0	0	3	0	0	3
8	5 TH Street / Columbus Blvd (Signalized)	0	1	10	11	4	0	0	26
9	5 TH Street / Swan Rd (Signalized)	2	1	11	6	9	1	3	33
10	5 TH Street / Arcadia Ave (BikeHAWK)	2	0	2	1	1	0	2	8
11	5 TH Street / Rosemont Blvd (Signalized)	0	1	7	3	3	0	1	15
12	5 TH Street / Craycroft Rd (Signalized)	2	0	9	9	8	3	1	32
13	5 TH Street / Chantilly Dr (HAWK)	0	0	0	0	0	0	0	0
14	5 TH Street / Sahuara Ave (TWSC)	0	0	0	0	0	0	0	0
15	5 TH Street / Wilmot Rd (Signalized)	1	0	2	11	9	2	4	29
Total		19	4	89	97	73	13	16	311

CRASHES BY SEVERITY

Table 8 shows the number of crashes that occurred at each signalized intersection along the corridor by crash severity. Crash severity is determined by the reporting officer at the time of the crash based on the most severe injury sustained by all parties involved. Crash severity is designated as one of the following (from most to least severe): fatal, incapacitating injury, non-incapacitating injury, possible injury, and no injury. The fifteen study intersections reported a total of 311 crashes of which 1 were fatal (0.3%), 15 (4.8%) were incapacitating injury, 70 (22.5%) were non-incapacitating injury, 63 (20.3%) were possible injury, and 161 (51.8%) were no injury crashes.

Table 8. 6th/5th Street Crashes by Type – 2017 to 2021

#	Intersection	Fatal	Incapacitating Injury	Non-Incapacitating Injury	Possible Injury	Property Damage Only	Total
1	6 TH Street / Campbell Ave (Signalized)	0	1	7	4	21	33
2	6 TH Street / Wilson Ave (HAWK)	0	0	0	0	2	2
3	6 TH Street / Tucson Blvd (Signalized)	0	0	5	5	14	24
4	6 TH Street / Country Club Rd (Signalized)	0	2	8	4	26	40
5	5 TH Street / Dodge Blvd (Signalized)	0	0	2	4	6	12
6	5 TH Street / Alvernon Way (Signalized)	0	1	12	14	27	54
7	5 TH Street / Irving Cir (HAWK)	0	0	2	0	1	3
8	5 TH Street / Columbus Blvd (Signalized)	0	2	6	6	12	26
9	5 TH Street / Swan Rd (Signalized)	1	3	4	7	17	33
10	5 TH Street / Arcadia Ave (BikeHAWK)	0	1	2	2	3	8
11	5 TH Street / Rosemont Blvd (Signalized)	0	0	4	4	7	15
12	5 TH Street / Craycroft Rd (Signalized)	0	2	8	8	14	32
13	5 TH Street / Chantilly Dr (HAWK)	0	0	0	0	0	0
14	5 TH Street / Sahuara Ave (TWSC)	0	0	0	0	0	0
15	5 TH Street / Wilmot Rd (Signalized)	0	3	10	5	11	29
Total		1	15	70	63	161	311

INDIVIDUAL INTERSECTION CRASH ANALYSIS

Using the information presented on Tables 5-8, a more thorough analysis was conducted at locations designated as high-crash locations or locations with fatalities over the five-year analysis period. Figure 14 shows the crash heat map used to identify the high crash intersections.

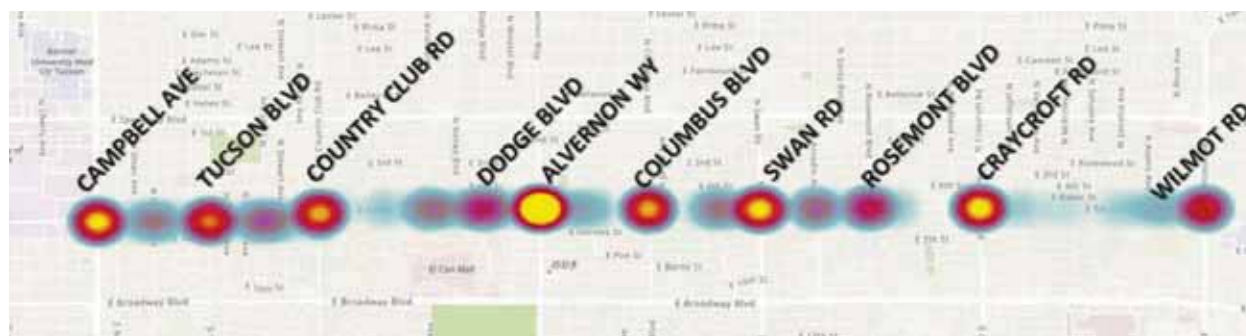


Figure 14. ADOT Crash Corridor Heatmap

6TH Street / Campbell Avenue

A total of 33 crashes occurred at this intersection, of which 9 (27%) were rear end crashes, 12 (36%) were left turn crashes, and 9 (27%) were angle crashes. One of these crashes involved a pedestrian in which a possible injury was sustained. A suspected serious injury was sustained by a passenger in an angle crash with a center median curb.

6TH Street / Plumer Avenue

One fatal crash occurred at Plumer Avenue along 6TH Street. The fatal crash at the two-way stop control intersection occurred in May of 2020 and involved a motorist fatality in which the motor vehicle ran off the road to the right of the centerline. The crash was identified as an angle crash during clear weather conditions in early morning with lighted conditions and dry pavement surface.

6TH Street / Country Club Road

A total of 40 crashes occurred at this intersection during the most recent 5-year period, of which 20 (50%) were left turn crashes and 13 (33%) were angle crashes. All the recorded crashes involved motorists. Two serious injury crashes occurred at the intersection, one of which involved a motorcycle collision with a curb where speed played a contributing factor. The other serious injury crash involved a vehicle-vehicle angle collision where speed also was a contributing factor.

5TH Street / Alvernon Way

A total of 54 crashes occurred at this intersection, 16 (27%) of which were rear end crashes, 14 (26%) were left turn crashes, and 13 (24%) were angle crashes. Two of these crashes involved a pedestrian, one resulting in a possible injury and one resulting in a suspected minor injury. One suspected serious injury crash occurred and involved an angle collision with two vehicles in which the traffic signal was disregarded.

5TH Street / Swan Road

A total of 33 crashes occurred at this intersection, nine (27%) of which were rear end crashes, six (18%) were left turn crashes, and 11 (33%) were angle crashes. Three of these crashes involved a pedestrian, one of which was a fatal crash, another sustained a suspected serious injury, and the last sustained a possible injury. The other suspected serious injuries resulted from a left turn and two angle collisions with other vehicles in which violations of failing to yield the right of way, exceeding lawful speed, and failed to yield to the traffic signal occurred.

The fatal crash at the intersection occurred in April of 2020 and involved a pedestrian fatality in which according to the crash report, the pedestrian failed to yield to the traffic control. The crash was identified

as “other” crash type during clear weather conditions in early morning, lighted conditions, with dry pavement surface.

5TH Street / Craycroft Road

A total of 32 crashes occurred at this intersection, nine (28%) of which were angle crashes, nine (28%) were left turn crashes, and eight (25%) were rear end crashes. Two crashes resulted in suspected serious injuries, one involving a left turn collision in which the driver failed to yield to right of way, the other involving a rear end collision in which the speed was too fast for the conditions.

SPEED DATA

COLLECTED DAILY SPEED PROFILES

To analyze vehicular speed (operating speed) on the corridor, speed data was collected along the following four roadway segments:

- 6th Street between Campbell Avenue and Tucson Boulevard
- 5th Street between Country Club Road and Alvernon Way
- 5th Street between Swan Road and Rosemont Boulevard
- 5th Street between Sahuara Avenue and Wilmot Road

The speed data was collected on Tuesday, 8/23/2022. The speed data was placed into bins of 1-15 mph, 16-20 mph, 21-25 mph, 26-30 mph, 31-35 mph, 36-40 mph, 41-45 mph, 46-50 mph, 51-55 mph, 56-60 mph, 61-65 mph, 66-70 mph, 71-75 mph, and 76+ mph. The speed volume counts were aggregated by 15-minute intervals. These speed segments included eastbound and westbound speed counts for vehicles along 6th Street and 5th Street. The western-most segment of speed data was collected along 6th Street from Campbell Avenue to Tucson Boulevard. The daily speed profile for the eastbound and westbound directions can be seen in Figure 15.

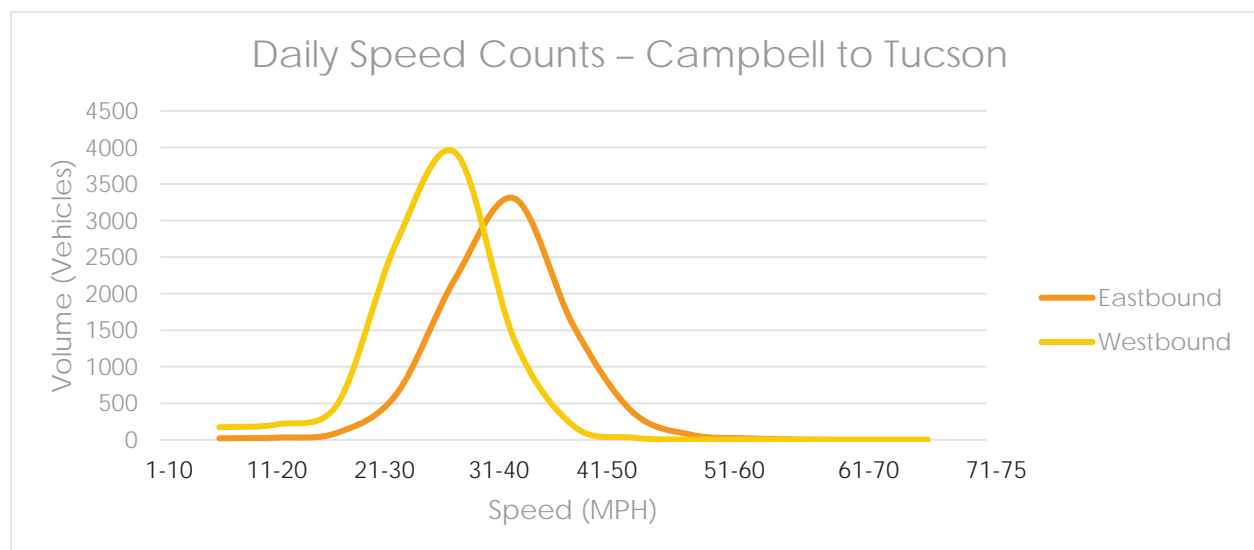


Figure 15. Campbell to Tucson Daily Speed Profile

The segment between Campbell Avenue and Tucson Boulevard:

- Posted Speed: **30 MPH**
- Average Speed: **37 MPH (EB), 31 MPH (WB)**
- 85th Percentile Speed: **41 MPH (EB), 35 MPH (WB)**

The same procedure was used for the eastern-most speed segment along 5th Street from Sahuara Avenue to Wilmot Road. The daily volume profile and speed data statistics can be observed below.

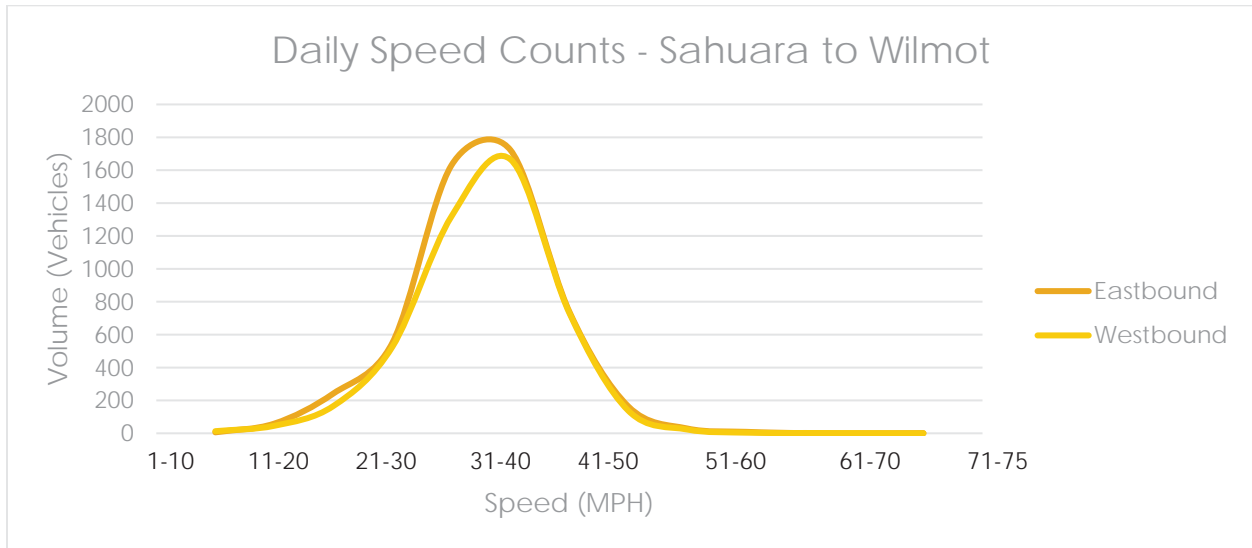


Figure 16. Sahuara Avenue to Wilmot Road Daily Speed Profile

- Posted Speed: **30 MPH**
- Average Speed: **35 MPH (EB), 35 MPH (WB)**
- 85th Percentile Speed: **41 MPH (EB), 41 MPH (WB)**

The collected speed data and the speed profiles for the additional segments are provided in *Appendix E*.

These speed profiles indicate that the corridor experiences speeds above the posted speed limit as both the average and 85th percentile speeds exceed the posted speed limit. The western segment of the corridor experiences a large variation between the eastbound and westbound speed profiles with both the average and 85th percentile speeds greater than 5 MPH apart. The eastern segment of the corridor experiences more uniform speed profiles with the average and 85th percentile speeds of the eastbound and westbound traffic being the same.



Section 3

Existing Conditions Traffic Analysis

EXISTING CONDITIONS TRAFFIC ANALYSIS

SYNCHRO/SIMTRAFFIC MODELS

Synchro models were obtained from the City of Tucson and updated for the study peak periods utilizing the traffic and roadway data collected for this project. The Simtraffic models were used to evaluate intersection operations for existing traffic conditions under both Scenario A and Scenario B. The simtraffic models were calibrated by comparing the HCM 6th Edition intersection delay to the average delay calculated during the Simtraffic model runs.

SCENARIO A EXISTING INTERSECTION TRAFFIC CONDITIONS

LEVEL OF SERVICE

Kittelson used the SimTraffic traffic model prepared for this study to evaluate traffic operations at intersections during AM and PM peak hours. Table 9 shows the projected intersection operations for existing conditions under Scenario A for both the AM and PM peak hours. Figure 17 and Figure 18 the overall intersection LOS for the AM and PM peak hours, respectively. Scenario A existing conditions SimTraffic reports are provided in *Appendix F*.



Figure 17. Scenario A Existing Conditions AM Peak Hour Intersection Operations



Figure 18. Scenario A Existing Conditions PM Peak Hour Intersection Operations

Table 9. Scenario A Existing Conditions Intersection Operations

Int. #	Intersection	Control Type	Level of Service (AM / PM)													
			Intersection	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
1	6 th Street / Campbell Avenue	Signal	E / F	E / F	C / D	A / C	D / D	E / C	E / C	E / E	D / D	A / B	D / F	E / F	E / F	
2	6 th Street / Wilson Avenue	HAWK	A / A	A / A	- / -	- / -	A / A	- / -	A / A	- / -	- / -	- / -	- / -	- / -	- / -	
3	6 th Street / Tucson Boulevard	Signal	C / D	A / B	A / B	A / B	B / D	B / B	B / B	F / F	C / D	C / D	E / F	E / E	E / E	
4	6 th Street / Country Club Road	Signal	C / C	C / C	- / -	- / -	D / D	A / A	A / A	D / D	C / C	A / B	D / D	C / D	C / C	
5	5 th Street / Dodge Boulevard	Signal	A / B	A / A	A / A	A / A	B / B	A / B	A / B	D / D	- / -	A / B	- / -	- / -	- / -	
6	5 th Street / Alvernon Way	Signal	C / D	C / C	B / C	B / C	C / D	C / C	C / C	D / E	C / E	C / E	D / E	D / D	C / D	
7	5 th Street / Irving Circle	HAWK	A / A	A / A	- / -	- / -	- / -	A / A	A / A	- / -	- / -	- / -	- / -	- / -	- / -	
8	5 th Street / Columbus Boulevard	Signal	B / C	A / B	A / B	A / B	C / C	B / B	B / B	C / E	B / D	B / D	C / E	B / D	B / C	
9	5 th Street / Swan Road	Signal	D / D	C / D	C / D	C / D	D / D	C / C	C / C	D / D	D / D	B / B	F / E	D / D	A / A	
10	5 th Street / Arcadia Avenue	Bike HAWK	A / A	A / A	- / -	- / -	- / -	A / A	- / -	- / -	- / -	- / -	- / -	- / -	- / -	
11	5 th Street / Rosemont Boulevard	Signal	C / C	A / B	A / B	A / B	C / C	B / A	B / A	F / E	D / D	C / D	F / E	E / C	E / C	
12	5 th Street / Craycroft Road	Signal	C / D	C / D	C / C	C / C	D / D	C / C	C / C	E / E	C / D	B / C	D / D	C / D	B / B	
13	5 th Street / Chantilly Drive	HAWK	A / A	A / A	- / -	- / -	- / -	A / A	- / -	- / -	- / -	- / -	- / -	- / -	- / -	
14	5 th Street / Sahuara Avenue	TWSC	A / A	A / A	A / A	A / A	E / A	A / A	A / A	B / A	B / A	A / B	A / A	B / -	A / A	
15	5 th Street / Wilmot Road	Signal	D / C	D / D	D / D	C / B	C / C	D / C	D / C	F / D	C / C	B / C	D / C	C / C	A / A	

Note: Dark grey cells indicate intersections that are anticipated operate at LOS E or worse during either peak hour; light gray cells indicate specific movements that would operate at LOS E or worse during either peak hour.

As can be seen in Table 9 under Scenario A existing conditions, the majority of the signalized intersections except for the Campbell Avenue/6th Street intersection operate at an acceptable intersection LOS (LOS D or better). However, there are several individual intersection movements that operate below LOS D during the evaluated peak periods.

QUEUE LENGTH

Queue lengths were calculated for all intersection movements. The maximum queue length observed across all ten simulations was recorded and compared to the existing storage length for each lane. The storage length for through movements was measured to the closest upstream signalized intersection or marked pedestrian crossing. Maximum queue lengths that extend beyond the upstream intersection pose operation and safety issues. Table 10 shows the observed maximum queue lengths during the simulation. Average queue lengths are reported in the Simtraffic reports.

Table 10. Scenario A Existing Conditions Intersection Maximum Queueing

Int. #	Intersection	Control Type	Maximum Queue Length (AM / PM)												
			EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
1	6 th Street / Campbell Avenue	Signal	419 / 424	790 / 610	200 / 200	380 / 231	769 / 283	770 / 304	390 / 291	2049 / 368	370 / 169	349 / 350	2033 / 2169	1962 / 2140	
2	6 th Street / Wilson Avenue	HAWK	- / -	68 / 75	- / -	- / -	273 / 176	- / -	- / -	- / -	- / -	- / -	- / -	- / -	
3	6 th Street / Tucson Boulevard	Signal	112 / 264	234 / 323	254 / 332	257 / 96	432 / 220	443 / 234	40 / 272	362 / 570	362 / 570	196 / 400	422 / 828	422 / 828	
4	6 th Street / Country Club Road	Signal	147 / 224	195 / 308	111 / 117	190 / 189	396 / 254	260 / 199	172 / 172	207 / 213	170 / 170	174 / 174	413 / 373	427 / 364	
5	5 th Street / Dodge Boulevard	Signal	- / -	127 / 178	147 / 180	126 / 177	298 / 235	- / -	304 / 244	- / -	83 / 164	- / -	- / -	- / -	
6	5 th Street / Alvernon Way	Signal	187 / 199	150 / 301	182 / 300	176 / 174	360 / 312	369 / 325	250 / 250	563 / 762	564 / 755	250 / 250	650 / 525	652 / 526	
7	5 th Street / Irving Circle	HAWK	- / -	155 / 233	- / -	- / -	75 / 71	- / -	- / -	- / -	- / -	- / -	- / -	- / -	
8	5 th Street / Columbus Boulevard	Signal	96 / 142	161 / 223	179 / 230	131 / 132	248 / 254	249 / 265	192 / 199	318 / 543	318 / 543	151 / 196	294 / 504	294 / 504	
9	5 th Street / Swan Road	Signal	149 / 200	238 / 349	259 / 356	165 / 194	289 / 275	314 / 284	303 / 264	408 / 577	213 / 380	314 / 294	388 / 458	210 / 245	
10	5 th Street / Arcadia Avenue	Bike HAWK	- / -	200 / 236	- / -	- / -	61 / 62	- / -	- / -	- / -	- / -	- / -	- / -	- / -	
11	5 th Street / Rosemont Boulevard	Signal	170 / 158	279 / 238	290 / 260	205 / 76	347 / 123	355 / 151	226 / 250	312 / 558	312 / 558	201 / 214	391 / 410	391 / 410	
12	5 th Street / Craycroft Road	Signal	210 / 175	225 / 318	262 / 333	165 / 209	248 / 207	271 / 211	330 / 329	524 / 655	230 / 230	299 / 299	418 / 488	300 / 300	
13	5 th Street / Chantilly Drive	HAWK	- / -	63 / 71	- / -	- / -	71 / 64	- / -	- / -	- / -	- / -	- / -	- / -	- / -	
14	5 th Street / Sahuara Avenue	TWSC	53 / 54	48 / 53	48 / 53	81 / 49	81 / 49	20 / 43	87 / 31	87 / 31	87 / 31	52 / 46	52 / 46	52 / 46	
15	5 th Street / Wilmot Road	Signal	159 / 242	198 / 310	161 / 216	154 / 86	154 / 86	154 / 86	359 / 332	786 / 373	499 / 335	230 / 200	561 / 306	399 / 90	

Note: Light gray cells indicate specific movement queues that are anticipated to exceed existing storage length during either peak hour.

As shown in Table 10, there are several calculated maximum queue lengths that are anticipated to exceed the existing storage capacity. These typically are expected to occur for turning movements along the corridor. However, the northbound and southbound through movement calculated maximum queue lengths at the Campbell Avenue/6th Street intersection are anticipated to extend beyond the adjacent upstream signalized intersection during the peak periods under the high-volume scenario (Scenario A).

SCENARIO B EXISTING INTERSECTION TRAFFIC CONDITIONS

LEVEL OF SERVICE

Kittelson used the SimTraffic traffic model to evaluate traffic operations at intersections during AM and PM peak hours. Table 11 shows the projected intersection operations for existing conditions under Scenario B for both the AM and PM peak hours. Figure 19 and Figure 20 show the overall intersection LOS for the AM and PM peak hours, respectively. Scenario B existing conditions SimTraffic reports are provided in *Appendix G*.



Figure 19. Scenario B Existing Conditions AM Peak Hour Intersection Operations



Figure 20. Scenario B Existing Conditions PM Peak Hour Intersection Operations

Table 11. Scenario B Existing Conditions Intersection Operations

Int. #	Intersection	Control Type	Level of Service (AM / PM)																
			Intersection	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR				
1	6 th Street / Campbell Avenue	Signal	D/D	D/E	C/D	A/C	C/D	D/C	D/C	D/C	D/C	E/D	C/C	C/C	A/B	D/D	C/D	C/D	
2	6 th Street / Wilson Avenue	HAWK	A/A	-/-	A/A	-/-	-/-	A/A	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-
3	6 th Street / Tucson Boulevard	Signal	C/C	D/C	B/B	A/B	C/D	B/B	B/B	B/A	B/A	E/D	C/D	C/C	C/C	D/E	D/D	D/D	C/C
4	6 th Street / Country Club Road	Signal	C/C	C/C	B/C	A/A	C/D	C/C	C/C	A/A	A/A	D/D	C/C	C/C	A/A	D/D	D/D	D/D	C/C
5	5 th Street / Dodge Boulevard	Signal	B/B	-/-	A/B	A/B	B/B	A/B	A/B	-/-	-/-	D/D	-/-	-/-	A/B	-/-	-/-	-/-	-/-
6	5 th Street / Alvernon Way	Signal	C/C	D/C	C/C	B/C	D/D	C/C	C/C	C/C	C/C	D/D	C/D	C/D	C/C	D/D	C/D	C/D	C/C
7	5 th Street / Irving Circle	HAWK	A/A	-/-	A/A	-/-	-/-	A/A	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-
8	5 th Street / Columbus Boulevard	Signal	B/B	C/C	A/B	A/B	C/C	B/B	B/B	B/B	B/B	C/D	B/C	B/C	B/B	C/C	B/C	B/C	B/C
9	5 th Street / Swan Road	Signal	C/C	D/D	C/D	C/D	D/D	C/C	C/C	C/C	C/C	D/D	C/C	C/C	A/A	D/D	C/C	C/C	A/A
10	5 th Street / Arcadia Avenue	Bike HAWK	A/A	-/-	A/A	-/-	-/-	A/A	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-
11	5 th Street / Rosemont Boulevard	Signal	B/B	C/C	B/B	A/B	C/D	B/B	B/B	B/B	B/B	C/C	B/B	B/B	A/B	B/C	B/B	B/B	B/B
12	5 th Street / Craycroft Road	Signal	C/C	D/D	C/D	C/D	D/D	D/C	D/C	C/C	C/C	D/D	C/C	C/C	A/A	D/D	C/C	C/C	A/A
13	5 th Street / Chantilly Drive	HAWK	A/A	-/-	A/A	-/-	-/-	A/A	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-
14	5 th Street / Sahuara Avenue	TWSC	A/A	A/A	A/A	A/A	A/A	A/A	A/A	A/A	A/A	A/B	A/-	A/A	A/A	A/A	A/A	A/-	A/A
15	5 th Street / Wilmot Road	Signal	C/C	D/D	D/C	B/B	C/C	D/C	D/C	B/B	B/B	E/D	B/B	B/B	B/B	C/C	B/B	B/B	A/A

Note: Dark grey cells indicate intersections that are expected to operate at LOS E or worse during either peak hour; light grey cells indicate specific movements that would operate at LOS E during either peak hour.

As shown in Table 11, all study intersections are anticipated to operate acceptably under Scenario B existing conditions. There are some left turning movements that are expected to operate at LOS E during the peak period at the Campbell Avenue/6th Street and Tucson Boulevard/6th Street intersections.

QUEUE LENGTH

Queue lengths were calculated for all intersection movements. The maximum queue length observed across all ten simulations was recorded and compared to the existing storage length for each lane. The storage length for through movements was measured to the closest upstream signalized intersection or marked pedestrian crossing. Maximum queue lengths that extend beyond the upstream intersection pose operation and safety issues. Table 12 shows the observed maximum queue lengths during the simulation. Average queue lengths are reported in the Simtraffic reports.

Table 12. Scenario B Existing Conditions Intersection Queueing

Int. #	Intersection	Control Type	Maximum Queue Length (AM / PM)											
			EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
1	6 th Street / Campbell Avenue	Signal	198 / 200	252 / 528	188 / 200	379 / 216	521 / 229	552 / 263	282 / 149	305 / 236	106 / 98	195 / 342	305 / 409	331 / 405
2	6 th Street / Wilson Avenue	HAWK	- / -	64 / 74	- / -	- / -	235 / 199	- / -	- / -	- / -	- / -	- / -	- / -	- / -
3	6 th Street / Tucson Boulevard	Signal	123 / 188	193 / 314	217 / 331	154 / 106	347 / 214	353 / 224	164 / 134	345 / 391	345 / 391	176 / 225	438 / 386	438 / 386
4	6 th Street / Country Club Road	Signal	173 / 165	141 / 190	73 / 115	189 / 78	315 / 266	260 / 196	167 / 168	189 / 202	103 / 156	174 / 174	289 / 298	295 / 295
5	5 th Street / Dodge Boulevard	Signal	- / -	151 / 266	185 / 298	130 / 168	249 / 231	- / -	253 / 244	- / -	146 / 227	- / -	- / -	- / -
6	5 th Street / Alvernon Way	Signal	181 / 199	219 / 343	237 / 339	173 / 174	316 / 282	325 / 294	250 / 250	447 / 468	446 / 466	249 / 250	411 / 420	411 / 416
7	5 th Street / Irving Circle	HAWK	- / -	134 / 232	- / -	- / -	72 / 75	- / -	- / -	- / -	- / -	- / -	- / -	- / -
8	5 th Street / Columbus Boulevard	Signal	74 / 153	167 / 243	182 / 266	115 / 118	233 / 212	237 / 219	165 / 164	232 / 310	232 / 310	103 / 166	218 / 339	218 / 339
9	5 th Street / Swan Road	Signal	160 / 200	231 / 328	259 / 367	190 / 196	279 / 276	280 / 283	251 / 238	332 / 317	100 / 88	247 / 187	314 / 320	80 / 103
10	5 th Street / Arcadia Avenue	Bike HAWK	- / -	62 / 258	- / -	- / -	0 / 62	- / -	- / -	- / -	- / -	- / -	- / -	- / -
11	5 th Street / Rosemont Boulevard	Signal	124 / 143	154 / 246	160 / 259	99 / 83	221 / 200	223 / 218	94 / 103	174 / 200	174 / 200	198 / 71	231 / 184	231 / 184
12	5 th Street / Craycroft Road	Signal	171 / 174	211 / 308	215 / 331	165 / 164	262 / 203	274 / 237	285 / 161	388 / 323	230 / 212	163 / 238	259 / 348	118 / 177
13	5 th Street / Chantilly Drive	HAWK	- / -	12 / 67	- / -	- / -	16 / 71	- / -	- / -	- / -	- / -	- / -	- / -	- / -
14	5 th Street / Sahuara Avenue	TWSC	97 / 47	115 / 9	115 / 9	93 / 57	93 / 57	86 / 5	72 / 33	72 / 33	72 / 33	33 / 45	33 / 45	33 / 45
15	5 th Street / Wilmot Road	Signal	128 / 184	156 / 201	122 / 167	138 / 107	138 / 107	138 / 107	343 / 273	378 / 295	226 / 280	229 / 169	279 / 298	127 / 68

Note: light gray cells indicate specific movement queues that are anticipated to exceed existing storage length during either peak hour.

As shown in Table 12, there are several maximum queue lengths that are expected exceed the existing storage length. These maximum queues are primarily left and right turn queues.

EXISTING CONDITIONS CORRIDOR ANALYSIS

TRAVEL TIME ANALYSIS

The corridor travel time was calculated for both the eastbound and westbound directions. The travel time was analyzed in three segments, Campbell Avenue to Country Club Road, Country Club Road to Alvernon Way, and Alvernon Way to Wilmot Road. Both the Campbell to Country Club and Country Club to Alvernon segments are one mile in length. While the Alvernon to Wilmot segment is three miles long. The calculated segment travel time was used to estimate the segment LOS. A summary of the travel time findings for each scenario can be found in Table 13 and Table 14.

Table 13. Eastbound Corridor Travel Time – Existing Conditions Scenario A and B

Segment	Existing Scenario A AM		Existing Scenario A PM		Existing Scenario B AM		Existing Scenario B PM	
	Travel Time (s)	LOS	Travel Time (s)	LOS	Travel Time (s)	LOS	Travel Time (s)	LOS
Campbell to Country Club	262.5	C	303.6	D	259.3	C	285.4	C
Country Club to Alvernon	148.1	C	161.9	C	153.0	C	159.4	C
Alvernon to Wilmot	514.9	C	521.2	C	543.4	C	514.8	C
Total	925.5	C	986.7	C	955.7	C	959.6	C

Table 14. Westbound Corridor Travel Time – Existing Conditions Scenario A and B

Segment	Existing Scenario A AM		Existing Scenario A PM		Existing Scenario B AM		Existing Scenario B PM	
	Travel Time (s)	LOS	Travel Time (s)	LOS	Travel Time (s)	LOS	Travel Time (s)	LOS
Wilmot to Alvernon	612.8	C	583.5	C	639.1	C	599.5	C
Alvernon to Country Club	172.6	C	171.6	C	174.8	C	170.1	C
Country Club to Campbell	258.6	C	227.5	D	230.7	C	210.2	D
Total	1044.0	C	982.6	C	1044.6	C	979.8	C

The tables above indicate that the travel time along the corridor is approximately fifteen to seventeen minutes under Scenario A existing conditions (4-lane cross-section). Additionally, the westbound travel time during the AM peak period experiences the longest travel time along the corridor.

SIDE STREET DELAY

A typical stop-controlled side street was modeled using the conservative volumes (Scenario A). The side street was modeled in Scenario A existing conditions to determine the expected delay for vehicles making a left onto 6th Street and 5th Street. The modeled typical side street was assumed to have 50 vehicles making a left turn movement from both the north and southbound approach. These volumes were determined based on traffic counts collected at the stop-controlled intersections where HAWK crossings are provided.

Table 15. Conservative (Scenario A) Existing Conditions Side Street Delay

Side Street	Side Street Approach	AM		PM	
		Delay (s)	LOS	Delay (s)	LOS
6 th Side Street	NB	22.0	C	23.5	C
	SB	27.6	C	23.5	C
5 th Side Street	NB	14.2	B	16.2	B
	SB	16.0	B	15.3	B

Under Scenario A existing conditions, the control delay for side street traffic to make a left-turn maneuver onto 6th/5th street is acceptable as the stop-control movements operate at LOS C or better.



Section 4

Proposed Road Diet Layout

PROPOSED ROAD DIET LAYOUT

BENEFITS OF A ROAD DIET

Road diets have the potential to enhance safety and multimodal operations. Road diet benefits may include:

Improved Safety: Road diets can reduce vehicle-to-vehicle conflicts that result in crashes through removing the interior travel lane on a four-lane undivided roadway that services both through and turning vehicles. According to the FHWA Road Diet Informational Guide⁸, reducing the number of lanes and adding a TWLTL may decrease overall crashes by 19 to 47 percent. This decrease in crashes can be attributed to lower corridor speeds, reduced vehicle-to-vehicle conflict points, and the separation of vehicles and bicycles. Additionally, road diets may reduce crashes at intersections by removing the number of conflicting lanes for vehicles. Furthermore, pedestrian exposure is decreased by reducing the number of vehicular lanes.

Operational Benefits: Road diets may provide operational benefits to the corridor dependent on the segment volumes. Separation of the left turn from the through traffic can reduce delays along the corridor that result from left turning vehicles waiting for an adequate gap in traffic. Additionally, side street traffic may experience reduced delay entering the main corridor due to the reduced conflicting lanes required to cross and potential reduction of traffic volumes associated with road diets.

Pedestrian and Bicycle Benefits: Road diets can reallocate space from vehicle lanes to provide designated bicycle and pedestrian facilities. In many cases stripped and/or buffered bike lanes can be added to a roadway undergoing a road diet. The reduced speeds along the corridor will also likely contribute to fewer and less severe bicycle and pedestrian crashes.

Livability Benefits: Road diets are shown to create more comfortable streets for all users through a combination of bike lanes, pedestrian improvements, and reduced speeds.

TRADE-OFFS OF A ROAD DIET

The reduction of travel lanes and addition of pedestrian and bicycle infrastructure can cause unintended or secondary impacts to roadway operations. These trade-offs may include:

Mail Service: Mail carriers may be required to stop in the bike lane and/or the travel lane to make deliveries. This might impact safety and operations on the corridor for both the carrier and the traffic utilizing the blocked lane as this may lead to increased corridor travel times, increased carrier delivery times and potential encroachment onto the center lane.

Transit Service: Transit services along a corridor that implements a road diet may see increased travel times as a result of the lane reduction. If a road diet results in increased vehicular congestion, it is expected that transit travel times along the corridor will increase. This may result in reduced transit reliability or the need for transit agency to increase headways along the corridor. At transit stop locations without a bus-pull out transit vehicles might be required to stop in the bike lane and/or the travel lane. This may increase vehicular queueing behind the transit vehicle.

Trash Collection: Garbage trucks may be required to stop in the bike lane and/or the travel lane to collect trash. This might impact safety and operations both the collector and the traffic utilizing the blocked lane.

⁸ Knapp, Keith K., et al. Road Diet Informational Guide. United States Department of Transportation, Federal Highway Administration, 2014.

This may lead to increased corridor travel times, increased collection times and potential encroachments onto the center lane.

May Reduce Traffic Volumes/Operations at Intersections: Road diets may result in a reduction in traffic volumes at intersections and along the roadway segments. Removal of through lanes at intersections can result in higher delays and traffic volumes rerouting to roadways with higher capacities which may result in higher delays along those roadways.

PROPOSED CROSS SECTION

Two conceptual level corridor plans have been prepared for the study corridor and provided to Kittelson for reference for this study. Kittelson utilized these conceptual layouts to build a Synchro and Simtraffic model for the corridor under road diet conditions. The lane widths and storage lengths shown in these plan sets were utilized as the proposed values in this study.

6TH STREET FROM CAMPBELL AVENUE TO COUNTRY CLUB ROAD

Psomas completed the 6th Street Road Reconfiguration Study in October 2016. This study included a conceptual layout of a road diet for 6th Street between Campbell Avenue and Country Club Road. The proposed cross section provided in that study is shown in Figure 21 includes the following:

- 9' TWLTL
- 9.5' Travel Lanes
- 5' Bike Lane
- Far-side bus bay at Campbell Avenue

The Psomas 6th Street conceptual road diet layout is provided in *Appendix H*.

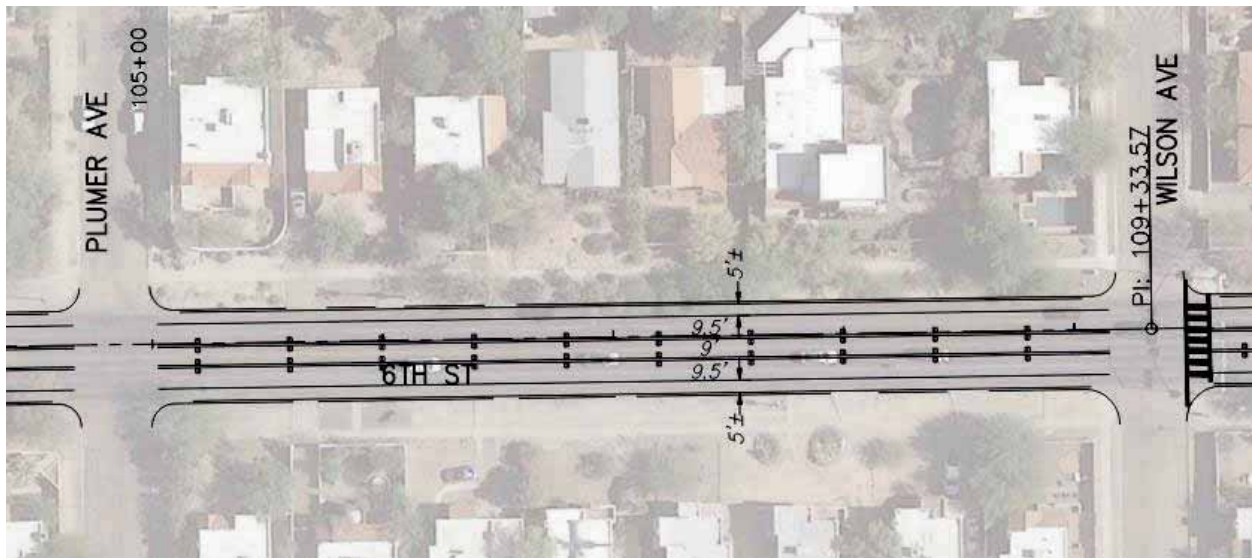


Figure 21. 6th Street Conceptual Road Diet Cross Section

Note that the City of Tucson requires special approval for travel lanes less than ten feet. Traditional travel lane widths would not accommodate bicycle facilities along this portion of roadway, the undersized lane widths would allow for bike lanes to be installed.

5TH STREET FROM COUNTRY CLUB ROAD TO WILMOT ROAD

Kimley-Horn developed a draft road diet layout for the 5th Street corridor from Country Club Road to Wilmot Road. The proposed cross section provided in that study is shown in Figure 22 includes the following:

- 12' TWLTL
- 11' Travel Lanes
- 5' Bike Lane w/ 2' Buffer

The Kimley-Horn 5th Street conceptual road diet layout is provided in *Appendix I*.

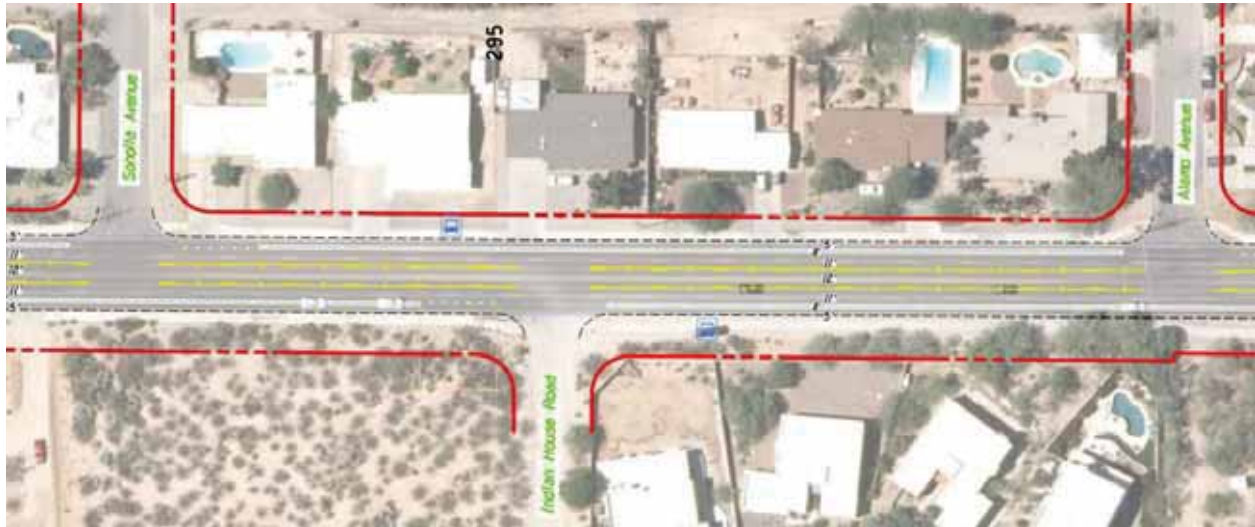


Figure 22. 5th Street Conceptual Road Diet Cross Section



Section 5

Year 2022 Road Diet

Traffic Analysis

YEAR 2022 ROAD DIET TRAFFIC ANALYSIS

SYNCHRO/SIMTRAFFIC MODELS

The Synchro and Simtraffic models developed as part of the existing conditions analysis were utilized as the basis for the 2022 road diet analysis. The lane configurations discussed in the Proposed Road Diet section of the report were used to adjust the model to the road diet conditions. The same volumes used for the Scenario A and Scenario B existing conditions analysis were used for the 2022 Road Diet Analysis. Traffic signal cycle lengths were updated in the road diet analysis to be 130 seconds at all signalized intersections. The HAWK crossings operations were maintained. Synchro’s signal split optimization tool was used to allocate the additional cycle length to the corresponding signal phases.

SCENARIO A 2022 ROAD DIET INTERSECTION TRAFFIC CONDITIONS

LEVEL OF SERVICE

Kittelson used the SimTraffic traffic model prepared for this study to evaluate traffic operations at intersections during AM and PM peak hours.

Table 16 shows the projected intersection operations for existing conditions under Scenario A for both the AM and PM peak hours. Figure 23 and Figure 24 show the overall intersection LOS for the AM and PM peak hours, respectively. The Scenario A 2022 Road Diet conditions SimTraffic reports are provided in *Appendix J*.

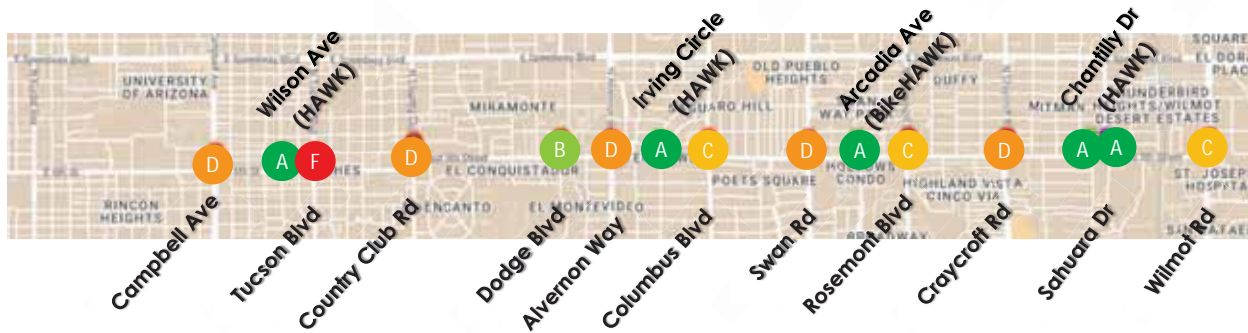


Figure 23. Scenario A 2022 Road Diet Conditions AM Peak Hour Intersection Operations

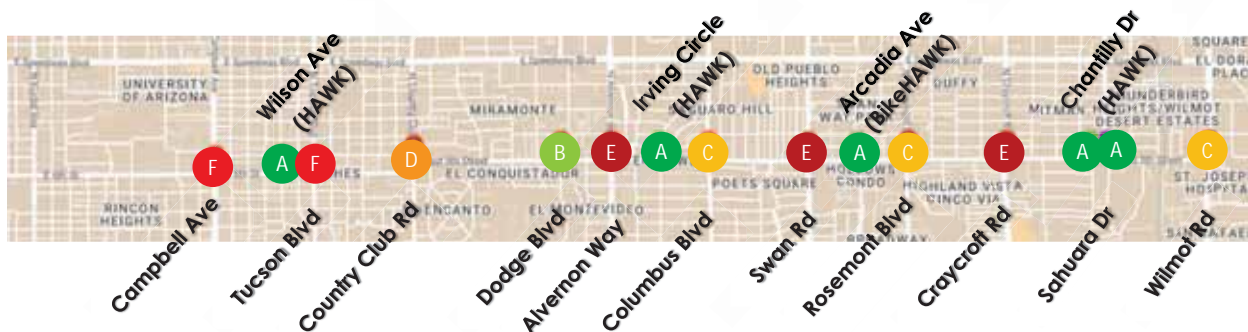


Figure 24. Scenario A 2022 Road Diet Conditions PM Peak Hour Intersection Operations

Table 16. Scenario A 2022 Road Diet Conditions Intersection Operations

Int. #	Intersection	Control Type	Level of Service (AM / PM)													
			Intersection	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
1	6 th Street / Campbell Avenue	Signal	D/F	E/F	D/F	A/F	D/E	D/C	D/C	D/C	F/E	D/D	A/B	E/F	E/F	E/F
2	6 th Street / Wilson Avenue	HAWK	A/A	-/-	A/A	-/-	A/A	-/-	A/A	-/-	-/-	-/-	-/-	-/-	-/-	-/-
3	6 th Street / Tucson Boulevard	Signal	F/F	E/E	C/D	C/C	F/F	F/F	F/F	F/F	F/F	E/E	E/E	F/F	F/F	F/F
4	6 th Street / Country Club Road	Signal	D/D	E/D	D/C	B/A	E/D	B/A	E/C	B/A	D/D	A/A	A/A	E/E	D/D	D/C
5	5 th Street / Dodge Boulevard	Signal	B/B	-/-	B/C	A/B	B/B	A/B	A/B	-/-	D/D	A/B	A/B	-/-	-/-	-/-
6	5 th Street / Alvernon Way	Signal	D/E	F/F	C/D	B/C	D/D	C/C	D/D	C/C	E/F	D/E	D/E	F/F	D/D	D/D
7	5 th Street / Irving Circle	HAWK	A/A	-/-	A/A	-/-	-/-	-/-	A/A	-/-	-/-	-/-	-/-	-/-	-/-	-/-
8	5 th Street / Columbus Boulevard	Signal	C/C	C/D	B/B	A/A	C/D	A/A	A/B	A/A	E/E	D/D	D/D	E/F	D/D	C/D
9	5 th Street / Swan Road	Signal	D/E	F/F	D/E	C/E	F/F	C/B	D/C	C/B	D/F	C/D	C/D	F/F	D/D	B/C
10	5 th Street / Arcadia Avenue	Bike HAWK	A/A	-/-	A/B	-/-	-/-	-/-	A/A	-/-	-/-	-/-	-/-	-/-	-/-	-/-
11	5 th Street / Rosemont Boulevard	Signal	C/C	E/D	C/C	B/B	C/D	B/A	B/B	B/A	F/F	D/D	D/D	E/F	D/D	D/C
12	5 th Street / Craycroft Road	Signal	D/E	F/D	C/D	B/C	D/F	C/B	D/C	C/B	F/F	D/E	C/D	F/E	D/D	C/B
13	5 th Street / Chantilly Drive	HAWK	A/A	-/-	A/A	-/-	-/-	-/-	A/A	-/-	-/-	-/-	-/-	-/-	-/-	-/-
14	5 th Street / Sahuara Avenue	TWSC	A/A	A/A	A/A	A/A	A/A	A/A	A/A	A/A	B/B	C/-	A/A	C/B	B/-	A/A
15	5 th Street / Wilmot Road	Signal	C/C	D/D	D/D	B/B	D/C	C/B	D/C	C/B	F/E	C/C	B/B	D/D	C/C	B/A

Note: Dark grey cells indicate intersections that are expected to operate at LOS E or worse during either peak hour; light gray cells indicate specific movements that would operate at LOS E or worse during either peak hour.

As shown in Table 16, most of the intersections with major north-south roadways are projected to operate below LOS D under 2022 Scenario A Road Diet Conditions. These intersections include:

- Campbell Avenue (PM Peak Hour)
- Tucson Boulevard (AM and PM Peak Hours)
- Alvernon Way (PM Peak Hour)
- Swan Road (PM Peak Hour)
- Craycroft Road (PM Peak Hour)

Additionally, several intersection movements are projected to operate below acceptable LOS under 2022 Scenario A Road Diet Conditions.

QUEUE LENGTH

Queue lengths were calculated for all intersection movements. The maximum queue length observed across all ten simulations was recorded and compared to the existing storage length for each lane. The storage length for through movements was measured to the closest upstream signalized intersection or marked pedestrian crossing. Maximum queue lengths that extend beyond the upstream intersection pose operation and safety issues. Table 17 shows the observed maximum queue lengths during the simulation. Average queue lengths are reported in the Simtraffic reports.

Table 17. Scenario A Road Diet Conditions Intersection Queueing

Int. #	Intersection	Control Type	Maximum Queue Length (AM / PM)													
			EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR		
1	6 th Street / Campbell Avenue	Signal	317 / 425	283 / 3033	200 / 200	232 / 232	273 / 272	282 / 267	389 / 362	517 / 410	310 / 229	350 / 350	535 / 2128	530 / 2097		
2	6 th Street / Wilson Avenue	HAWK	- / -	84 / 85	- / -	- / -	312 / 310	- / -	- / -	- / -	- / -	- / -	- / -	- / -		
3	6 th Street / Tucson Boulevard	Signal	300 / 300	368 / 374	368 / 374	300 / 300	1878 / 1254	1878 / 1254	236 / 399	536 / 835	536 / 835	400 / 400	1893 / 1682			
4	6 th Street / Country Club Road	Signal	265 / 280	340 / 441	111 / 125	190 / 190	516 / 416	260 / 119	177 / 177	220 / 218	170 / 170	174 / 175	550 / 416	551 / 401		
5	5 th Street / Dodge Boulevard	Signal	- / -	439 / 498	236 / 285	204 / 291	305 / 306	- / -	90 / 213	- / -	90 / 162	- / -	- / -	- / -		
6	5 th Street / Alvernon Way	Signal	343 / 399	431 / 714	285 / 285	400 / 400	587 / 596	285 / 285	260 / 260	565 / 827	568 / 829	260 / 260	622 / 552	627 / 551		
7	5 th Street / Irving Circle	HAWK	- / -	339 / 449	- / -	- / -	80 / 84	- / -	- / -	- / -	- / -	- / -	- / -	- / -		
8	5 th Street / Columbus Boulevard	Signal	163 / 170	354 / 480	127 / 194	164 / 150	347 / 357	95 / 175	199 / 199	472 / 497	472 / 497	200 / 200	417 / 502	417 / 502		
9	5 th Street / Swan Road	Signal	359 / 360	728 / 1285	254 / 254	416 / 404	566 / 559	210 / 210	304 / 305	444 / 922	341 / 450	379 / 376	487 / 725	370 / 370		
10	5 th Street / Arcadia Avenue	Bike HAWK	- / -	370 / 480	- / -	- / -	74 / 79	- / -	- / -	- / -	- / -	- / -	- / -	- / -		
11	5 th Street / Rosemont Boulevard	Signal	134 / 134	557 / 611	180 / 180	214 / 178	562 / 394	192 / 192	244 / 250	461 / 555	461 / 555	236 / 224	497 / 375	497 / 375		
12	5 th Street / Craycroft Road	Signal	377 / 400	502 / 874	210 / 210	457 / 415	722 / 496	210 / 210	330 / 330	580 / 857	230 / 230	300 / 299	446 / 507	300 / 300		
13	5 th Street / Chantilly Drive	HAWK	- / -	69 / 73	- / -	- / -	240 / 221	- / -	- / -	- / -	- / -	- / -	- / -	- / -		
14	5 th Street / Sahuara Avenue	TWSC	74 / 108	74 / 108	74 / 108	225 / 96	225 / 96	225 / 96	105 / 33	105 / 33	105 / 33	44 / 40	44 / 40	44 / 40		
15	5 th Street / Wilmot Road	Signal	150 / 240	177 / 388	144 / 243	155 / 110	155 / 110	155 / 110	359 / 349	562 / 388	349 / 379	230 / 229	433 / 378	306 / 116		

Note: Light gray cells indicate specific movement queues that are anticipated to exceed existing storage length during either peak hour.

As shown in Table 17, there are multiple calculated maximum queue lengths that exceed the existing storage length. These typically occur for turning movements along the corridor. However, the northbound and southbound through movement queue lengths at the Tucson Boulevard/6th Street intersection are expected to exceed the available storage lengths and would extend beyond the adjacent upstream signalized intersections during the peak periods. Additionally, the eastbound and southbound through movements at Campbell Avenue/6th Street are expected to exceed the available storage length during the PM peak period.

SCENARIO B 2022 ROAD DIET INTERSECTION TRAFFIC CONDITIONS

LEVEL OF SERVICE

Kittelson used the SimTraffic traffic model to evaluate traffic operations at intersections during AM and PM peak hours. Table 18 shows the projected intersection operations for existing conditions under Scenario A for both the AM and PM peak hours. Figure 25 and Figure 26 show the overall intersection LOS for the AM and PM peak hours, respectively. The Scenario B 2022 Road Diet conditions SimTraffic reports are provided in *Appendix K*.

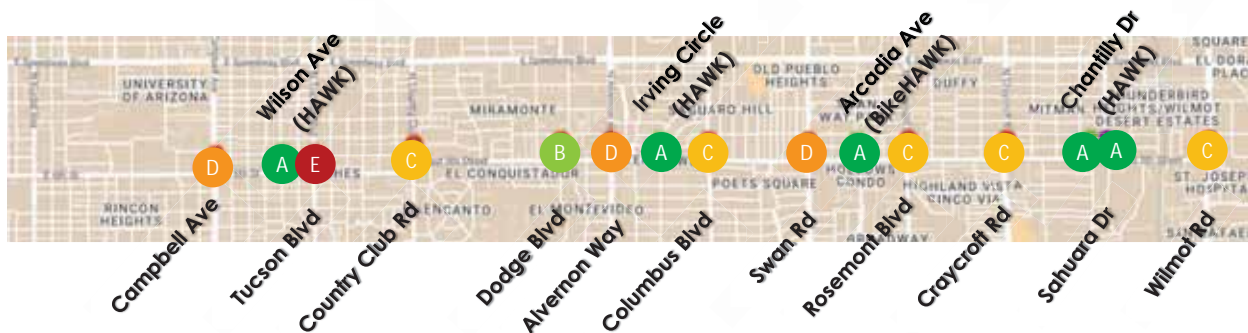


Figure 25. Scenario B 2022 Road Diet Conditions AM Peak Hour Intersection Operations

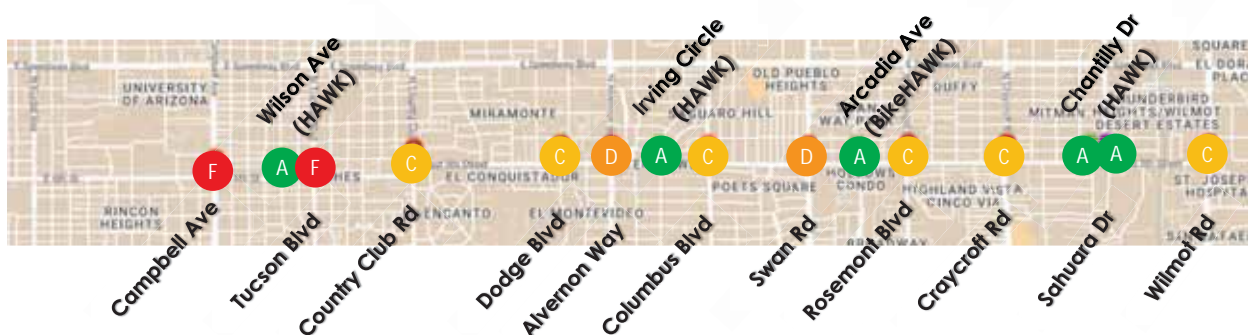


Figure 26. Scenario B 2022 Road Diet Conditions PM Peak Hour Intersection Operations

Table 18. Scenario B Road Diet Conditions Intersection Operations

Int. #	Intersection	Control Type	Level of Service (AM / PM)																	
			Intersection	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR					
1	6 th Street / Campbell Avenue	Signal	D/F	E/F	D/F	A/F	D/E	D/C	D/C	D/C	F/E	D/D	A/A	D/E	D/D	A/A	D/E	D/D	C/D	
2	6 th Street / Wilson Avenue	HAWK	A/A	-/-	A/A	-/-	-/-	A/A	A/A	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-
3	6 th Street / Tucson Boulevard	Signal	E/F	E/D	C/D	C/D	F/E	F/D	F/D	F/D	F/F	E/F	D/F	F/F	D/F	F/F	F/F	E/F	E/F	E/F
4	6 th Street / Country Club Road	Signal	C/C	D/C	B/B	A/A	C/D	C/B	C/B	A/A	D/D	A/A	A/A	D/D	A/A	A/A	D/D	D/D	C/C	C/C
5	5 th Street / Dodge Boulevard	Signal	B/C	-/-	B/C	A/B	B/B	A/B	A/B	-/-	D/E	B/B	B/B	-/-	B/B	B/B	-/-	-/-	-/-	-/-
6	5 th Street / Alvernon Way	Signal	D/D	F/E	D/E	B/D	E/E	E/D	E/D	D/B	F/F	D/D	D/D	E/E	D/D	D/D	E/E	D/D	D/D	D/D
7	5 th Street / Irving Circle	HAWK	A/A	-/-	A/A	-/-	-/-	A/A	A/A	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-
8	5 th Street / Columbus Boulevard	Signal	C/C	C/C	B/B	A/A	C/D	B/B	B/B	A/A	E/F	D/D	C/C	E/E	D/D	C/C	E/E	D/D	C/D	C/D
9	5 th Street / Swan Road	Signal	D/D	E/D	C/D	B/C	D/E	D/C	D/C	C/B	D/E	C/D	B/B	E/E	C/D	B/B	E/E	C/D	B/B	B/B
10	5 th Street / Arcadia Avenue	Bike HAWK	A/A	-/-	B/B	-/-	-/-	A/A	A/A	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-
11	5 th Street / Rosemont Boulevard	Signal	C/C	D/C	B/B	B/B	C/D	C/B	C/B	B/A	F/D	C/C	C/C	D/D	C/C	C/C	D/D	D/C	D/C	D/C
12	5 th Street / Craycroft Road	Signal	C/C	D/D	C/D	B/C	D/E	D/C	D/C	C/B	D/E	C/C	B/B	E/E	C/C	B/B	E/E	C/C	B/B	B/B
13	5 th Street / Chantilly Drive	HAWK	A/A	-/-	A/A	-/-	-/-	A/A	A/A	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-	-/-
14	5 th Street / Sahuara Avenue	TWSC	A/A	A/A	A/A	A/A	A/A	A/A	A/A	A/A	B/C	B/-	A/A	B/B	A/A	A/A	B/B	B/-	A/A	A/A
15	5 th Street / Wilmot Road	Signal	C/C	D/D	D/C	B/B	D/D	D/D	D/D	C/B	D/D	C/C	B/B	D/D	C/C	B/B	D/D	C/C	C/C	B/A

Note: Dark grey cells indicate intersections that are expected to operate at LOS E or worse during either peak hour; light gray cells indicate specific movements that would operate at LOS E or worse during either peak hour.

QUEUE LENGTH

Queue lengths were calculated for all intersection movements. The maximum queue length observed across all ten simulations was recorded and compared to the existing storage length for each lane. The storage length for through movements was measured to the closest upstream signalized intersection or marked pedestrian crossing. Maximum queue lengths that extend beyond the upstream intersection pose operation and safety issues. Table 19 shows the observed maximum queue lengths. Average queue lengths are reported in the Simtraffic reports.

Table 19. Scenario B Road Diet Conditions Intersection Queueing

Int. #	Intersection	Control Type	Maximum Queue Length (AM / PM)											
			EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
1	6 th Street / Campbell Avenue	Signal	249 / 425	278 / 3011	198 / 200	232 / 232	276 / 268	281 / 263	336 / 182	407 / 262	88 / 73	216 / 350	333 / 511	343 / 494
2	6 th Street / Wilson Avenue	HAWK	- / -	71 / 84	- / -	- / -	315 / 308	- / -	- / -	- / -	- / -	- / -	- / -	- / -
3	6 th Street / Tucson Boulevard	Signal	283 / 300	367 / 374	367 / 374	275 / 300	1828 / 791	1828 / 791	287 / 165	503 / 385	503 / 385	356 / 304	525 / 412	525 / 412
4	6 th Street / Country Club Road	Signal	207 / 279	268 / 376	77 / 81	190 / 188	411 / 336	180 / 26	176 / 204	196 / 209	154 / 174	174 / 374	336 / 374	348 / 356
5	5 th Street / Dodge Boulevard	Signal	- / -	397 / 751	171 / 285	192 / 250	302 / 307	- / -	151 / 273	- / -	102 / 208	- / -	- / -	- / -
6	5 th Street / Alvernon Way	Signal	333 / 400	576 / 896	285 / 285	400 / 375	597 / 587	285 / 285	260 / 260	618 / 541	613 / 536	260 / 260	508 / 455	523 / 459
7	5 th Street / Irving Circle	HAWK	- / -	323 / 404	- / -	- / -	86 / 80	- / -	- / -	- / -	- / -	- / -	- / -	- / -
8	5 th Street / Columbus Boulevard	Signal	154 / 170	401 / 480	164 / 194	154 / 150	434 / 357	148 / 175	200 / 199	500 / 497	500 / 497	200 / 200	439 / 502	439 / 502
9	5 th Street / Swan Road	Signal	293 / 359	463 / 794	254 / 254	196 / 196	562 / 512	210 / 210	304 / 304	398 / 422	133 / 181	256 / 372	334 / 426	94 / 232
10	5 th Street / Arcadia Avenue	Bike HAWK	- / -	458 / 487	- / -	- / -	77 / 72	- / -	- / -	- / -	- / -	- / -	- / -	- / -
11	5 th Street / Rosemont Boulevard	Signal	134 / 134	454 / 580	180 / 180	249 / 204	549 / 496	210 / 208	228 / 225	350 / 372	350 / 372	250 / 177	460 / 306	460 / 306
12	5 th Street / Craycroft Road	Signal	250 / 398	344 / 632	181 / 210	236 / 199	591 / 414	210 / 210	329 / 329	477 / 418	230 / 230	267 / 299	344 / 433	262 / 299
13	5 th Street / Chantilly Drive	HAWK	- / -	71 / 75	- / -	- / -	257 / 238	- / -	- / -	- / -	- / -	- / -	- / -	- / -
14	5 th Street / Sahuara Avenue	TWSC	87 / 159	87 / 159	87 / 159	287 / 93	287 / 93	287 / 93	91 / 33	91 / 33	91 / 33	39 / 46	39 / 46	39 / 46
15	5 th Street / Wilmot Road	Signal	148 / 214	172 / 297	126 / 236	140 / 119	140 / 119	140 / 119	343 / 323	395 / 356	311 / 314	230 / 216	335 / 390	190 / 132

Note: light gray cells indicate specific movement queues that are anticipated to exceed existing storage length during either peak hour.

As shown in Table 19, there are multiple calculated maximum queue lengths that exceed the existing storage length. These typically occur for turning movements along the corridor. However, the westbound through movement queue length at the Tucson Boulevard/6th Street intersection exceeds the available storage lengths and extends beyond the adjacent upstream signalized intersection during the AM peak period.

2022 ROAD DIET CORRIDOR ANALYSIS

TRAVEL TIME ANALYSIS

The corridor travel time was calculated for both the eastbound and westbound directions. The travel time was analyzed in three segments, Campbell Avenue to Country Club Road, Country Club Road to Alvernon Way, and Alvernon Way to Wilmot Road. Both the Campbell to Country Club and Country Club to Alvernon segments are one mile in length. While the Alvernon to Wilmot segment is three miles long. The calculated segment travel time was used to estimate the segment LOS. A summary of the travel time findings for each scenario can be found in Table 20 and Table 21.

Table 20. Eastbound Corridor Travel Time – 2022 Road Diet Scenarios A and B

Segment	2022 Road Diet Scenario A AM		2022 Road Diet Scenario A PM		2022 Road Diet Scenario B AM		2022 Road Diet Scenario B PM	
	Travel Time (s)	LOS	Travel Time (s)	LOS	Travel Time (s)	LOS	Travel Time (s)	LOS
Campbell to Country Club	310.7	D	1020.5	F	290.4	D	1006.5	F
Country Club to Alvernon	178.9	C	197.1	D	179.1	C	211.4	D
Alvernon to Wilmot	549.9	C	621.4	C	557.3	C	551.4	C
Total	1039.5	D	1839.0	D	1026.8	D	1769.3	D

Table 21. Westbound Corridor Travel Time – 2022 Road Diet Scenarios A and B

Segment	2022 Road Diet Scenario A AM		2022 Road Diet Scenario A PM		2022 Road Diet Scenario B AM		2022 Road Diet Scenario B PM	
	Travel Time (s)	LOS	Travel Time (s)	LOS	Travel Time (s)	LOS	Travel Time (s)	LOS
Wilmot to Alvernon	616.8	C	611.2	C	630.3	C	612.5	C
Alvernon to Country Club	252.5	E	204.8	D	216.7	D	188.0	D
Country Club to Campbell	561.5	E	323.8	D	370.6	E	257.1	D
Total	1430.8	D	1139.8	D	1217.6	D	1057.6	D

The tables above indicate that the travel time along the corridor is approximately seventeen to 31 minutes under 2022 Road Diet Conditions. Additionally, the eastbound Scenario A travel time during the PM peak period experiences the longest travel time along the corridor, with an 86% travel time increase compared to the Scenario A existing conditions. This is primarily due to a 236% increase in travel time from Campbell to Country Club. The peak directions, eastbound in the PM and westbound in the AM experience the highest increase in travel time particularly west of Country Club Road.

SIDE STREET DELAY

A typical stop-controlled side street was modeled using the conservative volumes (Scenario A). The side street was modeled in Scenario A existing conditions to determine the projected delay for vehicles making a left onto 6th Street and 5th Street. The modeled typical side street was assumed to have 50 vehicles making a left turn movement from both the north and southbound approach. These volumes were determined based on traffic counts collected at the stop-controlled intersections where HAWK crossings are provided.

Table 22. Scenario A Existing Conditions Side Street Delay

Side Street	Side Street Approach	AM		PM	
		Delay (s)	LOS	Delay (s)	LOS
6 th Side Street	NB	114.5	F	>600	F
	SB	58.4	F	>600	F
5 th Side Street	NB	42.3	E	45.8	E
	SB	42.4	E	42.3	E

Under 2022 Scenario A Road Diet Conditions, the control delay for side street traffic to make a left-turn maneuver onto 6th/5th Street operate below acceptable levels as the stop-control movements operate at LOS E and F. Side Street delay on 5th Street is within a range that may be seen as acceptable for drivers making a maneuver onto a major street as it does not operate at LOS F.

VEHICLES DENIED ENTRY/REROUTED TRAFFIC

The vehicles denied entry or rerouted traffic to the system is a measure of the potential impacts the road diet would have on the adjacent arterials. If the demand exceeds capacity, drivers will reroute along adjacent arterials to reach their destination. Based on the review of the traffic volumes along the corridor, it was determined that the PM peak hour experiences significantly more traffic. As such, the denied vehicles entry was analyzed for the PM peak hour for both Scenario A and Scenario B. The vehicles denied on 6th/5th Street indicate vehicles traveling east-west that would likely reroute to parallel east-west roadways. Vehicles denied entry on cross streets represent vehicles traveling north-south that could not enter the network due to congestion at the intersections crossing 6th/5th Street these include vehicles making turnings movements onto 6th/5th Street in addition to vehicles making through movement on the cross street. Table 23 provides the denied vehicles entry for both 2022 Road Diet PM Peak Hour Conditions.

Table 23. 2022 Road Diet Denied Vehicles Entry

Vehicles Denied Entry	2022 Road Diet Scenario A PM Peak Hour	2022 Road Diet Scenario B PM Peak Hour
On 6 th /5 th Street	206	54
On Cross-Street	93	6

The rerouted east-west traffic from 6th/5th Street is projected to utilize the Broadway Boulevard and Speedway Boulevard corridors. Speedway Boulevard runs parallel to 6th/5th Street half a mile to the north. It is a six-lane divided roadway. Based on PAG M2S segment count data taken October 23, 2019, Speedway Boulevard has a daily volume of 45,389, PM Peak Hour volume of 3,452 and PM Peak Hour Peak Direction volume of 1,881. Broadway Boulevard runs parallel to 6th/5th Street half a mile to the south. It is a six-lane divided roadway. Based on PAGs M2S count data taken September 5, 2018, Broadway Boulevard has a daily volume of 31,661, PM Peak Hour volume of 2,480 and PM Peak Hour Peak Direction volume of 1,320. Both roadways operate under capacity for a six-lane divided roadway. Florida Department of Transportation 2020 Quality/Level of Service Handbook⁹ indicates that for six-lane divided urban roadways with a speed limit under 35 MPH to be LOS D they should have less than 4,500 peak hour two-way trips.

⁹ Florida Department of Transportation. "Quality/Level of Service Handbook," Published 2020.

Using this metric both Speedway Boulevard and Broadway Boulevard could increase the PM peak hour volume to accommodate rerouted traffic from 6th/5th Street and still operate at an acceptable LOS.

TRANSIT IMPACTS

Kittelson used the SimTraffic models to evaluate travel times along 6th/5th Street during AM and PM peak hours. These travel times are estimated for vehicular traffic along the corridor and do not account for bus dwell time at a stop. However, the comparison of travel times by scenario is sufficient to compare the effects of the lane reduction and the potential roadway improvements. Table 24 and Table 25 show the corridor travel time increases that were calculated as part of the 2022 Road Diet Analysis.

Table 24. Travel Time Differential Between Existing and 2022 Road Diet Travel Time Eastbound

Segment	TT Increase (s) Scenario A AM	TT Increase (s) Scenario A PM	TT Increase (s) Scenario B AM	TT Increase (s) Scenario B PM
Campbell to Country Club	48.2	716.9	31.1	721.1
Country Club to Alvernon	30.8	35.2	26.1	52.0
Alvernon to Wilmot	35.0	100.2	13.9	36.6
Total	114.0	852.3	71.1	809.7

Table 25. Travel Time Differential Between Existing and 2022 Road Diet Travel Time Westbound

Segment	TT Increase (s) Scenario A AM	TT Increase (s) Scenario A PM	TT Increase (s) Scenario B AM	TT Increase (s) Scenario B PM
Wilmot to Alvernon	4.0	27.7	-8.8	13.0
Alvernon to Country Club	79.9	33.2	41.9	17.9
Country Club to Campbell	302.9	96.3	139.9	46.9
Total	386.8	157.2	173.0	77.8

The above tables highlight the projected increase in travel time along the corridor if a road diet were to be implemented. The transit route along the corridor could potentially experience an increase in travel time along the corridor of 14 minutes during the PM peak hour in the eastbound direction.

LEFT-TURN LANE AND LEFT-TURN ARROW WARRANT ANALYSIS

Left-turn lanes have been proposed at all signalized intersections as part of the road diet concept prepared for 6th/5th Street. Left turn lane warrants were verified using the City of Tucson 2021 Complete Streets Design Guide¹⁰. Left turn lanes are warranted at all signalized intersections.

Left turn arrow warrants were assessed using the ADOT Traffic Engineering Guidelines and Processes (TGP)¹¹. The hourly opposing through volume and hourly left-turn volumes were analyzed at the conservative peak hour (Scenario A) for considerations of protected left turns. The number of left turn crashes were also analyzed at each intersection for consideration of protected left turns. The following intersections were analyzed for the need of a permissive-protective left turn because the current signal timing phasing for left turns at these intersections is permitted:

- **Tucson Boulevard**
 - AM, PAG TMC
 - (55 left turns per hour) x (459 opposing vehicles per hour) = 25,245

¹⁰ City of Tucson "Street Design Guide," Adopted November 23, 2021.

¹¹ Arizona Department of Transportation "Traffic Engineering Guidelines and Processes Section 612," Adopted June 2015.

- PM, PAG TMC
 - $(136 \text{ left turns per hour}) \times (715 \text{ opposing vehicles per hour}) = 97,240$
- 8 Left Turn crashes over 5 years = 1.6 left turn crashes per year
- **Dodge Boulevard**
 - AM, PAG TMC
 - $(98 \text{ left turns per hour}) \times (461 \text{ opposing vehicles per hour}) = 45,178$
 - PM, PAG TMC
 - $(171 \text{ left turns per hour}) \times (701 \text{ opposing vehicles per hour}) = 119,871$
 - 2 Left Turn crashes over 5 years = 0.4 left turn crashes per year
- **Columbus Boulevard**
 - AM, PAG TMC
 - $(43 \text{ left turns per hour}) \times (493 \text{ opposing vehicles per hour}) = 21,199$
 - PM, PAG TMC
 - $(62 \text{ left turns per hour}) \times (705 \text{ opposing vehicles per hour}) = 43,710$
 - 11 Left Turn crashes over 5 years = 2.2 left turn crashes per year
- **Rosemont Boulevard**
 - AM, PAG TMC
 - $(79 \text{ left turns per hour}) \times (729 \text{ opposing vehicles per hour}) = 57,591$
 - PM, PAG TMC
 - $(93 \text{ left turns per hour}) \times (559 \text{ opposing vehicles per hour}) = 51,987$
 - 3 Left Turn crashes over 5 years = 0.6 left turn crashes per year
- **Wilmot Road**
 - AM, PAG TMC
 - $(133 \text{ left turns per hour}) \times (62 \text{ opposing vehicles per hour}) = 8,246$
 - PM, PAG TMC
 - $(399 \text{ left turns per hour}) \times (32 \text{ opposing vehicles per hour}) = 12,768$
 - 11 Left Turn crashes over 5 years = 2.2 left turn crashes per year

Appendix L displays the ADOT Left Turn Arrow Warrant Chart based on Traffic Volumes and the Left Turn Crash Experience Chart based on crashes used for the turn arrow determination at each intersection.

For **Tucson Boulevard** at 6th Street and **Dodge Boulevard** at 5th Street, a permissive-protective left turn is **warranted in the PM Peak**. The hourly left turn lane volume and opposing hourly volume exceeded the 2-lane urban street cross-product value of 75,000. A protected left turn is not warranted since the left turn crashes do not exceed 4 left turn crashes per year on average.

For **Columbus Boulevard, Rosemont Boulevard, and Wilmot Road along 5th Street, protective-permissive left turns are not warranted**. The hourly left turn lane volumes and opposing hourly volumes did not exceed the 2-lane urban street cross-product value of 75,000.

RIGHT TURN LANE WARRANT ANALYSIS

The addition of right turn lanes at intersections may increase capacity and enhance operations. Right turn lane warrants were assigned based on the 2021 Street Design Guide for the City of Tucson. The hourly major-road volume for one direction and hourly right-turn volumes were analyzed at the conservative peak hour (Scenario A). The speed along the major roads, 6th Street and 5th Street, was 30 mph. The following intersections were analyzed for the need of a right turn lane as the conceptual road diet layout did not provide a right turn lane:

- **Campbell Avenue, WB approach**
 - Conservative peak hour: PM, PAG TMC
 - 85 right turns per hour, 880 major-road vehicles per hour
- **Tucson Boulevard, WB & EB approaches**

- o Conservative peak hour: PM, Collected Counts TMC
- o WB: 87 right turns per hour, 1030 major-road vehicles per hour
- o EB: 45 right turns per hour, 1140 major-road vehicles per hour
- **Dodge Boulevard, EB approach**
 - o Conservative peak hour: PM, PAG TMC
 - o 140 right turns per hour, 760 major-road vehicles per hour
- **Swan Road, WB approach**
 - o Conservative peak hour: PM, PAG TMC
 - o 90 right turns per hour, 820 major-road vehicles per hour
- **Sahuara Avenue, WB & EB approaches**
 - o Conservative peak hour: AM, Collected Counts TMC
 - o WB: 11 right turns per hour, 556 major-road vehicles per hour
 - o EB: 42 right turns per hour, 431 major-road vehicles per hour

Appendix M displays the Right Turn Lane Guidelines for Two-Lane Roadways Chart used for the turn lane determination at each intersection.

Based on the parameters listed above for each intersection, **WB Campbell Avenue, WB & EB Tucson Boulevard, EB Dodge Boulevard, and WB Swan Road** warrant a right turn lane.

ROAD DIET FEASIBILITY ANALYSIS

To determine the feasibility of a road diet based on the weekday segment volume profiles, the Federal Highway Administration (FHWA) Road Diet Feasibility Criteria was used. Specifically, the City of Seattle Modeling Flow Chart for Road Diet Feasibility Determination was applied to each segment in a comparison for the weekday collected daily volume profiles and the PAG daily volume profiles.

The highest daily volume segment along 6th Street is from Campbell Avenue to Tucson Boulevard with 17,424 vehicles for the volume collected on August 23rd, 2022.

Figure 27 demonstrates the flowchart process of road diet determination from **Campbell Avenue to Tucson Boulevard**.

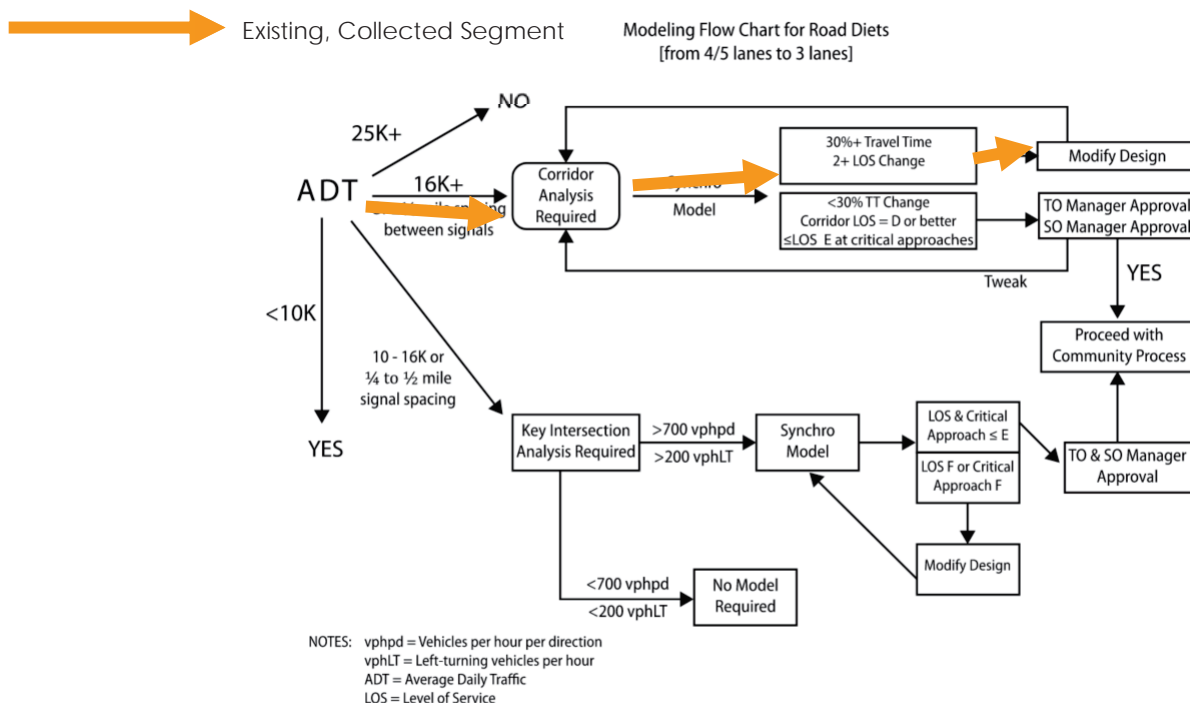


Figure 27. Road Diet Feasibility Flow Chart – Campbell Ave to Tucson Blvd

Based on the Road Diet Feasibility flowchart analysis and the travel time increase of over 30% projected for the segment from Campbell Avenue to Tucson Boulevard, a road diet with the current design is not recommended. Per the flow chart, the design for this portion of the road diet should be modified.

The highest daily volume segment along 5th Street is from Country Club Road to Alvernon Way with 16,739 vehicles for the conservative volume collected on August 23rd, 2022.

Figure 28 demonstrates the flowchart process of road diet determination from **Country Club Road to Alvernon Way**.

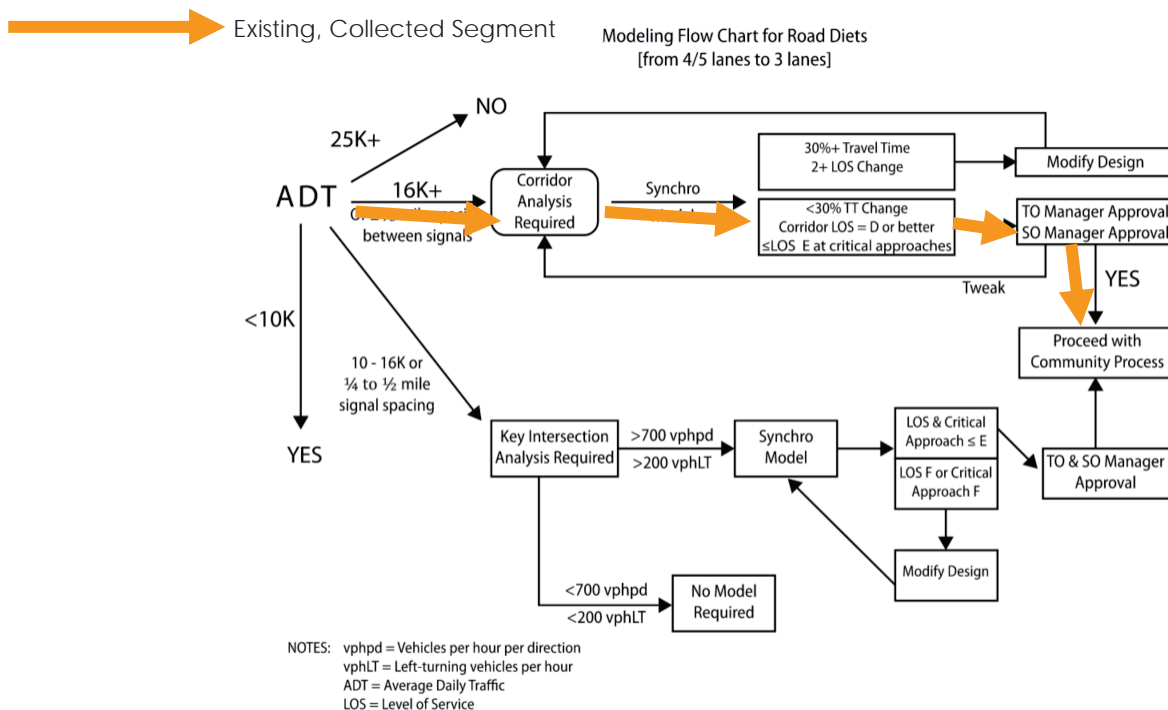


Figure 28. Road Diet Feasibility Flow Chart – Country Club Rd to Alvernon Wy

Based on the Road Diet Feasibility flowchart analysis, a road diet is feasible from Country Club Road to Alvernon Way along 5th Street based on the collected daily volume segment.

All remaining FHWA Road Diet Feasibility flow chart determinations for the 2022 Road Diet analysis can be found in *Appendix N*.

The City of Tucson has additional considerations for Road Diet feasibility described in the 2021 Complete Streets Design Guide. The City of Tucson guidelines are based on daily and peak hour volumes. The segment determinations as outlined in the Complete Streets guide are provided below:

Daily Volumes:

- Campbell Avenue to Tucson Boulevard Segment = 17,424 vehicles
 - **Consideration:** Good candidate but capacity may be affected
- Country Club Road to Alvernon Way Segment = 16,739 vehicles
 - **Consideration:** Good candidate but capacity may be affected
- Swan Road to Rosemont Boulevard Segment = 13,377 vehicles
 - **Consideration:** Good candidate but intersection analysis may be needed
- Sahuara Avenue to Wilmot Road Segment = 9,784 vehicles
 - **Consideration:** Strong candidate

Peak Hour Volumes (Collected Daily Volumes):

AM Peak

- Campbell Avenue to Tucson Boulevard Segment (7:30 AM)
 - Westbound = 950 vehicles
 - **Consideration:** Road redesign will be difficult, requiring additional study
- Country Club Road to Alvernon Way Segment (7:30 AM)
 - Westbound = 895 vehicles
 - **Consideration:** Road redesign will be difficult, requiring additional study
- Swan Road to Rosemont Boulevard Segment (8:00 AM)
 - Eastbound = 687 vehicles
 - **Consideration:** Feasible
- Sahuara Avenue to Wilmot Road Segment (7:30 AM)
 - Westbound = 534 vehicles
 - **Consideration:** Feasible

PM Peak

- Campbell Avenue to Tucson Boulevard Segment (4:45 PM)
 - Eastbound = 952 Vehicles
 - **Consideration:** Road redesign will be difficult, requiring additional study
- Country Club Road to Alvernon Way Segment (4:30 PM)
 - Eastbound = 933 vehicles
 - **Consideration:** Road redesign will be difficult, requiring additional study
- Swan Road to Rosemont Boulevard Segment (4:30 PM)
 - Eastbound = 751 Vehicles
 - **Consideration:** Caution
- Sahuara Avenue to Wilmot Road Segment (4:30 PM)
 - Eastbound = 580 vehicles
 - **Consideration:** Feasible



Section 6

Year 2045 Traffic Analysis

YEAR 2045 TRAFFIC ANALYSIS

Kittelson analyzed the year 2045 traffic conditions to determine potential impacts a road diet might have on the corridor in the long-term.

2045 TRAFFIC PROJECTIONS

Pima Association of Governments (PAG) provided Kittelson with the 2045 PAG travel demand model run for the study area. PAG uses an activity-based model to predict travel behavior and the resulting travel demand for future time frames. The PAG travel demand model projects a modest growth rate of 0.5% per year between 2022 and 2045. Review of historic segment counts along the corridor indicate that traffic along 6th/5th Street has been slightly declining in recent years. However, the PAG travel demand model indicates a minimal amount of growth on the corridor can occur from 2022 to 2045. To provide a comprehensive analysis, the 2045 traffic projection volumes were used to evaluate 2045 future conditions.

Year 2045 forecast turning movement volumes were developed through application of procedures outlined in National Cooperative Highway Research Program (NCHRP) *Report 255 Highway Traffic Data for Urbanized Area Project Planning and Design*¹². These procedures used year 2022 traffic counts and traffic volume projections provided by the PAG regional travel demand model for the years 2019 and 2045.

2045 INTERSECTION OPERATIONS

Kittelson used the SimTraffic traffic model to evaluate traffic operations at intersections during AM and PM peak hours. Table 26 shows the projected intersection operations for existing conditions under Scenario A for both the AM and PM peak hours. The 2045 Traffic Conditions SimTraffic reports, and 2045 Road Diet Feasibility Analysis are provided in *Appendix O*.

¹² National Cooperative Highway Research Program. NCHRP *Report 255: Highway Traffic Data for Urbanized Area Project Planning and Design*. 1982.

Table 26. 2045 Intersection Traffic Operations

No	Intersection	2045 No Build Scenario A (AM/PM)	2045 Road Diet Scenario A (AM/PM)	2045 No Build Scenario B (AM/PM)	2045 Road Diet Scenario B (AM/PM)
1	6 TH Street / Campbell Avenue	F / F	F / F	E / E	E / F
2	6 TH Street / Wilson Avenue	A / A	A / A	A / A	A / A
3	6 TH Street / Tucson Boulevard	C / D	F / F	C / C	F / D
4	6 TH Street / Country Club Road	C / D	D / D	C / C	D / C
5	5 TH Street / Dodge Boulevard	A / B	C / B	B / B	B / E
6	5 TH Street / Alvernon Way	D / E	F / F	D / E	F / F
7	5 TH Street / Irving Circle	A / A	A / A	A / A	A / A
8	5 TH Street / Columbus Boulevard	B / B	D / D	B / C	C / C
9	5 TH Street / Swan Road	C / D	F / F	C / C	D / D
10	5 TH Street / Arcadia Avenue	A / A	C / A	A / A	B / A
11	5 TH Street / Rosemont Boulevard	C / C	E / D	B / B	C / C
12	5 TH Street / Craycroft Road	D / D	E / F	C / C	D / D
13	5 TH Street / Chantilly Drive	A / A	A / A	A / A	A / A
14	5 TH Street / Sahuara Avenue	A / A	A / A	A / A	A / A
15	5 TH Street / Wilmot Road	D / C	D / C	C / C	C / C

Note: Dark grey cells indicate intersections that are expected to operate at LOS E or F during either peak hour

Under 2045 conditions (Scenario A and Scenario B), the no-build and road diet alternatives are projected to have the intersections of Campbell Avenue/6th Street and Alvernon Way/5th Street operating at unacceptable LOS. Additionally, the intersection of Tucson Boulevard/6th Street, Dodge Boulevard/5th Street, and Swan Road/5th Street are projected to operate at unacceptable LOS for Scenario A (high volume scenario).

2045 TRAVEL TIME ANALYSIS

The corridor travel time was calculated for both the eastbound and westbound directions. The travel time was analyzed in three segments, Campbell Avenue to Country Club Road, Country Club Road to Alvernon Way, and Alvernon Way to Wilmot Road. Both the Campbell to Country Club and Country Club to Alvernon segments are one mile in length. While the Alvernon to Wilmot segment is three miles long. The calculated segment travel time was used to estimate the segment LOS. A summary of the travel time findings for each scenario can be found in Table 27 and Table 28.

Table 27. Eastbound Corridor Travel Time in Seconds – 2045 Traffic Conditions

Segment	2045 No Build Scenario A (s) (AM/PM)	2045 Road Diet Scenario A (AM/PM)	2045 No Build Scenario B (AM/PM)	2045 Road Diet Scenario B (AM/PM)
Campbell to Country Club	296.2 / 387.8	323.0 / 1097.9	274.2 / 346.4	306.1 / 864.0
Country Club to Alvernon	147.8 / 200.9	286.7 / 222.7	153.3 / 172.3	198.8 / 439.8
Alvernon to Wilmot	527.9 / 564.7	1350.8 / 657.5	541.6 / 511.7	562.6 / 564.9
Total	971.9 / 1153.4	1956.5 / 1978.1	969.1 / 1030.4	1067.5 / 1868.7

Table 28. Westbound Corridor Travel Time in Seconds – 2045 Traffic Conditions

Segment	2045 No Build Scenario A (AM/PM)	2045 Road Diet Scenario A (AM/PM)	2045 No Build Scenario B (AM/PM)	2045 Road Diet Scenario B (AM/PM)
Wilmot to Alvernon	619.3 / 618.6	774.2 / 633.7	636.5 / 585.1	754.5 / 633.7
Alvernon to Country Club	173.4 / 197.9	276.3 / 214.7	175.3 / 172.1	238.8 / 204.1
Country Club to Campbell	309.6 / 241.9	579.9 / 299.0	246.0 / 237.1	467.2 / 280.5
Total	1102.3 / 1058.4	1630.4 / 1147.4	1057.8 / 994.3	1460.5 / 1118.3

The tables above indicate that the travel time along the corridor is approximately sixteen to 36 minutes under 2045 Traffic Conditions. Similarly, to the 2022 travel times, it is projected that the road diet will increase travel times along the corridor particularly in the eastbound direction.



Section 7

Special Event Traffic Analysis

SPECIAL EVENT TRAFFIC ANALYSIS

The west end of the study corridor borders the University of Arizona, a large traffic generator during periods where classes are in session. In addition, high traffic volumes and congestion are observed during special events at the University. A special event traffic analysis on the 6th/5th Street corridor was conducted to evaluate the traffic impacts of a home football game at the Arizona Stadium.

The traffic analysis was conducted on the home opener football game where the University of Arizona played host to Mississippi State university. The game took place on Saturday, September 10th, 2022, and the stadium gates opened at 6:30 p.m. ahead of the 8:00 p.m. kickoff. The attendance of the game was 80% of the stadium's capacity at 46,275. The University of Arizona has a gameday traffic management plan in which traffic along 6th Street before National Championship Lane to the west is restricted. The traffic is mainly directed to Cherry Avenue Garage that is directly adjacent to the Arizona Football Stadium. There is also the South Stadium Garage on the south side of 6th Street at National Championship Lane / Warren Avenue – see Figure 29.

After the football game, most of the traffic out of the Cherry Avenue Garage exits north towards Speedway Boulevard. The remaining traffic exits from the south towards 6th Street. At the South Stadium Garage, some traffic exits toward the east at Campbell Avenue and the majority of the vehicles exit north towards 6th Street. The traffic on 6th Street is directed to through and right turn movements only and the left turn lane is restricted on the west side of Campbell Avenue. The traffic on the east side of Campbell Avenue is restricted to only left- and right-turning movements. The details of the University of Arizona traffic management plan can be seen in the figure below. The full special event traffic plan is provided in *Appendix P*.

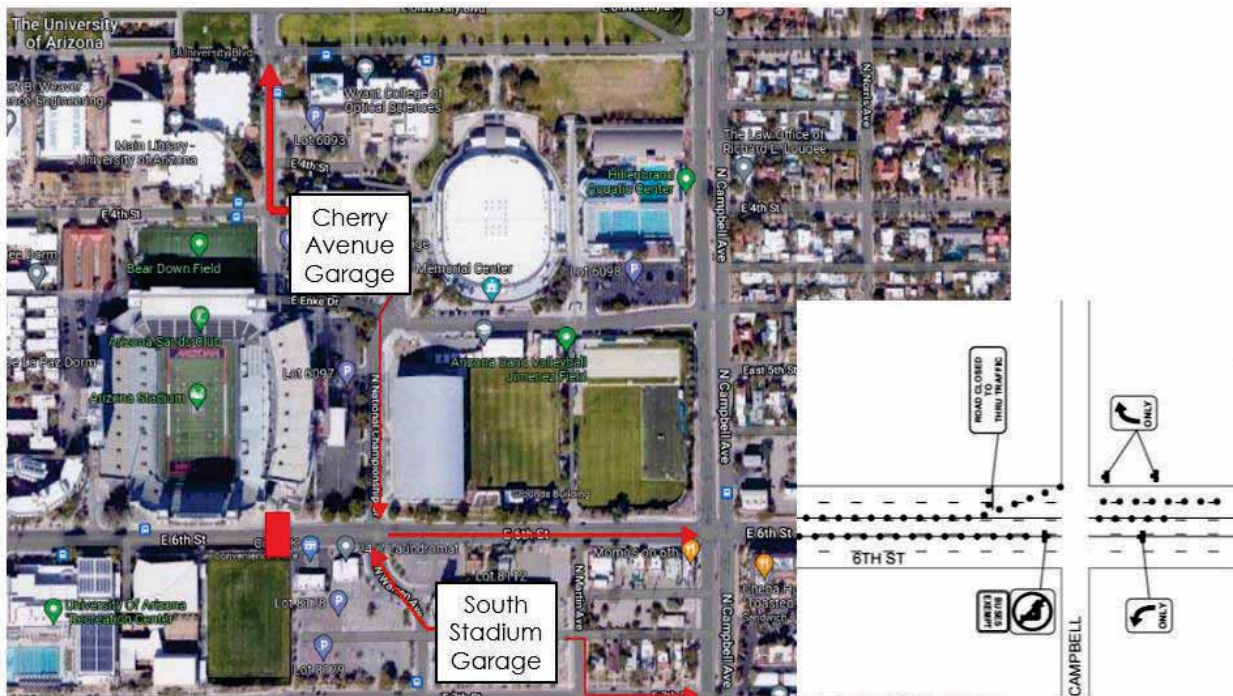


Figure 29. University of Arizona Special Event Traffic Plan

To determine the special event traffic generation, volumes and turning movement counts previously collected before the gameday were compared to volumes and turning movement counts collected during the game. Daily volume profiles were collected for various segments along the corridor. Turning

movement counts were collected when most of the traffic was exiting at the end of the football game from approximately 9:00 p.m. to 12:00 a.m.

The following TMCs were collected for comparison of gameday traffic to no gameday traffic:

- No gameday traffic, 8/23/2022 (Tuesday)
 - 6th Street/Campbell Avenue TMC, Weekday AM peak period
 - 6th Street/Campbell Avenue TMC, Weekday PM peak period
- Gameday traffic, 9/10/2022 (Saturday)
 - 6th Street/Campbell Avenue TMC, 9:00 p.m. – 12:00 a.m.

The following daily volume segments were collected for a Saturday comparison of gameday traffic (9/10/2022) to no gameday traffic (8/27/2022):

- 6th Street at Campbell Avenue
- 6th Street at Tucson Boulevard
- 5th Street at Country Club Road
- 5th Street at Swan Road
- 5th Street at Craycroft Road

Overall, the volumes stayed consistent throughout the corridor during the Saturday gameday and Saturday with no game. *Appendix Q* shows the daily traffic for each of the segments during the Saturday gameday and no game day comparisons.

Figure 30 below displays the overall comparison of volume profile by peak hours for the study corridor.

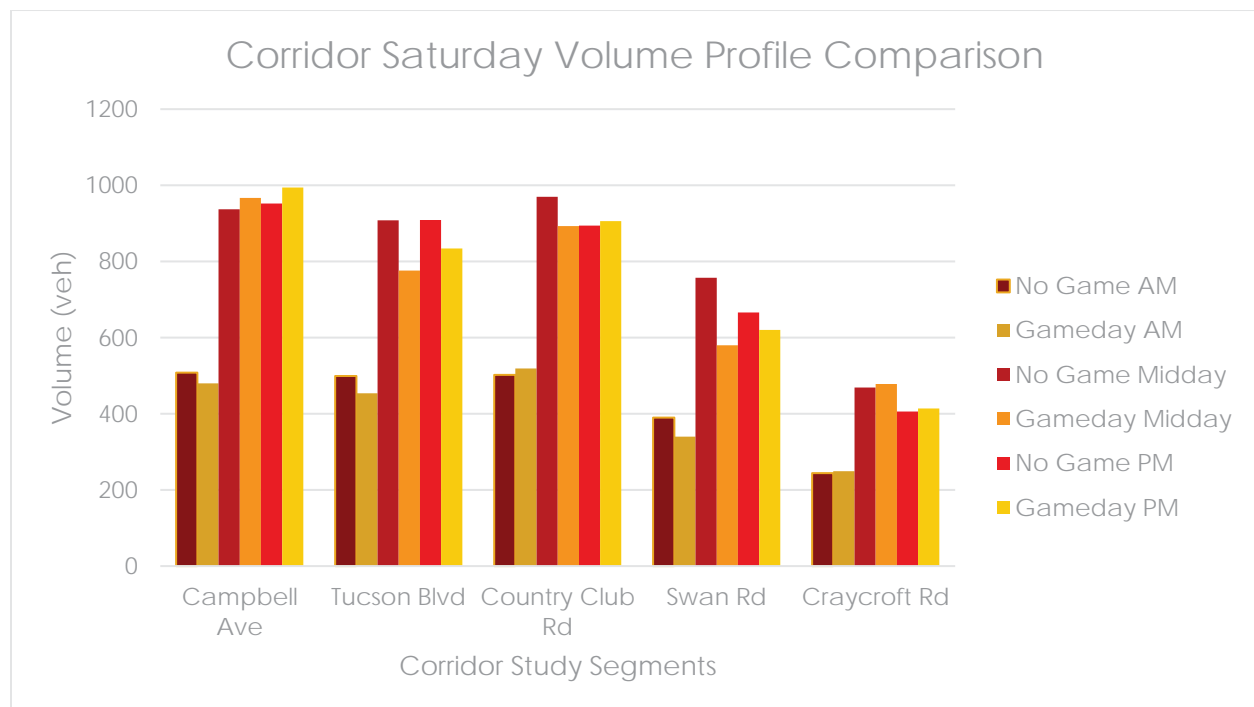


Figure 30. Corridor Saturday Volume Profile Comparison

Some key volume variations include:

- Campbell Ave Segment: Overall 1% decrease (-193 veh) in gameday traffic as compared to Saturday with no game day

- Tucson Blvd Segment: Overall 14% decrease (-1,579 veh) in gameday traffic as compared to Saturday with no game day
- Country Club Rd Segment: Overall 0.3% decrease (-37 veh) in gameday traffic as compared to Saturday with no game day
- Swan Rd Segment: Overall 4% decrease (-366 veh) in gameday traffic as compared to Saturday with no game day
- Craycroft Rd Segment: Overall 1% increase (+81 veh) in gameday traffic as compared to Saturday with no game day

The special event traffic counts indicate that volumes during a typical Saturday and special event Saturday are similar with little variation. The study roadway segments on the west side of the corridor are subject to higher fluctuations of traffic dependent on University of Arizona special events. Figure 31 shows the typical Saturday and special event Saturday traffic volumes for the 6th Street/Campbell area.

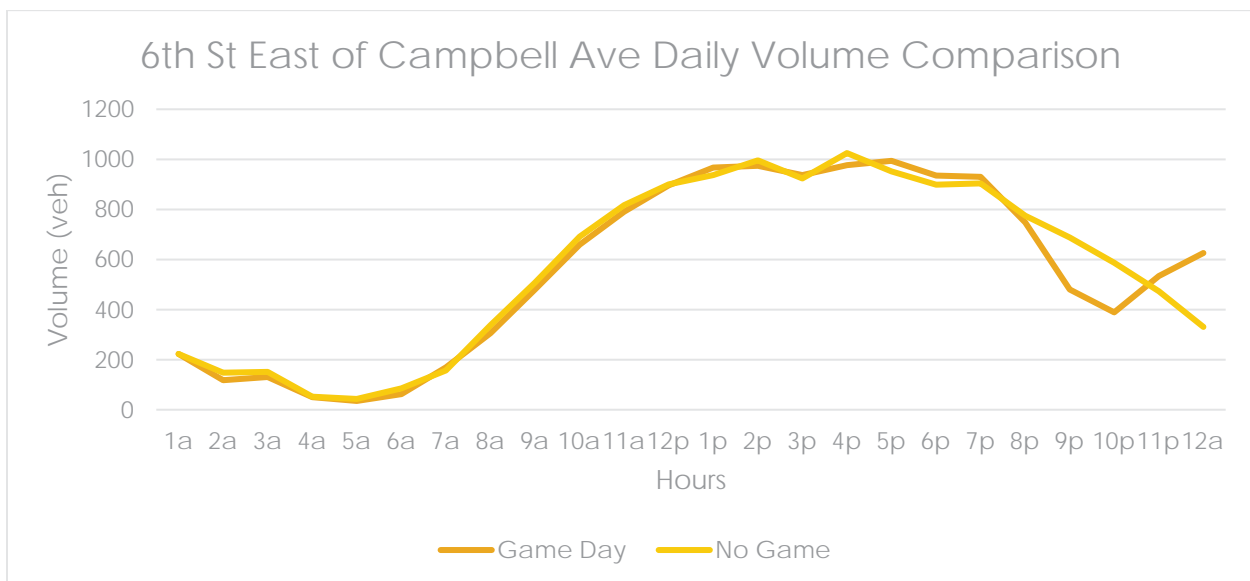


Figure 31. 6th Street at Campbell Road Daily Traffic Comparison

As shown in the figure above, the traffic volumes during a special event are very similar to the typical Saturday for the majority of the day. However, during the special event while the University of Arizona Traffic plan is in affect there is a significant drop in segment traffic (7:00 pm -10:00 pm in Figure 31 for the segment between Campbell Avenue and Tucson Boulevard). Additionally, post-game traffic increases the volume of traffic utilizing the corridor compared to a typical Saturday. Although the traffic volumes are higher during the post special event time period, the post-game traffic volume is still lower than the highest volumes observed during the day under both typical and special event traffic.

Figure 32 shows the typical Saturday and special event Saturday traffic volumes along the roadway segment east of Craycroft Boulevard. As shown in Figure 32, special event traffic dissipates before reaching the end of the corridor.

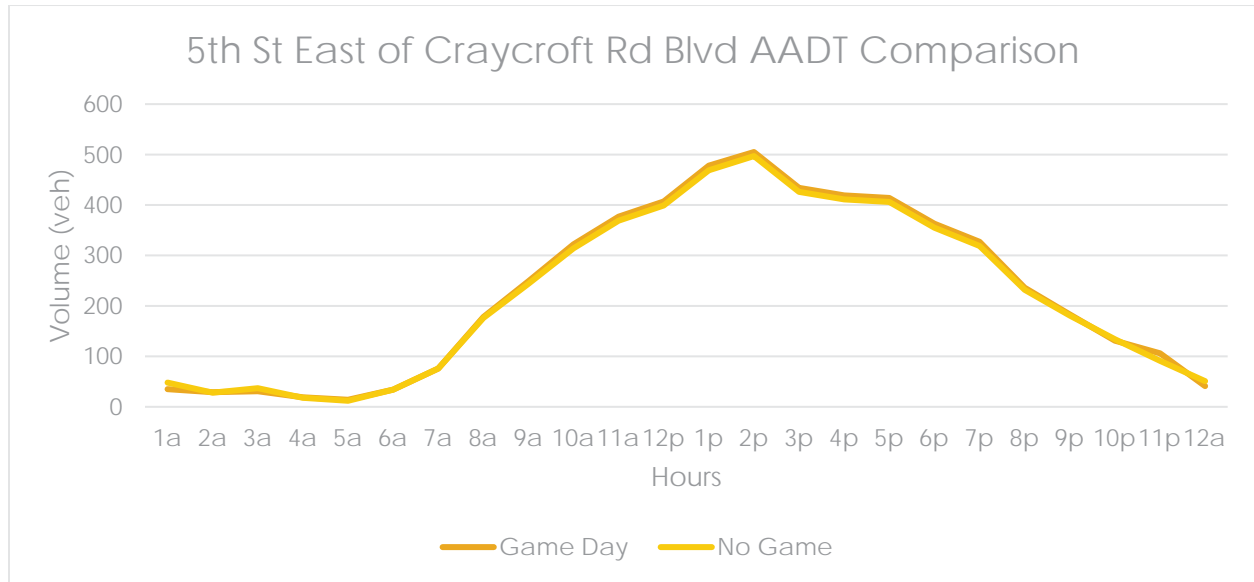


Figure 32. 5th Street at Craycroft Road Daily Traffic Comparison

SPECIAL EVENT ROAD DIET FEASIBILITY

To determine the feasibility of a road diet based on the Saturday game day and Saturday with no game volume profiles, the Federal Highway Administration (FHWA) Road Diet Feasibility Criteria was used. Specifically, the City of Seattle Modeling Flow Chart for Road Diet Feasibility Determination was applied to each segment in a comparison for the Saturday gameday and Saturday no game volume profiles.

Figure 33 demonstrates the flowchart process of road diet determination at **Campbell Avenue**. This represents the highest volume observed on 6th Street for the Saturday counts.

- Scenario X – Saturday, No Game
- Scenario Y – Saturday, Gameday

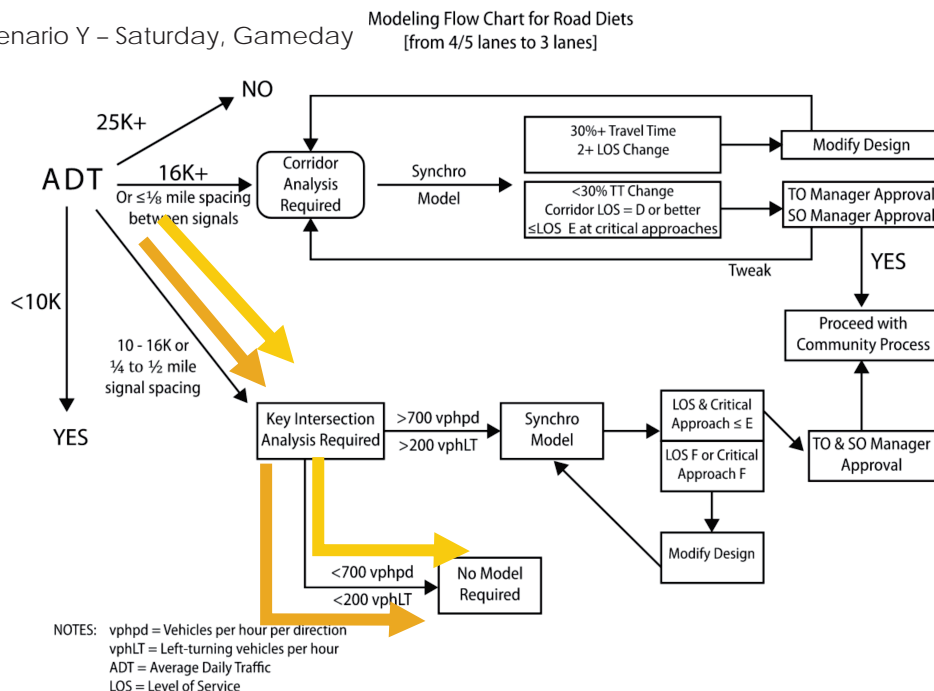


Figure 33. Road Diet Feasibility Flow Chart – Campbell Avenue

Based on the Road Diet Feasibility flowchart analysis, a road diet is feasible on Saturdays at Campbell Avenue and 6th Street.

Figure 34 demonstrates the flowchart process of road diet determination at **Country Club Road**. This represents the highest volume observed on 5th Street for the Saturday counts.

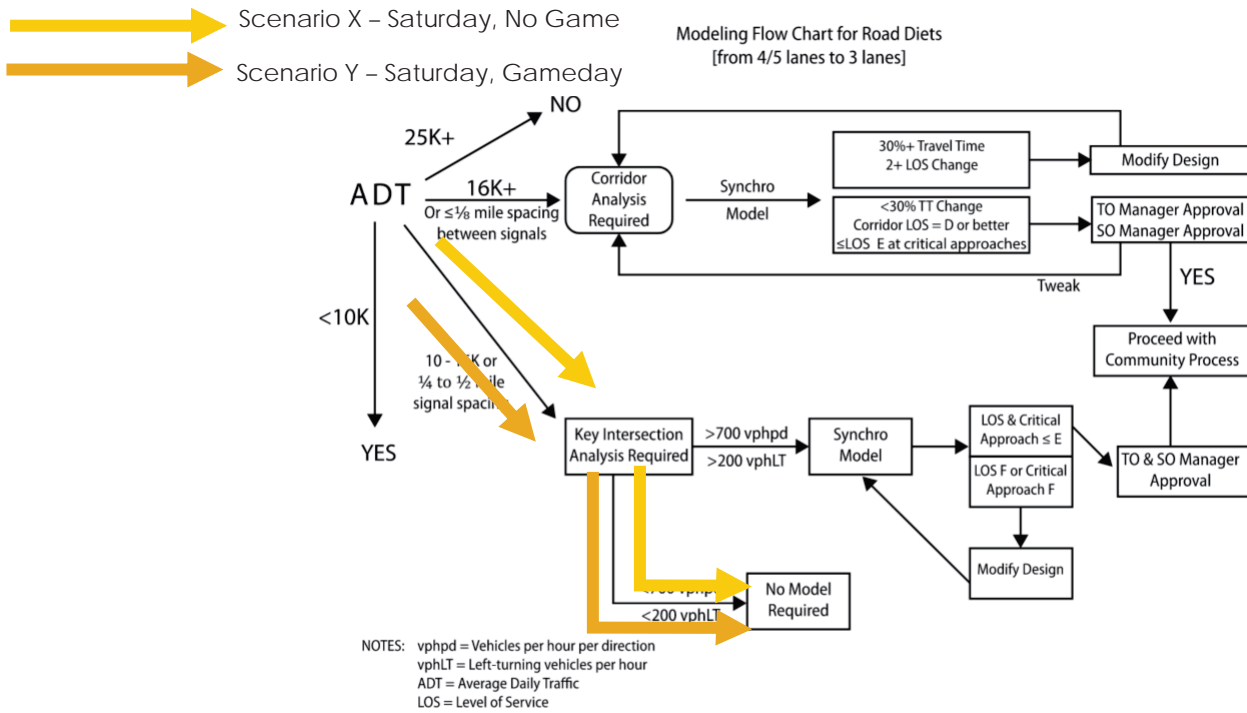


Figure 17. City of Seattle Modeling Flow Chart for Road Diet Feasibility Determination

Figure 34. Road Diet Feasibility Flow Chart – Country Club Road

Based on the Road Diet Feasibility flowchart analysis, a road diet is feasible on Saturdays east of Country Club Road and 5th Street.



Section 8

Findings and Recommendations

FINDINGS AND RECOMMENDATIONS

The results of the traffic analysis indicate that the proposed road diet is feasible along select sections of the study roadway. The findings of this analysis and recommendations are discussed below.

FINDINGS

FOUR-LANE CROSS SECTION (NO-BUILD)

Level of Service

- The majority of the study intersections were found to operate at acceptable LOS (LOS D or better) during 2022 weekday AM and PM peak hours with the exception of:
 - **6th Street/Campbell Avenue in the AM and PM Peak Hours (Scenario A – High Volume)**

Queue Length

- Several left and right turn calculated maximum queue lengths are expected to exceed the existing storage capacity under Scenario A and Scenario B. The calculated maximum queue lengths would extend into the through lane, but none extend beyond the upstream signalized intersection.
- The northbound and southbound through movement calculated maximum queues at the **6th Street/Campbell Avenue** intersection would extend beyond the upstream signalized intersection under Scenario A (High Volume) conditions.

Travel Time

- All study roadway segments were found to operate at acceptable LOS during the No-Build weekday AM and PM peak hours.

Side Street Delay

- The 6th and 5th Street typical side street intersections were found to operate at acceptable LOS during No-Build weekday AM and PM peak hours.

Bicycle and Pedestrian Experience

- Along the study corridor in the No-Build Alternative (existing cross-section) **NO DESIGNATED BICYCLE INFRASTRUCTURE** is provided.
- Cyclists along the corridor may utilize the vehicular travel lanes, with no dedicated infrastructure to enhance cyclist experience.
- Pedestrians crossing the 6th/5th Street corridor have to cross 4/5 lanes of traffic with vehicular speeds exceeding the speed limit

TWO LANE CROSS SECTION (ROAD DIET)

Level of Service

- The majority of the study intersections are anticipated to operate at acceptable LOS during 2022 weekday AM and PM peak hours except for:
 - **6th Street/Campbell Avenue in the PM peak hour (Both Scenarios)**
 - **6th Street/Tucson Boulevard in the AM and PM peak hours (Both Scenarios)**
 - **5th Street/Alvernon Way in the PM peak hour (Scenario A)**

- **5th Street/Swan Road in the PM peak hour (Scenario A)**
- **5th Street/Craycroft in the PM peak hour (Scenario A)**

Queue Length

- Several left and right turn calculated maximum queue lengths would exceed the existing storage lengths under both Scenario A and Scenario B. These maximum queues would extend into the through lane, but none extend beyond the upstream signalized intersection.
- The eastbound through movement (Both Scenarios) and southbound through movement (Scenario A) calculated maximum queues at the **6th Street/Campbell Avenue** intersection would extend beyond the upstream signalized intersection.
- The westbound through movement (Scenario A) and southbound through movement (Both Scenarios) calculated maximum queues at the **6th Street/ Tucson Boulevard** intersection would extend beyond the upstream signalized intersection.

Travel Time

- The majority of the of the Road Diet roadway segments are expected to operate at acceptable LOS with during the weekday AM and PM peak hours except for:
 - Eastbound Campbell Avenue to Country Club Road (Both Scenarios)
 - Westbound Country Club Road to Campbell Avenue (Both Scenarios)
 - Westbound Alvernon Way to Country Club Road (Scenario A)

Side Street Delay

- The 6th/5th Street typical side street intersections are expected to operate at unacceptable LOS during Road Diet weekday AM and PM peak hours.
 - **6th Street Typical Side Street in the AM and PM Peak Hours – LOS F (Both Scenarios)**
 - **5th Street Typical Side Street in the AM and PM Peak Hours – LOS E (Both Scenarios)**

Vehicles Denied Entry

- Simtraffic simulation projected the following vehicle denied entry volumes (i.e. rerouted traffic) traveling east-west along 6th/5th Street during the weekday PM peak hour:
 - 206 Vehicles (Scenario A)
 - 54 Vehicles (Scenario B)

Road Diet Feasibility

- The majority of the 6th/5th Street corridor segments were found to meet the FHWA Road Diet Feasibility criteria with the exception of:
 - **6th Street from Campbell Avenue to Country Club Road**
- All the 6th/5th Street corridor segments were found to be a good or strong candidate for a road redesign under the Tucson Complete Streets Design Guide criteria for daily traffic volumes.
- The majority of the 6th/5th Street corridor segments were found to be a feasible candidate based on the City of Tucson Complete Streets Design Guide criteria for peak hour peak direction volumes except for:
 - **6th Street from Campbell Avenue to Tucson Boulevard (AM and PM peak hours)**
 - Road redesign will be difficult, requiring additional study
 - **5th Street from Country Club Road to Alvernon Way (AM and PM peak hours)**
 - Road redesign will be difficult, requiring additional study
 - **5th Street from Swan to Rosemont Boulevard (PM peak hour)**
 - Caution

Bicycle Experience

- The Road Diet alternative would provide dedicated bicycle infrastructure including:
 - 6th Street from Campbell Avenue to Country Club Road: 5' Painted Bike Lane
 - 5th Street from Country Club Road to Wilmot Road: 5' Painted Bike Lane with 2' Buffer

RECOMMENDATIONS

A comparison matrix was prepared to visualize similarities and differences between the No-Build Alternative (current four lane cross-section) and the Road Diet Alternative (lane redesign to accommodate bicycle lanes). Table 29 provides a comparison of the alternatives.

Table 29. 6th/5th Street Alternatives Comparison

Criteria	6 th Street		5 th Street	
	No-Build	Road Diet	No Build	Road Diet
Multimodal Safety	✓	✓	✓	✓
Pedestrian/Cyclist Experience	✗	✓	✗	✓
Vehicle/Transit Operations	✓	✗	✓	✗
Side Street Circulation	✓	✗	✓	✓
FHWA Road Diet Feasibility		✗		✓

Note: **Green** indicates the scenario meets and exceeds criteria, **yellow** indicates scenario meets criteria and **red** indicates the scenario does not meet criteria.

Road Diet Recommendations

The results of the traffic analysis including the FHWA Road Diet Feasibility criteria indicate that a road diet should not be implemented along the entire 6th/5th Street corridor from Campbell Avenue to Wilmot Road. However, the corridor segments east of Country Club Road along 5th Street operates at acceptable LOS and meets the FHWA Road Diet Feasibility criteria under Road Diet conditions. Based on this analysis conducted for this study, the following is recommended for the Road Diet Alternative:

- 6th Street from Campbell Avenue to Country Club Road: **DO NOT PROCEED** with the community outreach process
- 5th Street from Country Club Road to Wilmot Road: **PROCEED** with the community outreach process.

In addition to completing a community outreach program, **the following actions should be completed prior to the implementation of a road diet:**

- Revise conceptual plan sets to accommodate projected queue lengths within storage lanes; may require median modifications and right-of-way acquisition
- Revise conceptual layout to provide all warranted right turn lanes; may require right-of-way acquisition
- Update cycle lengths at all intersections, except for HAWK crossings, along the corridor to 130 seconds
- Update signal timing plans to include warranted permitted-protected left turn phases
- Coordinate with SunTran to provide information on possible transit impacts and modifications

- Coordinate with Mail and Waste Collection services to minimize impact to these services

IMPLEMENTATION ASSESSMENT

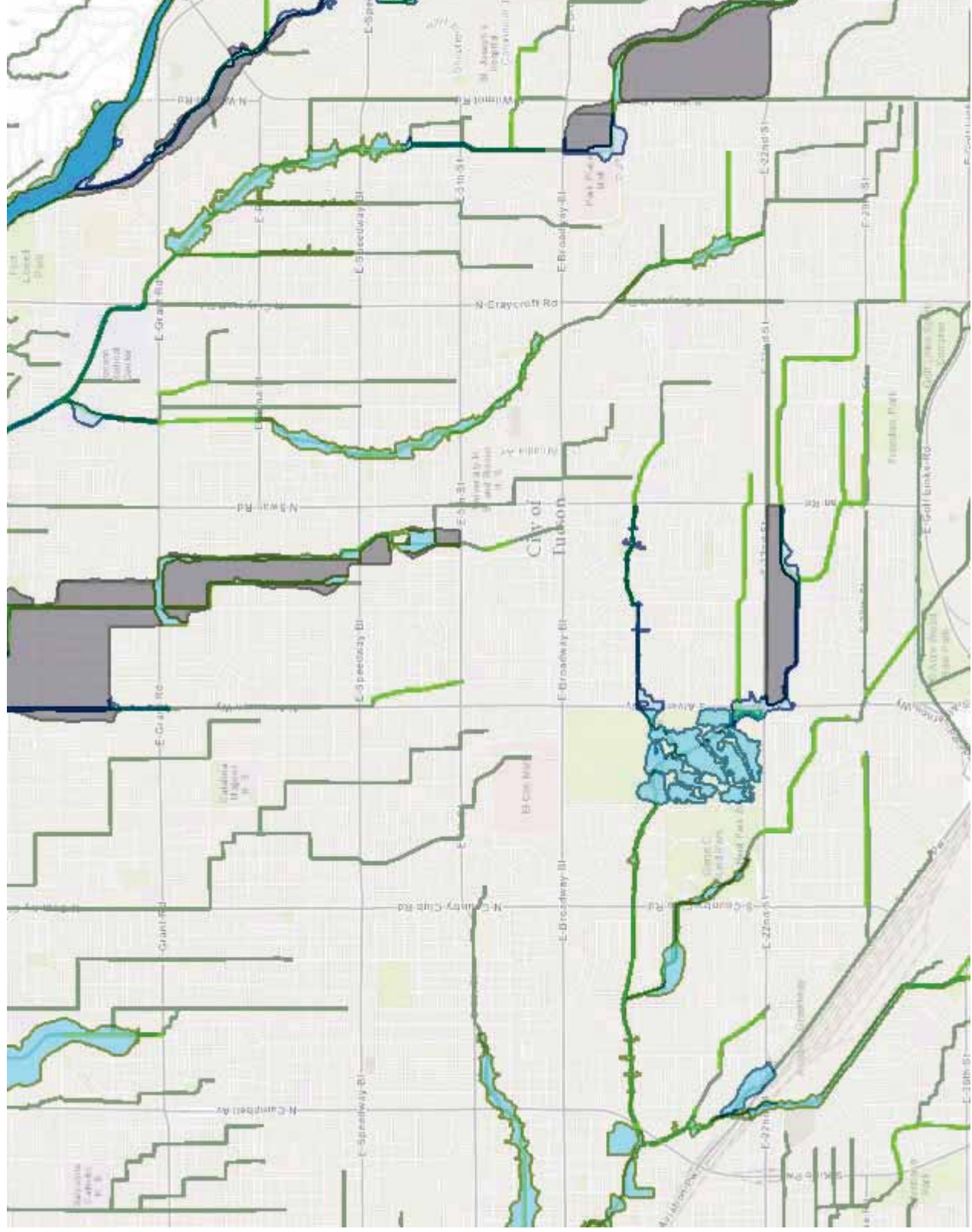
If a road diet is implemented along 5th Street, the following assessment is suggested:

- Pre-Implementation and Post-Implementation Safety Performance Analysis (Before and After Crash Analysis)
- Pre-Implementation and Post-Implementation Multimodal Usage at the Segment and Intersection level (Before and After Volume Comparison)
- Pre-Implementation and Post-Implementation Travel Time and Operating Speed Analysis
- Post-Implementation Transit Services Assessment
- Post-Implementation Mail and Trash Assessment
- Reevaluation of 6th Street Road Diet from Campbell Avenue to Country Club Road (if traffic patterns and volumes change due to 5th Street Road Diet implementation)



Appendix A Floodplain Map

6th/5th Street Floodplain Data



Legend

- A Zones
- ZONE A
- ZONE AE
- ZONE AH
- ZONE AO
- X Zones
- Areas of 100-year
- Areas of 500-year
- Floodway
- Street Washes
- Washes
- Light Gray Canvas
- Reference

Notes

Notes

This map is a user generated static output from an Internet mapping site and is for reference only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable.
THIS MAP IS NOT TO BE USED FOR NAVIGATION

1.7 Miles

0.87

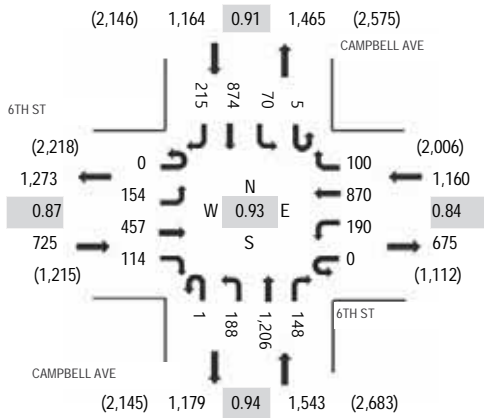
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1.7

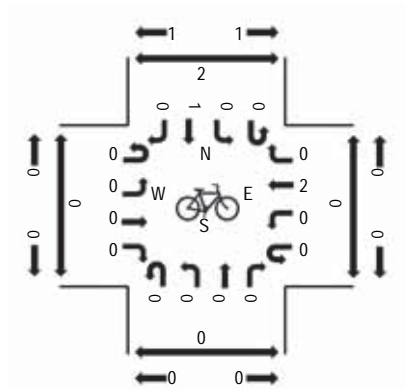


Appendix B Traffic Count Data

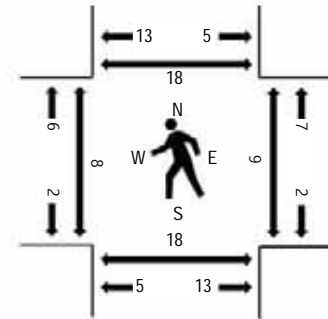
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

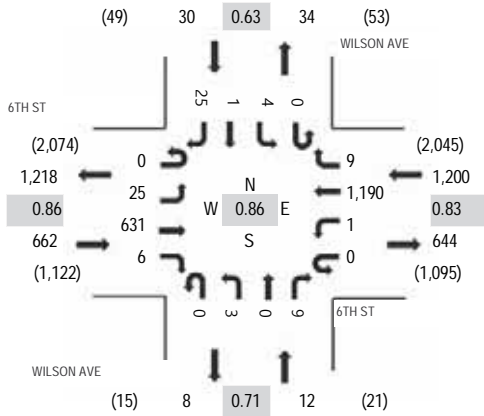


Note: Total study counts contained in parentheses.

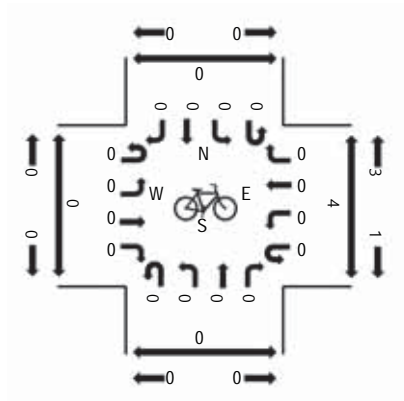
Traffic Counts - Motorized Vehicles

Interval Start Time	6TH ST Eastbound				6TH ST Westbound				CAMPBELL AVE Northbound				CAMPBELL AVE Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	17	35	10	0	32	90	10	0	23	179	14	0	13	165	39	627	3,906	2	3	3	4
7:15 AM	0	27	56	11	0	30	141	19	0	40	288	32	0	9	194	47	894	4,470	1	0	2	0
7:30 AM	0	31	106	16	0	45	231	23	0	63	292	18	0	16	240	63	1,144	4,592	2	3	10	4
7:45 AM	0	41	123	26	0	67	255	25	0	43	336	38	2	19	210	56	1,241	4,350	4	4	4	4
8:00 AM	0	45	130	37	0	35	192	30	0	42	324	48	2	19	231	56	1,191	4,144	2	2	3	5
8:15 AM	0	37	98	35	0	43	192	22	1	40	254	44	1	16	193	40	1,016		0	0	1	5
8:30 AM	0	40	101	27	0	61	196	20	0	23	176	14	1	21	176	46	902		1	4	3	3
8:45 AM	0	34	99	33	0	36	192	19	0	40	280	31	0	12	191	68	1,035		2	5	2	4
Count Total	0	272	748	195	0	349	1,489	168	1	314	2,129	239	6	125	1,600	415	8,050		14	21	28	29
Peak Hour	0	154	457	114	0	190	870	100	1	188	1,206	148	5	70	874	215	4,592		8	9	18	18

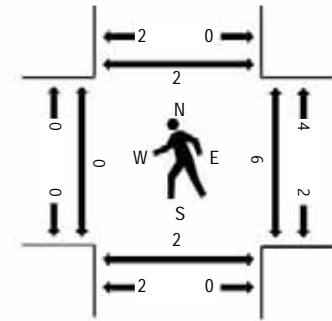
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

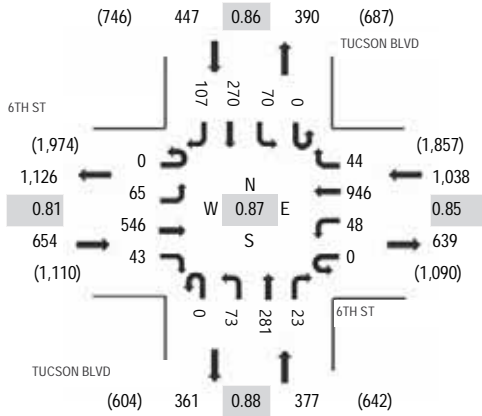
Traffic Counts - Motorized Vehicles

Interval Start Time	6TH ST Eastbound				6TH ST Westbound				WILSON AVE Northbound				WILSON AVE Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	1	63	2	0	0	136	2	0	0	0	1	0	1	1	4	211	1,586	0	3	2	1
7:15 AM	0	2	92	0	0	0	226	2	0	0	0	0	0	0	0	1	323	1,814	0	1	0	0
7:30 AM	0	2	138	1	0	0	352	2	0	1	0	0	0	0	0	3	499	1,904	0	1	0	0
7:45 AM	0	9	168	3	0	0	361	1	0	1	0	1	0	2	0	7	553	1,821	0	4	0	0
8:00 AM	0	6	191	1	0	0	232	2	0	0	0	3	0	0	0	4	439	1,651	0	0	0	0
8:15 AM	0	8	134	1	0	1	245	4	0	1	0	5	0	2	1	11	413		0	1	2	2
8:30 AM	0	6	150	2	0	0	243	1	0	3	0	3	0	1	0	7	416		0	4	0	2
8:45 AM	0	4	137	1	0	0	234	1	0	0	0	2	0	1	1	2	383		0	4	1	1
Count Total	0	38	1,073	11	0	1	2,029	15	0	6	0	15	0	7	3	39	3,237		0	18	5	6
Peak Hour	0	25	631	6	0	1	1,190	9	0	3	0	9	0	4	1	25	1,904		0	6	2	2

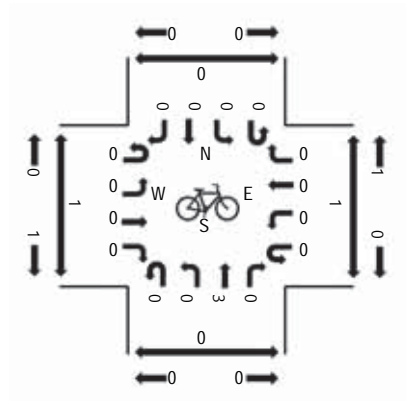


Location: 3 TUCSON BLVD & 6TH ST AM
 Date: Tuesday, August 23, 2022
 Peak Hour: 07:30 AM - 08:30 AM
 Peak 15-Minutes: 07:45 AM - 08:00 AM

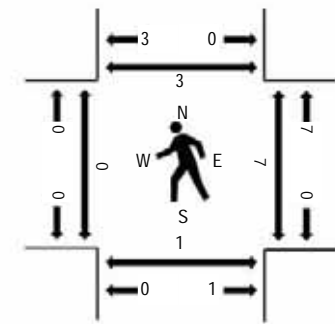
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	6TH ST Eastbound				6TH ST Westbound				TUCSON BLVD Northbound				TUCSON BLVD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	2	59	5	0	3	117	6	0	8	37	3	0	5	27	13	285	2,036	1	0	0	0
7:15 AM	0	10	84	5	0	8	210	10	0	10	43	4	0	11	21	13	429	2,361	0	0	0	0
7:30 AM	0	9	114	14	0	9	260	8	0	17	68	3	0	17	52	30	601	2,516	0	3	1	1
7:45 AM	0	16	149	7	0	17	277	12	0	19	83	6	0	13	84	38	721	2,507	0	2	0	0
8:00 AM	0	24	165	15	0	11	202	8	0	15	63	6	0	22	66	13	610	2,319	0	0	0	0
8:15 AM	0	16	118	7	0	11	207	16	0	22	67	8	0	18	68	26	584		0	2	0	2
8:30 AM	0	19	122	7	0	13	211	12	0	9	74	6	0	21	76	22	592		6	1	1	0
8:45 AM	0	16	113	14	0	11	208	10	0	6	58	7	0	16	53	21	533		0	1	0	1
Count Total	0	112	924	74	0	83	1,692	82	0	106	493	43	0	123	447	176	4,355		7	9	2	4
Peak Hour	0	65	546	43	0	48	946	44	0	73	281	23	0	70	270	107	2,516		0	7	1	3



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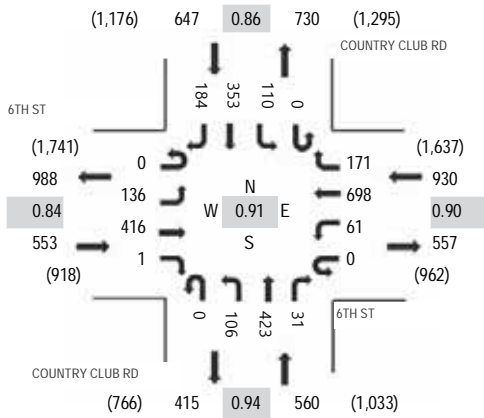
Location: 4 COUNTRY CLUB RD & 6TH ST AM

Date: Tuesday, August 23, 2022

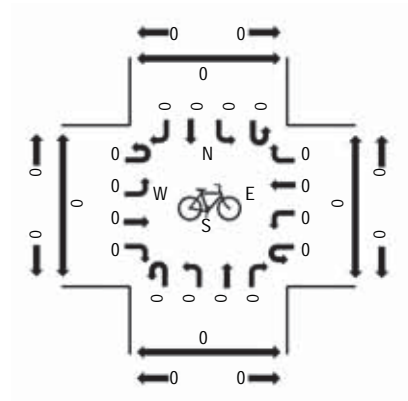
Peak Hour: 07:30 AM - 08:30 AM

Peak 15-Minutes: 07:45 AM - 08:00 AM

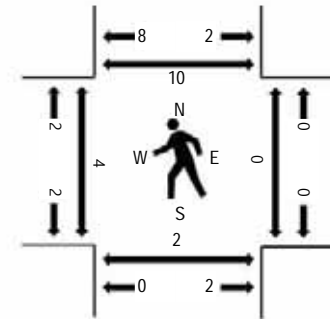
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	6TH ST Eastbound				6TH ST Westbound				COUNTRY CLUB RD Northbound				COUNTRY CLUB RD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	11	46	0	0	5	86	18	0	14	72	6	0	11	56	21	346	2,273	0	0	0	2
7:15 AM	0	19	62	0	0	8	155	31	0	26	91	13	0	11	70	36	522	2,575	0	0	0	0
7:30 AM	0	27	98	1	0	16	201	42	0	30	111	7	0	20	67	50	670	2,690	3	0	2	7
7:45 AM	0	46	104	0	0	15	176	47	0	32	105	12	0	34	101	63	735	2,651	1	0	0	1
8:00 AM	0	36	128	0	0	15	165	42	0	25	94	7	0	24	86	26	648	2,491	0	0	0	0
8:15 AM	0	27	86	0	0	15	156	40	0	19	113	5	0	32	99	45	637		0	0	0	2
8:30 AM	0	18	100	0	0	10	162	42	0	26	93	7	0	30	107	36	631		0	0	1	0
8:45 AM	0	26	83	0	0	13	125	52	0	23	92	10	0	26	82	43	575		0	0	0	1
Count Total	0	210	707	1	0	97	1,226	314	0	195	771	67	0	188	668	320	4,764		4	0	3	13
Peak Hour	0	136	416	1	0	61	698	171	0	106	423	31	0	110	353	184	2,690		4	0	2	10



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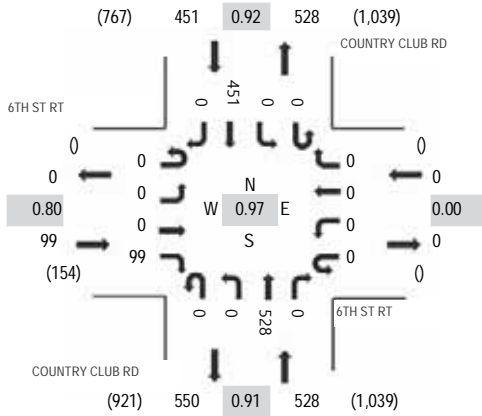
Location: 5 COUNTRY CLUB RD & 6TH ST RT AM

Date: Tuesday, August 23, 2022

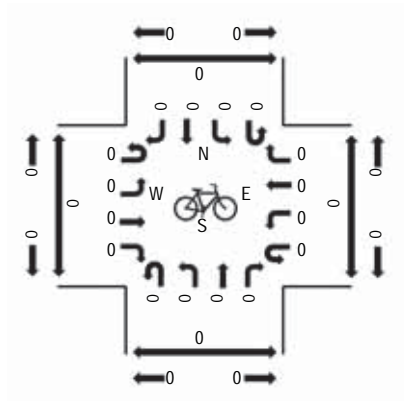
Peak Hour: 07:45 AM - 08:45 AM

Peak 15-Minutes: 07:45 AM - 08:00 AM

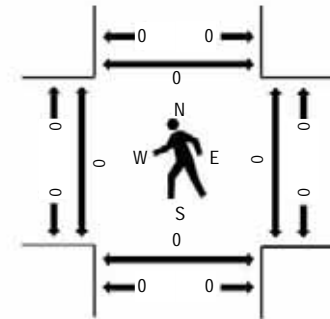
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	6TH ST RT Eastbound				6TH ST RT Westbound				COUNTRY CLUB RD Northbound				COUNTRY CLUB RD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	0	0	8	0	0	0	0	0	0	87	0	0	0	61	0	156	901	1	0	0	0
7:15 AM	0	0	0	11	0	0	0	0	0	0	132	0	0	0	74	0	217	1,008	0	0	0	0
7:30 AM	0	0	0	13	0	0	0	0	0	0	154	0	0	0	83	0	250	1,058	1	0	0	0
7:45 AM	0	0	0	19	0	0	0	0	0	0	143	0	0	0	116	0	278	1,078	0	0	0	0
8:00 AM	0	0	0	32	0	0	0	0	0	0	131	0	0	0	100	0	263	1,059	0	0	0	0
8:15 AM	0	0	0	23	0	0	0	0	0	0	132	0	0	0	112	0	267		0	0	0	0
8:30 AM	0	0	0	25	0	0	0	0	0	0	122	0	0	0	123	0	270		0	0	0	0
8:45 AM	0	0	0	23	0	0	0	0	0	0	138	0	0	0	98	0	259		0	0	0	0
Count Total	0	0	0	154	0	0	0	0	0	0	1,039	0	0	0	767	0	1,960		2	0	0	0
Peak Hour	0	0	0	99	0	0	0	0	0	0	528	0	0	0	451	0	1,078		0	0	0	0



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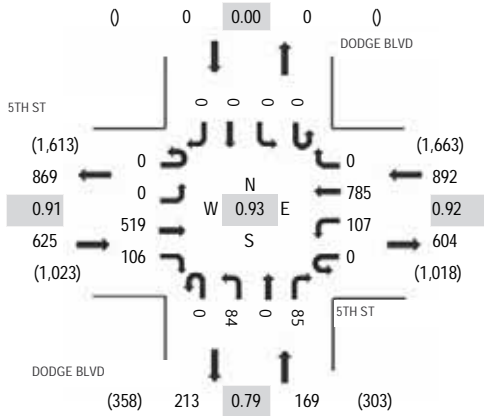
Location: 6 DODGE BLVD & 5TH ST AM

Date: Tuesday, August 23, 2022

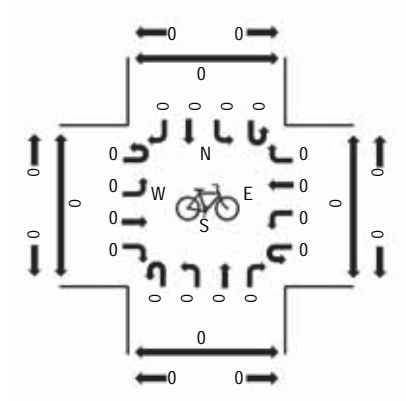
Peak Hour: 07:45 AM - 08:45 AM

Peak 15-Minutes: 07:45 AM - 08:00 AM

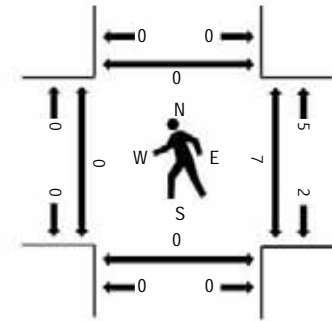
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				DODGE BLVD Northbound				DODGE BLVD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	0	50	12	0	17	111	0	0	9	0	10	0	0	0	0	209	1,388	0	2	0	0
7:15 AM	0	0	76	11	0	17	183	0	0	13	0	24	0	0	0	0	324	1,612	0	1	0	0
7:30 AM	0	0	114	9	0	21	226	0	0	14	0	19	0	0	0	0	403	1,681	0	1	0	0
7:45 AM	0	0	145	19	0	26	228	0	0	16	0	18	0	0	0	0	452	1,686	0	2	0	0
8:00 AM	0	0	140	31	0	27	203	0	0	14	0	18	0	0	0	0	433	1,601	0	4	0	0
8:15 AM	0	0	114	20	0	22	180	0	0	29	0	28	0	0	0	0	393		0	0	0	0
8:30 AM	0	0	120	36	0	32	174	0	0	25	0	21	0	0	0	0	408		0	1	0	0
8:45 AM	0	0	98	28	0	30	166	0	0	22	0	23	0	0	0	0	367		0	1	0	0
Count Total	0	0	857	166	0	192	1,471	0	0	142	0	161	0	0	0	0	2,989		0	12	0	0
Peak Hour	0	0	519	106	0	107	785	0	0	84	0	85	0	0	0	0	1,686		0	7	0	0



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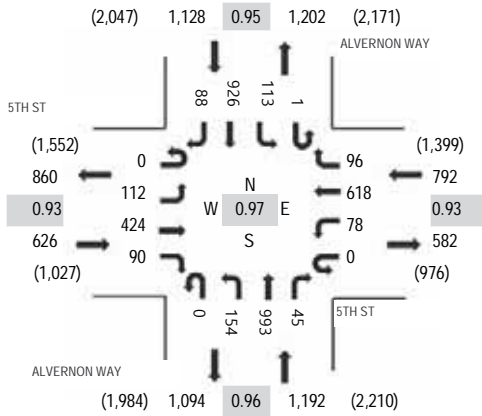
Location: 7 ALVERNON WAY & 5TH ST AM

Date: Tuesday, August 23, 2022

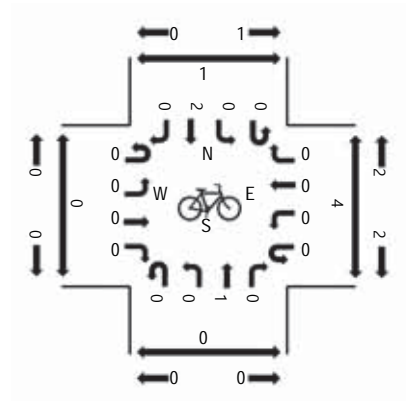
Peak Hour: 07:30 AM - 08:30 AM

Peak 15-Minutes: 08:00 AM - 08:15 AM

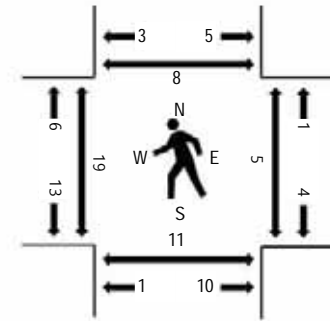
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

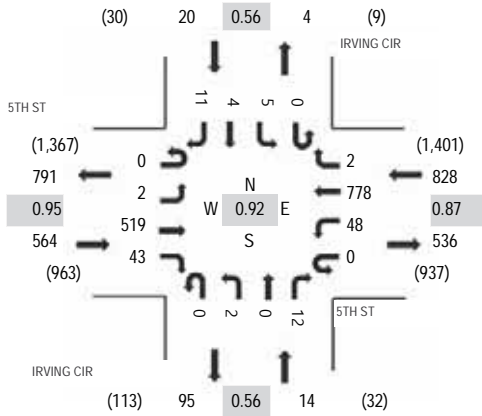


Note: Total study counts contained in parentheses.

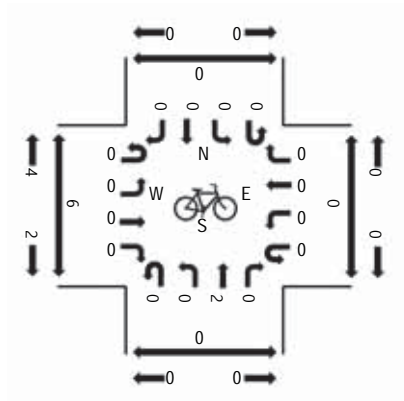
Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				ALVERNON WAY Northbound				ALVERNON WAY Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	12	38	12	0	15	80	3	0	14	173	11	1	9	169	17	554	3,217	2	0	2	4
7:15 AM	0	13	59	8	0	17	141	16	0	35	248	13	0	9	191	19	769	3,625	0	1	0	1
7:30 AM	0	22	112	28	0	23	161	28	0	44	242	6	0	30	234	18	948	3,738	4	1	2	1
7:45 AM	0	41	103	25	0	16	166	17	0	36	259	7	0	29	226	21	946	3,631	6	4	1	0
8:00 AM	0	27	107	19	0	23	148	29	0	37	255	21	1	30	242	23	962	3,466	5	0	4	7
8:15 AM	0	22	102	18	0	16	143	22	0	37	237	11	0	24	224	26	882		4	0	4	0
8:30 AM	0	18	98	16	0	18	134	20	0	38	228	9	0	22	218	22	841		1	0	1	0
8:45 AM	0	16	99	12	0	12	132	19	0	39	202	8	0	19	202	21	781		1	0	0	0
Count Total	0	171	718	138	0	140	1,105	154	0	280	1,844	86	2	172	1,706	167	6,683		23	6	14	13
Peak Hour	0	112	424	90	0	78	618	96	0	154	993	45	1	113	926	88	3,738		19	5	11	8

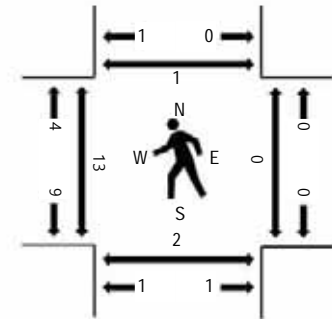
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				IRVING CIR Northbound				IRVING CIR Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	1	58	1	0	0	87	0	0	2	0	3	0	0	0	1	153	1,128	0	0	1	0
7:15 AM	0	0	76	3	0	3	167	0	0	0	0	1	0	2	0	2	254	1,341	0	0	2	0
7:30 AM	0	1	133	4	0	4	233	1	0	1	0	2	0	1	4	4	388	1,426	1	0	1	1
7:45 AM	0	0	137	1	0	11	171	1	0	1	0	6	0	2	0	3	333	1,348	4	0	1	0
8:00 AM	0	0	127	22	0	13	200	0	0	0	0	2	0	1	0	1	366	1,298	6	0	0	0
8:15 AM	0	1	122	16	0	20	174	0	0	0	0	2	0	1	0	3	339		2	0	0	0
8:30 AM	0	0	128	7	0	2	158	3	0	5	1	3	0	3	0	0	310		2	0	0	0
8:45 AM	0	0	125	0	0	2	151	0	0	2	0	1	0	1	0	1	283		2	0	0	1
Count Total	0	3	906	54	0	55	1,341	5	0	11	1	20	0	11	4	15	2,426		17	0	5	2
Peak Hour	0	2	519	43	0	48	778	2	0	2	0	12	0	5	4	11	1,426		13	0	2	1



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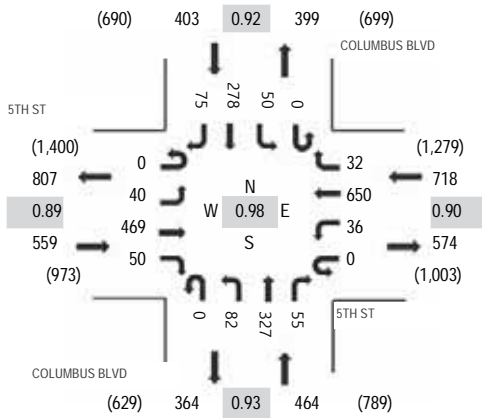
Location: 9 COLUMBUS BLVD & 5TH ST AM

Date: Tuesday, August 23, 2022

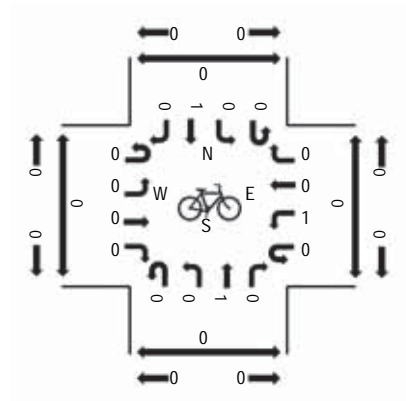
Peak Hour: 07:30 AM - 08:30 AM

Peak 15-Minutes: 07:45 AM - 08:00 AM

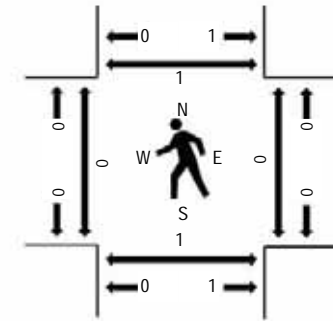
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

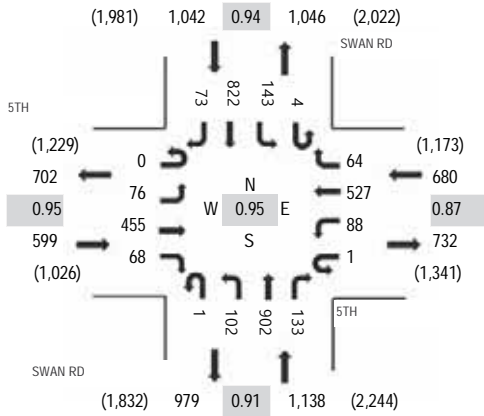
Interval Start Time	5TH ST Eastbound				5TH ST Westbound				COLUMBUS BLVD Northbound				COLUMBUS BLVD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	5	42	1	0	4	70	4	0	11	44	7	0	4	35	8	235	1,717	2	0	0	1
7:15 AM	0	6	75	9	0	5	154	6	0	18	50	4	0	10	51	12	400	2,026	0	0	0	0
7:30 AM	0	9	117	8	0	3	182	8	0	24	79	10	0	8	69	18	535	2,144	0	0	0	0
7:45 AM	0	6	121	15	0	11	151	9	0	18	93	14	0	13	80	16	547	2,092	0	0	1	0
8:00 AM	0	10	105	11	0	13	179	10	0	16	76	16	0	17	70	21	544	2,014	0	0	0	0
8:15 AM	0	15	126	16	0	9	138	5	0	24	79	15	0	12	59	20	518		0	0	0	1
8:30 AM	0	8	112	15	0	11	127	18	0	19	78	11	0	12	58	14	483		0	0	0	0
8:45 AM	0	11	119	11	0	8	140	14	0	11	56	16	0	17	57	9	469		0	0	0	0
Count Total	0	70	817	86	0	64	1,141	74	0	141	555	93	0	93	479	118	3,731		2	0	1	2
Peak Hour	0	40	469	50	0	36	650	32	0	82	327	55	0	50	278	75	2,144		0	0	1	1



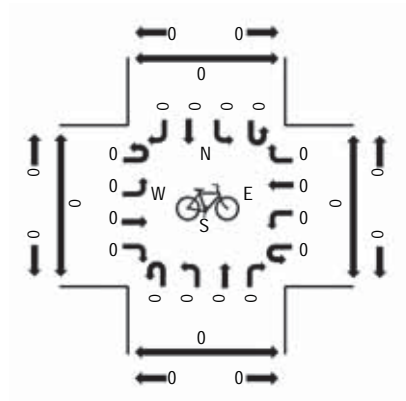
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Location: 10 SWAN RD & 5TH AM
Date: Tuesday, August 23, 2022
Peak Hour: 07:30 AM - 08:30 AM
Peak 15-Minutes: 08:00 AM - 08:15 AM

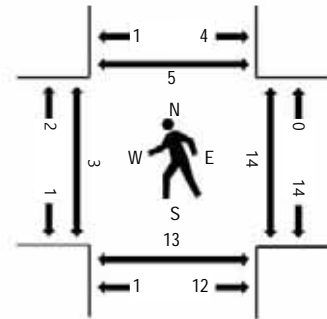
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

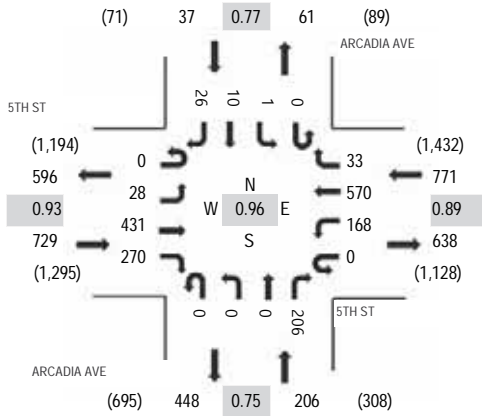
Interval Start Time	5TH Eastbound				5TH Westbound				SWAN RD Northbound				SWAN RD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	7	46	4	0	14	56	10	0	14	168	14	0	19	185	8	545	3,027	0	0	0	0
7:15 AM	0	17	67	9	0	16	115	8	0	18	240	34	0	19	173	16	732	3,394	0	4	4	3
7:30 AM	0	18	121	18	0	23	155	18	0	21	231	36	1	34	203	16	895	3,459	0	0	2	0
7:45 AM	0	28	109	21	0	32	125	11	1	24	207	28	0	40	202	27	855	3,392	1	10	6	2
8:00 AM	0	15	105	19	0	21	140	13	0	29	258	35	1	31	226	19	912	3,397	1	2	0	1
8:15 AM	0	15	120	10	1	12	107	22	0	28	206	34	2	38	191	11	797		1	2	5	2
8:30 AM	0	23	107	15	0	20	100	18	0	25	201	61	0	46	187	25	828		0	2	3	3
8:45 AM	0	16	101	15	0	18	98	20	0	30	243	58	5	37	197	22	860		0	16	11	0
Count Total	0	139	776	111	1	156	896	120	1	189	1,754	300	9	264	1,564	144	6,424		3	36	31	11
Peak Hour	0	76	455	68	1	88	527	64	1	102	902	133	4	143	822	73	3,459		3	14	13	5



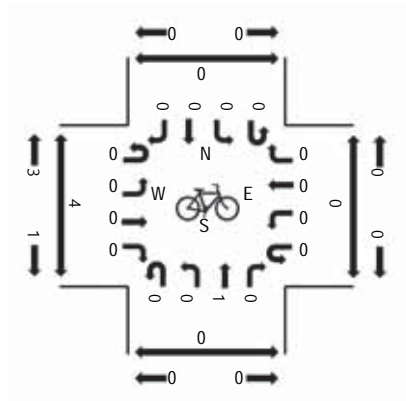
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Location: 11 ARCADIA AVE & 5TH ST AM
Date: Tuesday, August 23, 2022
Peak Hour: 08:00 AM - 09:00 AM
Peak 15-Minutes: 08:30 AM - 08:45 AM

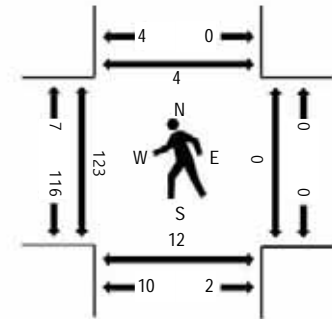
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				ARCADIA AVE Northbound				ARCADIA AVE Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	1	60	23	0	5	83	4	0	3	0	9	0	1	0	2	191	1,363	11	0	1	1
7:15 AM	1	1	73	34	0	14	151	1	0	0	0	28	0	0	0	4	307	1,608	3	0	1	0
7:30 AM	0	2	142	52	0	28	182	7	0	0	0	25	0	2	1	9	450	1,703	24	0	1	2
7:45 AM	0	6	111	60	0	28	152	6	0	0	0	37	0	2	2	11	415	1,707	16	0	0	0
8:00 AM	0	6	99	54	0	38	170	18	0	0	0	37	0	0	0	14	436	1,743	27	0	3	1
8:15 AM	0	7	117	56	0	33	139	8	0	0	0	37	0	1	1	3	402		13	0	4	0
8:30 AM	0	6	101	86	0	54	130	6	0	0	0	63	0	0	5	3	454		45	0	3	3
8:45 AM	0	9	114	74	0	43	131	1	0	0	0	69	0	0	4	6	451		38	0	2	0
Count Total	1	38	817	439	0	243	1,138	51	0	3	0	305	0	6	13	52	3,106		177	0	15	7
Peak Hour	0	28	431	270	0	168	570	33	0	0	0	206	0	1	10	26	1,743		123	0	12	4



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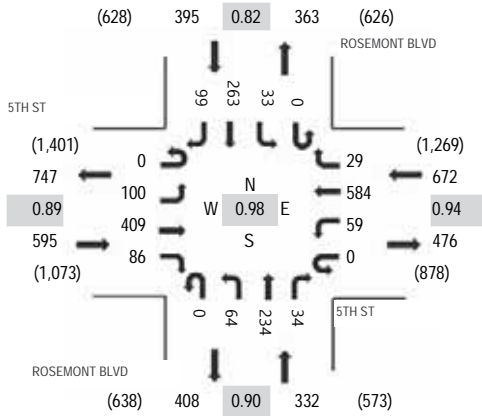
Location: 12 ROSEMONT BLVD & 5TH ST AM

Date: Tuesday, August 23, 2022

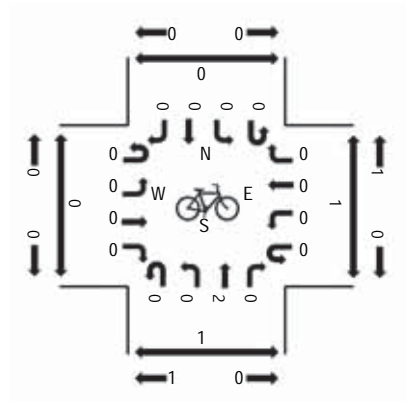
Peak Hour: 07:45 AM - 08:45 AM

Peak 15-Minutes: 08:00 AM - 08:15 AM

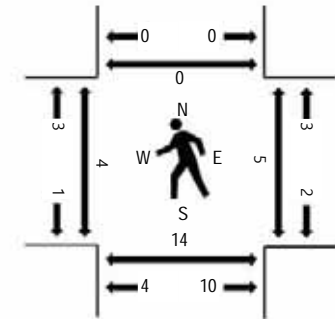
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

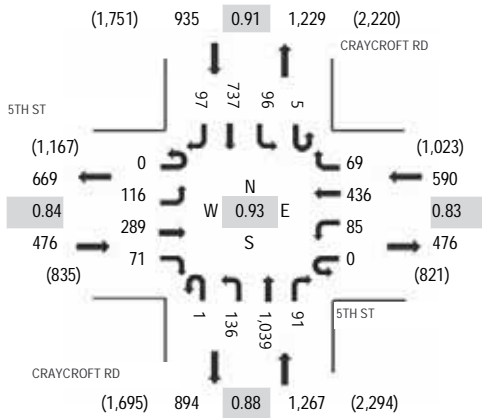


Note: Total study counts contained in parentheses.

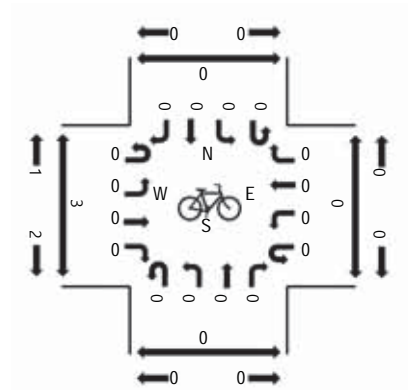
Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				ROSEMONT BLVD Northbound				ROSEMONT BLVD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	6	48	8	0	8	97	4	0	5	17	3	0	2	32	9	239	1,595	0	3	1	0
7:15 AM	0	14	84	8	0	1	158	7	0	13	33	7	0	6	26	14	371	1,866	0	0	0	0
7:30 AM	1	23	95	13	0	5	178	5	0	13	68	7	0	11	46	11	476	1,972	0	2	3	0
7:45 AM	0	26	94	21	0	17	158	10	0	7	52	4	0	9	85	26	509	1,994	3	1	3	0
8:00 AM	0	23	107	19	0	11	153	5	0	24	58	6	0	8	72	24	510	1,948	0	2	5	0
8:15 AM	0	24	108	15	0	19	120	10	0	20	67	9	0	10	52	23	477		1	1	2	0
8:30 AM	0	27	100	31	0	12	153	4	0	13	57	15	0	6	54	26	498		0	1	4	0
8:45 AM	0	32	121	25	0	6	122	6	0	17	48	10	0	8	52	16	463		0	0	0	0
Count Total	1	175	757	140	0	79	1,139	51	0	112	400	61	0	60	419	149	3,543		4	10	18	0
Peak Hour	0	100	409	86	0	59	584	29	0	64	234	34	0	33	263	99	1,994		4	5	14	0

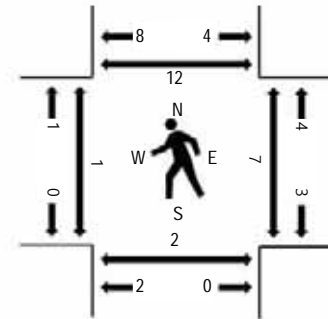
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

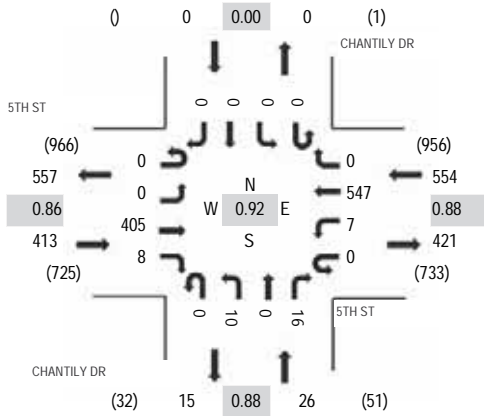
Interval Start Time	5TH ST Eastbound				5TH ST Westbound				CRAYCROFT RD Northbound				CRAYCROFT RD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	12	32	9	0	7	56	9	0	16	192	12	0	12	159	11	527	2,895	2	2	1	1
7:15 AM	0	21	42	16	0	25	88	9	2	36	218	24	1	20	149	18	669	3,131	1	1	0	1
7:30 AM	0	29	74	17	0	26	136	16	0	33	266	17	1	19	164	19	817	3,268	0	2	1	3
7:45 AM	0	28	63	21	0	22	108	14	1	36	293	31	1	26	212	26	882	3,199	0	0	1	6
8:00 AM	0	28	61	13	0	19	98	21	0	35	215	24	1	35	183	30	763	3,008	1	3	0	2
8:15 AM	0	31	91	20	0	18	94	18	0	32	265	19	2	16	178	22	806		0	2	0	1
8:30 AM	0	24	59	19	0	14	96	18	0	24	249	14	0	21	183	27	748		4	0	4	6
8:45 AM	0	31	73	21	0	21	80	10	0	28	195	17	2	19	176	18	691		0	5	4	3
Count Total	0	204	495	136	0	152	756	115	3	240	1,893	158	8	168	1,404	171	5,903		8	15	11	23
Peak Hour	0	116	289	71	0	85	436	69	1	136	1,039	91	5	96	737	97	3,268		1	7	2	12



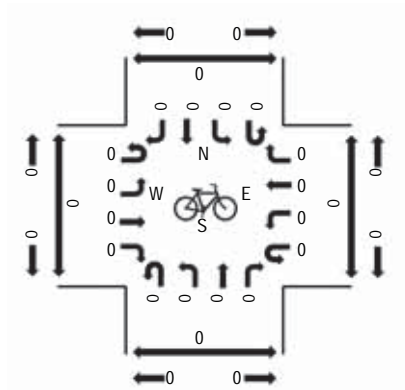
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Location: 14 CHANTILY DR & 5TH ST AM
Date: Tuesday, August 23, 2022
Peak Hour: 07:30 AM - 08:30 AM
Peak 15-Minutes: 07:45 AM - 08:00 AM

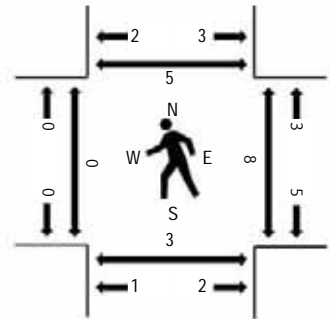
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				CHANTILY DR Northbound				CHANTILY DR Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	0	48	0	0	1	68	0	0	0	0	4	0	0	0	0	121	825	0	1	0	0
7:15 AM	0	0	68	1	0	1	111	0	0	6	0	3	0	0	0	0	190	934	0	3	1	3
7:30 AM	0	0	79	1	0	2	155	0	0	5	0	2	0	0	0	0	244	993	0	4	3	2
7:45 AM	0	0	121	2	0	2	142	0	0	1	0	2	0	0	0	0	270	962	0	0	0	0
8:00 AM	0	0	99	2	0	0	121	0	0	2	0	6	0	0	0	0	230	907	0	3	0	3
8:15 AM	0	0	106	3	0	3	129	0	0	2	0	6	0	0	0	0	249		0	1	0	0
8:30 AM	0	1	86	5	0	0	116	0	0	2	0	3	0	0	0	0	213		0	2	0	0
8:45 AM	0	0	96	7	0	2	103	0	0	3	0	4	0	0	0	0	215		0	2	0	2
Count Total	0	1	703	21	0	11	945	0	0	21	0	30	0	0	0	0	1,732		0	16	4	10
Peak Hour	0	0	405	8	0	7	547	0	0	10	0	16	0	0	0	0	993		0	8	3	5



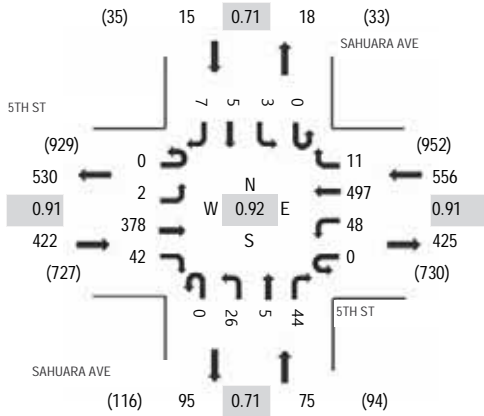
Location: 15 SAHUARA AVE & 5TH ST AM

Date: Tuesday, August 23, 2022

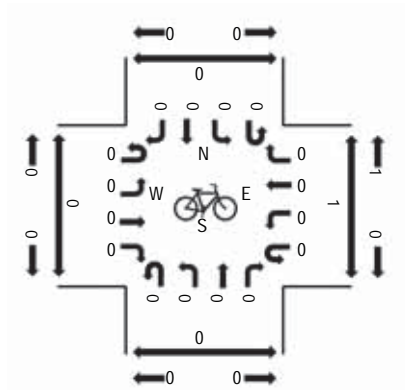
Peak Hour: 07:30 AM - 08:30 AM

Peak 15-Minutes: 07:45 AM - 08:00 AM

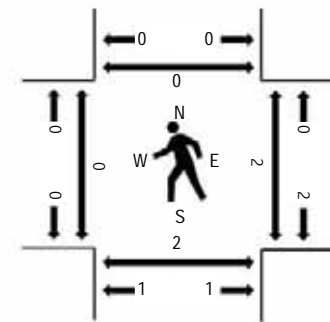
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				SAHUARA AVE Northbound				SAHUARA AVE Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	3	46	2	0	1	64	0	0	2	0	2	0	1	0	1	122	841	0	0	0	0
7:15 AM	0	2	64	2	0	2	117	2	0	0	0	2	0	3	0	4	198	983	0	1	1	0
7:30 AM	0	1	79	4	0	3	133	2	0	3	0	5	0	1	0	1	232	1,068	0	0	2	0
7:45 AM	0	0	113	5	0	10	140	2	0	3	0	11	0	1	1	3	289	1,052	0	1	0	0
8:00 AM	0	0	85	23	0	20	106	3	0	10	2	13	0	0	2	0	264	967	0	1	0	0
8:15 AM	0	1	101	10	0	15	118	4	0	10	3	15	0	1	2	3	283		0	0	0	0
8:30 AM	0	0	86	4	0	5	100	2	0	6	1	5	0	3	1	3	216		0	0	0	0
8:45 AM	0	2	91	3	0	1	99	3	0	1	0	0	0	2	0	2	204		0	0	1	1
Count Total	0	9	665	53	0	57	877	18	0	35	6	53	0	12	6	17	1,808		0	3	4	1
Peak Hour	0	2	378	42	0	48	497	11	0	26	5	44	0	3	5	7	1,068		0	2	2	0



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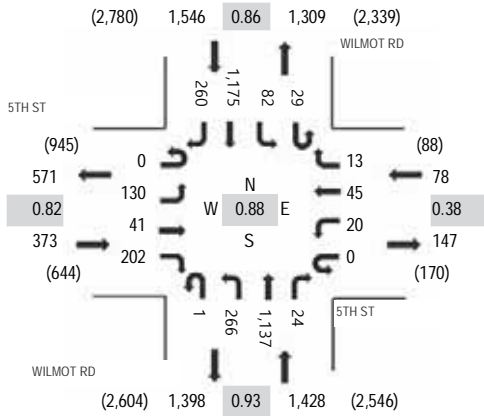
Location: 16 WILMOT RD & 5TH ST AM

Date: Tuesday, August 23, 2022

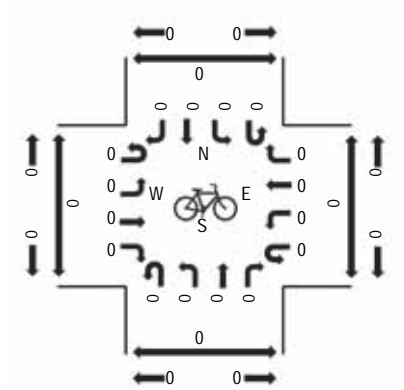
Peak Hour: 07:30 AM - 08:30 AM

Peak 15-Minutes: 07:45 AM - 08:00 AM

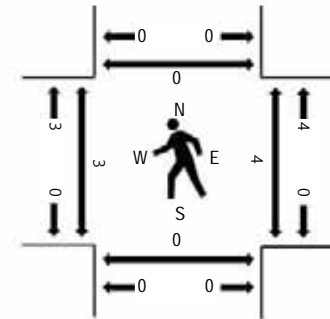
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

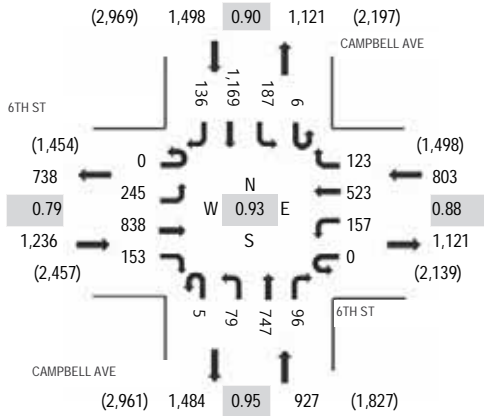


Note: Total study counts contained in parentheses.

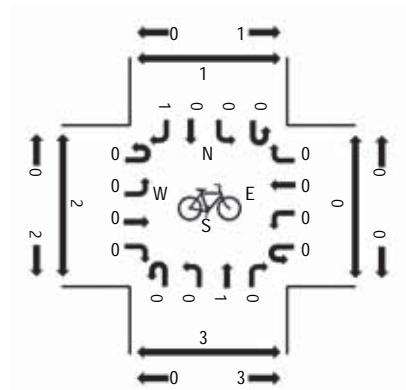
Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				WILMOT RD Northbound				WILMOT RD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	11	1	34	0	0	0	1	0	38	171	0	1	5	175	28	465	2,964	1	0	0	2
7:15 AM	0	25	2	28	0	1	3	1	0	50	215	3	1	10	269	50	658	3,345	2	0	1	3
7:30 AM	0	33	13	40	0	3	10	4	0	72	301	10	10	29	279	65	869	3,425	0	0	0	0
7:45 AM	0	38	24	52	0	14	32	8	0	63	272	13	12	50	331	63	972	3,325	0	0	0	0
8:00 AM	0	29	0	49	0	2	3	1	1	61	305	1	4	3	321	66	846	3,094	0	4	0	0
8:15 AM	0	30	4	61	0	1	0	0	0	70	259	0	3	0	244	66	738		3	0	0	0
8:30 AM	0	27	0	53	0	0	1	0	0	52	264	0	2	2	321	47	769		2	0	1	0
8:45 AM	0	37	0	53	0	1	0	2	1	52	272	0	0	0	270	53	741		2	0	0	0
Count Total	0	230	44	370	0	22	49	17	2	458	2,059	27	33	99	2,210	438	6,058		10	4	2	5
Peak Hour	0	130	41	202	0	20	45	13	1	266	1,137	24	29	82	1,175	260	3,425		3	4	0	0

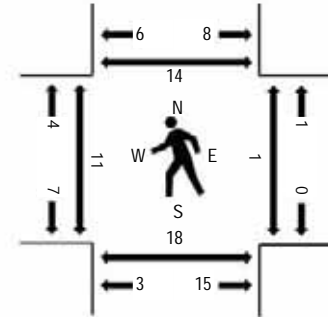
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

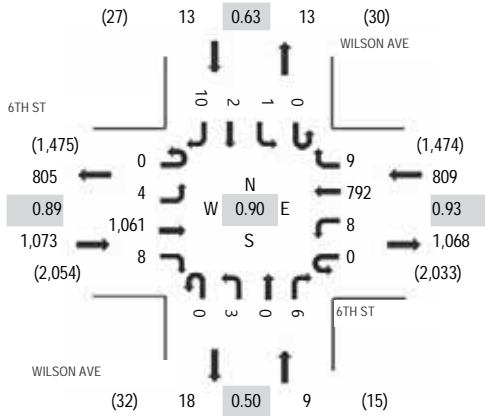


Note: Total study counts contained in parentheses.

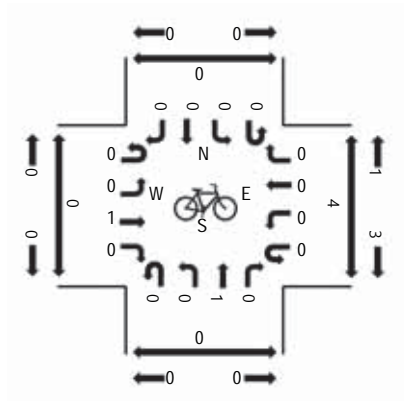
Traffic Counts - Motorized Vehicles

Interval Start Time	6TH ST Eastbound				6TH ST Westbound				CAMPBELL AVE Northbound				CAMPBELL AVE Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	93	243	77	0	42	102	18	1	34	169	19	3	41	271	45	1,158	4,418	2	0	8	0
4:15 PM	0	59	176	50	0	34	96	26	1	26	195	29	1	39	337	42	1,111	4,369	1	0	8	3
4:30 PM	0	63	200	43	0	35	100	32	1	16	200	20	0	37	269	43	1,059	4,464	6	0	3	4
4:45 PM	0	71	193	35	0	38	147	29	2	14	197	25	2	40	262	35	1,090	4,462	3	0	5	1
5:00 PM	0	52	197	37	0	42	116	26	1	26	178	25	4	50	329	26	1,109	4,333	1	1	2	6
5:15 PM	0	59	248	38	0	42	160	36	1	23	172	26	0	60	309	32	1,206		1	0	8	3
5:30 PM	0	67	188	33	0	50	127	27	4	19	163	27	1	45	271	35	1,057		5	0	5	9
5:45 PM	0	56	150	29	0	22	122	29	0	24	168	21	1	40	255	44	961		0	1	3	4
Count Total	0	520	1,595	342	0	305	970	223	11	182	1,442	192	12	352	2,303	302	8,751		19	2	42	30
Peak Hour	0	245	838	153	0	157	523	123	5	79	747	96	6	187	1,169	136	4,464		11	1	18	14

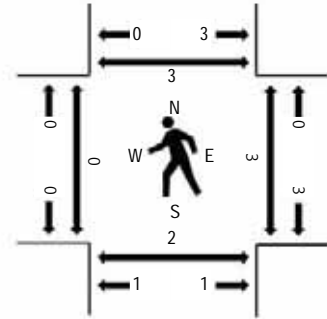
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

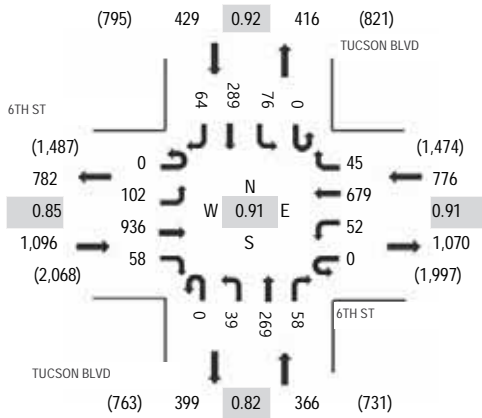


Note: Total study counts contained in parentheses.

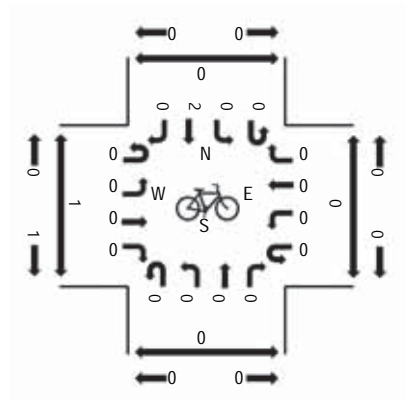
Traffic Counts - Motorized Vehicles

Interval Start Time	6TH ST Eastbound				6TH ST Westbound				WILSON AVE Northbound				WILSON AVE Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	4	292	3	0	2	155	1	0	0	0	4	0	0	0	3	464	1,762	0	1	1	0
4:15 PM	0	4	247	1	0	0	150	0	0	0	0	0	0	0	0	6	408	1,782	0	0	0	1
4:30 PM	0	1	245	2	0	3	194	3	0	1	0	0	0	1	0	3	453	1,904	0	0	0	0
4:45 PM	0	1	240	2	0	3	187	3	0	0	0	0	0	0	0	1	437	1,876	0	2	2	2
5:00 PM	0	1	276	2	0	1	195	2	0	1	0	2	0	0	0	4	484	1,808	0	0	0	0
5:15 PM	0	1	300	2	0	1	216	1	0	1	0	4	0	0	2	2	530		0	1	0	1
5:30 PM	0	1	227	3	0	0	190	1	0	0	0	1	0	1	1	0	425		0	0	0	0
5:45 PM	0	3	193	3	0	0	163	3	0	1	0	0	0	0	1	2	369		0	2	0	1
Count Total	0	16	2,020	18	0	10	1,450	14	0	4	0	11	0	2	4	21	3,570		0	6	3	5
Peak Hour	0	4	1,061	8	0	8	792	9	0	3	0	6	0	1	2	10	1,904		0	3	2	3

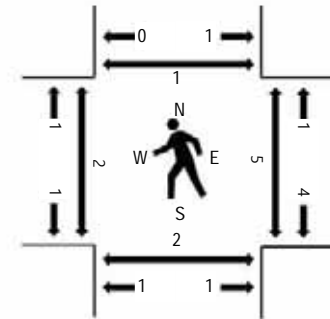
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

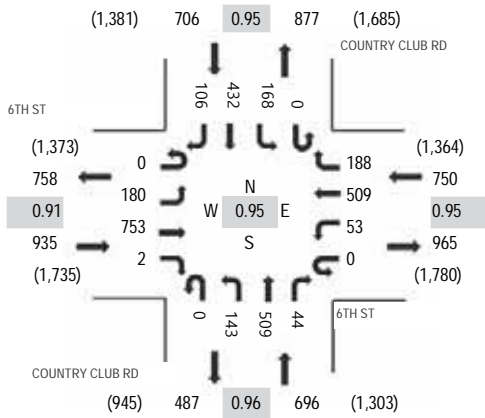


Note: Total study counts contained in parentheses.

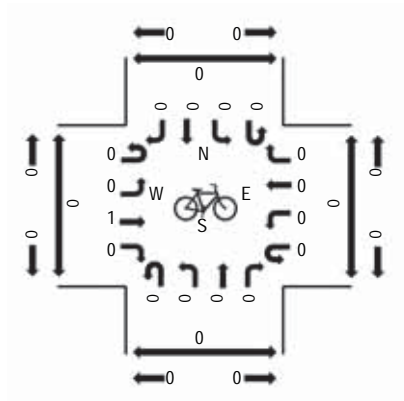
Traffic Counts - Motorized Vehicles

Interval Start Time	6TH ST Eastbound				6TH ST Westbound				TUCSON BLVD Northbound				TUCSON BLVD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	25	229	10	0	15	143	9	0	10	78	13	0	13	68	19	632	2,429	0	0	0	0
4:15 PM	0	21	221	18	0	12	152	14	0	10	70	15	0	11	66	18	628	2,533	0	1	0	0
4:30 PM	0	25	188	8	0	10	158	8	0	10	61	12	0	10	65	9	564	2,630	0	1	0	0
4:45 PM	0	23	195	12	0	11	173	7	0	7	50	17	0	28	72	10	605	2,667	1	0	0	0
5:00 PM	0	27	270	8	0	14	194	9	0	9	88	18	0	18	68	13	736	2,639	0	3	1	0
5:15 PM	0	32	271	19	0	18	167	18	0	13	61	9	0	13	86	18	725		1	0	1	1
5:30 PM	0	20	200	19	0	9	145	11	0	10	70	14	0	17	63	23	601		0	2	0	0
5:45 PM	0	24	192	11	0	12	157	8	0	11	62	13	0	10	69	8	577		0	1	0	0
Count Total	0	197	1,766	105	0	101	1,289	84	0	80	540	111	0	120	557	118	5,068		2	8	2	1
Peak Hour	0	102	936	58	0	52	679	45	0	39	269	58	0	76	289	64	2,667		2	5	2	1

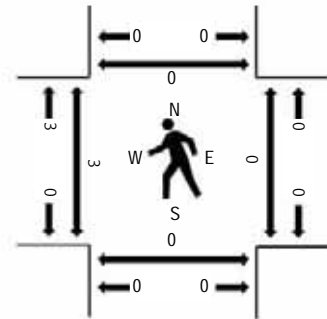
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	6TH ST Eastbound				6TH ST Westbound				COUNTRY CLUB RD Northbound				COUNTRY CLUB RD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	41	167	0	0	17	90	46	0	20	130	13	0	43	103	26	696	2,842	2	1	2	1
4:15 PM	0	49	167	0	0	9	97	41	0	22	111	6	0	36	94	30	662	2,939	2	0	0	0
4:30 PM	0	37	176	2	0	14	116	41	0	34	114	14	0	40	118	28	734	3,087	2	0	0	0
4:45 PM	0	34	178	0	0	11	138	49	0	36	121	9	0	43	105	26	750	3,080	1	0	0	0
5:00 PM	0	58	193	0	0	13	132	46	0	36	138	9	0	37	104	27	793	2,941	0	0	0	0
5:15 PM	0	51	206	0	0	15	123	52	0	37	136	12	0	48	105	25	810		0	0	0	0
5:30 PM	0	41	162	0	0	7	110	49	0	35	131	12	0	32	116	32	727		0	0	0	0
5:45 PM	0	41	132	0	0	16	88	44	0	36	84	7	0	38	96	29	611		0	0	0	0
Count Total	0	352	1,381	2	0	102	894	368	0	256	965	82	0	317	841	223	5,783		7	1	2	1
Peak Hour	0	180	753	2	0	53	509	188	0	143	509	44	0	168	432	106	3,087		3	0	0	0

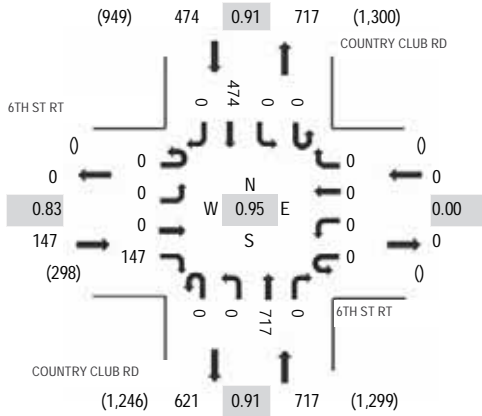
Location: 5 COUNTRY CLUB RD & 6TH ST RT PM

Date: Tuesday, August 23, 2022

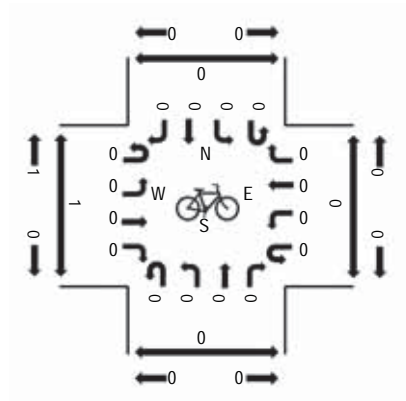
Peak Hour: 04:45 PM - 05:45 PM

Peak 15-Minutes: 05:00 PM - 05:15 PM

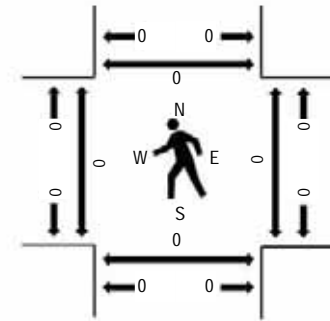
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

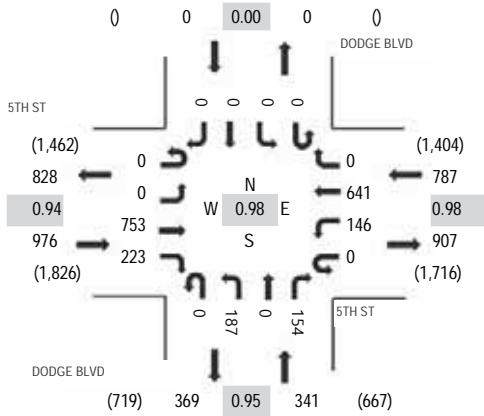


Note: Total study counts contained in parentheses.

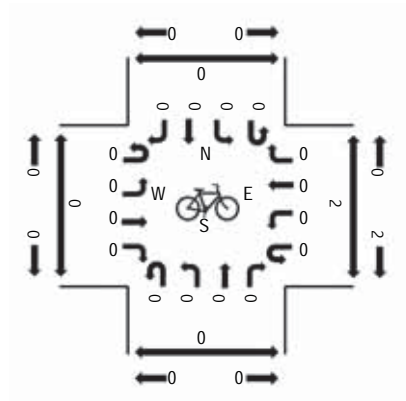
Traffic Counts - Motorized Vehicles

Interval Start Time	6TH ST RT Eastbound				6TH ST RT Westbound				COUNTRY CLUB RD Northbound				COUNTRY CLUB RD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	0	0	45	0	0	0	0	0	0	154	0	0	0	122	0	321	1,259	0	0	0	0
4:15 PM	0	0	0	48	0	0	0	0	0	0	142	0	0	0	106	0	296	1,290	0	0	0	0
4:30 PM	0	0	0	29	0	0	0	0	0	0	164	0	0	0	134	0	327	1,334	0	0	0	0
4:45 PM	0	0	0	37	0	0	0	0	0	0	158	0	0	0	120	0	315	1,338	0	0	0	0
5:00 PM	0	0	0	35	0	0	0	0	0	0	197	0	0	0	120	0	352	1,287	0	0	0	0
5:15 PM	0	0	0	37	0	0	0	0	0	0	190	0	0	0	113	0	340		0	0	0	0
5:30 PM	0	0	0	38	0	0	0	0	0	0	172	0	0	0	121	0	331		0	0	0	0
5:45 PM	0	1	0	28	0	0	0	0	0	0	122	0	0	0	113	0	264		0	0	0	0
Count Total	0	1	0	297	0	0	0	0	0	0	1,299	0	0	0	949	0	2,546		0	0	0	0
Peak Hour	0	0	0	147	0	0	0	0	0	0	717	0	0	0	474	0	1,338		0	0	0	0

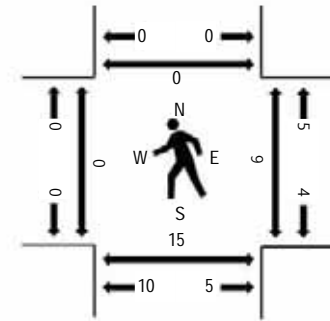
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

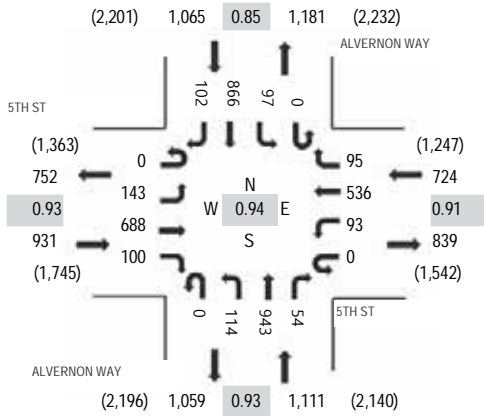


Note: Total study counts contained in parentheses.

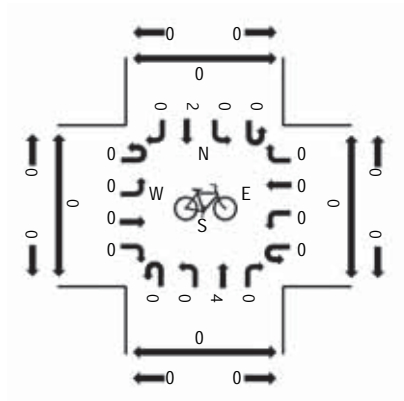
Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				DODGE BLVD Northbound				DODGE BLVD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	0	165	48	0	35	127	0	0	33	0	29	0	0	0	0	437	1,925	1	1	1	0
4:15 PM	0	0	169	53	0	38	99	0	0	43	0	46	0	0	0	0	448	2,025	0	2	0	0
4:30 PM	0	0	178	52	0	38	163	0	0	49	0	42	0	0	0	0	522	2,104	0	2	4	0
4:45 PM	0	0	175	64	0	40	156	0	0	45	0	38	0	0	0	0	518	2,065	0	3	5	0
5:00 PM	0	0	204	55	0	37	160	0	0	44	0	37	0	0	0	0	537	1,972	0	3	6	0
5:15 PM	0	0	196	52	0	31	162	0	0	49	0	37	0	0	0	0	527		0	1	0	0
5:30 PM	0	0	171	48	0	46	139	0	0	39	0	40	0	0	0	0	483		0	3	0	0
5:45 PM	0	0	141	55	0	27	106	0	0	48	0	48	0	0	0	0	425		0	3	0	0
Count Total	0	0	1,399	427	0	292	1,112	0	0	350	0	317	0	0	0	0	3,897		1	18	16	0
Peak Hour	0	0	753	223	0	146	641	0	0	187	0	154	0	0	0	0	2,104		0	9	15	0

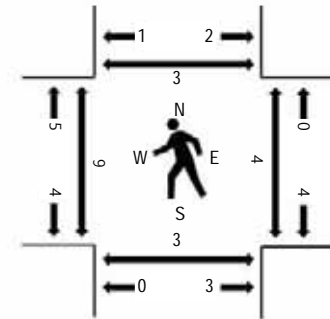
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

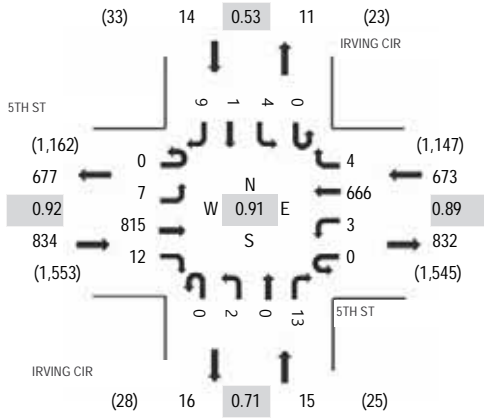


Note: Total study counts contained in parentheses.

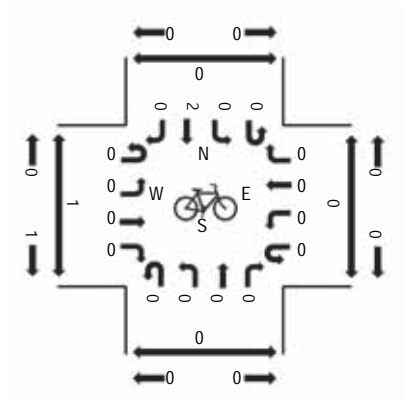
Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				ALVERNON WAY Northbound				ALVERNON WAY Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	34	151	23	0	26	105	20	0	34	177	11	0	16	230	26	853	3,623	2	2	4	1
4:15 PM	0	33	138	32	0	27	92	14	0	27	253	9	0	32	269	27	953	3,767	8	2	5	2
4:30 PM	0	37	154	34	0	26	115	24	0	41	201	14	0	25	206	25	902	3,831	3	4	2	2
4:45 PM	0	36	175	21	0	19	140	18	0	18	226	19	0	19	194	30	915	3,809	4	0	1	0
5:00 PM	0	31	173	21	0	28	146	24	0	20	262	10	0	30	233	19	997	3,710	2	0	0	0
5:15 PM	0	39	186	24	0	20	135	29	0	35	254	11	0	23	233	28	1,017		0	0	0	1
5:30 PM	0	47	145	28	0	15	112	6	0	35	205	12	1	21	225	28	880		4	0	2	1
5:45 PM	0	23	135	25	0	11	82	13	0	24	225	17	0	16	226	19	816		2	0	2	3
Count Total	0	280	1,257	208	0	172	927	148	0	234	1,803	103	1	182	1,816	202	7,333		25	8	16	10
Peak Hour	0	143	688	100	0	93	536	95	0	114	943	54	0	97	866	102	3,831		9	4	3	3

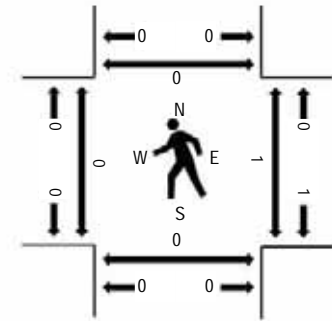
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

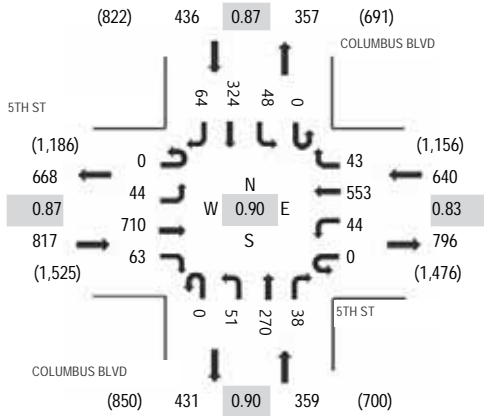


Note: Total study counts contained in parentheses.

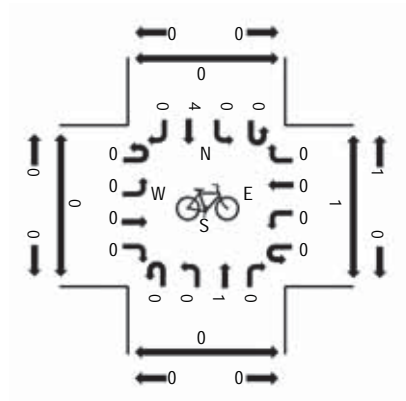
Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				IRVING CIR Northbound				IRVING CIR Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	2	173	2	0	1	130	0	0	0	1	2	0	0	0	2	313	1,369	0	0	0	0
4:15 PM	0	1	192	2	0	1	120	0	0	1	0	2	0	1	1	6	327	1,479	0	0	0	0
4:30 PM	0	0	185	3	0	0	149	1	0	1	0	5	0	0	0	3	347	1,536	0	0	0	0
4:45 PM	0	4	209	2	0	0	161	0	0	1	0	2	0	0	1	2	382	1,497	0	1	0	0
5:00 PM	0	1	221	4	0	2	186	1	0	0	0	5	0	2	0	1	423	1,389	0	0	0	0
5:15 PM	0	2	200	3	0	1	170	2	0	0	0	1	0	2	0	3	384		0	0	0	0
5:30 PM	0	0	183	3	0	0	115	0	0	0	0	1	0	2	0	4	308		2	0	2	0
5:45 PM	0	6	154	1	0	1	104	2	0	1	0	2	0	1	0	2	274		1	0	1	0
Count Total	0	16	1,517	20	0	6	1,135	6	0	4	1	20	0	8	2	23	2,758		3	1	3	0
Peak Hour	0	7	815	12	0	3	666	4	0	2	0	13	0	4	1	9	1,536		0	1	0	0

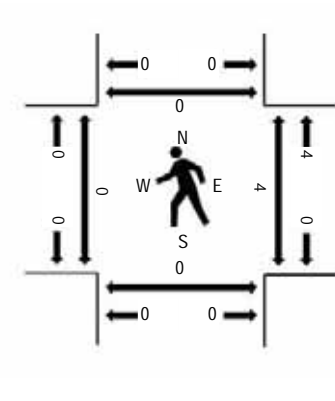
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

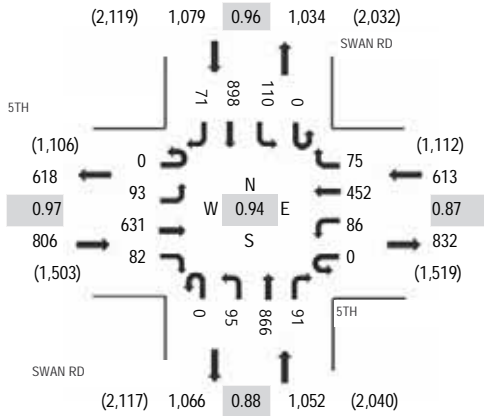


Note: Total study counts contained in parentheses.

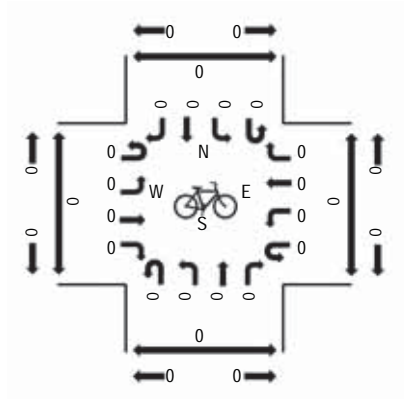
Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				COLUMBUS BLVD Northbound				COLUMBUS BLVD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	11	139	12	0	9	109	11	0	9	73	14	0	14	83	11	495	2,123	0	2	0	0
4:15 PM	0	12	174	18	0	11	146	14	0	5	72	7	0	15	97	15	586	2,252	0	1	0	0
4:30 PM	0	9	166	7	0	3	106	12	0	18	72	13	0	14	75	14	509	2,239	0	0	0	0
4:45 PM	0	9	170	17	0	18	131	7	0	9	63	5	0	7	78	19	533	2,198	0	3	0	0
5:00 PM	0	14	200	21	0	12	170	10	0	19	63	13	0	12	74	16	624	2,080	0	0	0	0
5:15 PM	0	11	175	19	0	13	138	12	0	15	80	10	0	4	86	10	573		0	0	0	0
5:30 PM	0	10	144	22	0	11	97	8	0	11	54	5	0	14	85	7	468		0	0	0	0
5:45 PM	0	8	142	15	0	4	99	5	0	9	51	10	0	9	60	3	415		0	0	1	0
Count Total	0	84	1,310	131	0	81	996	79	0	95	528	77	0	89	638	95	4,203		0	6	1	0
Peak Hour	0	44	710	63	0	44	553	43	0	51	270	38	0	48	324	64	2,252		0	4	0	0

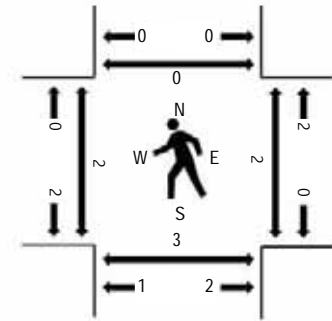
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

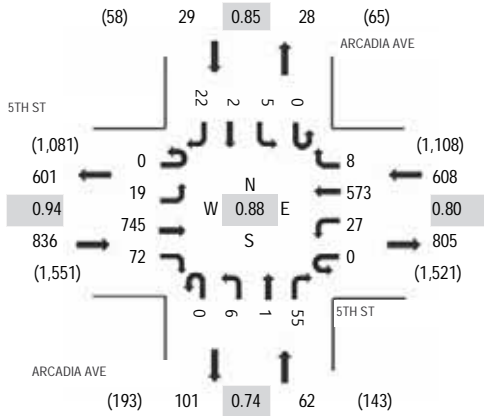


Note: Total study counts contained in parentheses.

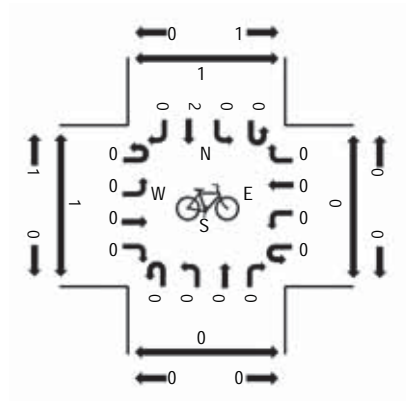
Traffic Counts - Motorized Vehicles

Interval Start Time	5TH Eastbound				5TH Westbound				SWAN RD Northbound				SWAN RD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	33	135	26	0	24	111	26	0	20	209	24	1	22	246	15	892	3,495	0	0	2	0
4:15 PM	0	24	148	23	0	19	88	12	0	17	260	28	0	20	244	19	902	3,546	1	0	1	0
4:30 PM	0	22	157	25	0	16	89	19	0	20	183	27	0	30	216	21	825	3,550	0	0	0	0
4:45 PM	0	22	154	18	0	20	116	16	0	22	225	18	0	19	228	18	876	3,484	0	2	1	0
5:00 PM	0	21	158	22	0	29	132	16	0	28	225	26	0	23	244	19	943	3,279	1	0	2	0
5:15 PM	0	28	162	17	0	21	115	24	0	25	233	20	0	38	210	13	906		1	0	0	0
5:30 PM	0	25	125	18	0	24	84	10	1	14	187	22	0	27	204	18	759		0	0	0	0
5:45 PM	0	21	102	17	0	16	70	15	0	18	175	13	0	21	189	14	671		1	0	0	0
Count Total	0	196	1,141	166	0	169	805	138	1	164	1,697	178	1	200	1,781	137	6,774		4	2	6	0
Peak Hour	0	93	631	82	0	86	452	75	0	95	866	91	0	110	898	71	3,550		2	2	3	0

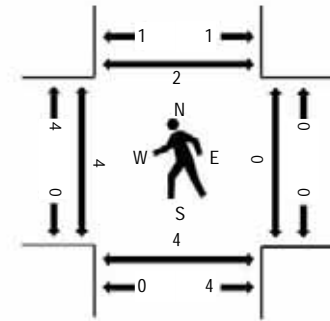
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

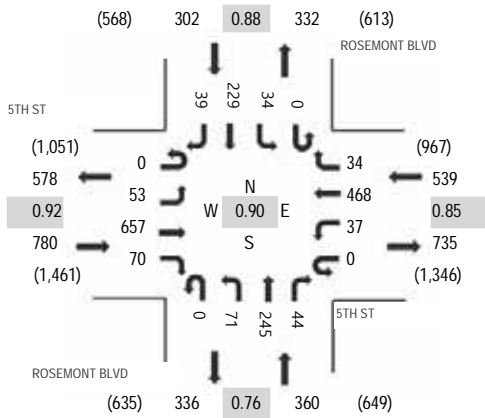


Note: Total study counts contained in parentheses.

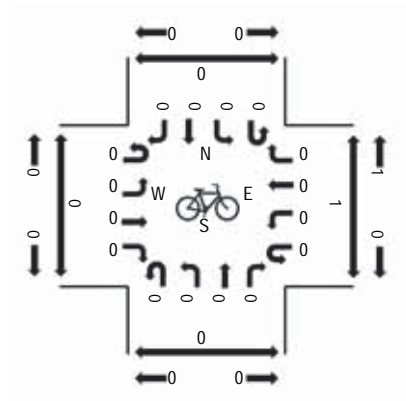
Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				ARCADIA AVE Northbound				ARCADIA AVE Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	3	168	10	0	7	140	5	0	0	0	16	0	1	0	9	359	1,429	0	0	2	0
4:15 PM	0	5	180	19	0	10	111	5	0	1	0	13	0	1	1	5	351	1,504	1	0	0	0
4:30 PM	0	3	203	13	0	6	115	1	0	3	0	17	0	2	1	7	371	1,535	3	0	1	0
4:45 PM	0	1	171	13	0	4	137	2	0	1	1	11	0	1	0	6	348	1,487	1	0	1	1
5:00 PM	0	10	193	20	0	4	184	3	0	1	0	13	0	2	0	4	434	1,431	0	0	0	1
5:15 PM	0	5	178	26	0	13	137	2	0	1	0	14	0	0	1	5	382		0	0	2	0
5:30 PM	0	5	161	15	0	7	101	3	0	2	1	21	0	1	0	6	323		0	0	0	0
5:45 PM	0	7	131	11	0	11	97	3	0	4	0	23	0	0	1	4	292		5	0	2	0
Count Total	0	39	1,385	127	0	62	1,022	24	0	13	2	128	0	8	4	46	2,860		10	0	8	2
Peak Hour	0	19	745	72	0	27	573	8	0	6	1	55	0	5	2	22	1,535		4	0	4	2

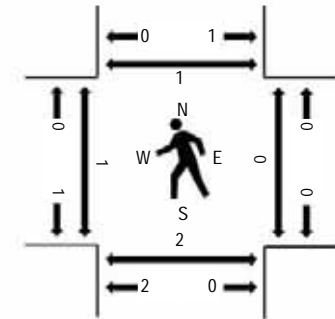
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				ROSEMONT BLVD Northbound				ROSEMONT BLVD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	23	123	20	0	14	115	8	0	22	78	11	0	5	68	12	499	1,885	0	0	0	1
4:15 PM	0	20	157	16	0	9	88	3	0	10	52	14	0	6	57	16	448	1,934	0	4	0	0
4:30 PM	0	23	161	28	0	10	86	6	0	18	63	4	0	6	57	9	471	1,981	0	0	0	0
4:45 PM	0	10	148	15	0	14	115	13	0	18	53	15	0	6	50	10	467	1,903	0	0	0	0
5:00 PM	0	11	166	12	0	6	146	8	0	25	78	18	0	12	56	10	548	1,760	1	0	1	1
5:15 PM	0	9	182	15	0	7	121	7	0	10	51	7	0	10	66	10	495		0	0	1	0
5:30 PM	0	12	146	15	0	9	87	9	0	14	36	11	0	1	41	12	393		0	0	0	0
5:45 PM	0	13	125	11	0	6	77	3	0	12	24	5	0	7	33	8	324		0	0	0	0
Count Total	0	121	1,208	132	0	75	835	57	0	129	435	85	0	53	428	87	3,645		1	4	2	2
Peak Hour	0	53	657	70	0	37	468	34	0	71	245	44	0	34	229	39	1,981		1	0	2	1



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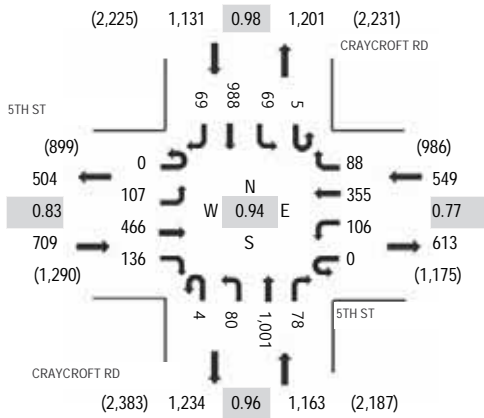
Location: 13 CRAYCROFT RD & 5TH ST PM

Date: Tuesday, August 23, 2022

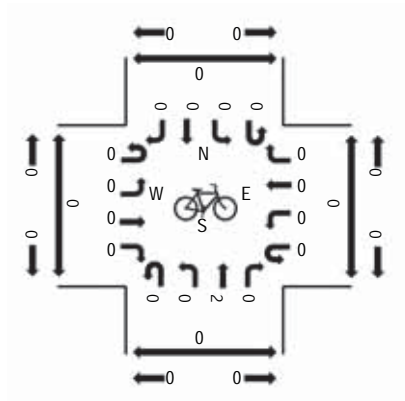
Peak Hour: 04:30 PM - 05:30 PM

Peak 15-Minutes: 05:00 PM - 05:15 PM

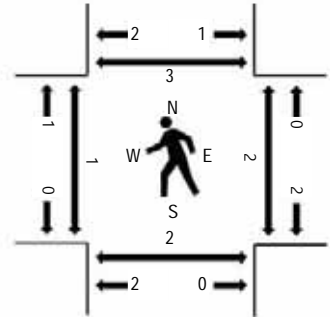
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

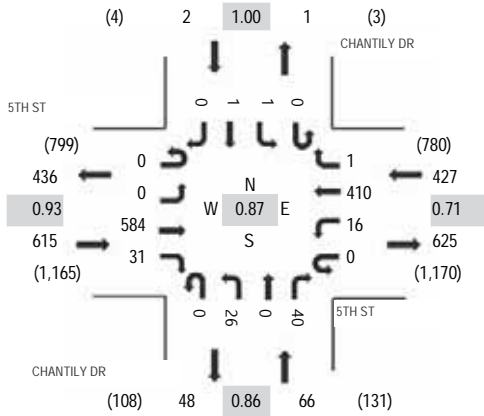


Note: Total study counts contained in parentheses.

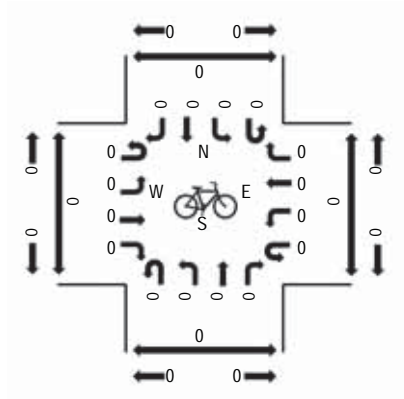
Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				CRAYCROFT RD Northbound				CRAYCROFT RD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	21	97	21	0	26	77	22	0	20	218	26	1	25	245	16	815	3,378	2	0	0	3
4:15 PM	0	30	111	33	0	26	65	28	0	21	244	25	0	20	239	12	854	3,504	2	1	0	2
4:30 PM	0	26	111	32	0	29	76	17	1	22	239	15	2	27	224	12	833	3,552	0	1	0	0
4:45 PM	0	30	107	28	0	20	81	19	2	22	260	19	1	14	258	15	876	3,504	0	0	2	2
5:00 PM	0	24	107	31	0	39	112	28	0	23	265	18	1	14	259	20	941	3,310	1	0	0	1
5:15 PM	0	27	141	45	0	18	86	24	1	13	237	26	1	14	247	22	902		0	1	0	0
5:30 PM	0	17	100	26	0	28	67	14	0	14	211	16	0	22	253	17	785		0	0	0	1
5:45 PM	0	18	90	17	0	19	54	11	1	17	194	17	1	13	215	15	682		2	1	1	0
Count Total	0	193	864	233	0	205	618	163	5	152	1,868	162	7	149	1,940	129	6,688		7	4	3	9
Peak Hour	0	107	466	136	0	106	355	88	4	80	1,001	78	5	69	988	69	3,552		1	2	2	3

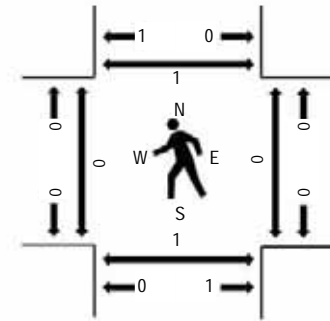
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				CHANTILY DR Northbound				CHANTILY DR Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	0	151	12	0	4	99	1	0	9	0	8	0	1	0	0	285	1,064	0	0	0	1
4:15 PM	0	1	134	11	0	12	84	0	0	8	0	9	0	0	0	1	260	1,097	0	0	0	0
4:30 PM	0	0	134	9	0	2	87	1	0	6	0	9	0	1	0	0	249	1,110	0	0	0	0
4:45 PM	0	0	148	8	0	3	90	0	0	10	0	10	0	0	1	0	270	1,101	0	0	1	1
5:00 PM	0	0	146	4	0	6	145	0	0	5	0	12	0	0	0	0	318	1,016	0	0	0	0
5:15 PM	0	0	156	10	0	5	88	0	0	5	0	9	0	0	0	0	273		0	0	0	0
5:30 PM	0	0	123	10	0	3	89	0	0	7	0	8	0	0	0	0	240		0	0	0	0
5:45 PM	0	0	102	6	0	2	59	0	0	7	0	9	0	0	0	0	185		0	1	0	0
Count Total	0	1	1,094	70	0	37	741	2	0	57	0	74	0	2	1	1	2,080		0	1	1	2
Peak Hour	0	0	584	31	0	16	410	1	0	26	0	40	0	1	1	0	1,110		0	0	1	1



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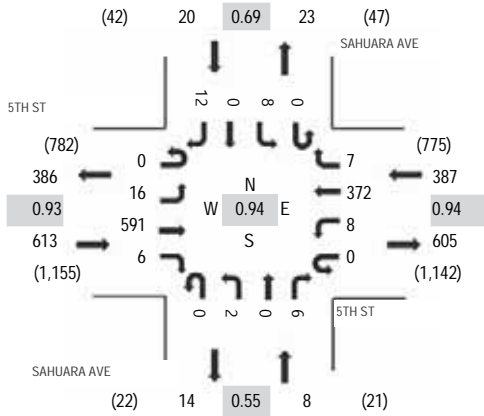
Location: 15 SAHUARA AVE & 5TH ST PM

Date: Tuesday, August 23, 2022

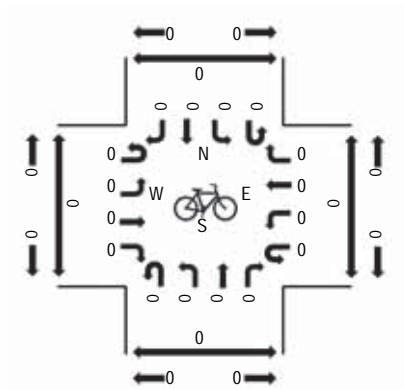
Peak Hour: 04:30 PM - 05:30 PM

Peak 15-Minutes: 05:15 PM - 05:30 PM

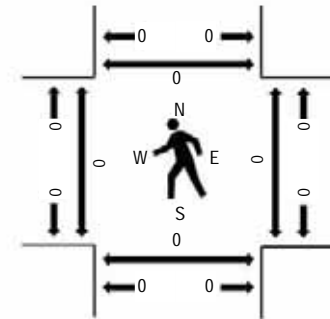
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				SAHUARA AVE Northbound				SAHUARA AVE Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	5	149	1	0	0	100	2	0	0	0	2	0	1	0	1	261	1,005	0	0	0	0
4:15 PM	0	3	142	0	0	1	100	2	0	1	0	3	0	4	0	4	260	1,015	0	0	0	0
4:30 PM	0	3	135	2	0	3	87	0	0	1	0	1	0	0	0	3	235	1,028	0	0	0	0
4:45 PM	0	4	144	2	0	2	89	2	0	0	0	2	0	2	0	2	249	1,026	0	0	0	0
5:00 PM	0	4	153	2	0	2	97	2	0	1	0	1	0	3	0	6	271	988	0	0	0	0
5:15 PM	0	5	159	0	0	1	99	3	0	0	0	2	0	3	0	1	273		0	0	0	0
5:30 PM	0	4	125	0	0	1	87	3	0	3	0	2	0	1	0	7	233		0	0	0	0
5:45 PM	0	3	107	3	0	2	88	2	0	2	0	0	0	1	0	3	211		0	0	0	0
Count Total	0	31	1,114	10	0	12	747	16	0	8	0	13	0	15	0	27	1,993		0	0	0	0
Peak Hour	0	16	591	6	0	8	372	7	0	2	0	6	0	8	0	12	1,028		0	0	0	0



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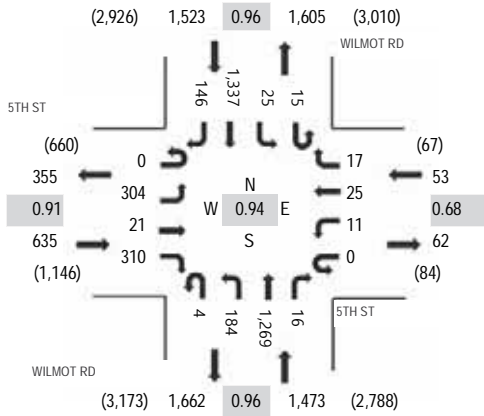
Location: 16 WILMOT RD & 5TH ST PM

Date: Tuesday, August 23, 2022

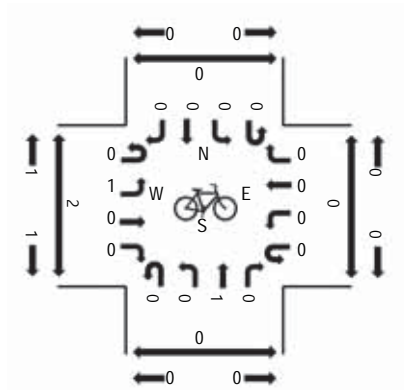
Peak Hour: 04:30 PM - 05:30 PM

Peak 15-Minutes: 05:00 PM - 05:15 PM

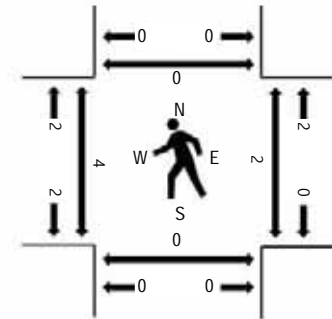
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	5TH ST Eastbound				5TH ST Westbound				WILMOT RD Northbound				WILMOT RD Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	75	1	79	0	1	1	1	0	45	291	3	1	3	383	42	926	3,519	2	0	1	0
4:15 PM	0	48	6	78	0	2	3	0	0	43	333	4	2	4	277	36	836	3,577	1	0	0	0
4:30 PM	0	69	4	83	0	0	2	2	0	49	285	2	3	4	306	36	845	3,684	1	0	0	0
4:45 PM	0	67	9	65	0	4	6	6	1	35	343	7	4	14	316	35	912	3,653	1	2	0	0
5:00 PM	0	94	6	75	0	2	11	7	2	53	327	6	4	6	346	45	984	3,408	2	0	0	0
5:15 PM	0	74	2	87	0	5	6	2	1	47	314	1	4	1	369	30	943		0	0	0	0
5:30 PM	0	67	0	54	0	0	2	3	0	35	277	0	1	1	334	40	814		1	0	0	0
5:45 PM	0	54	0	49	0	0	1	0	0	33	251	0	1	0	254	24	667		1	0	0	0
Count Total	0	548	28	570	0	14	32	21	4	340	2,421	23	20	33	2,585	288	6,927		9	2	1	0
Peak Hour	0	304	21	310	0	11	25	17	4	184	1,269	16	15	25	1,337	146	3,684		4	2	0	0

Site Code: 17
6TH ST BTW CAMPBELL AVE & TUCSON BLVD

Start Time	23-Aug-22 Tue	EB	WB	Total
12:00 AM		26	22	48
01:00		19	16	35
02:00		17	6	23
03:00		12	14	26
04:00		15	31	46
05:00		54	92	146
06:00		146	232	378
07:00		435	838	1273
08:00		516	879	1395
09:00		347	611	958
10:00		416	484	900
11:00		428	520	948
12:00 PM		572	559	1131
01:00		525	536	1061
02:00		627	619	1246
03:00		799	717	1516
04:00		920	677	1597
05:00		919	742	1661
06:00		501	486	987
07:00		403	365	768
08:00		252	286	538
09:00		214	181	395
10:00		96	131	227
11:00		68	53	121
Total		8327	9097	17424
Percent		47.8%	52.2%	
AM Peak	-	08:00	08:00	-
Vol.	-	516	879	-
PM Peak	-	16:00	17:00	-
Vol.	-	920	742	-
Grand Total		8327	9097	17424
Percent		47.8%	52.2%	
ADT		ADT 17,424	ADT 17,424	ADT 17,424

Start Time	23-Aug-22 Tue	EB	WB	Total
12:00 AM		26	21	47
01:00		18	12	30
02:00		10	4	14
03:00		9	9	18
04:00		9	26	35
05:00		43	77	120
06:00		143	204	347
07:00		418	769	1187
08:00		535	783	1318
09:00		386	542	928
10:00		428	500	928
11:00		496	487	983
12:00 PM		584	563	1147
01:00		527	500	1027
02:00		660	565	1225
03:00		810	745	1555
04:00		878	645	1523
05:00		840	688	1528
06:00		473	505	978
07:00		367	345	712
08:00		234	237	471
09:00		197	165	362
10:00		86	89	175
11:00		44	37	81
Total		8221	8518	16739
Percent		49.1%	50.9%	
AM Peak	-	08:00	08:00	-
Vol.	-	535	783	-
PM Peak	-	16:00	15:00	-
Vol.	-	878	745	-
Grand Total		8221	8518	16739
Percent		49.1%	50.9%	
ADT		ADT 16,739	ADT 16,739	AADT 16,739

Site Code: 19
5TH ST BTW SWAN RD & ROSEMONT BLVD

Start Time	23-Aug-22 Tue	EB	WB	Total
12:00 AM		24	19	43
01:00		12	13	25
02:00		11	9	20
03:00		9	11	20
04:00		19	44	63
05:00		49	89	138
06:00		166	221	387
07:00		497	569	1066
08:00		687	583	1270
09:00		370	409	779
10:00		338	345	683
11:00		356	396	752
12:00 PM		371	380	751
01:00		427	368	795
02:00		520	428	948
03:00		701	569	1270
04:00		725	540	1265
05:00		670	544	1214
06:00		348	305	653
07:00		261	216	477
08:00		171	185	356
09:00		103	106	209
10:00		75	63	138
11:00		35	20	55
Total		6945	6432	13377
Percent		51.9%	48.1%	
AM Peak	-	08:00	08:00	-
Vol.	-	687	583	-
PM Peak	-	16:00	15:00	-
Vol.	-	725	569	-
Grand Total		6945	6432	13377
Percent		51.9%	48.1%	
ADT		ADT 13,377	ADT 13,377	AADT 13,377

Start Time	23-Aug-22 Tue	EB	WB	Total
12:00 AM		13	11	24
01:00		3	4	7
02:00		8	7	15
03:00		7	5	12
04:00		7	28	35
05:00		34	46	80
06:00		112	128	240
07:00		326	450	776
08:00		393	466	859
09:00		291	292	583
10:00		305	280	585
11:00		311	329	640
12:00 PM		356	278	634
01:00		351	290	641
02:00		389	355	744
03:00		485	374	859
04:00		567	367	934
05:00		526	386	912
06:00		273	187	460
07:00		168	148	316
08:00		104	91	195
09:00		58	63	121
10:00		39	36	75
11:00		25	12	37
Total		5151	4633	9784
Percent		52.6%	47.4%	
AM Peak	-	08:00	08:00	-
Vol.	-	393	466	-
PM Peak	-	16:00	17:00	-
Vol.	-	567	386	-
Grand Total		5151	4633	9784
Percent		52.6%	47.4%	
ADT		ADT 9,784	ADT 9,784	ADT 9,784



Appendix C
2045 PAG Travel Demand Model
Segment Volumes

2045 PAG Travel Demand Model Projections

Street	Segment	Year	Count		2019 Base Projection	2045 Build
			Count	2022 Count		
6th St	Warren Ave to Campbell Ave	2017	21,441		21,000	23,000
	Campbell Ave to Wilson Ave	2017	15,139			
	Wilson Ave to Tucson Blvd	2017	15,139			
5th St	Campbell Ave to Tucson Blvd	2017	15,139		17,500	17,900
	Tucson Blvd to Country Club Rd	2017	15,139		13,800	14,250
	Country Club Rd to Dodge Blvd	2018	15,835		13,300	15,667
	Dodge Blvd to Alvernon Wy	2018	15,835		13,900	17,000
	Alvernon Wy to Irving Circle	2018	13,686		12,400	14,100
	Irving Circle to Columbus Blvd	2018	13,686		12,100	13,600
	Alvernon Wy to Columbus Blvd	2018	13,686		12,250	13,850
	Columbus Blvd to Swan Rd	2018	11,303		11,950	12,750
	Swan Rd to Arcadia Ave	2018	15,158		13,466	15,433
	Arcadia Ave to Rosemont Ave	2018	15,158		12,900	14,800
	Swan Rd to Rosemont Ave	2018	15,158		13,183	15,116
	Rosemont Ave to Craycroft Rd	2018	15,158		12,650	13,900
	Craycroft Rd to Chantily Dr	2018	11,699			
	Chantily Dr to Sahuara Ave	2018	11,699			
	Craycroft Rd to Wilmot Rd	2018	11,699		14,800	14,967
Campbell Ave	North of 6th St	2017	42,518		37,000	45,000
	South of 6th St	2017	36,720		46,000	53,000
Tucson Blvd	North of 6th St	2017	10,966		12,100	13,100
	South of 6th St	2018	8,966		9,700	10,700
Country Club Rd	North of 6th St	2018	21,429		23,000	27,000
	South of 6th St	2018	16,920		19,000	23,000
Dodge Blvd	South of 5th St					
Alvernon Wy	North of 5th St	2018	26,636		31,000	35,000
	South of 5th St	2018	26,585		29,000	34,000
Columbus Blvd	North of 5th St	2017	9,278		9,600	11,100
	South of 5th St	2017	8,251		8,700	10,200
Swan Rd	North of 5th St	2019	28,901		36,000	40,000
	South of 5th St	2019	27,977		34,000	37,000
Rosemont Ave	North of 5th St	2017	6,589		6,400	6,500
	South of 5th St	2017	6,589		10,000	10,200
Craycroft Rd	North of 5th St	2018	29,450		35,000	38,000
	South of 5th St	2018	26,574		35,000	38,000
Wilmot Rd	North of 5th St	2019	39,777		49,000	53,000
	South of 5th St	2019	39,815		48,000	53,000



Appendix D

Crash Data



Appendix E Speed Data

Site Code: 17
6TH ST BTW CAMPBELL AVE & TUCSON BLVD

EB	Start Time	15	16	20	21	25	26	30	31	35	36	40	41	45	46	50	51	55	56	60	61	65	66	70	71	75	76	999	Total	Pace Speed	Number in Pace
	08/23/22	0	0	0	0	0	1	2	3	5	10	10	10	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	26	36-45	20
	01:00	0	0	0	0	0	2	7	5	4	7	7	5	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	19	36-45	12
	02:00	0	0	0	0	0	0	4	4	4	7	7	4	4	1	0	1	1	1	1	0	0	0	0	0	0	0	17	31-40	11	
	03:00	0	0	0	1	0	0	3	3	3	5	5	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	12	31-40	8	
	04:00	0	0	0	0	0	0	3	3	3	4	4	3	3	5	5	0	0	0	0	0	0	0	0	0	0	0	15	39-48	8	
	05:00	0	0	0	1	1	2	14	14	16	16	16	16	16	3	2	2	2	2	2	0	0	0	0	0	0	0	54	35-44	32	
	06:00	1	1	4	4	4	5	19	19	50	50	44	44	44	17	5	5	5	0	0	0	0	0	0	0	0	0	146	36-45	94	
	07:00	14	8	4	5	4	33	93	114	171	227	104	104	89	15	4	4	4	3	3	0	0	1	0	0	0	0	435	31-40	264	
	08:00	2	5	0	3	3	29	64	114	227	104	104	104	104	26	4	4	4	0	0	0	0	0	0	0	0	0	516	31-40	341	
	09:00	1	0	0	3	3	13	149	149	149	149	149	91	23	3	3	3	3	0	0	0	0	0	0	0	0	0	347	36-45	240	
	10:00	0	1	2	2	2	19	164	164	164	164	102	102	26	5	1	1	1	1	1	0	0	0	0	0	0	0	416	36-45	266	
	11:00	0	0	0	2	2	26	80	80	80	187	94	94	29	9	0	0	0	0	0	1	1	0	0	0	0	0	428	36-45	281	
	12 PM	0	1	7	7	7	39	144	144	250	250	99	99	27	3	1	1	1	1	1	1	1	0	0	0	0	0	572	31-40	394	
	13:00	0	0	5	5	5	27	127	127	227	227	106	106	25	6	2	2	2	2	2	0	0	0	0	0	0	0	525	31-40	354	
	14:00	0	1	4	4	4	41	154	154	258	258	134	134	30	3	2	3	3	2	2	0	0	0	0	0	0	0	627	31-40	412	
	15:00	0	6	12	12	12	78	211	211	323	323	128	128	32	5	4	5	5	4	4	0	0	0	0	0	0	0	799	31-40	534	
	16:00	0	1	23	23	23	80	319	319	345	345	121	121	26	4	1	4	4	1	1	0	0	0	0	0	0	0	920	31-40	664	
	17:00	3	6	15	15	15	118	304	342	304	304	108	108	17	3	2	3	3	2	2	1	1	0	0	0	0	0	919	31-40	646	
	18:00	0	1	0	0	0	47	195	127	195	195	94	94	32	3	2	3	3	2	2	0	0	0	0	0	0	0	501	31-40	322	
	19:00	0	0	4	4	4	32	131	131	148	148	70	70	18	0	0	0	0	0	0	0	0	0	0	0	0	0	403	31-40	279	
	20:00	0	0	2	2	2	13	67	67	104	104	50	50	11	5	0	5	5	0	0	0	0	0	0	0	0	0	252	31-40	171	
	21:00	0	0	2	2	2	12	61	61	88	88	44	44	6	1	0	1	1	0	0	0	0	0	0	0	0	0	214	31-40	149	
	22:00	0	0	0	0	0	4	14	14	39	39	30	30	6	2	1	2	2	1	1	0	0	0	0	0	0	0	96	36-45	69	
	23:00	0	0	1	1	1	4	16	16	25	25	15	15	6	1	0	1	1	0	0	0	0	0	0	0	0	0	68	31-40	41	
	Total	21	31	0.3%	97	1.2%	625	3303	2211	3303	1563	1563	383	69	3	20	69	0.8%	0.2%	0.2%	3	0.0%	0.0%	1	0.0%	0.0%	0	8327			
	Percent	0.3%	0.4%	0.7%	1.2%	1.2%	7.5%	39.7%	26.6%	39.7%	18.8%	18.8%	4.6%	0.8%	4.6%	0.2%	0.8%	0.2%	0.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			
	AM Peak	07:00	07:00	0.4%	08:00	08:00	07:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	11:00	11:00	11:00	11:00	07:00	07:00	11:00	11:00	07:00	07:00	07:00	07:00	07:00	08:00			
	Vol.	14	8	0.4%	5	5	33	114	114	227	104	104	29	9	1	3	9	0.8%	0.2%	0.2%	1	0.0%	0.0%	1	0.0%	0.0%	0	516			
	PM Peak	17:00	15:00	0.4%	16:00	16:00	17:00	17:00	17:00	17:00	16:00	14:00	15:00	15:00	15:00	15:00	13:00	13:00	15:00	15:00	12:00	12:00	16:00	16:00	16:00	16:00	16:00	16:00			
	Vol.	3	6	0.4%	23	23	118	342	342	345	134	134	32	6	3	4	6	0.8%	0.2%	0.2%	3	0.0%	0.0%	1	0.0%	0.0%	0	8327			
	Percent	0.3%	0.4%	0.4%	1.2%	1.2%	7.5%	39.7%	26.6%	39.7%	18.8%	18.8%	4.6%	0.8%	4.6%	0.2%	0.8%	0.2%	0.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			

Stats
 10 MPH Pace Speed : 31-40 MPH
 Number in Pace : 5514
 Percent in Pace : 66.2%
 Number of Vehicles > 30 MPH : 7553
 Percent of Vehicles > 30 MPH : 90.7%
 Mean Speed(Average) : 37 MPH

6TH ST BTW CAMPBELL AVE & TUCSON BLVD
Site Code: 17

WB	Start Time	15	16	20	21	25	26	30	31	35	36	40	41	45	46	50	51	55	56	60	61	65	66	70	71	75	76	999	Total	Pace Speed	Number in Pace
	08/23/22	0	0	0	1	1	5	2	8	8	7	7	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	22	30-39	15
	01:00	0	0	0	0	0	2	11	11	11	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	16	26-35	13
	02:00	0	0	0	1	1	0	0	5	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	26-35	5	
	03:00	0	0	0	0	0	1	7	7	7	6	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14	31-40	13		
	04:00	0	0	0	1	1	6	14	14	14	8	8	0	0	2	2	0	0	0	0	0	0	0	0	0	0	31	31-40	22		
	05:00	0	1	1	2	2	22	38	38	38	23	23	5	10	1	1	0	0	0	0	0	0	0	0	0	0	92	31-40	61		
	06:00	2	1	1	5	5	41	103	103	103	70	70	10	10	0	0	0	0	0	0	0	0	0	0	0	0	232	31-40	173		
	07:00	117	118	118	83	83	222	246	246	246	40	40	10	10	1	1	0	0	0	1	1	0	0	0	0	0	838	26-35	468		
	08:00	17	30	30	104	104	346	295	295	295	75	75	10	10	2	2	0	0	0	0	0	0	0	0	0	0	879	26-35	641		
	09:00	5	4	4	23	23	191	280	280	280	96	96	10	10	2	2	0	0	0	0	0	0	0	0	0	0	611	26-35	471		
	10:00	0	1	1	14	14	129	237	237	237	95	95	7	7	1	1	0	0	0	0	0	0	0	0	0	0	484	26-35	366		
	11:00	0	3	3	26	26	149	257	257	257	77	77	6	6	2	2	0	0	0	0	0	0	0	0	0	0	520	26-35	406		
	12 PM	1	5	5	11	11	152	278	278	278	93	93	15	15	3	3	1	1	0	0	0	0	0	0	0	0	559	26-35	430		
	13:00	0	1	1	12	12	142	257	257	257	109	109	12	12	3	3	0	0	0	0	0	0	0	0	0	0	536	26-35	399		
	14:00	3	11	11	25	25	210	266	266	266	91	91	10	10	3	3	0	0	0	0	0	0	0	0	0	0	619	26-35	476		
	15:00	1	17	17	31	31	251	292	292	292	107	107	14	14	2	2	0	0	0	2	2	0	0	0	0	0	717	26-35	543		
	16:00	7	4	4	32	32	168	314	314	314	129	129	17	17	4	4	0	0	0	2	2	0	0	0	0	0	677	26-35	482		
	17:00	6	7	7	58	58	260	291	291	291	97	97	20	20	1	1	1	1	0	1	1	0	0	0	0	0	742	26-35	551		
	18:00	5	3	3	15	15	143	231	231	231	78	78	11	11	0	0	0	0	0	0	0	0	0	0	0	0	486	26-35	374		
	19:00	0	6	6	17	17	93	184	184	184	54	54	9	9	2	2	0	0	0	0	0	0	0	0	0	0	365	26-35	277		
	20:00	3	1	1	16	16	89	133	133	133	40	40	3	3	1	1	0	0	0	0	0	0	0	0	0	0	286	26-35	222		
	21:00	4	1	1	4	4	41	88	88	88	35	35	8	8	0	0	0	0	0	0	0	0	0	0	0	0	181	26-35	129		
	22:00	0	0	0	4	4	24	71	71	71	23	23	7	7	1	1	1	1	0	0	0	0	0	0	0	0	131	26-35	95		
	23:00	0	0	0	2	2	11	23	23	23	13	13	4	4	0	0	0	0	0	0	0	0	0	0	0	0	53	31-40	36		
	Total	171	214	214	487	487	2698	3929	3929	3929	1367	1367	191	191	31	31	3	3	6	6	0	0	0	0	0	0	9097				
	Percent	1.9%	2.4%	2.4%	5.4%	5.4%	29.7%	43.2%	43.2%	43.2%	15.0%	15.0%	2.1%	2.1%	0.3%	0.3%	0.0%	0.0%	0.1%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			
	AM Peak	07:00	07:00	07:00	08:00	08:00	08:00	08:00	08:00	08:00	09:00	09:00	06:00	06:00	04:00	04:00	07:00	07:00	07:00	07:00	07:00	07:00	07:00	07:00	07:00	07:00	07:00	08:00			
	Vol.	117	118	118	104	104	346	295	295	295	96	96	10	10	2	2	1	1	1	1	0	0	0	0	0	0	0	879			
	PM Peak	16:00	15:00	15:00	17:00	17:00	17:00	16:00	16:00	16:00	16:00	16:00	17:00	17:00	16:00	16:00	12:00	12:00	15:00	15:00	15:00	15:00	15:00	15:00	15:00	15:00	17:00				
	Vol.	7	17	17	58	58	260	314	314	314	129	129	20	20	4	4	1	1	2	2	2	2	2	2	2	2	742				
	Total	171	214	214	487	487	2698	3929	3929	3929	1367	1367	191	191	31	31	3	3	6	6	0	0	0	0	0	0	9097				
	Percent	1.9%	2.4%	2.4%	5.4%	5.4%	29.7%	43.2%	43.2%	43.2%	15.0%	15.0%	2.1%	2.1%	0.3%	0.3%	0.0%	0.0%	0.1%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%				

Stats	10 MPH Pace Speed :	26-35 MPH
Number in Pace :	6627	
Percent in Pace :	72.8%	
Number of Vehicles > 30 MPH :	5527	
Percent of Vehicles > 30 MPH :	60.8%	
Mean Speed(Average) :	31 MPH	

EB	Start Time	15	16	20	21	25	26	30	31	35	36	40	41	45	46	50	51	55	56	60	61	65	66	70	71	75	76	999	Total	Pace Speed	Number in Pace
	08/23/22	0	0	1	1	1	9	12	12	12	4	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	26	26-35	21
	01:00	1	1	2	2	2	5	8	8	8	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	18	26-35	13	
	02:00	0	0	0	0	0	4	6	6	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	26-35	10		
	03:00	0	0	0	0	0	5	4	4	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	26-35	9		
	04:00	0	0	0	0	0	3	5	5	5	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	26-35	8		
	05:00	0	0	0	0	0	10	23	23	23	9	9	0	0	1	1	0	0	0	0	0	0	0	0	0	0	43	26-35	33		
	06:00	1	0	4	4	4	35	77	77	77	22	22	4	4	4	4	0	0	0	0	0	0	0	0	0	0	143	26-35	112		
	07:00	3	2	1	1	1	115	217	217	217	67	67	12	12	1	1	0	0	0	0	0	0	0	0	0	0	418	26-35	332		
	08:00	3	1	12	12	12	156	281	281	281	78	78	4	4	0	0	0	0	0	0	0	0	0	0	0	0	535	26-35	437		
	09:00	3	1	11	11	11	133	189	189	189	41	41	8	8	0	0	0	0	0	0	0	0	0	0	0	0	386	26-35	322		
	10:00	1	0	7	7	7	153	219	219	219	42	42	5	5	0	0	0	0	1	1	0	0	0	0	0	0	428	26-35	372		
	11:00	3	1	13	13	13	153	258	258	258	59	59	8	8	1	1	0	0	0	0	0	0	0	0	0	0	496	26-35	411		
	12 PM	5	1	14	14	14	227	285	285	285	47	47	5	5	0	0	0	0	0	0	0	0	0	0	0	0	584	26-35	512		
	13:00	3	0	16	16	16	163	289	289	289	49	49	5	5	2	2	0	0	0	0	0	0	0	0	0	0	527	26-35	452		
	14:00	2	1	5	5	5	231	372	372	372	44	44	4	4	1	1	0	0	0	0	0	0	0	0	0	0	660	26-35	603		
	15:00	4	1	5	5	5	303	411	411	411	73	73	10	10	3	3	0	0	0	0	0	0	0	0	0	0	810	26-35	714		
	16:00	2	1	24	24	24	357	435	435	435	53	53	6	6	0	0	0	0	0	0	0	0	0	0	0	0	878	26-35	792		
	17:00	1	1	20	20	20	352	401	401	401	59	59	6	6	0	0	0	0	0	0	0	0	0	0	0	0	840	26-35	753		
	18:00	1	0	10	10	10	173	249	249	249	35	35	5	5	0	0	0	0	0	0	0	0	0	0	0	0	473	26-35	422		
	19:00	0	0	13	13	13	159	165	165	165	28	28	2	2	0	0	0	0	0	0	0	0	0	0	0	0	367	26-35	324		
	20:00	0	1	9	9	9	89	109	109	109	22	22	4	4	0	0	0	0	0	0	0	0	0	0	0	0	234	26-35	198		
	21:00	0	0	7	7	7	67	99	99	99	21	21	3	3	0	0	0	0	0	0	0	0	0	0	0	0	197	26-35	166		
	22:00	0	1	2	2	2	19	49	49	49	13	13	2	2	0	0	0	0	0	0	0	0	0	0	0	0	86	26-35	68		
	23:00	0	0	3	3	3	18	19	19	19	4	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	44	26-35	37		
	Total	33	13	179	179	179	2939	4182	4182	4182	772	772	93	93	9	9	0	0	0	1	1	0	0	0	0	0	0	8221			
	Percent	0.4%	0.2%	2.2%	2.2%	2.2%	35.7%	50.9%	50.9%	50.9%	9.4%	9.4%	1.1%	1.1%	0.1%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			
	AM Peak	07:00	07:00	11:00	11:00	11:00	08:00	08:00	08:00	08:00	08:00	08:00	07:00	07:00	05:00	05:00	0.0%	0.0%	0.0%	0.0%	10:00	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	08:00		
	Vol.	3	2	13	13	13	156	281	281	281	78	78	12	12	1	1	0	0	0	1	1	0	0	0	0	0	0	535			
	PM Peak	12:00	12:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	15:00	15:00	15:00	15:00	15:00	15:00	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	16:00			
	Vol.	5	1	24	24	24	357	435	435	435	73	73	10	10	3	3	0	0	0	1	1	0	0	0	0	0	0	878			
	Total	33	13	179	179	179	2939	4182	4182	4182	772	772	93	93	9	9	0	0	0	1	1	0	0	0	0	0	0	8221			
	Percent	0.4%	0.2%	2.2%	2.2%	2.2%	35.7%	50.9%	50.9%	50.9%	9.4%	9.4%	1.1%	1.1%	0.1%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			

Stats	10 MPH Pace Speed :	26-35 MPH
Number in Pace :	7121	
Percent in Pace :	86.6%	
Number of Vehicles > 30 MPH :	5057	
Percent of Vehicles > 30 MPH :	61.5%	
Mean Speed(Average) :	31 MPH	

WB	Start Time	15	16	20	21	25	26	30	31	35	36	40	41	45	46	50	51	55	56	60	61	65	66	70	71	75	76	999	Total	Pace Speed	Number in Pace
	08/23/22	0	0	0	1	1	3	0	5	3	7	8	2	1	1	0	0	0	2	0	0	0	0	0	0	0	0	0	21	31-40	12
	01:00	0	0	0	0	0	0	0	3	3	8	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12	31-40	11
	02:00	0	0	0	0	0	0	0	3	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	29-38	4	
	03:00	0	2	0	0	0	1	1	1	3	3	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	9	36-45	4	
	04:00	0	0	0	0	0	2	2	5	10	10	4	4	4	3	2	2	0	0	0	0	0	0	0	0	0	0	26	31-40	15	
	05:00	0	0	0	2	2	7	7	19	22	22	18	18	18	6	3	3	0	0	0	0	0	0	0	0	0	0	77	31-40	41	
	06:00	1	1	1	1	1	4	4	49	74	74	53	53	4	4	3	4	0	3	0	0	0	0	0	0	0	0	204	36-45	127	
	07:00	5	24	2	39	28	111	98	259	242	266	266	77	77	9	10	3	0	0	0	0	0	0	0	0	0	0	769	31-40	501	
	08:00	1	2	4	3	12	48	55	182	202	202	202	77	77	20	20	4	0	1	1	0	0	0	0	0	0	0	783	31-40	584	
	09:00	1	4	5	3	12	48	55	175	173	173	173	57	57	17	17	2	0	2	2	0	0	0	0	0	0	0	542	31-40	384	
	10:00	2	5	5	12	12	55	55	175	173	173	173	57	57	17	17	2	0	2	2	0	0	0	0	0	0	0	500	31-40	348	
	11:00	0	1	1	4	4	40	40	197	167	167	167	62	62	15	15	0	0	0	0	0	0	1	1	0	0	0	487	31-40	364	
	12 PM	2	7	7	3	3	46	46	213	216	216	216	62	62	12	12	2	0	0	0	0	0	0	0	0	0	0	563	31-40	429	
	13:00	1	1	1	14	14	32	32	160	180	180	81	81	81	25	25	6	0	0	0	0	0	0	0	0	0	0	500	31-40	340	
	14:00	2	2	2	1	1	62	62	194	230	230	61	61	61	13	13	0	0	0	0	0	0	0	0	0	0	0	565	31-40	424	
	15:00	0	3	3	23	23	104	104	313	237	237	52	52	52	13	13	0	0	0	0	0	0	0	0	0	0	0	745	31-40	550	
	16:00	0	1	1	8	8	75	75	252	227	227	67	67	67	14	14	1	0	0	0	0	0	0	0	0	0	0	645	31-40	479	
	17:00	0	2	2	10	10	96	96	318	189	189	59	59	59	13	13	1	0	0	0	0	0	0	0	0	0	0	688	31-40	507	
	18:00	0	3	3	7	7	45	45	207	191	191	40	40	40	9	9	2	0	1	1	0	0	0	0	0	0	0	505	31-40	398	
	19:00	0	0	0	2	2	31	31	161	119	119	27	27	27	2	2	1	0	1	1	1	1	0	0	0	0	0	345	31-40	280	
	20:00	0	0	0	0	0	26	26	71	89	89	33	33	33	14	14	3	0	1	1	0	0	0	0	0	0	0	237	31-40	160	
	21:00	0	1	1	2	2	12	12	51	69	69	26	26	26	4	4	0	0	0	0	0	0	0	0	0	0	0	165	31-40	120	
	22:00	0	0	0	3	3	9	9	31	35	35	8	8	8	2	2	1	0	0	0	0	0	0	0	0	0	0	89	31-40	66	
	23:00	0	0	0	0	0	3	3	8	12	12	10	10	10	2	2	2	0	0	0	0	0	0	0	0	0	0	37	35-44	22	
	Total	15	59	163	1.9%	1.9%	910	10.7%	3195	2969	2969	937	937	219	219	38	11	1	1	1	1	1	1	1	1	0	0	8518			
	Percent	0.2%	0.7%	1.9%	10.7%	10.7%	37.5%	37.5%	34.9%	34.9%	34.9%	11.0%	11.0%	2.6%	2.6%	0.4%	0.1%	0.0%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			
	AM Peak	07:00	07:00	07:00	07:00	07:00	07:00	07:00	08:00	08:00	08:00	08:00	07:00	07:00	09:00	09:00	06:00	06:00	06:00	06:00	06:00	06:00	11:00	11:00	0.0%	0.0%	0.0%	0.0%	08:00		
	Vol.	5	24	39	39	39	111	111	318	266	266	77	77	20	20	4	4	4	3	3	3	19:00	19:00	1	1	0.0%	0.0%	783			
	PM Peak	12:00	12:00	15:00	15:00	15:00	15:00	15:00	17:00	15:00	15:00	13:00	13:00	13:00	13:00	13:00	13:00	13:00	18:00	18:00	18:00	19:00	19:00	1	1	0.0%	0.0%	15:00			
	Vol.	2	7	23	23	23	104	104	318	237	237	81	81	25	25	6	6	6	1	1	1	1	1	1	1	0.0%	0.0%	745			
	Total	15	59	163	1.9%	1.9%	910	10.7%	3195	2969	2969	937	937	219	219	38	11	1	1	1	1	1	1	1	1	0.0%	0.0%	8518			
	Percent	0.2%	0.7%	1.9%	10.7%	10.7%	37.5%	37.5%	34.9%	34.9%	34.9%	11.0%	11.0%	2.6%	2.6%	0.4%	0.1%	0.0%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			

Stats
 10 MPH Pace Speed : 31-40 MPH
 Number in Pace : 6164
 Percent in Pace : 72.4%
 Number of Vehicles > 30 MPH : 7371
 Percent of Vehicles > 30 MPH : 86.5%
 Mean Speed(Average) : 35 MPH

Site Code: 19
5TH ST BTW SWAN RD & ROSEMONT BLVD

EB	Start Time	15	16	20	21	25	26	30	31	35	36	40	41	45	46	50	51	55	56	60	61	65	66	70	71	75	76	999	Total	Pace Speed	Number in Pace
	08/23/22	0	0	0	0	0	4	5	6	6	11	11	3	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	24	31-40	17
	01:00	0	0	0	0	0	5	4	4	4	2	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12	26-35	9
	02:00	0	0	0	0	0	4	2	2	2	4	4	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	31-40	6
	03:00	0	0	0	0	0	1	5	5	5	2	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	9	31-40	7	
	04:00	0	0	0	0	0	0	1	1	1	10	10	7	7	0	0	1	1	0	0	0	0	0	0	0	0	0	19	36-45	17	
	05:00	0	0	0	1	1	7	14	14	14	19	19	3	3	2	2	2	2	1	1	0	0	0	0	0	0	0	49	31-40	33	
	06:00	0	0	0	4	4	14	52	52	52	62	62	25	25	7	7	1	1	0	0	0	1	0	0	0	0	0	166	31-40	114	
	07:00	5	2	35	35	90	208	208	208	208	122	122	31	31	4	4	0	0	0	0	0	0	0	0	0	0	0	497	31-40	330	
	08:00	9	15	64	64	206	269	269	269	269	102	102	20	20	1	1	1	1	0	0	0	0	0	0	0	0	0	687	26-35	475	
	09:00	2	0	1	1	23	144	144	144	154	154	36	36	36	9	9	1	1	0	0	0	0	0	0	0	0	0	370	31-40	298	
	10:00	1	2	3	3	38	112	112	112	138	138	35	35	35	5	5	2	2	1	1	0	0	0	0	0	0	0	338	31-40	250	
	11:00	2	0	2	2	21	131	131	131	150	150	40	40	40	10	10	0	0	0	0	0	0	0	0	0	0	0	356	31-40	281	
	12 PM	0	0	0	2	23	110	110	110	170	170	57	57	57	6	6	2	2	1	1	0	0	0	0	0	0	0	371	31-40	280	
	13:00	0	1	3	3	20	130	130	130	184	184	74	74	74	10	10	4	4	0	0	0	0	0	1	0	0	0	427	31-40	314	
	14:00	1	1	11	11	74	202	202	202	178	178	45	45	45	8	8	0	0	0	0	0	0	0	0	0	0	0	520	31-40	380	
	15:00	39	38	63	63	132	238	238	238	165	165	24	24	24	2	2	0	0	0	0	0	0	0	0	0	0	0	701	31-40	403	
	16:00	6	0	6	6	65	264	264	264	297	297	74	74	74	11	11	1	1	1	1	0	0	0	0	0	0	0	725	31-40	561	
	17:00	4	1	5	5	49	244	244	244	280	280	79	79	79	7	7	1	1	0	0	0	0	0	0	0	0	0	670	31-40	524	
	18:00	0	0	2	2	17	89	89	89	161	161	65	65	65	12	12	2	2	0	0	0	0	0	0	0	0	0	348	31-40	250	
	19:00	0	1	5	5	34	114	114	114	79	79	24	24	24	4	4	0	0	0	0	0	0	0	0	0	0	0	261	31-40	193	
	20:00	0	0	9	9	17	79	79	79	48	48	13	13	13	3	3	0	0	2	2	0	0	0	0	0	0	0	171	31-40	127	
	21:00	1	1	1	1	11	45	45	45	31	31	12	12	12	1	1	0	0	0	0	0	0	0	0	0	0	0	103	31-40	76	
	22:00	0	0	1	1	9	27	27	27	28	28	8	8	8	2	2	0	0	0	0	0	0	0	0	0	0	0	75	31-40	55	
	23:00	0	0	1	1	5	12	12	12	12	12	3	3	3	2	2	0	0	0	0	0	0	0	0	0	0	0	35	31-40	24	
	Total	70	62	219	219	869	2502	2502	2502	2409	2409	681	681	681	106	106	18	18	6	6	1	1	1	1	1	1	0	6945			
	Percent	1.0%	0.9%	3.2%	3.2%	12.5%	36.0%	36.0%	36.0%	34.7%	34.7%	9.8%	9.8%	9.8%	1.5%	1.5%	0.3%	0.3%	0.1%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			
	AM Peak	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	09:00	09:00	11:00	11:00	11:00	11:00	05:00	05:00	05:00	05:00	06:00	06:00	06:00	06:00	06:00	06:00	06:00	08:00			
	Vol.	9	15	64	64	206	269	269	269	154	154	40	40	40	10	10	2	2	1	1	1	1	1	1	1	1	1	687			
	PM Peak	15:00	15:00	15:00	15:00	15:00	16:00	16:00	16:00	16:00	16:00	17:00	17:00	17:00	18:00	18:00	13:00	13:00	20:00	20:00	20:00	20:00	20:00	20:00	20:00	20:00	20:00	16:00			
	Vol.	39	38	63	63	132	264	264	264	297	297	79	79	79	12	12	4	4	2	2	1	1	1	1	1	1	1	725			
	Total	70	62	219	219	869	2502	2502	2502	2409	2409	681	681	681	106	106	18	18	6	6	1	1	1	1	1	1	0	6945			
	Percent	1.0%	0.9%	3.2%	3.2%	12.5%	36.0%	36.0%	36.0%	34.7%	34.7%	9.8%	9.8%	9.8%	1.5%	1.5%	0.3%	0.3%	0.1%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			

Stats	10 MPH Pace Speed :	31-40 MPH
Number in Pace :	4911	
Percent in Pace :	70.7%	
Number of Vehicles > 35 MPH :	3223	
Percent of Vehicles > 35 MPH :	46.4%	
Mean Speed(Average) :	35 MPH	

Site Code: 19
5TH ST BTW SWAN RD & ROSEMONT BLVD

WB	Start Time	15	16	20	21	25	26	30	31	35	36	40	41	45	46	50	51	55	56	60	61	65	66	70	71	75	76	999	Total	Pace Speed	Number in Pace
	08/23/22	0	0	0	0	0	2	1	5	5	9	9	2	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	19	31-40	14
	01:00	0	0	0	0	0	1	1	5	5	6	6	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	13	31-40	11	
	02:00	0	0	0	0	0	0	0	4	4	3	3	1	1	0	0	1	1	0	0	0	0	0	0	0	0	0	9	31-40	7	
	03:00	0	0	0	0	0	0	0	1	1	6	6	4	4	0	0	0	0	0	0	0	0	0	0	0	0	0	11	36-45	10	
	04:00	1	0	0	0	0	4	4	16	16	8	8	8	8	5	5	2	2	0	0	0	0	0	0	0	0	0	44	31-40	24	
	05:00	0	0	0	0	0	5	5	25	25	41	41	15	15	3	3	0	0	0	0	0	0	0	0	0	0	0	89	31-40	66	
	06:00	5	7	7	7	7	16	16	79	79	62	62	34	34	9	9	2	2	0	0	0	0	0	0	0	0	0	221	31-40	141	
	07:00	9	20	20	32	108	115	115	205	205	154	154	36	36	4	4	0	0	1	1	0	0	0	0	0	0	0	569	31-40	359	
	08:00	5	9	9	58	115	115	115	210	210	153	153	29	29	4	4	0	0	0	0	0	0	0	0	0	0	0	583	31-40	363	
	09:00	0	1	1	12	51	51	51	173	173	123	123	40	40	9	9	0	0	0	0	0	0	0	0	0	0	0	409	31-40	296	
	10:00	1	3	3	9	69	69	69	124	124	111	111	23	23	4	4	0	0	0	0	0	0	0	0	0	0	0	345	31-40	235	
	11:00	1	4	4	12	52	52	52	168	168	104	104	46	46	9	9	0	0	0	0	0	0	0	0	0	0	0	396	31-40	272	
	12 PM	0	3	3	12	47	47	47	128	128	152	152	32	32	4	4	1	1	0	0	0	0	0	0	0	0	0	380	31-40	280	
	13:00	1	1	1	4	4	45	45	124	124	146	146	39	39	4	4	4	4	0	0	0	0	0	0	0	0	0	368	31-40	270	
	14:00	0	4	4	9	9	66	66	190	190	115	115	34	34	8	8	2	2	0	0	0	0	0	0	0	0	0	428	31-40	305	
	15:00	6	15	15	26	130	130	130	232	232	127	127	29	29	4	4	0	0	0	0	0	0	0	0	0	0	0	569	26-35	362	
	16:00	7	10	10	26	80	80	80	218	218	157	157	36	36	6	6	0	0	0	0	0	0	0	0	0	0	0	540	31-40	375	
	17:00	7	5	5	34	93	93	93	191	191	155	155	50	50	7	7	2	2	0	0	0	0	0	0	0	0	0	544	31-40	346	
	18:00	0	3	3	9	33	33	33	110	110	112	112	34	34	3	3	1	1	0	0	0	0	0	0	0	0	0	305	31-40	222	
	19:00	0	4	4	4	22	22	22	84	84	82	82	15	15	5	5	0	0	0	0	0	0	0	0	0	0	0	216	31-40	166	
	20:00	2	14	14	10	31	31	31	65	65	45	45	14	14	1	1	2	2	1	1	0	0	0	0	0	0	0	185	31-40	110	
	21:00	0	2	2	4	4	9	9	36	36	40	40	13	13	2	2	0	0	0	0	0	0	0	0	0	0	0	106	31-40	76	
	22:00	0	0	0	0	0	7	7	22	22	23	23	10	10	1	1	0	0	0	0	0	0	0	0	0	0	0	63	31-40	45	
	23:00	0	0	0	0	0	2	2	4	4	5	5	7	7	2	2	0	0	0	0	0	0	0	0	0	0	0	20	36-45	12	
	Total	45	105	105	268	988	988	988	2419	2419	1939	1939	552	552	95	95	17	17	4	4	0	0	0	0	0	0	0	6432			
	Percent	0.7%	1.6%	1.6%	4.2%	15.4%	15.4%	15.4%	37.6%	37.6%	30.1%	30.1%	8.6%	8.6%	1.5%	1.5%	0.3%	0.3%	0.1%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			
	AM Peak	07:00	07:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	07:00	07:00	11:00	11:00	06:00	06:00	04:00	04:00	07:00	07:00											
	Vol.	9	20	20	58	115	115	115	210	210	154	154	46	46	9	9	2	2	1	1											
	PM Peak	16:00	15:00	17:00	17:00	15:00	15:00	15:00	15:00	15:00	16:00	16:00	17:00	17:00	14:00	14:00	13:00	13:00	12:00	12:00											
	Vol.	7	15	15	34	130	130	130	232	232	157	157	50	50	8	8	4	4	1	1											
	Total	45	105	105	268	988	988	988	2419	2419	1939	1939	552	552	95	95	17	17	4	4											
	Percent	0.7%	1.6%	1.6%	4.2%	15.4%	15.4%	15.4%	37.6%	37.6%	30.1%	30.1%	8.6%	8.6%	1.5%	1.5%	0.3%	0.3%	0.1%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			

Stats	10 MPH Pace Speed	31-40 MPH
Number in Pace	988	4358
Percent in Pace	67.8%	2607
Number of Vehicles > 35 MPH	2607	40.5%
Percent of Vehicles > 35 MPH	40.5%	34 MPH
Mean Speed(Average)	34 MPH	

All Traffic Data Services, LLC
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Site Code: 20
5TH ST BTW SAHUARA AVE & WILMOT RD

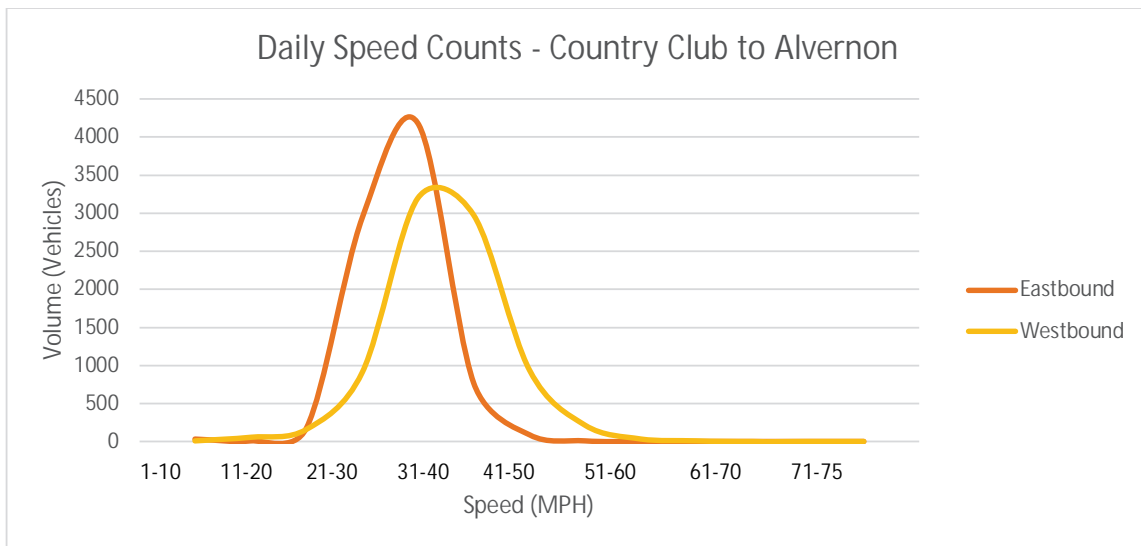
EB	Start Time	15	16	20	21	25	26	30	31	35	36	40	41	45	46	50	51	55	56	60	61	65	66	70	71	75	76	999	Total	Pace Speed	Number in Pace
	08/23/22	0	0	0	0	0	2	4	4	4	7	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	13	31-40	11
	01:00	0	0	0	0	0	1	1	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	24-33	2
	02:00	0	1	0	0	0	0	3	3	3	3	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	31-40	6	
	03:00	0	0	0	0	0	1	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	0	7	29-38	4	
	04:00	0	0	0	0	0	1	2	2	2	1	1	0	0	2	2	2	0	1	1	0	0	0	0	0	0	0	7	31-40	3	
	05:00	0	2	4	4	4	2	11	11	11	11	11	4	4	0	0	0	0	0	0	0	0	0	0	0	0	0	34	31-40	22	
	06:00	1	1	3	4	4	7	28	28	28	34	34	23	23	9	9	4	4	1	1	0	0	0	0	0	0	0	112	31-40	62	
	07:00	1	3	7	24	24	56	115	115	115	88	88	29	29	9	9	1	1	0	0	0	0	0	0	0	0	0	326	31-40	203	
	08:00	0	11	42	42	42	83	143	143	143	83	83	28	28	3	3	0	0	0	0	0	0	0	0	0	0	0	393	26-35	226	
	09:00	0	3	9	9	9	25	94	94	94	113	113	44	44	1	1	2	2	0	0	0	0	0	0	0	0	0	291	31-40	207	
	10:00	0	5	10	10	10	31	102	102	102	89	89	56	56	12	12	0	0	0	0	0	0	0	0	0	0	0	305	31-40	191	
	11:00	2	3	15	15	15	25	107	107	107	121	121	31	31	6	6	0	0	1	1	0	0	0	0	0	0	0	311	31-40	228	
	12 PM	0	7	12	12	12	35	119	119	119	124	124	49	49	8	8	1	1	0	0	0	1	0	0	0	0	0	356	31-40	243	
	13:00	0	0	6	6	6	29	96	96	96	123	123	71	71	23	23	0	0	0	3	0	0	0	0	0	0	0	351	31-40	219	
	14:00	1	15	53	53	53	110	127	127	127	65	65	11	11	5	5	2	2	0	0	0	0	0	0	0	0	0	389	26-35	237	
	15:00	0	1	10	10	10	18	166	166	166	194	194	78	78	13	13	3	3	1	1	0	0	0	0	0	1	0	485	31-40	360	
	16:00	0	1	14	14	14	34	147	147	147	223	223	114	114	26	26	8	8	0	0	0	0	0	0	0	0	0	567	31-40	370	
	17:00	0	2	10	10	10	36	172	172	172	197	197	93	93	15	15	0	0	1	1	0	0	0	0	0	0	0	526	31-40	369	
	18:00	0	1	10	10	10	23	61	61	61	114	114	44	44	16	16	4	4	0	0	0	0	0	0	0	0	0	273	31-40	175	
	19:00	0	0	7	7	7	14	58	58	58	60	60	22	22	7	7	0	0	0	0	0	0	0	0	0	0	0	168	31-40	118	
	20:00	0	1	2	2	2	6	33	33	33	36	36	21	21	4	4	1	1	0	0	0	0	0	0	0	0	0	104	31-40	69	
	21:00	1	0	8	8	8	6	15	15	15	15	15	10	10	2	2	0	0	1	1	0	0	0	0	0	0	0	58	31-40	30	
	22:00	0	1	1	1	1	4	13	13	13	13	13	3	3	2	2	2	2	0	0	0	0	0	0	0	0	0	39	31-40	26	
	23:00	0	1	1	1	1	1	10	10	10	6	6	5	5	0	0	1	1	0	0	0	0	0	0	0	0	0	25	31-40	16	
	Total	6	59	242	242	242	550	1629	1629	1629	1723	1723	739	739	163	163	29	29	9	9	1	1	0	0	0	1	0	5151			
	Percent	0.1%	1.1%	4.7%	4.7%	4.7%	10.7%	31.6%	31.6%	31.6%	33.4%	33.4%	14.3%	14.3%	3.2%	3.2%	0.6%	0.6%	0.2%	0.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			
	AM Peak	11:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	11:00	11:00	10:00	10:00	10:00	10:00	06:00	06:00	04:00	04:00									08:00		
	Vol.	2	11	42	42	42	83	143	143	143	121	121	56	56	12	12	4	4	1	1								393			
	PM Peak	14:00	14:00	14:00	14:00	14:00	14:00	14:00	14:00	14:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	13:00	13:00	12:00	12:00	12:00	12:00	12:00	15:00	15:00	16:00			
	Vol.	1	15	53	53	53	110	172	172	172	223	223	114	114	26	26	8	8	3	3	1	1	1	1	1	1	1	567			
	Total	6	59	242	242	242	550	1629	1629	1629	1723	1723	739	739	163	163	29	29	9	9	1	1	0	0	0	0	0	5151			
	Percent	0.1%	1.1%	4.7%	4.7%	4.7%	10.7%	31.6%	31.6%	31.6%	33.4%	33.4%	14.3%	14.3%	3.2%	3.2%	0.6%	0.6%	0.2%	0.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			

Stats	10 MPH Pace Speed	31-40 MPH
Number in Pace	3352	3352
Percent in Pace	65.1%	65.1%
Number of Vehicles > 40 MPH	942	942
Percent of Vehicles > 40 MPH	18.3%	18.3%
Mean Speed(Average)	36 MPH	36 MPH

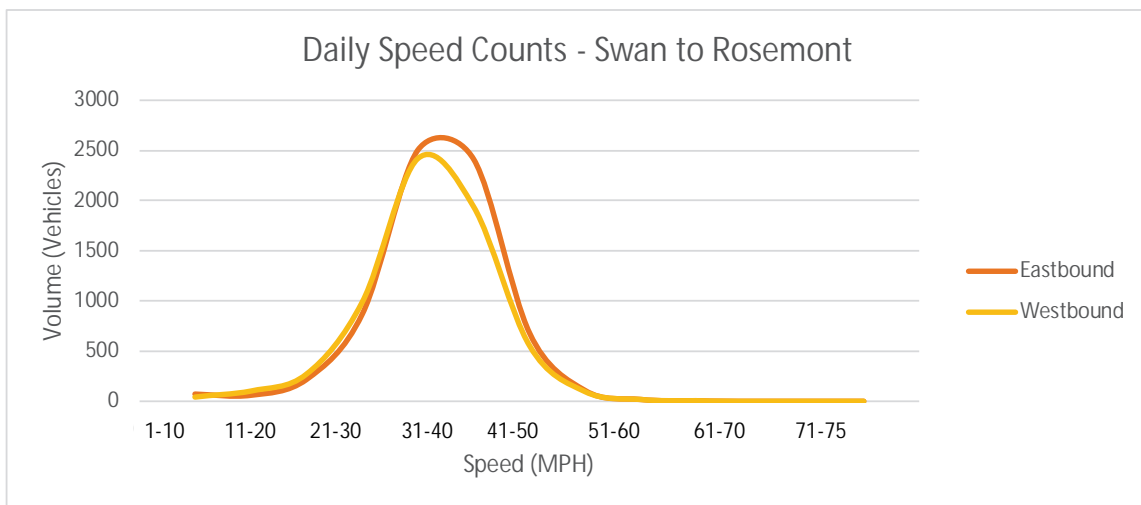
Site Code: 20
5TH ST BTW SAHUARA AVE & WILMOT RD

WB	Start Time	15	16	20	21	25	26	30	31	35	36	40	41	45	46	50	51	55	56	60	61	65	66	70	71	75	76	999	Total	Pace Speed	Number in Pace
	08/23/22	0	2	0	1	1	1	1	0	0	5	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	36-45	7
	01:00	0	0	0	0	0	1	0	0	0	2	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	4	29-38	2	
	02:00	0	0	0	1	1	0	0	3	1	1	1	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	7	31-40	4	
	03:00	0	0	0	1	1	0	0	2	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	31-40	3	
	04:00	0	0	0	1	1	2	2	13	5	5	3	3	3	2	2	2	2	0	0	0	0	0	0	0	0	0	28	31-40	18	
	05:00	0	1	1	1	1	3	3	15	17	17	7	7	7	2	2	0	0	0	0	0	0	0	0	0	0	0	46	31-40	32	
	06:00	1	1	1	2	2	7	7	31	50	50	24	24	24	10	10	1	1	1	1	0	0	0	0	0	0	0	128	31-40	81	
	07:00	1	8	32	102	144	121	144	154	104	104	34	34	34	7	7	1	1	0	0	0	0	0	0	0	0	0	450	31-40	265	
	08:00	5	13	38	116	154	117	116	81	117	117	32	32	61	11	11	2	2	0	0	0	0	0	0	0	0	466	26-35	270		
	09:00	1	0	2	7	7	19	19	68	131	131	42	42	42	10	10	1	1	0	0	0	0	0	0	0	0	0	292	31-40	198	
	10:00	0	2	4	4	4	17	17	94	138	138	60	60	60	8	8	1	1	0	0	0	0	0	0	0	0	0	280	31-40	199	
	11:00	1	3	3	4	4	21	21	75	99	99	64	64	64	10	10	2	2	0	0	0	0	1	1	0	0	0	329	31-40	232	
	12 PM	0	1	0	6	6	19	19	87	120	120	52	52	52	7	7	1	1	0	0	0	0	0	0	0	0	0	278	31-40	174	
	13:00	1	0	0	3	3	19	19	81	114	114	83	83	83	8	8	0	0	0	0	0	0	0	0	0	0	0	290	31-40	207	
	14:00	3	12	31	81	81	28	28	92	165	165	72	72	72	8	8	0	0	0	0	0	0	0	0	0	0	0	355	30-39	197	
	15:00	0	0	0	5	5	19	19	96	155	155	74	74	74	15	15	4	4	0	0	0	0	0	0	0	0	0	374	31-40	257	
	16:00	0	1	0	6	6	23	23	93	158	158	86	86	86	15	15	1	1	0	0	0	0	0	0	0	0	0	367	31-40	251	
	17:00	0	0	0	9	9	15	15	42	69	69	43	43	43	9	9	3	3	1	1	0	0	0	0	0	0	0	386	31-40	251	
	18:00	0	0	0	5	5	15	15	42	69	69	43	43	43	9	9	3	3	1	1	0	0	0	0	0	0	0	187	34-43	112	
	19:00	0	1	1	6	6	16	16	49	58	58	17	17	17	0	0	1	1	0	0	0	0	0	0	0	0	0	148	31-40	107	
	20:00	0	1	0	0	0	10	10	29	33	33	15	15	15	3	3	0	0	0	0	0	0	0	0	0	0	0	91	31-40	62	
	21:00	0	0	0	3	3	6	6	18	20	20	11	11	11	3	3	2	2	0	0	0	0	0	0	0	0	0	63	31-40	38	
	22:00	0	0	0	2	2	2	2	14	14	10	5	5	5	3	3	0	0	0	0	0	0	0	0	0	0	0	36	31-40	24	
	23:00	0	0	0	0	0	1	1	4	4	4	4	2	2	0	0	0	0	0	0	0	1	1	0	0	0	0	12	31-40	8	
	Total	13	46	166	366	526	731	1318	1666	1666	1666	731	731	731	137	137	23	23	4	4	2	2	1	1	0	0	0	4633			
	Percent	0.3%	1.0%	3.6%	11.4%	11.4%	15.8%	28.4%	36.0%	36.0%	36.0%	15.8%	15.8%	15.8%	3.0%	3.0%	0.5%	0.5%	0.1%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			
	AM Peak	08:00	08:00	08:00	08:00	08:00	09:00	09:00	09:00	09:00	09:00	04:00	04:00	04:00	06:00	06:00	06:00	06:00	06:00	06:00	11:00	11:00	11:00	11:00	11:00	11:00	11:00	08:00			
	Vol.	5	13	38	116	116	61	61	138	138	138	61	61	61	11	11	2	2	1	1	1	1	1	1	1	1	1	466			
	PM Peak	14:00	14:00	14:00	14:00	14:00	14:00	14:00	14:00	15:00	15:00	17:00	17:00	17:00	16:00	16:00	15:00	15:00	17:00	17:00	23:00	23:00	23:00	23:00	23:00	23:00	17:00				
	Vol.	3	12	31	81	81	86	86	165	165	165	86	86	86	15	15	4	4	4	4	1	1	1	1	1	1	1	386			
	Total	13	46	166	526	526	731	1318	1666	1666	1666	731	731	731	137	137	23	23	4	4	2	2	1	1	0	0	0	4633			
	Percent	0.3%	1.0%	3.6%	11.4%	11.4%	15.8%	28.4%	36.0%	36.0%	36.0%	15.8%	15.8%	15.8%	3.0%	3.0%	0.5%	0.5%	0.1%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			
	Stats	10 MPH Pace Speed :		31-40 MPH		Number in Pace :		2984		Percent in Pace :		64.4%		Number of Vehicles > 40 MPH :		898		Percent of Vehicles > 40 MPH :		19.4%		Mean Speed(Average) :		36 MPH							

Daily Speed Profiles: Country Club to Alvernon Way & Swan Road to Rosemont Boulevard



- Posted Speed: **30 MPH**
- Average Speed: **31 MPH (EB), 35 MPH (WB)**
- 85th Percentile Speed: **34 MPH (EB), 39 MPH (WB)**



- Posted Speed: **30 MPH**
- Average Speed: **34 MPH (EB), 35 MPH (WB)**
- 85th Percentile Speed: **39 MPH (EB), 39 MPH (WB)**



Appendix F
Existing Scenario A
Simtraffic Reports

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	6:45	6:45	6:45	6:45	6:45	6:45	6:45
End Time	8:00	8:00	8:00	8:00	8:00	8:00	8:00
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	23596	23654	23702	23755	23763	23600	23720
Vehs Exited	23363	23421	23433	23443	23470	23264	23410
Starting Vehs	896	956	908	883	865	875	900
Ending Vehs	1129	1189	1177	1195	1158	1211	1210
Travel Distance (mi)	24648	24670	24632	24756	24883	24527	24840
Travel Time (hr)	1187.5	1159.0	1135.6	1183.5	1231.0	1129.6	1193.1
Total Delay (hr)	419.5	390.2	368.1	412.0	453.9	365.5	417.2
Total Stops	28944	27946	27195	28767	30145	27229	28959
Fuel Used (gal)	861.0	855.0	848.5	866.0	878.9	845.6	869.4

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	6:45	6:45	6:45	6:45
End Time	8:00	8:00	8:00	8:00
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	23798	23948	23813	23733
Vehs Exited	23414	23591	23588	23441
Starting Vehs	881	853	923	880
Ending Vehs	1265	1210	1148	1176
Travel Distance (mi)	24469	24915	24812	24715
Travel Time (hr)	1165.7	1208.1	1184.8	1177.8
Total Delay (hr)	404.6	431.6	410.9	407.3
Total Stops	27875	28874	28486	28439
Fuel Used (gal)	852.4	873.4	863.0	861.3

Interval #0 Information Seeding

Start Time	6:45
End Time	7:00
Total Time (min)	15
No data recorded this interval.	

Interval #1 Information Recording

Start Time	7:00
End Time	8:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	23596	23654	23702	23755	23763	23600	23720
Vehs Exited	23363	23421	23433	23443	23470	23264	23410
Starting Vehs	896	956	908	883	865	875	900
Ending Vehs	1129	1189	1177	1195	1158	1211	1210
Travel Distance (mi)	24648	24670	24632	24756	24883	24527	24840
Travel Time (hr)	1187.5	1159.0	1135.6	1183.5	1231.0	1129.6	1193.1
Total Delay (hr)	419.5	390.2	368.1	412.0	453.9	365.5	417.2
Total Stops	28944	27946	27195	28767	30145	27229	28959
Fuel Used (gal)	861.0	855.0	848.5	866.0	878.9	845.6	869.4

Interval #1 Information Recording

Start Time	7:00
End Time	8:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	23798	23948	23813	23733
Vehs Exited	23414	23591	23588	23441
Starting Vehs	881	853	923	880
Ending Vehs	1265	1210	1148	1176
Travel Distance (mi)	24469	24915	24812	24715
Travel Time (hr)	1165.7	1208.1	1184.8	1177.8
Total Delay (hr)	404.6	431.6	410.9	407.3
Total Stops	27875	28874	28486	28439
Fuel Used (gal)	852.4	873.4	863.0	861.3

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.3	0.2	0.2	1.4	0.9	0.9	1.2	0.2	1.0	1.2	0.1	0.2
Total Del/Veh (s)	79.7	31.2	9.0	42.4	70.8	77.5	56.6	49.7	8.9	50.7	62.0	63.1

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	55.4

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0		0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	15.9	3.3	2.1		0.5	0.1	25.4	9.8	47.1	24.0	13.4	2.0

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.0	0.0	0.3	0.0	0.1	1.8	0.3	0.4	1.6	0.5	0.6
Total Del/Veh (s)	50.2	9.8	9.6	17.8	14.6	15.2	85.0	32.0	25.5	70.5	59.1	55.1

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	26.8

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.2	0.0	0.2	0.1	0.2	0.0	0.0	0.0	1.5	0.2	0.2	0.1
Total Del/Veh (s)	39.0	20.9	35.0	31.7	5.6	42.1	26.7	5.8	40.8	31.8	28.0	28.5

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.2	0.0	0.2	1.0	0.1
Total Del/Veh (s)	5.8	4.5	10.2	9.2	44.3	6.9	8.8

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.4	0.2	0.3	1.4	0.3	0.3
Total Del/Veh (s)	36.5	20.8	17.3	34.7	32.5	28.6	46.9	33.9	30.3	52.9	36.0	32.9

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	33.8

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	7.0	0.6	0.3	7.0	2.8	1.4	23.9	4.5	11.3	18.7	10.5	2.2

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.0	0.0	0.2	0.0	0.0	1.6	0.4	0.4	1.8	0.4	0.4
Total Del/Veh (s)	18.5	8.4	6.7	20.5	14.6	13.0	29.6	18.1	12.7	27.4	18.6	14.7

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	14.8

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.3	0.0	0.0	0.3	0.0	0.0	1.6	0.3	1.5	1.6	0.3	1.5
Total Del/Veh (s)	38.5	32.0	28.4	41.9	34.8	32.4	46.1	35.2	15.3	81.4	36.4	9.9

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	35.5

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.3		0.1	0.1	0.0
Total Del/Veh (s)	6.3	1.2	0.7	15.3	4.6	2.4	5.7		40.9	12.6	4.6

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.4	0.0	0.0	0.4	0.0	0.1	1.3	0.4	0.4	0.7	0.4	0.4
Total Del/Veh (s)	28.8	8.8	8.3	23.4	10.4	10.0	156.0	42.1	34.5	85.3	67.7	59.7

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	29.1

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.2	0.0	0.0	0.2	0.0	0.0	1.5	0.3	1.5	1.6	0.3	1.4
Total Del/Veh (s)	45.4	29.8	25.9	36.7	31.1	27.3	57.4	32.6	12.2	48.1	31.4	12.3

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	32.3

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	1.9	1.3	3.4	0.5	8.0	5.0	1.3

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.1
Total Del/Veh (s)	4.3	1.0	0.7	5.5	1.6	0.9	10.6	16.3	4.9	8.8	15.4	4.1

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	2.0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.2	0.2	1.5	0.2	0.2	1.3	0.2	1.2
Total Del/Veh (s)	47.8	47.4	20.8	32.6	35.8	23.2	250.8	28.9	14.5	35.8	24.1	8.9

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	43.7

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.1	0.0
Total Del/Veh (s)	1.4	3.5	2.8

40: Country Club Rd Performance by movement

Movement	EBR	NBT	SBT	All
Denied Del/Veh (s)	0.0	0.2	0.0	0.1
Total Del/Veh (s)	9.1	5.5	2.2	4.3

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	3.4	1.7	2.4

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWT	SWR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0
Total Del/Veh (s)	2.9	0.2	2.2	2.4	2.5

100: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	5.0	2.5	3.9

111: 5th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.0
Total Del/Veh (s)	2.2	2.4	3.7	4.2	14.2	6.3	16.0	3.9

201: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.2	0.2	0.0
Total Del/Veh (s)	1.5	1.7	2.2	2.2	22.0	27.6	17.7	3.9

1000: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.0
Total Del/Veh (s)	2.6	2.6	2.8	6.1	2.8

1100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	0.8	2.0	1.9	5.1	1.5

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.2	0.0
Total Del/Veh (s)	3.0	3.7	1.4	12.5	5.5	3.3

1300: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.8	2.9	2.4

1400: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	1.2	1.5	1.5	3.2	1.4

3000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	2.9	0.3	4.6	8.1	1.4

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.2	0.0
Total Del/Veh (s)	1.4	1.3	4.4	14.1	8.1	4.1

5000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.0
Total Del/Veh (s)	1.2	1.1	2.2	7.4	2.2

6000: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	1.6	1.2	1.1	5.6	1.4

7000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.2	0.0
Total Del/Veh (s)	0.9	2.0	5.1	5.9	1.8

8000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	2.3	3.1	5.5	6.8	3.0

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.2	0.0
Total Del/Veh (s)	3.2	3.6	0.9	18.9	6.4	2.7

Total Network Performance

Denied Del/Veh (s)	0.5
Total Del/Veh (s)	59.1

Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	31.2	98.8	0.6	21
	1000	3.2	23.6	0.2	26
	2	3.5	23.2	0.2	26
Wilson HAWK	20	1.3	3.9	0.0	18
6th Side Street	201	1.5	9.0	0.1	26
Tucson Blvd	3	8.8	18.1	0.1	16
	3000	3.1	45.4	0.4	29
6th/5th Street	72	3.0	6.6	0.0	16
Country Club Rd	4	20.9	33.9	0.1	12
	9922	1.5	13.5	0.1	26
	4000	1.4	30.2	0.2	29
Dodge Blvd	5	5.8	52.6	0.4	28
	5000	1.3	16.6	0.1	28
Alvernon Wy	6	20.8	35.2	0.1	13
	6000	2.1	17.3	0.1	26
	70	3.3	16.5	0.1	23
	7	0.6	2.6	0.0	26
Columbus Blvd	7000	0.9	16.6	0.1	28
	8	8.4	21.7	0.1	19
Swan Rd	8000	2.6	33.4	0.3	28
	9	31.7	60.1	0.3	15
	9000	4.5	19.4	0.1	23
	100	5.0	18.0	0.1	22
	10	1.2	3.3	0.0	21
	1100	0.9	15.7	0.1	29
Rosemont Blvd	11	8.7	22.4	0.1	19
5th Side Street	111	2.5	31.6	0.3	29
Craycroft Rd	12	30.2	58.3	0.2	15
	1200	3.9	30.5	0.2	26
	13	1.9	27.8	0.2	28
Chantilly Dr HAWK	1300	1.8	4.4	0.0	16
Sahuara Ave	14	1.0	8.1	0.1	27
	1400	1.3	31.1	0.3	29
Wilmot Rd	15	47.3	76.1	0.3	12
Total		267.4	925.3	5.6	22

Arterial Level of Service
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Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	35.8	138.4	0.4	11
	1400	1.2	31.6	0.3	29
Sahuara Ave	14	1.6	31.2	0.3	29
	1300	2.8	10.4	0.1	21
	13	0.5	2.5	0.0	27
	1200	1.4	27.4	0.2	28
Craycroft Rd	12	30.7	56.2	0.2	14
5th Side Street	111	4.7	34.2	0.2	26
Rosemont Blvd	11	10.3	38.9	0.3	23
	1100	2.2	16.3	0.1	27
	10	4.6	19.6	0.1	23
Arcadia Ave HAWK	100	2.5	5.0	0.0	14
	9000	0.9	13.4	0.1	29
Swan Rd	9	34.6	48.2	0.1	9
	8000	3.7	33.4	0.3	27
Columbus Blvd	8	14.7	53.6	0.3	22
	7000	2.2	15.8	0.1	26
	7	2.9	18.5	0.1	25
Irving Ave HAWK	70	1.7	4.4	0.0	16
	6000	1.2	13.8	0.1	28
Alvernon Wy	6	32.5	46.9	0.1	10
	5000	2.7	17.8	0.1	25
Dodge Blvd	5	9.8	24.7	0.1	19
	4000	4.5	51.6	0.4	28
	9922	2.2	31.6	0.2	28
Country Club Rd	4	31.7	42.5	0.1	8
6th/5th Street	72	3.1	16.9	0.1	24
	3000	0.3	4.3	0.0	25
Tucson Blvd	3	14.5	56.4	0.4	23
6th Side Street	201	2.5	11.9	0.1	24
Wilson HAWK	20	3.5	11.1	0.1	21
	2	0.5	2.5	0.0	27
	1000	2.6	22.5	0.2	27
Campbell Ave	1	70.8	90.5	0.2	7
Total		341.4	1044.0	5.5	19

Queuing and Blocking Report
Scenario A

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Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	199	329	292	199	364	673	684	370	448	442	436	252
Average Queue (ft)	147	164	149	53	196	408	428	145	303	306	280	37
95th Queue (ft)	220	304	247	132	431	717	731	305	429	427	405	156
Link Distance (ft)		2992	2992			786	786		2020	2020	2020	
Upstream Blk Time (%)						0	1					
Queuing Penalty (veh)						3	4					
Storage Bay Dist (ft)	100			50	180			290				150
Storage Blk Time (%)	46	16	47	3	2	47		0	12		38	
Queuing Penalty (veh)	113	31	54	9	7	90		2	21		29	

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	330	543	537	536
Average Queue (ft)	120	342	346	352
95th Queue (ft)	322	535	531	538
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	0	40		
Queuing Penalty (veh)	0	32		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	171	175	48	55	35	76
Average Queue (ft)	41	30	5	5	11	26
95th Queue (ft)	118	115	29	30	35	57
Link Distance (ft)	843	843	55	55	433	438
Upstream Blk Time (%)			0	0		
Queuing Penalty (veh)			1	1		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario A

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Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	128	162	187	141	353	368	92	377	399	717
Average Queue (ft)	50	61	70	37	140	155	16	207	106	383
95th Queue (ft)	103	125	140	91	266	281	63	333	319	655
Link Distance (ft)		356	356		1877	1877		1935		2179
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	100			100			300		300	
Storage Blk Time (%)	3	3		0	12			2		24
Queuing Penalty (veh)	8	2		1	8			0		16

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	NB	NB	NB	NB	SB
Directions Served	L	T	T	L	T	T	R	L	T	T	R	L
Maximum Queue (ft)	144	149	153	190	356	384	260	171	204	210	170	174
Average Queue (ft)	53	66	80	81	219	230	61	96	163	171	39	83
95th Queue (ft)	116	127	137	184	322	341	241	168	209	217	141	179
Link Distance (ft)		488	488		423	423			172	172		
Upstream Blk Time (%)							0	1	9	12	0	
Queuing Penalty (veh)							0	0	49	64	0	
Storage Bay Dist (ft)	180			90			160	100			70	100
Storage Blk Time (%)	0	0		2	36	25	0	10	30	39	0	2
Queuing Penalty (veh)	1	0		8	34	38	0	42	45	26	0	6

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	TR
Maximum Queue (ft)	378	402
Average Queue (ft)	217	231
95th Queue (ft)	329	343
Link Distance (ft)	2000	2000
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)	32	
Queuing Penalty (veh)	30	

Queuing and Blocking Report
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Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	WB	NB	NB
Directions Served	T	TR	L	T	T	L	R
Maximum Queue (ft)	123	123	138	252	265	83	79
Average Queue (ft)	34	37	49	118	136	35	40
95th Queue (ft)	93	95	102	229	252	73	71
Link Distance (ft)	2071	2071		619	619	2122	
Upstream Blk Time (%)							
Queuing Penalty (veh)							
Storage Bay Dist (ft)			300			220	
Storage Blk Time (%)				0			
Queuing Penalty (veh)				0			

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	TR	L	T	TR
Maximum Queue (ft)	171	176	179	174	312	336	250	464	464	250	512	513
Average Queue (ft)	79	78	91	83	180	198	107	276	283	150	301	307
95th Queue (ft)	146	148	158	177	285	304	240	421	423	282	463	464
Link Distance (ft)		581	581		578	578		2106	2106		1977	1977
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	100			75			150			150		
Storage Blk Time (%)	7	5		9	38		1	29		3	32	
Queuing Penalty (veh)	13	6		28	37		3	30		17	44	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	45	42	137	148	35	43
Average Queue (ft)	3	4	42	30	11	15
95th Queue (ft)	23	23	104	107	35	40
Link Distance (ft)	56	56	627	627	304	320
Upstream Blk Time (%)	0	0				
Queuing Penalty (veh)	0	0				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario A

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Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	95	168	171	148	244	259	118	242	126	247
Average Queue (ft)	25	61	71	37	129	143	36	114	40	120
95th Queue (ft)	65	121	127	97	224	240	84	197	88	205
Link Distance (ft)		533	533		1685	1685		2072		1936
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			75			100		100	
Storage Blk Time (%)	0	4		1	14		0	11	0	13
Queuing Penalty (veh)	1	1		2	7		1	6	1	7

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	165	266	281	200	308	311	233	414	414	299	349	446
Average Queue (ft)	61	130	153	85	164	181	91	265	262	91	183	279
95th Queue (ft)	127	222	241	170	265	275	220	376	373	200	328	409
Link Distance (ft)		1226	1226		560	560		1988	1988			2060
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	100			100			140			350	200	
Storage Blk Time (%)	3	17		5	26		0	35	1		12	22
Queuing Penalty (veh)	8	17		15	31		3	29	4		65	43

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	447	236
Average Queue (ft)	275	42
95th Queue (ft)	404	137
Link Distance (ft)	2060	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	5	
Queuing Penalty (veh)	5	

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Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	69	59	195	158	103	71
Average Queue (ft)	21	11	86	41	53	27
95th Queue (ft)	62	43	160	120	85	58
Link Distance (ft)	52	52	616	616	261	294
Upstream Blk Time (%)	2	0				
Queuing Penalty (veh)	7	2				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	125	172	188	85	189	188	249	544	248	631
Average Queue (ft)	53	75	91	38	96	102	108	192	77	315
95th Queue (ft)	101	139	159	73	163	170	236	418	226	684
Link Distance (ft)		570	570		1260	1260		1970		2041
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			100			150		150	
Storage Blk Time (%)	7	6		0	6		18	15	0	39
Queuing Penalty (veh)	20	6		1	4		56	11	0	15

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Scenario A

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Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	173	231	230	205	263	282	329	409	411	230	292	370
Average Queue (ft)	89	111	134	84	148	163	140	258	259	99	106	237
95th Queue (ft)	158	189	206	161	231	245	277	375	375	251	224	334
Link Distance (ft)		1212	1212		1092	1092		1951	1951			2065
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	75			150			180			180	150	
Storage Blk Time (%)	18	15		2	8		5	21	22	0	1	25
Queuing Penalty (veh)	35	21		5	11		25	36	25	0	7	30

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	383	279
Average Queue (ft)	236	75
95th Queue (ft)	337	208
Link Distance (ft)	2065	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	25	0
Queuing Penalty (veh)	38	0

Intersection: 13: 6th/5th Street

Movement	EB	EB	WB	WB	NB
Directions Served	LT	TR	LT	TR	LTR
Maximum Queue (ft)	74	100	52	49	35
Average Queue (ft)	8	12	6	4	19
95th Queue (ft)	42	55	32	25	43
Link Distance (ft)	1086	1086	56	56	422
Upstream Blk Time (%)			0	0	
Queuing Penalty (veh)			1	0	
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Queuing and Blocking Report
Scenario A

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Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	37	39	74	41	74	37
Average Queue (ft)	2	2	20	2	36	13
95th Queue (ft)	19	19	57	18	61	38
Link Distance (ft)	275	275	1271	1271	614	699
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	143	188	167	159	360	1264	1211	946	230	382	368	348
Average Queue (ft)	67	102	76	68	330	738	691	216	98	232	228	202
95th Queue (ft)	124	160	138	127	428	1444	1398	574	218	338	332	307
Link Distance (ft)		1225	1225	2261		1997	1997	1997		2032	2032	2032
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	150				240				130			
Storage Blk Time (%)	0	2			69	1			1	27		0
Queuing Penalty (veh)	0	1			305	2			7	33		1

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	204
Average Queue (ft)	75
95th Queue (ft)	143
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Queuing and Blocking Report

Scenario A

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Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	61	73	241	251
Average Queue (ft)	18	23	59	66
95th Queue (ft)	58	68	184	196
Link Distance (ft)	55	55	298	298
Upstream Blk Time (%)	2	4	0	0
Queuing Penalty (veh)	8	14	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 40: Country Club Rd

Movement	EB	NB	NB	SB
Directions Served	R	T	T	T
Maximum Queue (ft)	88	226	247	9
Average Queue (ft)	38	55	64	0
95th Queue (ft)	69	163	180	6
Link Distance (ft)	451	2094	2094	172
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	162	180	79	76
Average Queue (ft)	30	36	26	30
95th Queue (ft)	107	122	74	79
Link Distance (ft)	513	513	56	56
Upstream Blk Time (%)			4	6
Queuing Penalty (veh)			17	24
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

Scenario A

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Intersection: 72: 6th/5th Street

Movement

Directions Served
 Maximum Queue (ft)
 Average Queue (ft)
 95th Queue (ft)
 Link Distance (ft)
 Upstream Blk Time (%)
 Queuing Penalty (veh)
 Storage Bay Dist (ft)
 Storage Blk Time (%)
 Queuing Penalty (veh)

Intersection: 100: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	165	224	63	76
Average Queue (ft)	38	58	25	29
95th Queue (ft)	115	165	67	76
Link Distance (ft)	526	526	52	52
Upstream Blk Time (%)			5	7
Queuing Penalty (veh)			14	21
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 111: 5th Side Street & 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	TR	T	TR	LTR	LTR
Maximum Queue (ft)	3	16	10	76	75
Average Queue (ft)	0	1	0	35	33
95th Queue (ft)	3	10	6	63	64
Link Distance (ft)	1260	1212	1212	332	276
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Queuing and Blocking Report
Scenario A

10/20/2022

Intersection: 201: 6th Side Street & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	T	TR	T	TR	LTR	LTR
Maximum Queue (ft)	3	17	21	9	91	150
Average Queue (ft)	0	1	1	0	36	65
95th Queue (ft)	3	9	12	7	70	119
Link Distance (ft)	298	298	356	356	419	408
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 1000: 6th/5th Street

Movement	WB	WB	NB
Directions Served	T	TR	LTR
Maximum Queue (ft)	64	77	77
Average Queue (ft)	5	8	36
95th Queue (ft)	49	59	64
Link Distance (ft)	843	843	448
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1100: 6th/5th Street

Movement	WB	WB	NB
Directions Served	T	TR	LTR
Maximum Queue (ft)	6	3	33
Average Queue (ft)	0	0	11
95th Queue (ft)	5	3	35
Link Distance (ft)	570	570	214
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Queuing and Blocking Report
Scenario A

10/20/2022

Intersection: 1200: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	6	5	94	81
Average Queue (ft)	0	0	44	38
95th Queue (ft)	5	5	76	63
Link Distance (ft)	1092	1086	211	276
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1300: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	68	68	120	127
Average Queue (ft)	19	23	30	34
95th Queue (ft)	59	66	91	96
Link Distance (ft)	56	56	275	275
Upstream Blk Time (%)	2	3		
Queuing Penalty (veh)	5	8		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1400: 6th/5th Street

Movement	WB	NB
Directions Served	T	LTR
Maximum Queue (ft)	2	54
Average Queue (ft)	0	22
95th Queue (ft)	2	47
Link Distance (ft)	1225	305
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario A

10/20/2022

Intersection: 3000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	39	59
Average Queue (ft)	13	21
95th Queue (ft)	37	50
Link Distance (ft)	405	430
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 4000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	3	2	89	97
Average Queue (ft)	0	0	42	42
95th Queue (ft)	3	2	74	73
Link Distance (ft)	1235	2071	547	583
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 5000: 6th/5th Street

Movement	SB
Directions Served	LTR
Maximum Queue (ft)	76
Average Queue (ft)	38
95th Queue (ft)	66
Link Distance (ft)	280
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	
Storage Blk Time (%)	
Queuing Penalty (veh)	

Queuing and Blocking Report

Scenario A

10/20/2022

Intersection: 6000: 6th/5th Street

Movement	WB	WB	NB
Directions Served	T	TR	LTR
Maximum Queue (ft)	4	1	35
Average Queue (ft)	0	0	9
95th Queue (ft)	4	1	32
Link Distance (ft)	513	513	355
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 7000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	30	70
Average Queue (ft)	4	35
95th Queue (ft)	20	61
Link Distance (ft)	260	304
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 8000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	68	62
Average Queue (ft)	29	25
95th Queue (ft)	54	52
Link Distance (ft)	319	333
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario A

10/20/2022

Intersection: 9000: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	16	32	47	88
Average Queue (ft)	1	2	14	40
95th Queue (ft)	9	15	41	69
Link Distance (ft)	560	560	204	310
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 2019

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	4:45	4:45	4:45	4:45	4:45	4:45	4:45
End Time	6:00	6:00	6:00	6:00	6:00	6:00	6:00
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	24699	24885	24510	24880	24720	24410	24709
Vehs Exited	24213	24483	24141	24428	24365	24100	24354
Starting Vehs	1130	1220	1138	1181	1155	1204	1213
Ending Vehs	1616	1622	1507	1633	1510	1514	1568
Travel Distance (mi)	26788	26899	26866	27001	26837	26314	26895
Travel Time (hr)	1341.0	1479.4	1444.1	1425.0	1416.1	1417.4	1386.9
Total Delay (hr)	505.0	639.2	603.6	582.0	577.5	597.8	548.2
Total Stops	32169	35931	34644	34647	33800	32508	33859
Fuel Used (gal)	948.9	986.0	977.0	976.1	968.2	958.7	964.9

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	4:45	4:45	4:45	4:45
End Time	6:00	6:00	6:00	6:00
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	24585	24571	24577	24652
Vehs Exited	24214	24135	24125	24253
Starting Vehs	1175	1150	1148	1155
Ending Vehs	1546	1586	1600	1566
Travel Distance (mi)	26788	26493	26533	26741
Travel Time (hr)	1389.2	1423.5	1415.4	1413.8
Total Delay (hr)	552.0	596.9	586.8	578.9
Total Stops	33245	34190	33639	33861
Fuel Used (gal)	962.9	962.4	960.6	966.6

Interval #0 Information Seeding

Start Time	4:45
End Time	5:00
Total Time (min)	15
No data recorded this interval.	

Interval #1 Information Recording

Start Time	5:00
End Time	6:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	24699	24885	24510	24880	24720	24410	24709
Vehs Exited	24213	24483	24141	24428	24365	24100	24354
Starting Vehs	1130	1220	1138	1181	1155	1204	1213
Ending Vehs	1616	1622	1507	1633	1510	1514	1568
Travel Distance (mi)	26788	26899	26866	27001	26837	26314	26895
Travel Time (hr)	1341.0	1479.4	1444.1	1425.0	1416.1	1417.4	1386.9
Total Delay (hr)	505.0	639.2	603.6	582.0	577.5	597.8	548.2
Total Stops	32169	35931	34644	34647	33800	32508	33859
Fuel Used (gal)	948.9	986.0	977.0	976.1	968.2	958.7	964.9

Interval #1 Information Recording

Start Time	5:00
End Time	6:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	24585	24571	24577	24652
Vehs Exited	24214	24135	24125	24253
Starting Vehs	1175	1150	1148	1155
Ending Vehs	1546	1586	1600	1566
Travel Distance (mi)	26788	26493	26533	26741
Travel Time (hr)	1389.2	1423.5	1415.4	1413.8
Total Delay (hr)	552.0	596.9	586.8	578.9
Total Stops	33245	34190	33639	33861
Fuel Used (gal)	962.9	962.4	960.6	966.6

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.3	0.2	0.2	0.0	0.0	0.0	1.2	0.1	1.1	5.7	5.4	5.9
Total Del/Veh (s)	92.1	48.8	28.0	45.5	31.9	28.4	55.3	39.5	15.0	241.5	281.0	279.7

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	1.9
Total Del/Veh (s)	118.9

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1		0.1	0.1	0.0
Total Del/Veh (s)	10.0	3.8	2.9	8.8	0.5	0.1	42.4	13.4		30.1	6.1	2.6

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.3	0.1	0.1	0.2	0.0	0.0	1.9	0.5	0.5	1.5	0.6	0.6
Total Del/Veh (s)	52.3	14.6	16.1	43.9	10.9	11.1	149.9	48.0	44.7	195.8	70.3	65.6

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	35.9

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	1.6	0.2	0.2	0.1
Total Del/Veh (s)	33.6	29.1	54.8	34.6	5.2	44.7	30.3	10.7	53.7	36.2	30.1	32.4

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.3	1.0	0.1
Total Del/Veh (s)	8.5	7.8	14.1	10.3	41.9	11.1	11.5

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.4	0.2	0.3	1.4	0.2	0.3
Total Del/Veh (s)	48.7	28.1	27.4	39.2	30.2	29.8	70.0	72.7	72.2	59.9	43.7	42.7

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	47.9

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.1
Total Del/Veh (s)	6.1	0.5	0.2	6.9	2.0	2.3	15.2	8.6	5.5	10.1	11.0	6.9

7: 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	1.3

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.7	0.4	0.4	1.9	0.4	0.4
Total Del/Veh (s)	26.9	10.0	10.6	28.5	14.6	15.2	62.4	39.9	35.1	62.6	37.1	31.1

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	22.8

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.6	0.3	1.5	1.7	0.3	1.5
Total Del/Veh (s)	38.3	37.1	37.0	52.1	29.2	28.9	53.5	45.5	15.9	59.5	36.2	9.8

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	38.0

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.1
Total Del/Veh (s)	4.2	0.8	0.3	11.1	2.8	0.7	32.8	26.2	9.3	22.2	35.2	9.2

10: 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	2.4

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.0	0.0	0.1	0.0	0.0	1.3	0.4	0.4	0.6	0.4	0.4
Total Del/Veh (s)	23.2	11.1	10.3	27.7	8.8	8.4	79.7	42.2	38.7	71.0	31.4	24.7

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	22.1

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.5	0.3	1.4	1.4	0.3	1.6
Total Del/Veh (s)	38.9	35.4	33.9	49.9	25.3	20.1	56.4	47.7	24.5	49.2	38.6	11.8

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	38.8

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.1	0.1		0.1	0.0
Total Del/Veh (s)	3.0	2.5	7.1	0.6	0.1	13.0	9.0		22.7	2.6

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	4.2	0.8	0.5	5.5	1.2	0.7	5.1	4.6	11.3	3.8	1.2

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.1	1.4	0.2	0.2	1.5	0.2	1.4
Total Del/Veh (s)	46.5	35.8	19.1	21.0	28.7	13.6	54.9	25.9	20.6	34.8	24.1	5.5

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	27.5

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.4	3.1	2.1

40: Country Club Rd Performance by movement

Movement	EBT	EBR	NBT	SBT	All
Denied Del/Veh (s)	0.0	0.0	0.2	0.0	0.1
Total Del/Veh (s)	0.2	9.1	7.2	2.4	5.4

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	3.9	1.7	2.9

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0
Total Del/Veh (s)	1.9	0.5	4.3	2.7

100: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	4.9	2.1	3.7

111: 5th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	3.2	3.3	3.0	3.1	16.2	9.3	15.3	8.0	4.0

201: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	1.0	1.3	1.3	23.5	13.7	23.5	9.1	2.1

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.0
Total Del/Veh (s)	5.3	6.0	1.4	7.1	3.9

1100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.1	0.0
Total Del/Veh (s)	1.2	1.9	2.0	8.9	17.4	2.5

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.2	0.0
Total Del/Veh (s)	4.1	5.1	1.2	16.0	5.2	4.0

1300: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.7	2.8	2.1

1400: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.1	0.0
Total Del/Veh (s)	1.7	1.5	1.5	5.4	12.4	2.4

3000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	5.5	0.4	13.9	5.0	3.9

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0
Total Del/Veh (s)	3.9	5.1	4.8	5.7	4.5

5000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.2	0.0
Total Del/Veh (s)	1.3	2.2	19.5	3.6	8.8	2.5

6000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.2	0.2	0.2	0.0
Total Del/Veh (s)	2.0	1.1	20.3	9.5	7.0	2.6

7000: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	1.2	2.3	2.4	6.7	1.7

8000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	2.9	3.1	7.0	6.6	3.2

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.2	0.0
Total Del/Veh (s)	3.7	4.3	0.9	24.7	7.5	4.2

Total Network Performance

Denied Del/Veh (s)	0.8
Total Del/Veh (s)	80.0

Arterial Level of Service
Scenario A

10/20/2022

Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	48.8	115.9	0.6	18
	1000	6.5	27.1	0.2	22
	2	3.8	24.1	0.2	26
Wilson HAWK	20	1.4	3.9	0.0	17
6th Side Street	201	1.0	8.5	0.1	27
Tucson Blvd	3	14.1	23.3	0.1	12
	3000	5.9	48.9	0.4	27
6th/5th Street	72	2.0	7.0	0.0	16
Country Club Rd	4	29.1	44.9	0.1	9
	9922	2.4	14.4	0.1	24
	4000	3.9	32.9	0.2	27
Dodge Blvd	5	8.5	55.6	0.4	26
	5000	1.5	16.9	0.1	28
Alvernon Wy	6	28.1	42.1	0.1	11
	6000	2.4	17.2	0.1	26
	70	3.9	16.9	0.1	23
	7	0.5	2.6	0.0	27
Columbus Blvd	7000	1.2	16.8	0.1	28
	8	10.0	23.2	0.1	18
Swan Rd	8000	3.2	33.1	0.3	28
	9	36.9	65.7	0.3	14
	9000	4.6	19.6	0.1	23
	100	4.9	17.8	0.1	22
	10	0.8	2.9	0.0	24
	1100	1.3	16.4	0.1	28
Rosemont Blvd	11	11.2	25.3	0.1	17
5th Side Street	111	3.4	33.4	0.3	27
Craycroft Rd	12	35.4	64.3	0.2	14
	1200	4.9	32.3	0.2	25
	13	3.0	28.2	0.2	28
Chantilly Dr HAWK	1300	1.6	4.2	0.0	16
Sahuara Ave	14	0.8	7.7	0.1	29
	1400	1.7	31.9	0.3	28
Wilmot Rd	15	36.8	61.7	0.3	15
Total		325.7	986.5	5.6	20

Arterial Level of Service
Scenario A

10/20/2022

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	28.7	128.6	0.4	12
	1400	0.9	29.2	0.3	31
Sahuara Ave	14	1.2	30.4	0.3	30
	1300	2.8	10.0	0.1	22
	13	0.6	2.6	0.0	26
	1200	1.2	26.6	0.2	29
Craycroft Rd	12	24.2	50.1	0.2	16
5th Side Street	111	3.7	33.9	0.2	26
Rosemont Blvd	11	8.7	38.3	0.3	24
	1100	2.2	16.7	0.1	26
	10	2.8	17.9	0.1	26
Arcadia Ave HAWK	100	2.1	4.6	0.0	15
	9000	0.9	13.6	0.1	29
Swan Rd	9	28.8	42.7	0.1	10
	8000	3.7	33.6	0.3	27
Columbus Blvd	8	14.7	53.2	0.3	22
	7000	2.4	15.8	0.1	26
	7	2.0	17.6	0.1	27
Irving Ave HAWK	70	1.7	4.4	0.0	16
	6000	1.1	13.7	0.1	28
Alvernon Wy	6	30.3	44.3	0.1	10
	5000	2.6	17.4	0.1	26
Dodge Blvd	5	11.2	26.2	0.1	18
	4000	5.2	52.4	0.4	28
	9922	2.0	31.3	0.2	28
Country Club Rd	4	34.6	45.4	0.1	8
6th/5th Street	72	5.5	21.7	0.1	19
	3000	0.4	9.9	0.0	11
Tucson Blvd	3	10.5	53.4	0.4	25
6th Side Street	201	1.4	10.9	0.1	26
Wilson HAWK	20	3.0	11.1	0.1	21
	2	0.5	2.6	0.0	27
	1000	1.4	21.7	0.2	28
Campbell Ave	1	31.7	50.8	0.2	12
Total		274.9	982.5	5.5	20

Queuing and Blocking Report
Scenario A

10/20/2022

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	424	610	610	200	231	283	304	291	368	370	348	169
Average Queue (ft)	269	333	340	143	114	160	179	123	258	260	232	48
95th Queue (ft)	450	573	570	266	201	246	270	229	343	344	317	114
Link Distance (ft)		2992	2992			786	786		2020	2020	2020	
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	250			50	180			290				150
Storage Blk Time (%)	24	16	62	11	3	5		0	3		24	0
Queuing Penalty (veh)	116	45	107	55	9	9		0	5		32	0

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	350	2185	2169	2140
Average Queue (ft)	287	1446	1414	1350
95th Queue (ft)	483	2452	2402	2310
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)		12	8	8
Queuing Penalty (veh)		0	0	0
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	1	73		
Queuing Penalty (veh)	5	131		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	186	211	63	34	35	37
Average Queue (ft)	46	48	10	2	9	11
95th Queue (ft)	150	159	45	18	31	35
Link Distance (ft)	843	843	55	55	433	438
Upstream Blk Time (%)			1	0		
Queuing Penalty (veh)			6	0		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario A

10/20/2022

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	264	323	332	96	220	234	272	570	400	828
Average Queue (ft)	101	150	165	37	74	90	75	338	180	475
95th Queue (ft)	199	276	294	78	162	182	221	571	409	861
Link Distance (ft)		356	356		1877	1877		1935		2179
Upstream Blk Time (%)		0	0							
Queuing Penalty (veh)		1	3							
Storage Bay Dist (ft)	100			100			300		300	
Storage Blk Time (%)	22	19		1	6			16	1	33
Queuing Penalty (veh)	120	29		5	3			6	7	24

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	NB	NB	NB	NB	SB
Directions Served	L	T	T	L	T	T	R	L	T	T	R	L
Maximum Queue (ft)	224	308	319	189	254	267	199	172	213	217	170	174
Average Queue (ft)	91	170	179	91	152	161	23	104	172	175	53	131
95th Queue (ft)	181	274	281	165	230	239	126	179	210	216	162	212
Link Distance (ft)		488	488		423	423			172	172		
Upstream Blk Time (%)								1	14	15	0	
Queuing Penalty (veh)								0	76	82	0	
Storage Bay Dist (ft)	180			90			160	100			70	100
Storage Blk Time (%)	0	10		19	32	13	1	12	35	42	0	17
Queuing Penalty (veh)	2	19		51	39	27	2	51	52	35	2	64

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	TR
Maximum Queue (ft)	373	364
Average Queue (ft)	225	225
95th Queue (ft)	340	331
Link Distance (ft)	2000	2000
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)	37	
Queuing Penalty (veh)	55	

Queuing and Blocking Report
Scenario A

10/20/2022

Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	WB	NB	NB
Directions Served	T	TR	L	T	T	L	R
Maximum Queue (ft)	178	180	177	235	244	164	130
Average Queue (ft)	56	69	82	127	144	81	63
95th Queue (ft)	133	148	146	216	230	143	109
Link Distance (ft)	2071	2071		619	619	2122	
Upstream Blk Time (%)							
Queuing Penalty (veh)							
Storage Bay Dist (ft)			300				220
Storage Blk Time (%)						0	
Queuing Penalty (veh)						0	

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	TR	L	T	TR
Maximum Queue (ft)	199	301	300	174	312	325	250	762	755	250	525	526
Average Queue (ft)	133	156	158	79	187	207	133	484	488	152	320	326
95th Queue (ft)	219	267	250	169	283	298	294	869	864	286	484	482
Link Distance (ft)		581	581		578	578		2106	2106		1977	1977
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	100			75			150			150		
Storage Blk Time (%)	26	25		11	38		0	50		4	38	
Queuing Penalty (veh)	81	51		33	34		2	48		18	48	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	64	41	111	125	35	36
Average Queue (ft)	8	3	20	21	12	10
95th Queue (ft)	38	21	76	81	37	32
Link Distance (ft)	56	56	627	627	304	320
Upstream Blk Time (%)	1	0				
Queuing Penalty (veh)	2	0				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario A

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Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	142	223	230	132	254	265	199	543	196	504
Average Queue (ft)	42	96	113	38	130	151	57	223	61	212
95th Queue (ft)	96	181	199	97	227	247	159	445	166	454
Link Distance (ft)		533	533		1685	1685		2072		1936
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			75			100		100	
Storage Blk Time (%)	3	11		2	15		2	36	3	32
Queuing Penalty (veh)	11	7		6	8		9	17	14	17

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	200	349	356	194	275	284	264	577	581	380	294	458
Average Queue (ft)	96	200	221	97	156	173	125	334	333	96	120	259
95th Queue (ft)	194	309	329	182	243	257	278	540	541	291	236	403
Link Distance (ft)		1226	1226		560	560		1988	1988			2060
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	100			100			140			350	200	
Storage Blk Time (%)	3	35		13	22		1	43	9		1	19
Queuing Penalty (veh)	13	43		40	31		7	44	16		6	27

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	456	245
Average Queue (ft)	258	44
95th Queue (ft)	404	157
Link Distance (ft)	2060	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	4	
Queuing Penalty (veh)	3	

Queuing and Blocking Report
Scenario A

10/20/2022

Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	61	51	133	135	99	71
Average Queue (ft)	18	6	37	26	37	23
95th Queue (ft)	55	32	98	92	73	54
Link Distance (ft)	52	52	616	616	261	294
Upstream Blk Time (%)	1	0				
Queuing Penalty (veh)	5	1				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	158	238	260	76	123	151	250	558	214	410
Average Queue (ft)	58	119	138	31	59	75	104	226	60	163
95th Queue (ft)	115	206	233	67	105	126	235	488	157	324
Link Distance (ft)		570	570		1260	1260		1970		2041
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			100			150		150	
Storage Blk Time (%)	6	13		0	1		7	23	3	13
Queuing Penalty (veh)	24	14		0	0		29	23	10	7

Queuing and Blocking Report
Scenario A

10/20/2022

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	175	318	333	209	207	211	329	655	647	230	299	488
Average Queue (ft)	103	189	216	108	98	116	143	370	371	141	82	303
95th Queue (ft)	184	292	309	188	169	181	333	583	584	298	234	454
Link Distance (ft)		1212	1212		1092	1092		1951	1951			2065
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	75			150			180			180	150	
Storage Blk Time (%)	17	33		5	1		0	39	38	0	0	35
Queuing Penalty (veh)	56	56		11	2		1	42	50	0	0	21

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	480	300
Average Queue (ft)	305	91
95th Queue (ft)	461	275
Link Distance (ft)	2065	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	34	
Queuing Penalty (veh)	34	

Intersection: 13: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	134	153	62	31	83	24
Average Queue (ft)	18	24	15	2	37	1
95th Queue (ft)	80	94	52	16	67	12
Link Distance (ft)	1086	1086	56	56	422	473
Upstream Blk Time (%)			2	0		
Queuing Penalty (veh)			4	0		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario A

10/20/2022

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	54	53	49	43	31	46
Average Queue (ft)	7	3	5	2	6	17
95th Queue (ft)	32	23	29	17	26	43
Link Distance (ft)	275	275	1271	1271	614	699
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	242	310	216	86	332	373	363	335	200	304	306	276
Average Queue (ft)	134	161	106	36	175	226	226	202	45	213	207	170
95th Queue (ft)	205	241	180	75	296	333	326	304	145	293	292	268
Link Distance (ft)		1225	1225	2261		1997	1997	1997		2032	2032	2032
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	150				240				130			
Storage Blk Time (%)	7	17			4	5				26		0
Queuing Penalty (veh)	30	36			20	12				12		0

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	90
Average Queue (ft)	43
95th Queue (ft)	73
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Queuing and Blocking Report
Scenario A

10/20/2022

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	75	72	176	177
Average Queue (ft)	26	27	46	48
95th Queue (ft)	72	73	132	137
Link Distance (ft)	55	55	298	298
Upstream Blk Time (%)	5	5		
Queuing Penalty (veh)	30	31		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 40: Country Club Rd

Movement	EB	NB	NB
Directions Served	R	T	T
Maximum Queue (ft)	117	243	253
Average Queue (ft)	54	71	72
95th Queue (ft)	94	187	192
Link Distance (ft)	451	2094	2094
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	233	244	71	76
Average Queue (ft)	48	54	26	28
95th Queue (ft)	158	172	72	76
Link Distance (ft)	513	513	56	56
Upstream Blk Time (%)			4	4
Queuing Penalty (veh)			15	16
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report
Scenario A

10/20/2022

Intersection: 72: 6th/5th Street

Movement	SW	SW
Directions Served	R	R
Maximum Queue (ft)	20	12
Average Queue (ft)	1	0
95th Queue (ft)	11	7
Link Distance (ft)	488	488
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 100: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	236	246	62	75
Average Queue (ft)	60	62	22	25
95th Queue (ft)	186	193	65	71
Link Distance (ft)	526	526	52	52
Upstream Blk Time (%)			5	6
Queuing Penalty (veh)			17	20
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 111: 5th Side Street & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	T	TR	T	TR	LTR	LTR
Maximum Queue (ft)	4	6	21	7	81	104
Average Queue (ft)	0	0	1	0	37	43
95th Queue (ft)	5	4	11	5	69	77
Link Distance (ft)	1260	1260	1212	1212	332	276
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario A

10/20/2022

Intersection: 201: 6th Side Street & 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	TR	LTR	LTR
Maximum Queue (ft)	26	22	2	97	68
Average Queue (ft)	1	1	0	42	26
95th Queue (ft)	13	12	2	76	56
Link Distance (ft)	298	298	356	419	408
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 1000: 6th/5th Street

Movement	EB	EB	SB
Directions Served	T	TR	LTR
Maximum Queue (ft)	20	20	85
Average Queue (ft)	1	1	36
95th Queue (ft)	12	13	65
Link Distance (ft)	786	786	416
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1100: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	T	LTR	LTR
Maximum Queue (ft)	2	5	89	95
Average Queue (ft)	0	0	44	37
95th Queue (ft)	2	4	75	74
Link Distance (ft)	616	570	214	185
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report
Scenario A

10/20/2022

Intersection: 1200: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	10	8	99	79
Average Queue (ft)	0	0	43	38
95th Queue (ft)	7	6	79	63
Link Distance (ft)	1092	1092	211	276
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1300: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	71	64	104	107
Average Queue (ft)	22	27	29	29
95th Queue (ft)	65	70	83	82
Link Distance (ft)	56	56	275	275
Upstream Blk Time (%)	3	5		
Queuing Penalty (veh)	12	16		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1400: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	2	6	68	80
Average Queue (ft)	0	0	38	33
95th Queue (ft)	2	5	60	63
Link Distance (ft)	1271	1271	305	274
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report
Scenario A

10/20/2022

Intersection: 3000: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	8	8	90	56
Average Queue (ft)	0	0	36	26
95th Queue (ft)	8	6	71	47
Link Distance (ft)	1877	1877	405	430
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 4000: 6th/5th Street

Movement	EB	EB	B9922	WB	WB
Directions Served	T	TR	T	T	TR
Maximum Queue (ft)	24	26	3	4	13
Average Queue (ft)	1	1	0	0	0
95th Queue (ft)	12	13	3	4	7
Link Distance (ft)	1235	1235	423	2071	2071
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 5000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	67	88
Average Queue (ft)	25	44
95th Queue (ft)	56	76
Link Distance (ft)	310	280
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario A

10/20/2022

Intersection: 6000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	92	81
Average Queue (ft)	41	39
95th Queue (ft)	74	65
Link Distance (ft)	355	360
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 7000: 6th/5th Street

Movement	WB	NB
Directions Served	T	LTR
Maximum Queue (ft)	6	35
Average Queue (ft)	0	7
95th Queue (ft)	7	29
Link Distance (ft)	533	260
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 8000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	64	57
Average Queue (ft)	31	20
95th Queue (ft)	55	47
Link Distance (ft)	319	333
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario A

10/20/2022

Intersection: 9000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	TR	LTR	LTR
Maximum Queue (ft)	10	24	4	128	94
Average Queue (ft)	0	1	0	60	44
95th Queue (ft)	6	12	3	106	76
Link Distance (ft)	560	560	526	204	310
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Network Summary

Network wide Queuing Penalty: 2571

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	4:45	4:45	4:45	4:45	4:45	4:45	4:45
End Time	6:00	6:00	6:00	6:00	6:00	6:00	6:00
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	24699	24885	24510	24880	24720	24410	24709
Vehs Exited	24213	24483	24141	24428	24365	24100	24354
Starting Vehs	1130	1220	1138	1181	1155	1204	1213
Ending Vehs	1616	1622	1507	1633	1510	1514	1568
Denied Entry After	4	4	9	3	8	108	1
Travel Distance (mi)	26788	26899	26866	27001	26837	26314	26895
Travel Time (hr)	1341.0	1479.4	1444.1	1425.0	1416.1	1417.4	1386.9
Total Delay (hr)	505.0	639.2	603.6	582.0	577.5	597.8	548.2
Total Stops	32169	35931	34644	34647	33800	32508	33859
Fuel Used (gal)	948.9	986.0	977.0	976.1	968.2	958.7	964.9

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	4:45	4:45	4:45	4:45
End Time	6:00	6:00	6:00	6:00
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	24585	24571	24577	24652
Vehs Exited	24214	24135	24125	24253
Starting Vehs	1175	1150	1148	1155
Ending Vehs	1546	1586	1600	1566
Denied Entry After	21	91	26	26
Travel Distance (mi)	26788	26493	26533	26741
Travel Time (hr)	1389.2	1423.5	1415.4	1413.8
Total Delay (hr)	552.0	596.9	586.8	578.9
Total Stops	33245	34190	33639	33861
Fuel Used (gal)	962.9	962.4	960.6	966.6

Interval #0 Information Seeding

Start Time	4:45
End Time	5:00
Total Time (min)	15

No data recorded this interval.

Interval #1 Information Recording

Start Time	5:00
End Time	6:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	24699	24885	24510	24880	24720	24410	24709
Vehs Exited	24213	24483	24141	24428	24365	24100	24354
Starting Vehs	1130	1220	1138	1181	1155	1204	1213
Ending Vehs	1616	1622	1507	1633	1510	1514	1568
Denied Entry After	4	4	9	3	8	108	1
Travel Distance (mi)	26788	26899	26866	27001	26837	26314	26895
Travel Time (hr)	1341.0	1479.4	1444.1	1425.0	1416.1	1417.4	1386.9
Total Delay (hr)	505.0	639.2	603.6	582.0	577.5	597.8	548.2
Total Stops	32169	35931	34644	34647	33800	32508	33859
Fuel Used (gal)	948.9	986.0	977.0	976.1	968.2	958.7	964.9

Interval #1 Information Recording

Start Time	5:00
End Time	6:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	24585	24571	24577	24652
Vehs Exited	24214	24135	24125	24253
Starting Vehs	1175	1150	1148	1155
Ending Vehs	1546	1586	1600	1566
Denied Entry After	21	91	26	26
Travel Distance (mi)	26788	26493	26533	26741
Travel Time (hr)	1389.2	1423.5	1415.4	1413.8
Total Delay (hr)	552.0	596.9	586.8	578.9
Total Stops	33245	34190	33639	33861
Fuel Used (gal)	962.9	962.4	960.6	966.6

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	2	21	3

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	26

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Entry After	0	0	0	0	0	0	0

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

7: 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

10: 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

40: Country Club Rd Performance by movement

Movement	EBT	EBR	NBT	SBT	All
Denied Entry After	0	0	0	0	0

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWR	All
Denied Entry After	0	0	0	0

100: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

111: 5th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0

201: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBR	All
Denied Entry After	0	0	0	0	0

1100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Entry After	0	0	0	0	0	0

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Entry After	0	0	0	0	0	0

1300: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

1400: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Entry After	0	0	0	0	0	0

3000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Entry After	0	0	0	0	0

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	All
Denied Entry After	0	0	0	0	0

5000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0

6000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0

7000: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	All
Denied Entry After	0	0	0	0	0

8000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Entry After	0	0	0	0	0

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Entry After	0	0	0	0	0	0

Total Network Performance

Denied Entry After	26				
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Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	48.8	115.9	0.6	18
	1000	6.5	27.1	0.2	22
	2	3.8	24.1	0.2	26
Wilson HAWK	20	1.4	3.9	0.0	17
6th Side Street	201	1.0	8.5	0.1	27
Tucson Blvd	3	14.1	23.3	0.1	12
	3000	5.9	48.9	0.4	27
6th/5th Street	72	2.0	7.0	0.0	16
Country Club Rd	4	29.1	44.9	0.1	9
	9922	2.4	14.4	0.1	24
	4000	3.9	32.9	0.2	27
Dodge Blvd	5	8.5	55.6	0.4	26
	5000	1.5	16.9	0.1	28
Alvernon Wy	6	28.1	42.1	0.1	11
	6000	2.4	17.2	0.1	26
	70	3.9	16.9	0.1	23
	7	0.5	2.6	0.0	27
Columbus Blvd	7000	1.2	16.8	0.1	28
	8	10.0	23.2	0.1	18
	8000	3.2	33.1	0.3	28
Swan Rd	9	36.9	65.7	0.3	14
	9000	4.6	19.6	0.1	23
	100	4.9	17.8	0.1	22
	10	0.8	2.9	0.0	24
Rosemont Blvd	1100	1.3	16.4	0.1	28
	11	11.2	25.3	0.1	17
	5th Side Street	111	3.4	33.4	0.3
Craycroft Rd	12	35.4	64.3	0.2	14
	1200	4.9	32.3	0.2	25
	13	3.0	28.2	0.2	28
Chantilly Dr HAWK	1300	1.6	4.2	0.0	16
Sahuara Ave	14	0.8	7.7	0.1	29
	1400	1.7	31.9	0.3	28
Wilmot Rd	15	36.8	61.7	0.3	15
Total		325.7	986.5	5.6	20

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	28.7	128.6	0.4	12
	1400	0.9	29.2	0.3	31
Sahuara Ave	14	1.2	30.4	0.3	30
	1300	2.8	10.0	0.1	22
	13	0.6	2.6	0.0	26
	1200	1.2	26.6	0.2	29
Craycroft Rd	12	24.2	50.1	0.2	16
5th Side Street	111	3.7	33.9	0.2	26
Rosemont Blvd	11	8.7	38.3	0.3	24
	1100	2.2	16.7	0.1	26
	10	2.8	17.9	0.1	26
Arcadia Ave HAWK	100	2.1	4.6	0.0	15
	9000	0.9	13.6	0.1	29
Swan Rd	9	28.8	42.7	0.1	10
	8000	3.7	33.6	0.3	27
Columbus Blvd	8	14.7	53.2	0.3	22
	7000	2.4	15.8	0.1	26
	7	2.0	17.6	0.1	27
Irving Ave HAWK	70	1.7	4.4	0.0	16
	6000	1.1	13.7	0.1	28
Alvernon Wy	6	30.3	44.3	0.1	10
	5000	2.6	17.4	0.1	26
Dodge Blvd	5	11.2	26.2	0.1	18
	4000	5.2	52.4	0.4	28
	9922	2.0	31.3	0.2	28
Country Club Rd	4	34.6	45.4	0.1	8
6th/5th Street	72	5.5	21.7	0.1	19
	3000	0.4	9.9	0.0	11
Tucson Blvd	3	10.5	53.4	0.4	25
6th Side Street	201	1.4	10.9	0.1	26
Wilson HAWK	20	3.0	11.1	0.1	21
	2	0.5	2.6	0.0	27
	1000	1.4	21.7	0.2	28
Campbell Ave	1	31.7	50.8	0.2	12
Total		274.9	982.5	5.5	20

Arterial Level of Service: NB Country Club Rd

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
	40	7.2	48.0	0.4	30
6th/5th Street	4	30.3	34.8	0.0	5
Total		37.4	82.8	0.4	20

Arterial Level of Service: SB Country Club Rd

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
6th/5th Street	4	36.2	75.1	0.4	19
	40	2.7	9.2	0.0	19
Total		38.9	84.3	0.4	19

Queuing and Blocking Report
Scenario A

11/07/2022

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	424	610	610	200	231	283	304	291	368	370	348	169
Average Queue (ft)	269	333	340	143	114	160	179	123	258	260	232	48
95th Queue (ft)	450	573	570	266	201	246	270	229	343	344	317	114
Link Distance (ft)		2992	2992			786	786		2020	2020	2020	
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	250			50	180			290				150
Storage Blk Time (%)	24	16	62	11	3	5		0	3		24	0
Queuing Penalty (veh)	116	45	107	55	9	9		0	5		32	0

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	350	2185	2169	2140
Average Queue (ft)	287	1446	1414	1350
95th Queue (ft)	483	2452	2402	2310
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)		12	8	8
Queuing Penalty (veh)		0	0	0
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	1	73		
Queuing Penalty (veh)	5	131		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	186	211	63	34	35	37
Average Queue (ft)	46	48	10	2	9	11
95th Queue (ft)	150	159	45	18	31	35
Link Distance (ft)	843	843	55	55	433	438
Upstream Blk Time (%)			1	0		
Queuing Penalty (veh)			6	0		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario A

11/07/2022

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	264	323	332	96	220	234	272	570	400	828
Average Queue (ft)	101	150	165	37	74	90	75	338	180	475
95th Queue (ft)	199	276	294	78	162	182	221	571	409	861
Link Distance (ft)		356	356		1877	1877		1935		2179
Upstream Blk Time (%)		0	0							
Queuing Penalty (veh)		1	3							
Storage Bay Dist (ft)	100			100			300		300	
Storage Blk Time (%)	22	19		1	6			16	1	33
Queuing Penalty (veh)	120	29		5	3			6	7	24

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	NB	NB	NB	NB	SB
Directions Served	L	T	T	L	T	T	R	L	T	T	R	L
Maximum Queue (ft)	224	308	319	189	254	267	199	172	213	217	170	174
Average Queue (ft)	91	170	179	91	152	161	23	104	172	175	53	131
95th Queue (ft)	181	274	281	165	230	239	126	179	210	216	162	212
Link Distance (ft)		488	488		423	423			172	172		
Upstream Blk Time (%)								1	14	15	0	
Queuing Penalty (veh)								0	76	82	0	
Storage Bay Dist (ft)	180			90			160	100			70	100
Storage Blk Time (%)	0	10		19	32	13	1	12	35	42	0	17
Queuing Penalty (veh)	2	19		51	39	27	2	51	52	35	2	64

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	TR
Maximum Queue (ft)	373	364
Average Queue (ft)	225	225
95th Queue (ft)	340	331
Link Distance (ft)	2000	2000
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)	37	
Queuing Penalty (veh)	55	

Queuing and Blocking Report
Scenario A

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Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	WB	NB	NB
Directions Served	T	TR	L	T	T	L	R
Maximum Queue (ft)	178	180	177	235	244	164	130
Average Queue (ft)	56	69	82	127	144	81	63
95th Queue (ft)	133	148	146	216	230	143	109
Link Distance (ft)	2071	2071		619	619	2122	
Upstream Blk Time (%)							
Queuing Penalty (veh)							
Storage Bay Dist (ft)			300				220
Storage Blk Time (%)						0	
Queuing Penalty (veh)						0	

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	TR	L	T	TR
Maximum Queue (ft)	199	301	300	174	312	325	250	762	755	250	525	526
Average Queue (ft)	133	156	158	79	187	207	133	484	488	152	320	326
95th Queue (ft)	219	267	250	169	283	298	294	869	864	286	484	482
Link Distance (ft)		581	581		578	578		2106	2106		1977	1977
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	100			75			150			150		
Storage Blk Time (%)	26	25		11	38		0	50		4	38	
Queuing Penalty (veh)	81	51		33	34		2	48		18	48	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	64	41	111	125	35	36
Average Queue (ft)	8	3	20	21	12	10
95th Queue (ft)	38	21	76	81	37	32
Link Distance (ft)	56	56	627	627	304	320
Upstream Blk Time (%)	1	0				
Queuing Penalty (veh)	2	0				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario A

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Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	142	223	230	132	254	265	199	543	196	504
Average Queue (ft)	42	96	113	38	130	151	57	223	61	212
95th Queue (ft)	96	181	199	97	227	247	159	445	166	454
Link Distance (ft)		533	533		1685	1685		2072		1936
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			75			100		100	
Storage Blk Time (%)	3	11		2	15		2	36	3	32
Queuing Penalty (veh)	11	7		6	8		9	17	14	17

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	200	349	356	194	275	284	264	577	581	380	294	458
Average Queue (ft)	96	200	221	97	156	173	125	334	333	96	120	259
95th Queue (ft)	194	309	329	182	243	257	278	540	541	291	236	403
Link Distance (ft)		1226	1226		560	560		1988	1988			2060
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	100			100			140			350	200	
Storage Blk Time (%)	3	35		13	22		1	43	9		1	19
Queuing Penalty (veh)	13	43		40	31		7	44	16		6	27

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	456	245
Average Queue (ft)	258	44
95th Queue (ft)	404	157
Link Distance (ft)	2060	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	4	
Queuing Penalty (veh)	3	

Queuing and Blocking Report
Scenario A

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Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	61	51	133	135	99	71
Average Queue (ft)	18	6	37	26	37	23
95th Queue (ft)	55	32	98	92	73	54
Link Distance (ft)	52	52	616	616	261	294
Upstream Blk Time (%)	1	0				
Queuing Penalty (veh)	5	1				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	158	238	260	76	123	151	250	558	214	410
Average Queue (ft)	58	119	138	31	59	75	104	226	60	163
95th Queue (ft)	115	206	233	67	105	126	235	488	157	324
Link Distance (ft)		570	570		1260	1260		1970		2041
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			100			150		150	
Storage Blk Time (%)	6	13		0	1		7	23	3	13
Queuing Penalty (veh)	24	14		0	0		29	23	10	7

Queuing and Blocking Report
Scenario A

11/07/2022

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	175	318	333	209	207	211	329	655	647	230	299	488
Average Queue (ft)	103	189	216	108	98	116	143	370	371	141	82	303
95th Queue (ft)	184	292	309	188	169	181	333	583	584	298	234	454
Link Distance (ft)		1212	1212		1092	1092		1951	1951			2065
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	75			150			180			180	150	
Storage Blk Time (%)	17	33		5	1		0	39	38	0	0	35
Queuing Penalty (veh)	56	56		11	2		1	42	50	0	0	21

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	480	300
Average Queue (ft)	305	91
95th Queue (ft)	461	275
Link Distance (ft)	2065	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	34	
Queuing Penalty (veh)	34	

Intersection: 13: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	134	153	62	31	83	24
Average Queue (ft)	18	24	15	2	37	1
95th Queue (ft)	80	94	52	16	67	12
Link Distance (ft)	1086	1086	56	56	422	473
Upstream Blk Time (%)			2	0		
Queuing Penalty (veh)			4	0		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario A

11/07/2022

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	54	53	49	43	31	46
Average Queue (ft)	7	3	5	2	6	17
95th Queue (ft)	32	23	29	17	26	43
Link Distance (ft)	275	275	1271	1271	614	699
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	242	310	216	86	332	373	363	335	200	304	306	276
Average Queue (ft)	134	161	106	36	175	226	226	202	45	213	207	170
95th Queue (ft)	205	241	180	75	296	333	326	304	145	293	292	268
Link Distance (ft)		1225	1225	2261		1997	1997	1997		2032	2032	2032
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	150				240				130			
Storage Blk Time (%)	7	17			4	5				26		0
Queuing Penalty (veh)	30	36			20	12				12		0

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	90
Average Queue (ft)	43
95th Queue (ft)	73
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Queuing and Blocking Report
Scenario A

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Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	75	72	176	177
Average Queue (ft)	26	27	46	48
95th Queue (ft)	72	73	132	137
Link Distance (ft)	55	55	298	298
Upstream Blk Time (%)	5	5		
Queuing Penalty (veh)	30	31		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 40: Country Club Rd

Movement	EB	NB	NB
Directions Served	R	T	T
Maximum Queue (ft)	117	243	253
Average Queue (ft)	54	71	72
95th Queue (ft)	94	187	192
Link Distance (ft)	451	2094	2094
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	233	244	71	76
Average Queue (ft)	48	54	26	28
95th Queue (ft)	158	172	72	76
Link Distance (ft)	513	513	56	56
Upstream Blk Time (%)			4	4
Queuing Penalty (veh)			15	16
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

Scenario A

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Intersection: 72: 6th/5th Street

Movement	SW	SW
Directions Served	R	R
Maximum Queue (ft)	20	12
Average Queue (ft)	1	0
95th Queue (ft)	11	7
Link Distance (ft)	488	488
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 100: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	236	246	62	75
Average Queue (ft)	60	62	22	25
95th Queue (ft)	186	193	65	71
Link Distance (ft)	526	526	52	52
Upstream Blk Time (%)			5	6
Queuing Penalty (veh)			17	20
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 111: 5th Side Street & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	T	TR	T	TR	LTR	LTR
Maximum Queue (ft)	4	6	21	7	81	104
Average Queue (ft)	0	0	1	0	37	43
95th Queue (ft)	5	4	11	5	69	77
Link Distance (ft)	1260	1260	1212	1212	332	276
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 201: 6th Side Street & 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	TR	LTR	LTR
Maximum Queue (ft)	26	22	2	97	68
Average Queue (ft)	1	1	0	42	26
95th Queue (ft)	13	12	2	76	56
Link Distance (ft)	298	298	356	419	408
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 1000: 6th/5th Street

Movement	EB	EB	SB
Directions Served	T	TR	LTR
Maximum Queue (ft)	20	20	85
Average Queue (ft)	1	1	36
95th Queue (ft)	12	13	65
Link Distance (ft)	786	786	416
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1100: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	T	LTR	LTR
Maximum Queue (ft)	2	5	89	95
Average Queue (ft)	0	0	44	37
95th Queue (ft)	2	4	75	74
Link Distance (ft)	616	570	214	185
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report
Scenario A

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Intersection: 1200: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	10	8	99	79
Average Queue (ft)	0	0	43	38
95th Queue (ft)	7	6	79	63
Link Distance (ft)	1092	1092	211	276
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1300: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	71	64	104	107
Average Queue (ft)	22	27	29	29
95th Queue (ft)	65	70	83	82
Link Distance (ft)	56	56	275	275
Upstream Blk Time (%)	3	5		
Queuing Penalty (veh)	12	16		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1400: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	2	6	68	80
Average Queue (ft)	0	0	38	33
95th Queue (ft)	2	5	60	63
Link Distance (ft)	1271	1271	305	274
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

Scenario A

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Intersection: 3000: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	8	8	90	56
Average Queue (ft)	0	0	36	26
95th Queue (ft)	8	6	71	47
Link Distance (ft)	1877	1877	405	430
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 4000: 6th/5th Street

Movement	EB	EB	B9922	WB	WB
Directions Served	T	TR	T	T	TR
Maximum Queue (ft)	24	26	3	4	13
Average Queue (ft)	1	1	0	0	0
95th Queue (ft)	12	13	3	4	7
Link Distance (ft)	1235	1235	423	2071	2071
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 5000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	67	88
Average Queue (ft)	25	44
95th Queue (ft)	56	76
Link Distance (ft)	310	280
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario A

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Intersection: 6000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	92	81
Average Queue (ft)	41	39
95th Queue (ft)	74	65
Link Distance (ft)	355	360
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 7000: 6th/5th Street

Movement	WB	NB
Directions Served	T	LTR
Maximum Queue (ft)	6	35
Average Queue (ft)	0	7
95th Queue (ft)	7	29
Link Distance (ft)	533	260
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 8000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	64	57
Average Queue (ft)	31	20
95th Queue (ft)	55	47
Link Distance (ft)	319	333
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario A

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Intersection: 9000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	TR	LTR	LTR
Maximum Queue (ft)	10	24	4	128	94
Average Queue (ft)	0	1	0	60	44
95th Queue (ft)	6	12	3	106	76
Link Distance (ft)	560	560	526	204	310
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Network Summary

Network wide Queuing Penalty: 2571



Appendix G
Existing Scenario B
Simtraffic Reports

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	6:45	6:45	6:45	6:45	6:45	6:45	6:45
End Time	8:00	8:00	8:00	8:00	8:00	8:00	8:00
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	20923	21096	21041	21067	21042	20997	20998
Vehs Exited	20763	20945	20834	20840	20892	20828	20893
Starting Vehs	835	886	848	815	870	833	900
Ending Vehs	995	1037	1055	1042	1020	1002	1005
Travel Distance (mi)	22252	22328	22412	22364	22338	22530	22374
Travel Time (hr)	984.4	991.8	994.7	1000.5	995.2	1012.4	994.1
Total Delay (hr)	285.7	290.2	290.8	296.0	292.6	301.9	291.0
Total Stops	25920	25830	26365	26476	26272	26511	26038
Fuel Used (gal)	760.8	763.7	767.6	766.2	765.5	774.1	766.1

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	6:45	6:45	6:45	6:45
End Time	8:00	8:00	8:00	8:00
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	21089	21081	21175	21048
Vehs Exited	20887	20910	21028	20877
Starting Vehs	841	803	862	836
Ending Vehs	1043	974	1009	1000
Travel Distance (mi)	22337	22428	22628	22399
Travel Time (hr)	990.7	998.5	1023.0	998.5
Total Delay (hr)	287.5	294.1	310.2	294.0
Total Stops	25760	26307	27360	26279
Fuel Used (gal)	761.9	769.3	777.8	767.3

Interval #0 Information Seeding

Start Time	6:45
End Time	7:00
Total Time (min)	15
No data recorded this interval.	

Interval #1 Information Recording

Start Time	7:00
End Time	8:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	20923	21096	21041	21067	21042	20997	20998
Vehs Exited	20763	20945	20834	20840	20892	20828	20893
Starting Vehs	835	886	848	815	870	833	900
Ending Vehs	995	1037	1055	1042	1020	1002	1005
Travel Distance (mi)	22252	22328	22412	22364	22338	22530	22374
Travel Time (hr)	984.4	991.8	994.7	1000.5	995.2	1012.4	994.1
Total Delay (hr)	285.7	290.2	290.8	296.0	292.6	301.9	291.0
Total Stops	25920	25830	26365	26476	26272	26511	26038
Fuel Used (gal)	760.8	763.7	767.6	766.2	765.5	774.1	766.1

Interval #1 Information Recording

Start Time	7:00
End Time	8:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	21089	21081	21175	21048
Vehs Exited	20887	20910	21028	20877
Starting Vehs	841	803	862	836
Ending Vehs	1043	974	1009	1000
Travel Distance (mi)	22337	22428	22628	22399
Travel Time (hr)	990.7	998.5	1023.0	998.5
Total Delay (hr)	287.5	294.1	310.2	294.0
Total Stops	25760	26307	27360	26279
Fuel Used (gal)	761.9	769.3	777.8	767.3

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.2	0.1	0.2	0.0	0.0	0.0	1.2	0.1	1.2	1.3	0.1	0.1
Total Del/Veh (s)	54.8	30.6	8.4	33.5	48.0	49.8	56.9	34.5	8.6	43.5	34.6	26.8

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	37.0

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	14.5	2.9	1.3	2.8	0.6	0.3	33.2	8.9	35.8	19.7	11.2	1.8

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.2	0.0	0.0	0.2	0.0	0.0	2.1	0.4	0.3	1.6	0.4	0.5
Total Del/Veh (s)	39.3	10.2	8.8	24.0	17.3	16.7	63.2	30.6	24.8	49.8	36.5	32.4

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	22.8

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.1	0.0	0.0	0.0	0.1	0.0	0.0	0.0	1.7	0.2	0.2	0.1
Total Del/Veh (s)	31.4	16.7	26.4	23.1	4.4	42.8	31.7	4.8	40.3	35.5	27.2	26.0

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.3	1.0	0.1
Total Del/Veh (s)	8.6	7.4	10.7	9.5	45.6	7.6	10.9

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.3	0.2	0.3	1.4	0.2	0.3
Total Del/Veh (s)	37.4	21.6	19.0	41.7	33.1	32.9	46.0	32.0	27.7	45.9	31.3	26.4

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	31.6

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.2	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	12.0	0.6	0.3	7.7	2.6	3.1	17.5	3.7	19.5	17.4	8.4	2.1

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.5	0.5	0.5	1.9	0.4	0.3
Total Del/Veh (s)	27.2	9.7	8.3	23.5	13.1	11.7	28.8	18.2	13.2	27.4	17.6	12.1

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	14.8

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.6	0.3	1.5	1.8	0.2	1.6
Total Del/Veh (s)	45.8	34.2	32.7	42.5	33.6	31.8	36.6	25.4	7.8	44.8	23.6	7.1

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	28.7

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.2	0.1	0.1	0.0	0.0
Total Del/Veh (s)	5.9	1.3	0.9	20.4	5.4	3.2	5.9	41.5	12.8	5.4	5.4

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.3	0.3	0.4	0.5	0.4	0.3
Total Del/Veh (s)	27.3	10.1	8.5	24.6	13.5	13.1	25.0	13.6	9.0	19.8	16.3	12.4

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	14.2

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.5	0.3	1.5	1.9	0.2	1.6
Total Del/Veh (s)	50.4	34.1	28.9	40.9	36.9	34.8	36.6	24.4	8.3	41.9	20.7	6.6

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	27.8

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	1.8	1.5	2.9	0.4	10.4	5.8	1.1

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.1
Total Del/Veh (s)	5.6	8.2	5.9	8.8	9.4	7.6	5.5	7.1	3.7	5.0	6.6	3.2

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	8.4

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.1	0.1	1.5	0.2	0.2	1.3	0.2	1.3
Total Del/Veh (s)	54.5	51.4	15.6	34.9	38.0	16.9	55.9	18.3	10.7	31.7	19.1	6.5

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	23.0

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.4	3.6	2.8

40: Country Club Rd Performance by movement

Movement	EBR	NBT	SBT	All
Denied Del/Veh (s)	0.0	0.2	0.0	0.1
Total Del/Veh (s)	7.0	1.9	2.0	2.4

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	2.9	1.8	2.3

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWT	SWR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0
Total Del/Veh (s)	2.9	0.4	1.8	2.3	2.4

100: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	1.0	1.2	2.2	2.3	15.1	5.3	2.0

110: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	2.4	2.6	3.2	3.5	4.0	4.8	3.0

120: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	2.7	3.1	0.7	10.5	4.5	1.9

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.7	4.4	3.2

140: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.1	0.1	0.0
Total Del/Veh (s)	9.1	6.8	8.0	6.0	5.1	3.1	3.2	7.7

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.2	0.0
Total Del/Veh (s)	3.1	3.5	2.2	2.4	37.3	24.8	4.0

1050: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.1	0.0
Total Del/Veh (s)	5.8	2.6	4.3

2000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.2	0.2	0.2	0.0
Total Del/Veh (s)	1.1	0.8	2.8	2.7	25.5	13.9	23.5	13.8	4.1

3000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.2	0.2	0.0
Total Del/Veh (s)	3.6	3.1	0.6	0.3	18.9	9.0	22.2	11.2	3.1

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.2	0.2	0.0
Total Del/Veh (s)	1.4	1.7	4.8	5.0	14.9	7.2	17.5	10.3	4.5

5000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.1	0.2	0.1	0.0
Total Del/Veh (s)	1.7	1.6	2.4	2.4	17.2	5.9	17.9	8.8	2.9

6000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.1	0.1	0.1	0.0
Total Del/Veh (s)	2.2	2.4	1.3	1.1	17.3	6.1	5.3	2.1	

7000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.1	0.1	0.1	0.0
Total Del/Veh (s)	1.2	1.1	2.2	2.1	16.0	6.0	11.7	6.2	2.4

8000: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.1	0.0
Total Del/Veh (s)	2.2	3.6	4.0	12.6	5.3	5.3	3.3

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.2	0.0
Total Del/Veh (s)	2.7	2.8	1.3	1.0	17.3	7.9	15.8	6.7	2.8

Total Network Performance

Denied Del/Veh (s)	0.4
Total Del/Veh (s)	48.0

Arterial Level of Service
Scenario B

11/03/2022

Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	30.6	97.8	0.6	21
	1000	4.1	24.7	0.2	24
	2	2.9	22.6	0.2	27
Wilson HAWK	20	1.4	3.9	0.0	18
	2000	1.1	8.6	0.1	27
Tucson Blvd	3	9.3	18.4	0.1	16
	3000	3.9	46.7	0.4	28
6th/5th Street	72	3.0	6.7	0.0	16
Country Club Rd	4	16.7	29.9	0.1	14
	9922	1.4	13.5	0.1	25
	4000	1.4	30.6	0.2	29
Dodge Blvd	5	8.7	56.6	0.4	26
	5000	1.8	17.5	0.1	26
Alvernon Wy	6	20.7	34.8	0.1	13
	6000	2.6	17.6	0.1	26
	70	2.9	16.0	0.1	24
	7	0.6	2.7	0.0	26
Columbus Blvd	7000	1.2	16.9	0.1	28
	8	9.7	22.8	0.1	18
	8000	2.5	32.2	0.3	29
Swan Rd	9	34.3	63.1	0.3	14
	9000	3.6	18.6	0.1	24
	1050	5.8	19.1	0.1	20
	10	1.3	3.3	0.0	21
	100	1.1	15.9	0.1	29
Rosemont Blvd	11	10.1	23.7	0.1	18
	110	2.6	31.5	0.3	29
Craycroft Rd	12	34.4	63.2	0.2	14
	120	3.6	30.5	0.2	26
	13	1.8	27.6	0.2	28
Chantilly Dr HAWK	130	1.7	4.3	0.0	16
Sahuara Ave	14	8.2	15.3	0.1	14
	140	9.4	39.6	0.3	23
Wilmot Rd	15	51.8	79.5	0.3	11
Total		296.3	955.7	5.6	21

Arterial Level of Service
Scenario B

11/03/2022

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	38.0	137.0	0.4	12
	140	7.6	38.9	0.3	23
Sahuara Ave	14	9.6	39.6	0.3	23
	130	4.4	12.0	0.1	18
	13	0.4	2.4	0.0	29
	120	0.7	26.8	0.2	29
Craycroft Rd	12	37.2	63.4	0.2	13
	110	4.2	34.1	0.2	26
Rosemont Blvd	11	13.7	42.6	0.3	21
	100	2.5	16.5	0.1	26
	10	5.5	20.4	0.1	22
Arcadia Ave HAWK	1050	2.6	5.1	0.0	14
	9000	1.3	14.2	0.1	27
Swan Rd	9	34.1	48.1	0.1	9
	8000	4.3	34.3	0.3	26
Columbus Blvd	8	13.2	51.5	0.3	23
	7000	2.5	15.7	0.1	26
	7	2.6	18.2	0.1	26
Irving Ave HAWK	70	1.8	4.4	0.0	15
	6000	1.3	13.9	0.1	28
Alvernon Wy	6	33.2	47.4	0.1	10
	5000	2.9	17.7	0.1	25
Dodge Blvd	5	9.8	25.1	0.1	18
	4000	5.1	53.2	0.4	27
	9922	2.0	31.4	0.2	28
Country Club Rd	4	23.1	34.1	0.1	10
	72	2.9	16.7	0.1	25
6th/5th Street	3000	0.6	4.6	0.0	24
	3	17.3	59.5	0.4	22
Tucson Blvd	2000	3.2	12.6	0.1	23
	20	3.5	11.6	0.1	20
Wilson HAWK	2	0.6	2.5	0.0	27
	1000	2.2	22.1	0.2	28
	1	47.9	67.0	0.2	9
Total		341.8	1044.5	5.5	19

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	198	252	234	188	379	521	552	282	305	299	266	106
Average Queue (ft)	115	132	139	53	144	288	311	146	202	203	171	41
95th Queue (ft)	190	211	210	132	318	503	526	252	270	275	245	82
Link Distance (ft)		2986	2986			780	780		2020	2020	2020	
Upstream Blk Time (%)						0	0					
Queuing Penalty (veh)						0	0					
Storage Bay Dist (ft)	100			50	180			290				150
Storage Blk Time (%)	23	16	46	4	2	31		1	0		11	0
Queuing Penalty (veh)	57	26	57	9	8	63		2	1		17	0

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	195	305	295	331
Average Queue (ft)	64	198	200	197
95th Queue (ft)	134	266	274	288
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	0	7		
Queuing Penalty (veh)	0	5		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	156	146	56	62	35	68
Average Queue (ft)	38	24	10	8	10	25
95th Queue (ft)	108	93	39	38	33	56
Link Distance (ft)	843	843	55	55	433	438
Upstream Blk Time (%)			0	0		
Queuing Penalty (veh)			2	1		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	123	193	217	154	347	353	164	345	176	438
Average Queue (ft)	55	81	91	38	174	192	75	173	60	220
95th Queue (ft)	105	153	166	97	295	309	146	295	128	365
Link Distance (ft)		356	356		1877	1877		1935		2179
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	100			100			300		300	
Storage Blk Time (%)	3	5		0	17			1		3
Queuing Penalty (veh)	9	4		2	9			1		3

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	NB	NB	NB	NB	SB
Directions Served	L	T	T	L	T	T	R	L	T	T	R	L
Maximum Queue (ft)	173	141	168	189	315	338	260	167	189	190	103	174
Average Queue (ft)	67	71	87	54	167	185	40	74	115	115	12	93
95th Queue (ft)	132	131	145	139	278	302	190	141	177	182	59	175
Link Distance (ft)		488	488		423	423			172	172		
Upstream Blk Time (%)						0		0	1	1	0	
Queuing Penalty (veh)						0		0	3	4	0	
Storage Bay Dist (ft)	180			90			160	100			70	100
Storage Blk Time (%)	0	0		1	23	15	0	6	14	25	0	7
Queuing Penalty (veh)	0	0		5	16	27	0	14	16	8	0	14

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	TR
Maximum Queue (ft)	289	295
Average Queue (ft)	152	173
95th Queue (ft)	243	264
Link Distance (ft)	2000	2000
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)	23	
Queuing Penalty (veh)	27	

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	WB	NB	NB
Directions Served	T	TR	L	T	T	L	R
Maximum Queue (ft)	151	185	130	249	253	146	86
Average Queue (ft)	59	81	51	122	139	70	40
95th Queue (ft)	127	166	98	217	234	127	71
Link Distance (ft)	2071	2071		619	619	2122	
Upstream Blk Time (%)							
Queuing Penalty (veh)							
Storage Bay Dist (ft)			300			220	
Storage Blk Time (%)				0			
Queuing Penalty (veh)				0			

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	TR	L	T	TR
Maximum Queue (ft)	181	219	237	173	316	325	250	447	446	249	411	411
Average Queue (ft)	80	105	130	78	191	211	138	254	263	108	241	247
95th Queue (ft)	148	188	215	173	294	310	261	396	396	223	364	365
Link Distance (ft)		581	581		578	578		2106	2106		1977	1977
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	100			75			150			150		
Storage Blk Time (%)	6	8		9	41		2	26		1	25	
Queuing Penalty (veh)	14	10		28	33		11	41		5	29	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	56	51	142	154	34	44
Average Queue (ft)	6	5	42	29	12	15
95th Queue (ft)	32	28	107	100	37	39
Link Distance (ft)	56	56	627	627	304	320
Upstream Blk Time (%)	0	0				
Queuing Penalty (veh)	1	0				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	74	167	182	115	233	237	165	232	103	218
Average Queue (ft)	29	70	89	29	107	122	54	126	36	113
95th Queue (ft)	65	131	156	74	193	208	112	206	82	188
Link Distance (ft)		533	533		1685	1685		2072		1936
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			75			100		100	
Storage Blk Time (%)	2	5		1	11		1	14	0	11
Queuing Penalty (veh)	4	2		3	4		5	11	1	6

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	160	231	259	190	279	280	251	332	322	100	247	314
Average Queue (ft)	58	128	157	75	156	178	81	203	203	39	109	178
95th Queue (ft)	118	209	241	161	253	271	179	300	300	76	192	274
Link Distance (ft)		1226	1226		560	560		1988	1988			2060
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	100			100			140			350	200	
Storage Blk Time (%)	2	17		3	22		1	22	0		1	5
Queuing Penalty (veh)	6	14		9	21		5	23	0		3	7

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	308	80
Average Queue (ft)	177	28
95th Queue (ft)	272	63
Link Distance (ft)	2060	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	0	
Queuing Penalty (veh)	0	

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	66	66	227	199	109	70
Average Queue (ft)	23	17	105	54	54	29
95th Queue (ft)	62	54	191	150	88	62
Link Distance (ft)	52	52	616	616	261	294
Upstream Blk Time (%)	2	1				
Queuing Penalty (veh)	8	3				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	124	154	160	99	221	223	94	174	98	231
Average Queue (ft)	58	70	85	40	107	121	41	84	24	121
95th Queue (ft)	103	126	143	79	189	199	79	142	67	202
Link Distance (ft)		570	570		1260	1260		1970		2041
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			100			150		150	
Storage Blk Time (%)	7	5		0	7			1		3
Queuing Penalty (veh)	15	5		1	4			0		1

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	171	211	215	165	262	274	285	388	395	230	163	259
Average Queue (ft)	82	95	116	57	130	151	98	221	222	56	72	153
95th Queue (ft)	146	174	188	122	217	236	206	343	346	177	127	237
Link Distance (ft)		1212	1212		1092	1092		1951	1951			2065
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	75			150			180			180	150	
Storage Blk Time (%)	17	12		0	7		0	14	13	0	0	8
Queuing Penalty (veh)	26	15		0	7		2	21	13	0	1	8

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	258	118
Average Queue (ft)	150	32
95th Queue (ft)	238	79
Link Distance (ft)	2065	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	8	
Queuing Penalty (veh)	8	

Intersection: 13: 6th/5th Street

Movement	EB	EB	WB	WB	NB
Directions Served	LT	TR	LT	TR	LTR
Maximum Queue (ft)	54	83	53	35	50
Average Queue (ft)	5	11	4	2	19
95th Queue (ft)	31	50	26	15	45
Link Distance (ft)	1086	1086	56	56	422
Upstream Blk Time (%)	0				
Queuing Penalty (veh)	1				
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	97	115	93	86	72	33
Average Queue (ft)	54	63	54	55	34	13
95th Queue (ft)	82	95	77	76	57	37
Link Distance (ft)	275	275	1271	1271	614	699
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	128	156	122	138	343	378	318	226	229	294	279	253
Average Queue (ft)	57	91	56	59	196	169	157	128	74	177	171	137
95th Queue (ft)	103	141	102	111	319	289	248	213	163	259	253	230
Link Distance (ft)		1219	1219	2255		1997	1997	1997		2032	2032	2032
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	150				240				130			
Storage Blk Time (%)	0	1			9	0			1	16		0
Queuing Penalty (veh)	0	1			40	1			4	20		0

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	127
Average Queue (ft)	55
95th Queue (ft)	98
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	64	73	235	244
Average Queue (ft)	22	25	57	69
95th Queue (ft)	64	69	173	191
Link Distance (ft)	55	55	298	298
Upstream Blk Time (%)	3	3		0
Queuing Penalty (veh)	10	13		0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 40: Country Club Rd

Movement	EB	NB	NB	SB
Directions Served	R	T	T	T
Maximum Queue (ft)	73	54	42	6
Average Queue (ft)	39	3	2	0
95th Queue (ft)	63	26	19	5
Link Distance (ft)	451	2094	2094	172
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	134	156	72	81
Average Queue (ft)	25	32	27	32
95th Queue (ft)	86	105	73	82
Link Distance (ft)	513	513	56	56
Upstream Blk Time (%)			4	5
Queuing Penalty (veh)			17	24
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 72: 6th/5th Street

Movement

Directions Served
Maximum Queue (ft)
Average Queue (ft)
95th Queue (ft)
Link Distance (ft)
Upstream Blk Time (%)
Queuing Penalty (veh)
Storage Bay Dist (ft)
Storage Blk Time (%)
Queuing Penalty (veh)

Intersection: 100: 6th/5th Street

Movement	NB	SB
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Directions Served	LTR	LTR
Maximum Queue (ft)	62	64
Average Queue (ft)	23	29
95th Queue (ft)	52	56
Link Distance (ft)	214	185
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 110: 6th/5th Street

Movement	EB	WB	WB	NB	SB
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Directions Served	TR	T	TR	LTR	LTR
Maximum Queue (ft)	2	10	3	64	55
Average Queue (ft)	0	0	0	29	28
95th Queue (ft)	2	8	3	55	51
Link Distance (ft)	1260	1212	1212	332	276
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 120: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	12	16	46	33
Average Queue (ft)	0	1	16	11
95th Queue (ft)	9	9	44	35
Link Distance (ft)	1092	1092	211	276
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	56	68	102	104
Average Queue (ft)	14	20	27	34
95th Queue (ft)	50	63	81	92
Link Distance (ft)	56	56	275	275
Upstream Blk Time (%)	1	3		
Queuing Penalty (veh)	3	7		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 140: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	T	TR	T	TR	LTR	LTR
Maximum Queue (ft)	73	81	86	92	62	49
Average Queue (ft)	45	49	55	59	33	24
95th Queue (ft)	68	73	78	78	52	47
Link Distance (ft)	1271	1271	1219	1219	305	274
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 1000: 6th/5th Street

Movement	EB	EB	WB	WB	SB
Directions Served	T	TR	T	TR	LTR
Maximum Queue (ft)	8	4	11	11	147
Average Queue (ft)	0	0	0	0	65
95th Queue (ft)	6	4	9	8	121
Link Distance (ft)	780	780	843	843	416
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 1050: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	199	246	68	83
Average Queue (ft)	45	68	27	32
95th Queue (ft)	136	189	69	78
Link Distance (ft)	526	526	52	52
Upstream Blk Time (%)			5	7
Queuing Penalty (veh)			15	23
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 2000: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	T	T	TR	LTR	LTR
Maximum Queue (ft)	11	21	20	156	119
Average Queue (ft)	0	1	1	63	52
95th Queue (ft)	8	12	9	119	95
Link Distance (ft)	298	356	356	419	408
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Queuing and Blocking Report

Scenario B

11/03/2022

Intersection: 3000: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	TR	T	TR	LTR	LTR
Maximum Queue (ft)	2	8	5	88	119
Average Queue (ft)	0	0	0	35	49
95th Queue (ft)	2	6	3	67	92
Link Distance (ft)	1877	100	100	405	430
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 4000: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	T	T	TR	LTR	LTR
Maximum Queue (ft)	3	11	4	91	102
Average Queue (ft)	0	0	0	41	49
95th Queue (ft)	3	7	3	71	86
Link Distance (ft)	1235	2071	2071	547	583
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 5000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	TR	LTR	LTR
Maximum Queue (ft)	6	3	2	66	83
Average Queue (ft)	0	0	0	29	41
95th Queue (ft)	5	3	2	57	71
Link Distance (ft)	619	619	581	310	280
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Queuing and Blocking Report

Scenario B

11/03/2022

Intersection: 6000: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	T	T	TR	LTR	LTR
Maximum Queue (ft)	12	19	2	74	52
Average Queue (ft)	0	1	0	32	20
95th Queue (ft)	7	11	2	61	46
Link Distance (ft)	578	513	513	355	360
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 7000: 6th/5th Street

Movement	EB	NB	SB
Directions Served	T	LTR	LTR
Maximum Queue (ft)	3	88	67
Average Queue (ft)	0	38	31
95th Queue (ft)	3	69	57
Link Distance (ft)	627	260	304
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 8000: 6th/5th Street

Movement	WB	NB	SB
Directions Served	T	LTR	LTR
Maximum Queue (ft)	14	61	57
Average Queue (ft)	0	29	27
95th Queue (ft)	8	54	51
Link Distance (ft)	1226	319	333
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Queuing and Blocking Report

Scenario B

11/03/2022

Intersection: 9000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	TR	LTR	LTR
Maximum Queue (ft)	7	8	3	77	102
Average Queue (ft)	0	0	0	36	43
95th Queue (ft)	5	6	3	67	75
Link Distance (ft)	560	560	526	204	310
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Network Summary

Network wide Queuing Penalty: 1028

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	4:45	4:45	4:45	4:45	4:45	4:45	4:45
End Time	6:00	6:00	6:00	6:00	6:00	6:00	6:00
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	20527	20200	20460	20255	20176	20345	20270
Vehs Exited	20508	20112	20369	20237	20054	20310	19979
Starting Vehs	1001	949	997	1021	999	960	970
Ending Vehs	1020	1037	1088	1039	1121	995	1261
Travel Distance (mi)	23770	23196	23653	23277	23149	23501	23296
Travel Time (hr)	1070.3	1021.0	1056.3	1029.1	1020.9	1040.1	1041.1
Total Delay (hr)	325.9	294.7	317.0	300.9	297.3	304.3	311.3
Total Stops	25428	23585	24891	24049	23726	24102	24519
Fuel Used (gal)	811.9	785.2	805.5	790.1	785.6	798.7	792.1

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	4:45	4:45	4:45	4:45
End Time	6:00	6:00	6:00	6:00
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	20309	20271	20447	20327
Vehs Exited	20177	20258	20328	20237
Starting Vehs	905	958	974	965
Ending Vehs	1037	971	1093	1052
Travel Distance (mi)	23306	23483	23580	23421
Travel Time (hr)	1035.1	1040.6	1053.2	1040.8
Total Delay (hr)	306.3	306.4	313.4	307.7
Total Stops	24263	24318	24708	24355
Fuel Used (gal)	792.8	797.5	803.4	796.3

Interval #0 Information Seeding

Start Time	4:45
End Time	5:00
Total Time (min)	15

No data recorded this interval.

Interval #1 Information Recording

Start Time	5:00
End Time	6:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	20527	20200	20460	20255	20176	20345	20270
Vehs Exited	20508	20112	20369	20237	20054	20310	19979
Starting Vehs	1001	949	997	1021	999	960	970
Ending Vehs	1020	1037	1088	1039	1121	995	1261
Travel Distance (mi)	23770	23196	23653	23277	23149	23501	23296
Travel Time (hr)	1070.3	1021.0	1056.3	1029.1	1020.9	1040.1	1041.1
Total Delay (hr)	325.9	294.7	317.0	300.9	297.3	304.3	311.3
Total Stops	25428	23585	24891	24049	23726	24102	24519
Fuel Used (gal)	811.9	785.2	805.5	790.1	785.6	798.7	792.1

Interval #1 Information Recording

Start Time	5:00
End Time	6:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	20309	20271	20447	20327
Vehs Exited	20177	20258	20328	20237
Starting Vehs	905	958	974	965
Ending Vehs	1037	971	1093	1052
Travel Distance (mi)	23306	23483	23580	23421
Travel Time (hr)	1035.1	1040.6	1053.2	1040.8
Total Delay (hr)	306.3	306.4	313.4	307.7
Total Stops	24263	24318	24708	24355
Fuel Used (gal)	792.8	797.5	803.4	796.3

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.3	0.2	0.2	0.0	0.0	0.0	1.3	0.1	1.3	1.2	0.1	0.2
Total Del/Veh (s)	61.4	43.2	22.3	40.6	24.9	22.7	43.9	31.6	12.0	52.1	43.7	37.3

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	38.5

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.2	0.1	0.1	0.0
Total Del/Veh (s)	11.0	4.3	4.8	8.6	0.5	0.1	12.1	9.8	24.6	18.6	6.7	2.8

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.0	0.0	0.0	0.0	0.0	1.8	0.4	0.4	1.6	0.4	0.4
Total Del/Veh (s)	25.0	11.7	11.9	35.6	10.5	9.5	52.7	35.6	28.9	62.2	36.0	30.8

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	20.3

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	1.7	0.2	0.2	0.1
Total Del/Veh (s)	30.9	20.9	41.4	24.2	4.1	40.0	31.9	8.6	44.6	36.0	26.5	27.4

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.4	1.0	0.1
Total Del/Veh (s)	11.6	11.7	16.0	11.7	41.3	10.1	14.3

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.4	0.2	0.3	1.4	0.2	0.2
Total Del/Veh (s)	30.6	25.4	24.1	40.3	29.3	27.7	51.8	37.4	32.0	49.1	36.2	32.4

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	33.6

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.1
Total Del/Veh (s)	4.1	0.5	0.1	9.4	1.8	1.9	15.3	20.0	6.8	15.7	14.8	7.0

7: 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	1.3

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.0	0.0	0.2	0.0	0.0	1.6	0.4	0.4	1.9	0.4	0.4
Total Del/Veh (s)	21.7	11.8	11.3	30.9	12.0	10.7	47.2	21.2	16.1	32.9	25.5	20.4

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	17.0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.7	0.2	1.5	1.8	0.2	1.6
Total Del/Veh (s)	40.8	37.9	36.0	52.4	34.8	31.9	42.3	23.9	7.8	41.8	24.8	6.2

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	29.9

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.1	0.1
Total Del/Veh (s)	5.4	0.9	0.3	12.1	2.6	1.5	29.3	17.0	9.1	21.3	21.2	8.8

10: 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	2.4

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.2	0.0	0.0	0.1	0.0	0.0	1.3	0.4	0.4	0.7	0.3	0.3
Total Del/Veh (s)	24.0	14.5	12.9	37.4	14.7	12.4	22.9	14.8	10.6	23.3	15.2	11.5

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	15.5

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.6	0.2	1.6	1.6	0.2	1.5
Total Del/Veh (s)	50.5	37.9	35.0	40.0	27.6	23.8	41.9	23.9	8.5	42.0	24.2	7.0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	28.1

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.2	0.1		0.1	0.0
Total Del/Veh (s)	2.5	2.4	5.2	0.5	0.1	13.4	7.8		14.6	2.2

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	3.3	0.7	0.6	7.0	1.2	0.6	14.8	5.1	8.1	3.7	1.1

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.1	1.5	0.2	0.2	1.5	0.1	1.3
Total Del/Veh (s)	48.2	24.9	17.2	26.8	31.4	13.3	44.7	19.5	12.0	33.6	19.7	4.1

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	22.8

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.4	3.1	2.1

40: Country Club Rd Performance by movement

Movement	EBR	NBT	SBT	All
Denied Del/Veh (s)	0.0	0.2	0.0	0.1
Total Del/Veh (s)	6.2	3.3	2.2	3.2

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	4.1	1.7	3.0

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0
Total Del/Veh (s)	0.7	0.3	3.7	1.9

100: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	1.3	1.5	2.2	2.4	4.7	1.9

110: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	3.3	3.8	2.5	4.4	3.1

120: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.2	0.1	0.0
Total Del/Veh (s)	3.3	3.6	1.1	13.0	7.7	4.8	3.1

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.5	2.5	1.9

140: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.2	0.2	0.0
Total Del/Veh (s)	1.6	1.4	1.2	9.1	4.1	11.2	4.8	2.0

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	4.4	4.3	1.8	1.7	5.7	3.3

1050: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	5.1	2.2	3.9

2000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	1.1	1.5	11.0	6.4	1.4

3000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	4.7	5.0	0.3	28.5	9.1	3.4

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	2.7	3.3	4.7	5.3	6.9	3.8

5000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.1	0.0
Total Del/Veh (s)	1.7	1.9	22.6	11.1	4.9	2.3

6000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.0
Total Del/Veh (s)	2.5	2.8	1.2	0.9	18.1	8.1	5.7	2.4

7000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	1.5	1.4	2.0	1.9	4.9	1.8

8000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	2.9	3.0	5.3	4.8	3.0

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.1	0.0
Total Del/Veh (s)	3.0	3.2	0.8	6.7	14.8	6.0	2.4

Total Network Performance

Denied Del/Veh (s)	0.4
Total Del/Veh (s)	51.7

Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	43.2	110.3	0.6	19
	1000	5.4	26.2	0.2	23
	2	4.3	24.5	0.2	25
Wilson HAWK	20	1.4	3.9	0.0	17
	2000	1.1	8.6	0.1	27
Tucson Blvd	3	11.5	20.9	0.1	14
	3000	5.1	48.6	0.4	27
6th/5th Street	72	0.7	5.7	0.0	19
Country Club Rd	4	20.9	36.7	0.1	11
	9922	1.9	13.9	0.1	25
	4000	2.7	31.6	0.2	28
Dodge Blvd	5	11.5	57.6	0.4	25
	5000	1.9	16.9	0.1	28
Alvernon Wy	6	25.1	39.4	0.1	11
	6000	2.9	18.2	0.1	25
	70	4.1	17.2	0.1	22
	7	0.5	2.6	0.0	27
Columbus Blvd	7000	1.5	17.0	0.1	28
	8	11.8	25.0	0.1	16
	8000	3.1	33.5	0.3	27
Swan Rd	9	38.0	66.8	0.3	13
	9000	3.6	18.9	0.1	24
	1050	5.2	18.4	0.1	21
	10	0.9	2.9	0.0	24
	100	1.4	16.4	0.1	28
Rosemont Blvd	11	14.5	28.6	0.1	15
	110	3.6	33.6	0.3	27
Craycroft Rd	12	37.9	66.3	0.2	13
	120	3.9	31.1	0.2	26
	13	2.5	27.7	0.2	28
Chantilly Dr HAWK	130	1.5	4.0	0.0	17
Sahuara Ave	14	0.7	7.8	0.1	28
	140	1.6	31.5	0.3	29
Wilmot Rd	15	24.7	47.3	0.3	19
Total		300.7	959.7	5.6	21

Arterial Level of Service
Scenario B

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Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	31.4	131.3	0.4	12
	140	0.9	29.7	0.3	30
Sahuara Ave	14	1.2	31.1	0.3	29
	130	2.4	9.9	0.1	22
	13	0.5	2.4	0.0	28
	120	1.1	26.6	0.2	29
Craycroft Rd	12	27.4	53.2	0.2	15
	110	3.1	32.9	0.2	27
Rosemont Blvd	11	14.9	44.5	0.3	20
	100	2.6	17.0	0.1	25
	10	2.6	17.6	0.1	26
Arcadia Ave HAWK	1050	2.1	4.7	0.0	15
	9000	0.8	13.7	0.1	28
Swan Rd	9	34.8	48.7	0.1	9
	8000	3.7	33.7	0.3	27
Columbus Blvd	8	12.0	51.1	0.3	23
	7000	2.2	15.9	0.1	26
	7	1.9	17.4	0.1	27
Irving Ave HAWK	70	1.7	4.3	0.0	16
	6000	1.2	13.8	0.1	28
Alvernon Wy	6	29.1	43.4	0.1	10
	5000	2.4	17.5	0.1	26
Dodge Blvd	5	12.0	26.8	0.1	17
	4000	5.3	51.3	0.4	28
	9922	1.8	31.1	0.2	28
Country Club Rd	4	24.2	35.0	0.1	10
6th/5th Street	72	4.7	20.8	0.1	20
	3000	0.3	9.9	0.0	11
Tucson Blvd	3	10.4	54.1	0.4	24
	2000	1.6	11.2	0.1	25
Wilson HAWK	20	3.1	11.2	0.1	21
	2	0.5	2.5	0.0	27
	1000	1.8	22.1	0.2	28
Campbell Ave	1	24.4	43.4	0.2	14
Total		270.2	979.8	5.5	20

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	200	528	527	200	216	229	263	149	236	230	191	98
Average Queue (ft)	181	309	316	129	114	116	140	63	161	159	117	34
95th Queue (ft)	240	478	480	253	191	202	230	122	217	216	199	76
Link Distance (ft)		2986	2986			780	780		2020	2020	2020	
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	100			50	180			290				150
Storage Blk Time (%)	40	44	60	10	2	2			0		2	
Queuing Penalty (veh)	178	116	98	46	5	3			0		2	

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	342	419	409	405
Average Queue (ft)	163	272	277	267
95th Queue (ft)	301	377	376	373
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	4	25		
Queuing Penalty (veh)	15	52		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	239	260	62	32	35	33
Average Queue (ft)	49	52	11	1	7	11
95th Queue (ft)	167	178	46	14	29	35
Link Distance (ft)	843	843	55	55	433	438
Upstream Blk Time (%)			1	0		
Queuing Penalty (veh)			5	0		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario B

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Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	188	314	331	106	214	224	134	391	225	386
Average Queue (ft)	62	123	139	41	88	102	41	200	76	203
95th Queue (ft)	133	263	282	84	168	182	100	326	164	336
Link Distance (ft)		356	356		1877	1877		1935		2179
Upstream Blk Time (%)		0	0							
Queuing Penalty (veh)		1	2							
Storage Bay Dist (ft)	100			100			300		300	
Storage Blk Time (%)	4	12		1	5			2		2
Queuing Penalty (veh)	18	13		5	3			1		2

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	NB	NB	NB	NB	SB
Directions Served	L	T	T	L	T	T	R	L	T	T	R	L
Maximum Queue (ft)	214	273	290	128	240	266	196	168	202	199	156	174
Average Queue (ft)	90	149	164	41	132	144	12	88	134	135	27	115
95th Queue (ft)	165	248	263	91	206	219	93	161	198	204	108	190
Link Distance (ft)		488	488		423	423			172	172		
Upstream Blk Time (%)								0	4	5	0	
Queuing Penalty (veh)								0	16	18	0	
Storage Bay Dist (ft)	180			90			160	100			70	100
Storage Blk Time (%)	1	6		2	20	6	0	10	19	31	0	16
Queuing Penalty (veh)	3	11		6	11	12	0	26	29	14	0	36

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	TR
Maximum Queue (ft)	298	295
Average Queue (ft)	159	167
95th Queue (ft)	260	261
Link Distance (ft)	2000	2000
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)	23	
Queuing Penalty (veh)	41	

Queuing and Blocking Report
Scenario B

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Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	WB	NB	NB
Directions Served	T	TR	L	T	T	L	R
Maximum Queue (ft)	266	298	168	231	244	227	113
Average Queue (ft)	105	128	77	127	146	120	55
95th Queue (ft)	213	243	136	211	226	199	93
Link Distance (ft)	2071	2071		619	619	2122	
Upstream Blk Time (%)							
Queuing Penalty (veh)							
Storage Bay Dist (ft)			300				220
Storage Blk Time (%)						1	
Queuing Penalty (veh)						1	

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	TR	L	T	TR
Maximum Queue (ft)	199	343	339	174	282	294	250	468	466	250	420	416
Average Queue (ft)	94	158	179	75	153	169	128	276	283	120	267	273
95th Queue (ft)	185	279	290	153	252	263	259	413	415	254	387	385
Link Distance (ft)		581	581		578	578		2106	2106		1977	1977
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	100			75			150			150		
Storage Blk Time (%)	8	23		12	36		3	32		1	30	
Queuing Penalty (veh)	30	36		33	35		13	38		3	31	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	49	15	129	130	40	32
Average Queue (ft)	6	1	19	19	13	10
95th Queue (ft)	32	10	77	78	39	32
Link Distance (ft)	56	56	627	627	304	320
Upstream Blk Time (%)	0	0				
Queuing Penalty (veh)	2	0				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario B

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Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	153	243	266	118	212	219	164	310	166	339
Average Queue (ft)	35	114	132	38	86	99	53	124	45	162
95th Queue (ft)	92	207	229	86	173	181	123	233	123	284
Link Distance (ft)		533	533		1685	1685		2072		1936
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			75			100		100	
Storage Blk Time (%)	1	14		3	8		3	14	0	24
Queuing Penalty (veh)	3	7		8	4		9	8	1	13

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	200	328	367	196	276	283	238	317	319	88	187	320
Average Queue (ft)	82	199	224	74	144	165	78	185	183	32	78	195
95th Queue (ft)	185	304	332	150	236	254	171	284	282	70	150	289
Link Distance (ft)		1226	1226		560	560		1988	1988			2060
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	100			100			140			350	200	
Storage Blk Time (%)	2	34		5	22		1	17	0		0	7
Queuing Penalty (veh)	8	33		11	20		5	17	0		0	8

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	333	103
Average Queue (ft)	194	27
95th Queue (ft)	295	74
Link Distance (ft)	2060	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	1	
Queuing Penalty (veh)	1	

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	69	45	134	135	95	66
Average Queue (ft)	17	4	37	24	35	23
95th Queue (ft)	56	26	100	85	70	54
Link Distance (ft)	52	52	616	616	261	294
Upstream Blk Time (%)	1	0				
Queuing Penalty (veh)	7	1				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	143	246	259	83	200	218	103	200	71	184
Average Queue (ft)	42	132	149	32	106	125	44	100	26	101
95th Queue (ft)	101	216	238	70	181	201	85	168	61	166
Link Distance (ft)		570	570		1260	1260		1970		2041
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			100			150		150	
Storage Blk Time (%)	2	16		1	7		0	1		1
Queuing Penalty (veh)	8	9		2	3		0	1		0

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	174	308	331	164	203	237	161	323	333	212	238	348
Average Queue (ft)	90	163	197	71	93	119	64	204	202	57	62	208
95th Queue (ft)	171	260	297	134	170	197	124	304	307	178	143	312
Link Distance (ft)		1212	1212		1092	1092		1951	1951			2065
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	75			150			180			180	150	
Storage Blk Time (%)	15	34		1	2		0	12	12		0	17
Queuing Penalty (veh)	38	39		2	2		0	11	10		1	13

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	351	177
Average Queue (ft)	207	36
95th Queue (ft)	317	128
Link Distance (ft)	2065	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	17	
Queuing Penalty (veh)	12	

Intersection: 13: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	113	122	58	13	74	25
Average Queue (ft)	12	17	9	0	37	2
95th Queue (ft)	61	74	39	7	62	13
Link Distance (ft)	1086	1086	56	56	422	473
Upstream Blk Time (%)			1	0		
Queuing Penalty (veh)			2	0		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	47	9	57	5	33	45
Average Queue (ft)	5	0	5	0	7	17
95th Queue (ft)	26	6	30	5	28	42
Link Distance (ft)	275	275	1271	1271	614	699
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	184	201	167	107	273	295	292	280	169	305	298	273
Average Queue (ft)	106	130	80	40	129	171	167	136	33	188	183	146
95th Queue (ft)	168	192	139	84	224	265	263	243	99	275	271	250
Link Distance (ft)		1219	1219	2255		1997	1997	1997		2032	2032	2032
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	150				240				130			
Storage Blk Time (%)	3	9			1	1				20		0
Queuing Penalty (veh)	11	14			4	3				8		0

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	68
Average Queue (ft)	34
95th Queue (ft)	61
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	74	73	199	202
Average Queue (ft)	28	29	43	48
95th Queue (ft)	74	75	135	149
Link Distance (ft)	55	55	298	298
Upstream Blk Time (%)	5	6		
Queuing Penalty (veh)	30	34		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 40: Country Club Rd

Movement	EB	NB	NB
Directions Served	R	T	T
Maximum Queue (ft)	115	146	134
Average Queue (ft)	51	16	14
95th Queue (ft)	88	76	70
Link Distance (ft)	451	2094	2094
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	232	253	75	79
Average Queue (ft)	51	59	26	29
95th Queue (ft)	165	190	72	78
Link Distance (ft)	513	513	56	56
Upstream Blk Time (%)			3	4
Queuing Penalty (veh)			13	16
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

Scenario B

11/03/2022

Intersection: 72: 6th/5th Street

Movement	EB	SW	SW
Directions Served	L	R	R
Maximum Queue (ft)	6	26	32
Average Queue (ft)	0	1	1
95th Queue (ft)	5	16	16
Link Distance (ft)	100	488	488
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 100: 6th/5th Street

Movement	WB	SB
Directions Served	TR	LTR
Maximum Queue (ft)	4	71
Average Queue (ft)	0	35
95th Queue (ft)	4	60
Link Distance (ft)	570	185
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 110: 6th/5th Street

Movement	EB	EB	SB
Directions Served	T	TR	LTR
Maximum Queue (ft)	3	8	56
Average Queue (ft)	0	0	22
95th Queue (ft)	3	6	49
Link Distance (ft)	1260	1260	276
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Queuing and Blocking Report

Scenario B

11/03/2022

Intersection: 120: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	4	4	114	70
Average Queue (ft)	0	0	45	31
95th Queue (ft)	4	4	83	55
Link Distance (ft)	1092	1092	211	276
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	67	71	101	100
Average Queue (ft)	21	26	22	26
95th Queue (ft)	62	71	71	77
Link Distance (ft)	56	56	275	275
Upstream Blk Time (%)	3	4		
Queuing Penalty (veh)	9	15		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 140: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	42	76
Average Queue (ft)	18	35
95th Queue (ft)	44	64
Link Distance (ft)	305	274
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 1000: 6th/5th Street

Movement	EB	WB	WB	SB
Directions Served	T	T	TR	LTR
Maximum Queue (ft)	15	14	10	53
Average Queue (ft)	0	0	0	26
95th Queue (ft)	8	8	6	50
Link Distance (ft)	780	843	843	416
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1050: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	258	293	62	75
Average Queue (ft)	51	60	27	31
95th Queue (ft)	165	190	68	77
Link Distance (ft)	526	526	52	52
Upstream Blk Time (%)			5	6
Queuing Penalty (veh)			16	21
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 2000: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	T	LTR	LTR
Maximum Queue (ft)	11	31	56	49
Average Queue (ft)	0	1	21	16
95th Queue (ft)	9	18	48	40
Link Distance (ft)	298	298	419	408
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 3000: 6th/5th Street

Movement	EB	EB	SB
Directions Served	T	TR	LTR
Maximum Queue (ft)	4	4	77
Average Queue (ft)	0	0	32
95th Queue (ft)	4	4	64
Link Distance (ft)	1877	1877	430
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 4000: 6th/5th Street

Movement	B9922	B9922	WB	WB	NB
Directions Served	T	T	T	TR	LTR
Maximum Queue (ft)	6	4	6	8	67
Average Queue (ft)	0	0	0	0	32
95th Queue (ft)	5	4	6	6	58
Link Distance (ft)	423	423	2071	2071	547
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 5000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	78	30
Average Queue (ft)	35	6
95th Queue (ft)	69	26
Link Distance (ft)	310	280
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario B

11/03/2022

Intersection: 6000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	4	6	70	63
Average Queue (ft)	0	0	29	28
95th Queue (ft)	4	5	58	51
Link Distance (ft)	578	513	355	360
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 7000: 6th/5th Street

Movement	EB	WB	SB
Directions Served	T	T	LTR
Maximum Queue (ft)	4	9	63
Average Queue (ft)	0	1	28
95th Queue (ft)	4	8	53
Link Distance (ft)	627	533	304
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 8000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	30	33
Average Queue (ft)	9	16
95th Queue (ft)	31	40
Link Distance (ft)	319	333
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario B

11/03/2022

Intersection: 9000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	T	LTR	LTR
Maximum Queue (ft)	13	4	50	70
Average Queue (ft)	0	0	21	26
95th Queue (ft)	13	4	47	57
Link Distance (ft)	560	526	204	310
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 1522

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	4:45	4:45	4:45	4:45	4:45	4:45	4:45
End Time	6:00	6:00	6:00	6:00	6:00	6:00	6:00
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	20527	20200	20460	20255	20176	20345	20270
Vehs Exited	20508	20112	20369	20237	20054	20310	19979
Starting Vehs	1001	949	997	1021	999	960	970
Ending Vehs	1020	1037	1088	1039	1121	995	1261
Denied Entry After	0	3	2	2	2	5	2
Travel Distance (mi)	23770	23196	23653	23277	23149	23501	23296
Travel Time (hr)	1070.3	1021.0	1056.3	1029.1	1020.9	1040.1	1041.1
Total Delay (hr)	325.9	294.7	317.0	300.9	297.3	304.3	311.3
Total Stops	25428	23585	24891	24049	23726	24102	24519
Fuel Used (gal)	811.9	785.2	805.5	790.1	785.6	798.7	792.1

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	4:45	4:45	4:45	4:45
End Time	6:00	6:00	6:00	6:00
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	20309	20271	20447	20327
Vehs Exited	20177	20258	20328	20237
Starting Vehs	905	958	974	965
Ending Vehs	1037	971	1093	1052
Denied Entry After	2	0	0	0
Travel Distance (mi)	23306	23483	23580	23421
Travel Time (hr)	1035.1	1040.6	1053.2	1040.8
Total Delay (hr)	306.3	306.4	313.4	307.7
Total Stops	24263	24318	24708	24355
Fuel Used (gal)	792.8	797.5	803.4	796.3

Interval #0 Information Seeding

Start Time	4:45
End Time	5:00
Total Time (min)	15

No data recorded this interval.

Interval #1 Information Recording

Start Time	5:00
End Time	6:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	20527	20200	20460	20255	20176	20345	20270
Vehs Exited	20508	20112	20369	20237	20054	20310	19979
Starting Vehs	1001	949	997	1021	999	960	970
Ending Vehs	1020	1037	1088	1039	1121	995	1261
Denied Entry After	0	3	2	2	2	5	2
Travel Distance (mi)	23770	23196	23653	23277	23149	23501	23296
Travel Time (hr)	1070.3	1021.0	1056.3	1029.1	1020.9	1040.1	1041.1
Total Delay (hr)	325.9	294.7	317.0	300.9	297.3	304.3	311.3
Total Stops	25428	23585	24891	24049	23726	24102	24519
Fuel Used (gal)	811.9	785.2	805.5	790.1	785.6	798.7	792.1

Interval #1 Information Recording

Start Time	5:00
End Time	6:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	20309	20271	20447	20327
Vehs Exited	20177	20258	20328	20237
Starting Vehs	905	958	974	965
Ending Vehs	1037	971	1093	1052
Denied Entry After	2	0	0	0
Travel Distance (mi)	23306	23483	23580	23421
Travel Time (hr)	1035.1	1040.6	1053.2	1040.8
Total Delay (hr)	306.3	306.4	313.4	307.7
Total Stops	24263	24318	24708	24355
Fuel Used (gal)	792.8	797.5	803.4	796.3

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Entry After	0	0	0	0	0	0	0

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

7: 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

10: 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

40: Country Club Rd Performance by movement

Movement	EBR	NBT	SBT	All
Denied Entry After	0	0	0	0

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWR	All
Denied Entry After	0	0	0	0

100: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	SBR	All
Denied Entry After	0	0	0	0	0	0

110: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBR	All
Denied Entry After	0	0	0	0	0

120: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0	0

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

140: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	SBR	All
Denied Entry After	0	0	0	0	0	0

1050: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

2000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Entry After	0	0	0	0	0

3000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	All
Denied Entry After	0	0	0	0	0	0

5000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0

6000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0

7000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	SBR	All
Denied Entry After	0	0	0	0	0	0

8000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Entry After	0	0	0	0	0

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0

Total Network Performance

Denied Entry After	0
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Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	43.2	110.3	0.6	19
	1000	5.4	26.2	0.2	23
	2	4.3	24.5	0.2	25
Wilson HAWK	20	1.4	3.9	0.0	17
	2000	1.1	8.6	0.1	27
Tucson Blvd	3	11.5	20.9	0.1	14
	3000	5.1	48.6	0.4	27
6th/5th Street	72	0.7	5.7	0.0	19
Country Club Rd	4	20.9	36.7	0.1	11
	9922	1.9	13.9	0.1	25
	4000	2.7	31.6	0.2	28
Dodge Blvd	5	11.5	57.6	0.4	25
	5000	1.9	16.9	0.1	28
Alvernon Wy	6	25.1	39.4	0.1	11
	6000	2.9	18.2	0.1	25
	70	4.1	17.2	0.1	22
	7	0.5	2.6	0.0	27
Columbus Blvd	7000	1.5	17.0	0.1	28
	8	11.8	25.0	0.1	16
	8000	3.1	33.5	0.3	27
Swan Rd	9	38.0	66.8	0.3	13
	9000	3.6	18.9	0.1	24
	1050	5.2	18.4	0.1	21
	10	0.9	2.9	0.0	24
Rosemont Blvd	100	1.4	16.4	0.1	28
	11	14.5	28.6	0.1	15
	110	3.6	33.6	0.3	27
Craycroft Rd	12	37.9	66.3	0.2	13
	120	3.9	31.1	0.2	26
	13	2.5	27.7	0.2	28
Chantilly Dr HAWK	130	1.5	4.0	0.0	17
Sahuara Ave	14	0.7	7.8	0.1	28
	140	1.6	31.5	0.3	29
Wilmot Rd	15	24.7	47.3	0.3	19
Total		300.7	959.7	5.6	21

Arterial Level of Service
Scenario B

11/07/2022

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	31.4	131.3	0.4	12
	140	0.9	29.7	0.3	30
Sahuara Ave	14	1.2	31.1	0.3	29
	130	2.4	9.9	0.1	22
	13	0.5	2.4	0.0	28
	120	1.1	26.6	0.2	29
Craycroft Rd	12	27.4	53.2	0.2	15
	110	3.1	32.9	0.2	27
Rosemont Blvd	11	14.9	44.5	0.3	20
	100	2.6	17.0	0.1	25
	10	2.6	17.6	0.1	26
Arcadia Ave HAWK	1050	2.1	4.7	0.0	15
	9000	0.8	13.7	0.1	28
Swan Rd	9	34.8	48.7	0.1	9
	8000	3.7	33.7	0.3	27
Columbus Blvd	8	12.0	51.1	0.3	23
	7000	2.2	15.9	0.1	26
	7	1.9	17.4	0.1	27
Irving Ave HAWK	70	1.7	4.3	0.0	16
	6000	1.2	13.8	0.1	28
Alvernon Wy	6	29.1	43.4	0.1	10
	5000	2.4	17.5	0.1	26
Dodge Blvd	5	12.0	26.8	0.1	17
	4000	5.3	51.3	0.4	28
	9922	1.8	31.1	0.2	28
Country Club Rd	4	24.2	35.0	0.1	10
6th/5th Street	72	4.7	20.8	0.1	20
	3000	0.3	9.9	0.0	11
Tucson Blvd	3	10.4	54.1	0.4	24
	2000	1.6	11.2	0.1	25
Wilson HAWK	20	3.1	11.2	0.1	21
	2	0.5	2.5	0.0	27
	1000	1.8	22.1	0.2	28
Campbell Ave	1	24.4	43.4	0.2	14
Total		270.2	979.8	5.5	20

Queuing and Blocking Report
Scenario B

11/07/2022

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	200	528	527	200	216	229	263	149	236	230	191	98
Average Queue (ft)	181	309	316	129	114	116	140	63	161	159	117	34
95th Queue (ft)	240	478	480	253	191	202	230	122	217	216	199	76
Link Distance (ft)		2986	2986			780	780		2020	2020	2020	
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	100			50	180			290				150
Storage Blk Time (%)	40	44	60	10	2	2			0		2	
Queuing Penalty (veh)	178	116	98	46	5	3			0		2	

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	342	419	409	405
Average Queue (ft)	163	272	277	267
95th Queue (ft)	301	377	376	373
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	4	25		
Queuing Penalty (veh)	15	52		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	239	260	62	32	35	33
Average Queue (ft)	49	52	11	1	7	11
95th Queue (ft)	167	178	46	14	29	35
Link Distance (ft)	843	843	55	55	433	438
Upstream Blk Time (%)			1	0		
Queuing Penalty (veh)			5	0		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario B

11/07/2022

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	188	314	331	106	214	224	134	391	225	386
Average Queue (ft)	62	123	139	41	88	102	41	200	76	203
95th Queue (ft)	133	263	282	84	168	182	100	326	164	336
Link Distance (ft)		356	356		1877	1877		1935		2179
Upstream Blk Time (%)		0	0							
Queuing Penalty (veh)		1	2							
Storage Bay Dist (ft)	100			100			300		300	
Storage Blk Time (%)	4	12		1	5			2		2
Queuing Penalty (veh)	18	13		5	3			1		2

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	NB	NB	NB	NB	SB
Directions Served	L	T	T	L	T	T	R	L	T	T	R	L
Maximum Queue (ft)	214	273	290	128	240	266	196	168	202	199	156	174
Average Queue (ft)	90	149	164	41	132	144	12	88	134	135	27	115
95th Queue (ft)	165	248	263	91	206	219	93	161	198	204	108	190
Link Distance (ft)		488	488		423	423			172	172		
Upstream Blk Time (%)								0	4	5	0	
Queuing Penalty (veh)								0	16	18	0	
Storage Bay Dist (ft)	180			90			160	100			70	100
Storage Blk Time (%)	1	6		2	20	6	0	10	19	31	0	16
Queuing Penalty (veh)	3	11		6	11	12	0	26	29	14	0	36

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	TR
Maximum Queue (ft)	298	295
Average Queue (ft)	159	167
95th Queue (ft)	260	261
Link Distance (ft)	2000	2000
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)	23	
Queuing Penalty (veh)	41	

Queuing and Blocking Report
Scenario B

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Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	WB	NB	NB
Directions Served	T	TR	L	T	T	L	R
Maximum Queue (ft)	266	298	168	231	244	227	113
Average Queue (ft)	105	128	77	127	146	120	55
95th Queue (ft)	213	243	136	211	226	199	93
Link Distance (ft)	2071	2071		619	619	2122	
Upstream Blk Time (%)							
Queuing Penalty (veh)							
Storage Bay Dist (ft)			300				220
Storage Blk Time (%)						1	
Queuing Penalty (veh)						1	

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	TR	L	T	TR
Maximum Queue (ft)	199	343	339	174	282	294	250	468	466	250	420	416
Average Queue (ft)	94	158	179	75	153	169	128	276	283	120	267	273
95th Queue (ft)	185	279	290	153	252	263	259	413	415	254	387	385
Link Distance (ft)		581	581		578	578		2106	2106		1977	1977
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	100			75			150			150		
Storage Blk Time (%)	8	23		12	36		3	32		1	30	
Queuing Penalty (veh)	30	36		33	35		13	38		3	31	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	49	15	129	130	40	32
Average Queue (ft)	6	1	19	19	13	10
95th Queue (ft)	32	10	77	78	39	32
Link Distance (ft)	56	56	627	627	304	320
Upstream Blk Time (%)	0	0				
Queuing Penalty (veh)	2	0				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario B

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Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	153	243	266	118	212	219	164	310	166	339
Average Queue (ft)	35	114	132	38	86	99	53	124	45	162
95th Queue (ft)	92	207	229	86	173	181	123	233	123	284
Link Distance (ft)		533	533		1685	1685		2072		1936
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			75			100		100	
Storage Blk Time (%)	1	14		3	8		3	14	0	24
Queuing Penalty (veh)	3	7		8	4		9	8	1	13

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	200	328	367	196	276	283	238	317	319	88	187	320
Average Queue (ft)	82	199	224	74	144	165	78	185	183	32	78	195
95th Queue (ft)	185	304	332	150	236	254	171	284	282	70	150	289
Link Distance (ft)		1226	1226		560	560		1988	1988			2060
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	100			100			140			350	200	
Storage Blk Time (%)	2	34		5	22		1	17	0		0	7
Queuing Penalty (veh)	8	33		11	20		5	17	0		0	8

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	333	103
Average Queue (ft)	194	27
95th Queue (ft)	295	74
Link Distance (ft)	2060	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	1	
Queuing Penalty (veh)	1	

Queuing and Blocking Report
Scenario B

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Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	69	45	134	135	95	66
Average Queue (ft)	17	4	37	24	35	23
95th Queue (ft)	56	26	100	85	70	54
Link Distance (ft)	52	52	616	616	261	294
Upstream Blk Time (%)	1	0				
Queuing Penalty (veh)	7	1				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	143	246	259	83	200	218	103	200	71	184
Average Queue (ft)	42	132	149	32	106	125	44	100	26	101
95th Queue (ft)	101	216	238	70	181	201	85	168	61	166
Link Distance (ft)		570	570		1260	1260		1970		2041
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			100			150		150	
Storage Blk Time (%)	2	16		1	7		0	1		1
Queuing Penalty (veh)	8	9		2	3		0	1		0

Queuing and Blocking Report
Scenario B

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Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	174	308	331	164	203	237	161	323	333	212	238	348
Average Queue (ft)	90	163	197	71	93	119	64	204	202	57	62	208
95th Queue (ft)	171	260	297	134	170	197	124	304	307	178	143	312
Link Distance (ft)		1212	1212		1092	1092		1951	1951			2065
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	75			150			180			180	150	
Storage Blk Time (%)	15	34		1	2		0	12	12		0	17
Queuing Penalty (veh)	38	39		2	2		0	11	10		1	13

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	351	177
Average Queue (ft)	207	36
95th Queue (ft)	317	128
Link Distance (ft)	2065	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	17	
Queuing Penalty (veh)	12	

Intersection: 13: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	113	122	58	13	74	25
Average Queue (ft)	12	17	9	0	37	2
95th Queue (ft)	61	74	39	7	62	13
Link Distance (ft)	1086	1086	56	56	422	473
Upstream Blk Time (%)			1	0		
Queuing Penalty (veh)			2	0		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
Scenario B

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Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	47	9	57	5	33	45
Average Queue (ft)	5	0	5	0	7	17
95th Queue (ft)	26	6	30	5	28	42
Link Distance (ft)	275	275	1271	1271	614	699
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	184	201	167	107	273	295	292	280	169	305	298	273
Average Queue (ft)	106	130	80	40	129	171	167	136	33	188	183	146
95th Queue (ft)	168	192	139	84	224	265	263	243	99	275	271	250
Link Distance (ft)		1219	1219	2255		1997	1997	1997		2032	2032	2032
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	150				240				130			
Storage Blk Time (%)	3	9			1	1				20		0
Queuing Penalty (veh)	11	14			4	3				8		0

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	68
Average Queue (ft)	34
95th Queue (ft)	61
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Queuing and Blocking Report
Scenario B

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Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	74	73	199	202
Average Queue (ft)	28	29	43	48
95th Queue (ft)	74	75	135	149
Link Distance (ft)	55	55	298	298
Upstream Blk Time (%)	5	6		
Queuing Penalty (veh)	30	34		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 40: Country Club Rd

Movement	EB	NB	NB
Directions Served	R	T	T
Maximum Queue (ft)	115	146	134
Average Queue (ft)	51	16	14
95th Queue (ft)	88	76	70
Link Distance (ft)	451	2094	2094
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	232	253	75	79
Average Queue (ft)	51	59	26	29
95th Queue (ft)	165	190	72	78
Link Distance (ft)	513	513	56	56
Upstream Blk Time (%)			3	4
Queuing Penalty (veh)			13	16
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

Scenario B

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Intersection: 72: 6th/5th Street

Movement	EB	SW	SW
Directions Served	L	R	R
Maximum Queue (ft)	6	26	32
Average Queue (ft)	0	1	1
95th Queue (ft)	5	16	16
Link Distance (ft)	100	488	488
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 100: 6th/5th Street

Movement	WB	SB
Directions Served	TR	LTR
Maximum Queue (ft)	4	71
Average Queue (ft)	0	35
95th Queue (ft)	4	60
Link Distance (ft)	570	185
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 110: 6th/5th Street

Movement	EB	EB	SB
Directions Served	T	TR	LTR
Maximum Queue (ft)	3	8	56
Average Queue (ft)	0	0	22
95th Queue (ft)	3	6	49
Link Distance (ft)	1260	1260	276
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Queuing and Blocking Report
Scenario B

11/07/2022

Intersection: 120: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	4	4	114	70
Average Queue (ft)	0	0	45	31
95th Queue (ft)	4	4	83	55
Link Distance (ft)	1092	1092	211	276
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	67	71	101	100
Average Queue (ft)	21	26	22	26
95th Queue (ft)	62	71	71	77
Link Distance (ft)	56	56	275	275
Upstream Blk Time (%)	3	4		
Queuing Penalty (veh)	9	15		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 140: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	42	76
Average Queue (ft)	18	35
95th Queue (ft)	44	64
Link Distance (ft)	305	274
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario B

11/07/2022

Intersection: 1000: 6th/5th Street

Movement	EB	WB	WB	SB
Directions Served	T	T	TR	LTR
Maximum Queue (ft)	15	14	10	53
Average Queue (ft)	0	0	0	26
95th Queue (ft)	8	8	6	50
Link Distance (ft)	780	843	843	416
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1050: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	258	293	62	75
Average Queue (ft)	51	60	27	31
95th Queue (ft)	165	190	68	77
Link Distance (ft)	526	526	52	52
Upstream Blk Time (%)			5	6
Queuing Penalty (veh)			16	21
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 2000: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	T	LTR	LTR
Maximum Queue (ft)	11	31	56	49
Average Queue (ft)	0	1	21	16
95th Queue (ft)	9	18	48	40
Link Distance (ft)	298	298	419	408
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

Scenario B

11/07/2022

Intersection: 3000: 6th/5th Street

Movement	EB	EB	SB
Directions Served	T	TR	LTR
Maximum Queue (ft)	4	4	77
Average Queue (ft)	0	0	32
95th Queue (ft)	4	4	64
Link Distance (ft)	1877	1877	430
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 4000: 6th/5th Street

Movement	B9922	B9922	WB	WB	NB
Directions Served	T	T	T	TR	LTR
Maximum Queue (ft)	6	4	6	8	67
Average Queue (ft)	0	0	0	0	32
95th Queue (ft)	5	4	6	6	58
Link Distance (ft)	423	423	2071	2071	547
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 5000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	78	30
Average Queue (ft)	35	6
95th Queue (ft)	69	26
Link Distance (ft)	310	280
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario B

11/07/2022

Intersection: 6000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	4	6	70	63
Average Queue (ft)	0	0	29	28
95th Queue (ft)	4	5	58	51
Link Distance (ft)	578	513	355	360
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 7000: 6th/5th Street

Movement	EB	WB	SB
Directions Served	T	T	LTR
Maximum Queue (ft)	4	9	63
Average Queue (ft)	0	1	28
95th Queue (ft)	4	8	53
Link Distance (ft)	627	533	304
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 8000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	30	33
Average Queue (ft)	9	16
95th Queue (ft)	31	40
Link Distance (ft)	319	333
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario B

11/07/2022

Intersection: 9000: 6th/5th Street

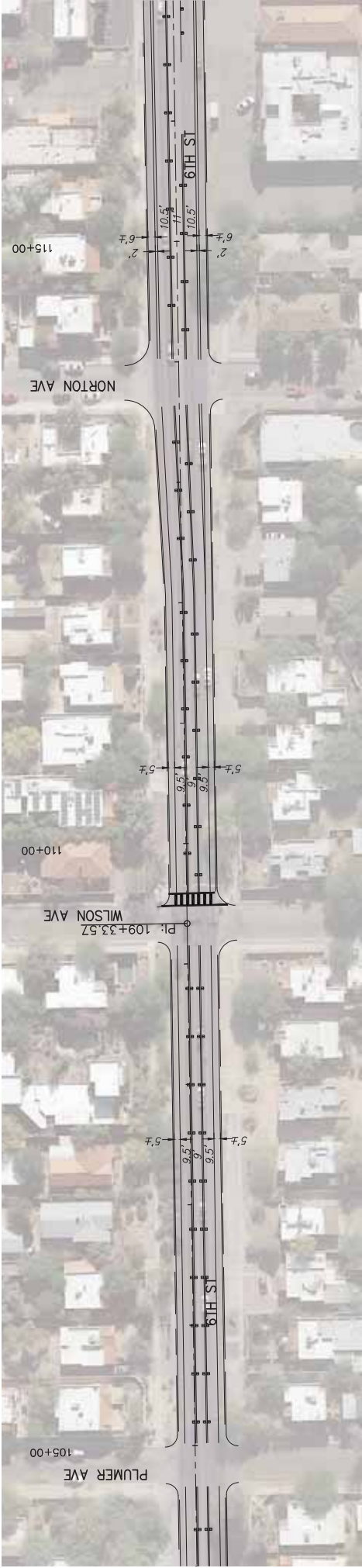
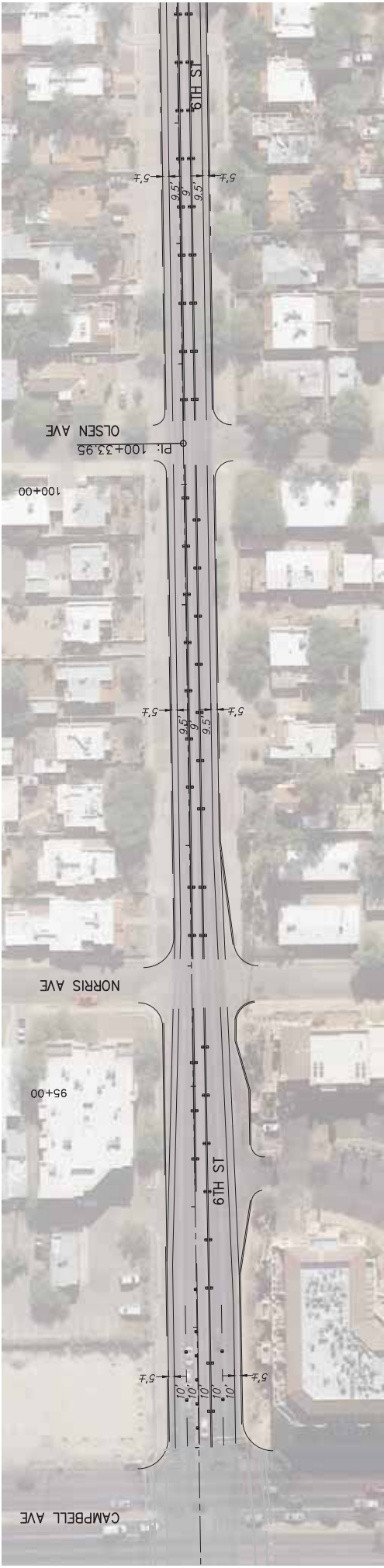
Movement	EB	WB	NB	SB
Directions Served	TR	T	LTR	LTR
Maximum Queue (ft)	13	4	50	70
Average Queue (ft)	0	0	21	26
95th Queue (ft)	13	4	47	57
Link Distance (ft)	560	526	204	310
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 1522

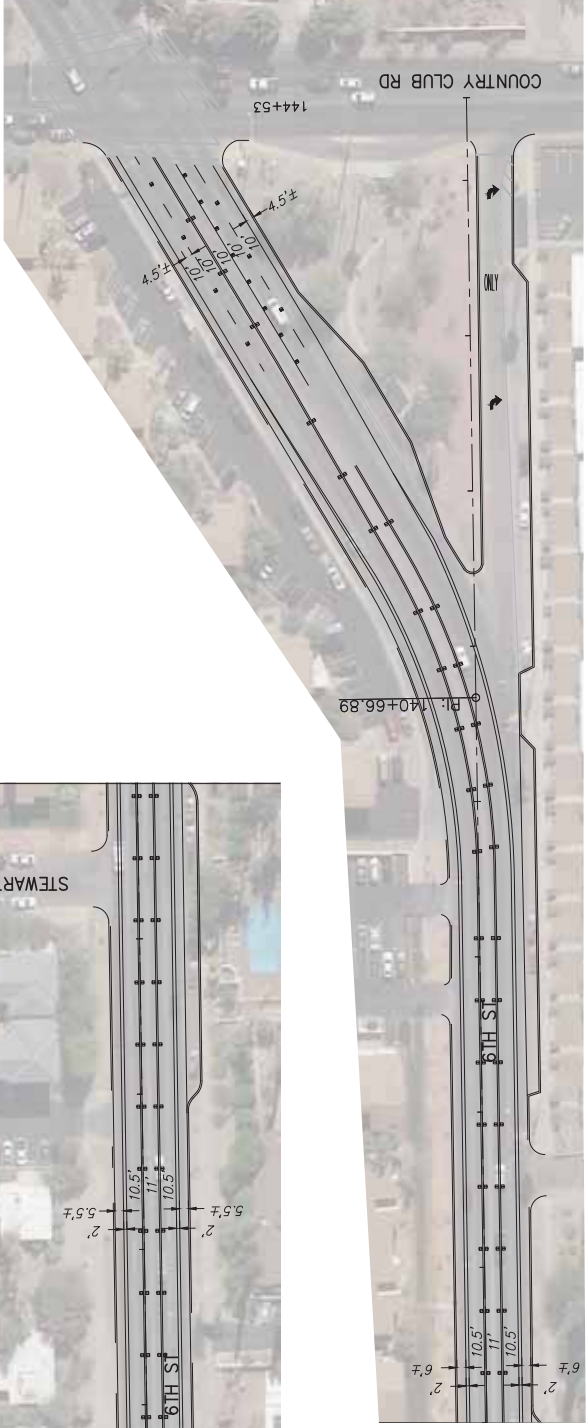
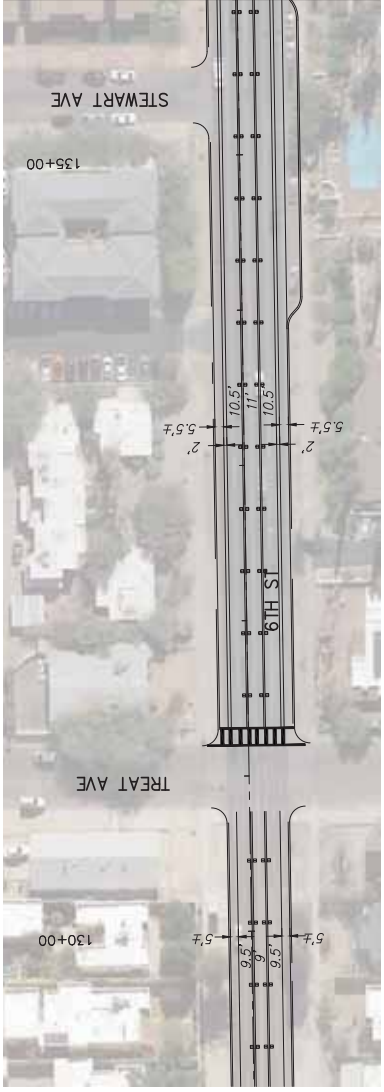


Appendix H
Psomas 6th Street
Conceptual Design



ALTERNATIVE 3
 CONCEPTUAL PLANS
 SHEET 1 OF 2
 OCTOBER 2016

PSOMAS
 333 E. Washington Blvd., Suite 450
 Tucson, AZ 85705
 (520) 292-2300 (520) 292-1280 fax
 www.psomas.com



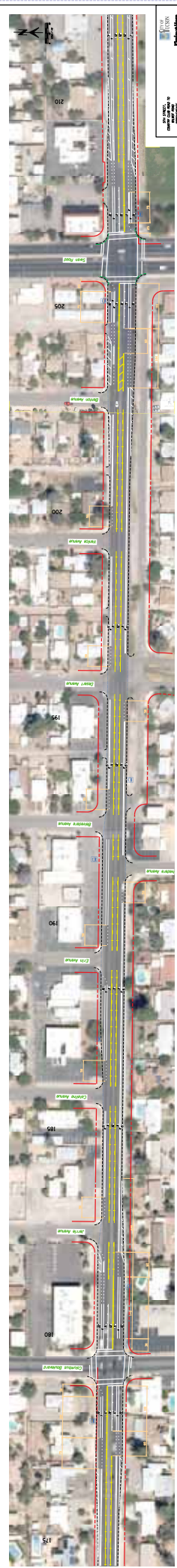
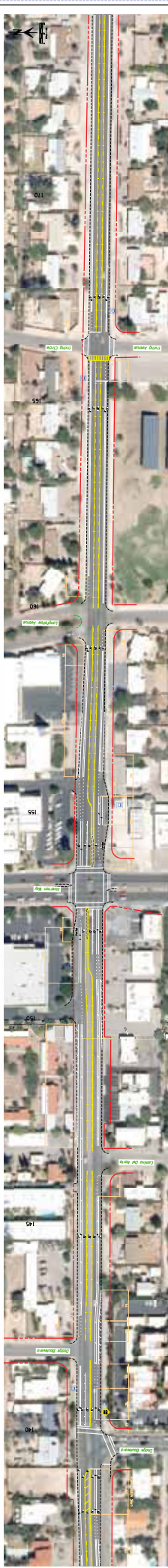
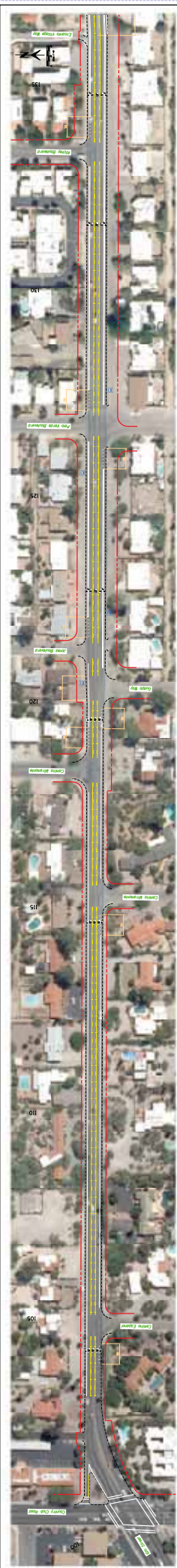
ALTERNATIVE 3
 CONCEPTUAL PLANS
 SHEET 2 OF 2
 OCTOBER 2016

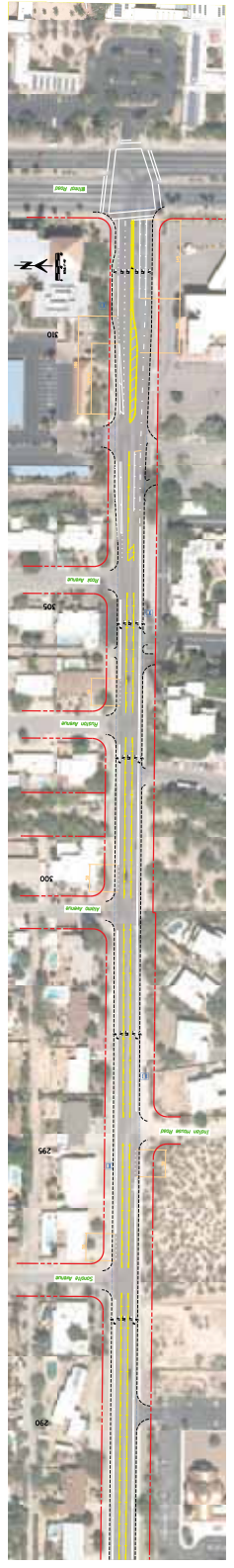
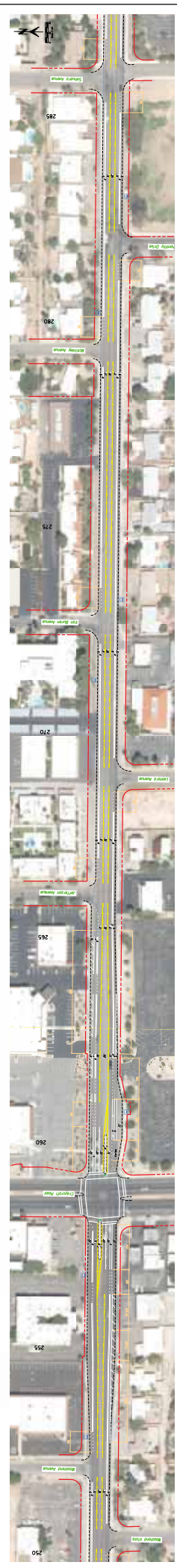
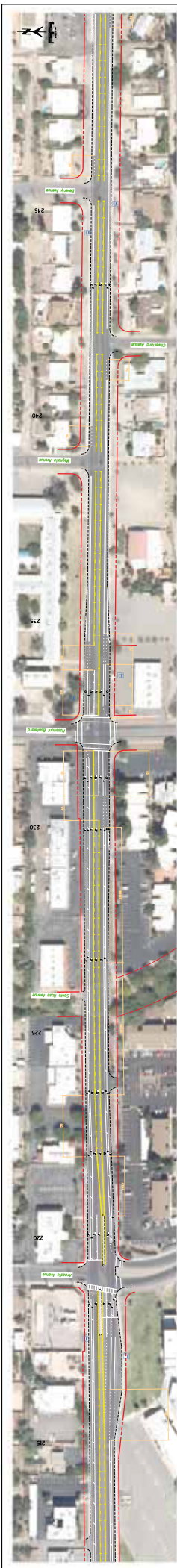
PSOMAS

151 E. McDowell Road, Suite 450
 Tucson, AZ 85715
 (520) 292-2300 (520) 292-1390 fax
 www.psomas.com



Appendix I
Kiley-Horn 5th Street
Conceptual Design







Appendix J
2022 Road Diet Scenario A
Simtraffic Reports

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	7:15	7:15	7:15	7:15	7:15	7:15	7:15
End Time	8:30	8:30	8:30	8:30	8:30	8:30	8:30
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	23771	23800	23865	23710	23492	23532	23818
Vehs Exited	23355	23097	23243	23270	23027	22993	23251
Starting Vehs	1054	969	1016	1050	1018	1003	1028
Ending Vehs	1470	1672	1638	1490	1483	1542	1595
Travel Distance (mi)	24720	24321	24576	24481	24232	24311	24546
Travel Time (hr)	1441.4	1389.2	1428.9	1387.0	1372.3	1386.8	1404.8
Total Delay (hr)	677.4	637.8	669.7	629.2	624.3	634.5	646.6
Total Stops	35528	33474	35699	33339	32889	33617	33785
Fuel Used (gal)	917.5	895.9	910.3	899.9	889.0	893.9	905.5

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	7:15	7:15	7:15	7:15
End Time	8:30	8:30	8:30	8:30
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	23781	23617	23779	23709
Vehs Exited	23242	23022	23365	23184
Starting Vehs	1023	961	1038	1001
Ending Vehs	1562	1556	1452	1540
Travel Distance (mi)	24679	24139	24666	24467
Travel Time (hr)	1391.1	1376.5	1404.7	1398.3
Total Delay (hr)	628.3	632.0	642.3	642.2
Total Stops	33859	33026	34132	33931
Fuel Used (gal)	902.6	886.0	905.1	900.6

Interval #0 Information Seeding

Start Time	7:15
End Time	7:30
Total Time (min)	15
No data recorded this interval.	

Interval #1 Information Recording

Start Time	7:30
End Time	8:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	23771	23800	23865	23710	23492	23532	23818
Vehs Exited	23355	23097	23243	23270	23027	22993	23251
Starting Vehs	1054	969	1016	1050	1018	1003	1028
Ending Vehs	1470	1672	1638	1490	1483	1542	1595
Travel Distance (mi)	24720	24321	24576	24481	24232	24311	24546
Travel Time (hr)	1441.4	1389.2	1428.9	1387.0	1372.3	1386.8	1404.8
Total Delay (hr)	677.4	637.8	669.7	629.2	624.3	634.5	646.6
Total Stops	35528	33474	35699	33339	32889	33617	33785
Fuel Used (gal)	917.5	895.9	910.3	899.9	889.0	893.9	905.5

Interval #1 Information Recording

Start Time	7:30
End Time	8:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	23781	23617	23779	23709
Vehs Exited	23242	23022	23365	23184
Starting Vehs	1023	961	1038	1001
Ending Vehs	1562	1556	1452	1540
Travel Distance (mi)	24679	24139	24666	24467
Travel Time (hr)	1391.1	1376.5	1404.7	1398.3
Total Delay (hr)	628.3	632.0	642.3	642.2
Total Stops	33859	33026	34132	33931
Fuel Used (gal)	902.6	886.0	905.1	900.6

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.5	0.2	0.4	0.1	0.7	0.7	1.2	0.2	1.1	1.3	0.1	0.2
Total Del/Veh (s)	68.4	35.1	8.4	44.1	39.4	35.7	111.8	51.5	7.8	55.7	55.7	57.1

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	49.9

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.4	0.1	0.2	0.0	0.0	0.0	0.1	0.1	0.1		0.1	0.0
Total Del/Veh (s)	33.2	6.0	4.2	7.7	1.0	0.2	55.1	17.4	155.4		65.8	4.7

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.3	0.5	102.7	88.6	67.1	1.7	0.3	0.3	4.7	4.1	3.9
Total Del/Veh (s)	62.6	25.3	23.1	152.2	144.1	141.7	328.7	59.4	56.0	395.1	247.4	246.6

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	37.5
Total Del/Veh (s)	132.8

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.1	0.0	2.5	0.7	0.9	0.0	0.0	0.0	1.5	0.2	0.2	0.4
Total Del/Veh (s)	79.1	36.3	72.6	76.3	15.0	52.3	34.7	5.3	58.3	46.0	41.4	48.8

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.1	0.2	0.1	0.4	0.3	1.3	0.3
Total Del/Veh (s)	17.0	7.5	11.0	9.7	52.4	9.5	12.9

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.4	0.2	0.3	1.4	0.3	0.3
Total Del/Veh (s)	95.2	34.6	15.6	46.0	47.4	27.0	75.8	44.9	40.9	92.5	48.3	45.0

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	48.3

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	9.5	1.0	0.3	9.8	6.4	3.9	27.7	7.1	29.8	31.1	27.4	4.7

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.1	0.0	0.1	0.0	0.2	2.1	0.4	0.4	2.2	0.4	0.3
Total Del/Veh (s)	31.0	10.8	5.2	20.6	9.4	4.4	69.9	43.7	36.9	73.9	42.1	34.2

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	23.8

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	1.6	1.0	1.1	2.6	2.4	2.1	1.7	0.4	1.7	1.6	0.3	1.5
Total Del/Veh (s)	155.3	39.8	25.0	155.0	49.6	31.5	54.7	40.7	25.8	116.8	42.0	19.3

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	1.0
Total Del/Veh (s)	49.6

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.3	0.1	0.1	0.0	
Total Del/Veh (s)	5.9	2.2	0.8	17.2	5.6	3.9	12.3	55.1	24.8	6.6	

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.7	0.8	1.2	0.5	0.2	0.6	1.9	0.4	0.5	1.6	0.4	0.4
Total Del/Veh (s)	76.2	20.4	13.1	29.9	18.6	11.4	152.3	42.8	39.5	66.2	46.3	44.6

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.5
Total Del/Veh (s)	33.3

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.2	0.0	0.3	0.3	0.1	0.2	1.8	0.3	1.7	1.6	0.3	1.5
Total Del/Veh (s)	138.1	26.9	13.8	52.9	44.3	29.5	156.6	46.7	22.9	102.3	41.0	23.6

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	49.7

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	3.9	3.1	4.4	0.5	13.6	9.0	2.2

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.1
Total Del/Veh (s)	5.5	1.6	0.8	8.2	4.3	3.5	19.3	21.0	8.0	24.2	15.6	5.7

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	4.1

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.1	1.6	0.2	0.2	1.3	0.2	1.2
Total Del/Veh (s)	44.4	44.0	17.4	44.5	43.3	25.7	84.0	28.2	18.3	37.7	29.4	15.9

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	32.3

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	1.5	0.9
Total Del/Veh (s)	1.5	6.8	4.8

40: Country Club Rd Performance by movement

Movement	EBR	NBT	SBT	All
Denied Del/Veh (s)	0.0	0.2	0.0	0.1
Total Del/Veh (s)	12.3	13.4	2.3	9.0

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	5.6	1.7	3.3

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWT	SWR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0
Total Del/Veh (s)	3.2	0.4	2.9	20.9	14.4

100: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	9.0	2.0	5.9

111: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.0
Total Del/Veh (s)	3.6	2.9	6.2	5.0	42.3	26.9	42.4	7.5

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.6	4.2	3.1

201: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.3	0.2	0.2	0.0
Total Del/Veh (s)	5.4	3.3	4.9	4.2	114.5	94.5	58.4	13.2

1000: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.0
Total Del/Veh (s)	5.0	33.5	31.2	12.4	23.1

1100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	1.6	4.2	3.6	6.5	3.1

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.2	0.0
Total Del/Veh (s)	5.5	4.5	2.4	32.2	12.1	6.4

1400: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	2.0	2.9	2.5	4.9	2.6

3000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	5.6	4.9	8.6	136.4	7.3

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.6	0.0
Total Del/Veh (s)	2.7	2.1	11.2	82.8	72.2	15.8

5000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.0
Total Del/Veh (s)	1.8	1.1	6.7	20.7	5.7

6000: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	2.5	6.9	4.8	6.8	40.1	5.4

7000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	1.9	2.0	6.4	14.0	2.7

8000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	2.3	5.9	9.0	10.7	4.6

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.2	0.0
Total Del/Veh (s)	5.2	4.0	3.1	46.7	17.1	5.4

Total Network Performance

Denied Del/Veh (s)	5.1
Total Del/Veh (s)	88.6

Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	35.1	102.0	0.6	20
	1000	6.0	14.2	0.1	16
	2	6.1	37.7	0.3	26
Wilson HAWK	20	1.5	4.1	0.0	17
6th Side Street	201	5.4	12.8	0.1	18
Tucson Blvd	3	26.0	35.5	0.1	8
	3000	6.0	48.3	0.4	27
6th/5th Street	72	3.3	6.9	0.0	16
Country Club Rd	4	36.3	49.2	0.1	8
	9922	5.4	17.5	0.1	20
	4000	2.7	31.6	0.2	28
Dodge Blvd	5	17.0	63.5	0.4	23
	5000	2.0	10.0	0.1	24
Alvernon Wy	6	34.6	56.3	0.2	12
	6000	3.0	18.1	0.1	25
	70	5.6	18.7	0.1	21
	7	1.0	3.0	0.0	23
	7000	1.9	18.9	0.1	27
Columbus Blvd	8	10.7	22.7	0.1	16
	8000	2.7	20.1	0.1	26
Swan Rd	9	38.9	80.2	0.4	16
	9000	7.0	21.9	0.1	20
	100	9.0	22.1	0.1	18
	10	2.2	4.2	0.0	16
	1100	1.9	16.0	0.1	27
Rosemont Blvd	11	20.3	35.5	0.1	13
	111	4.0	31.4	0.2	27
Craycroft Rd	12	26.6	56.5	0.3	17
	1200	7.1	35.3	0.2	24
	13	3.9	28.4	0.2	26
Chantilly Dr HAWK	130	1.6	4.2	0.0	16
Sahuara Ave	14	1.6	8.7	0.1	25
	1400	2.2	30.0	0.2	28
Wilmot Rd	15	43.9	74.0	0.3	13
Total		382.7	1039.7	5.6	19

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	43.3	93.4	0.4	17
	1400	4.5	33.3	0.3	29
Sahuara Ave	14	4.3	32.0	0.2	27
	130	4.2	11.7	0.1	19
	13	0.5	2.6	0.0	27
	1200	2.4	26.7	0.2	27
Craycroft Rd	12	44.0	70.6	0.2	12
	111	7.6	38.7	0.3	24
Rosemont Blvd	11	18.3	45.4	0.2	19
	1100	4.8	19.7	0.1	23
	10	5.6	19.8	0.1	22
Arcadia Ave HAWK	100	2.0	4.6	0.0	15
	9000	3.1	15.6	0.1	25
Swan Rd	9	49.0	65.4	0.1	7
	8000	6.8	49.6	0.4	26
Columbus Blvd	8	8.8	26.0	0.1	20
	7000	2.2	14.5	0.1	25
	7	6.4	23.3	0.1	22
Irving Ave HAWK	70	1.7	4.4	0.0	16
	6000	6.9	19.5	0.1	20
Alvernon Wy	6	47.4	61.6	0.1	7
	5000	8.1	30.6	0.2	22
Dodge Blvd	5	10.4	18.6	0.1	13
	4000	11.4	58.6	0.4	25
	9922	54.5	83.1	0.2	11
Country Club Rd	4	76.3	87.7	0.1	4
6th/5th Street	72	24.5	38.2	0.1	11
	3000	4.9	9.1	0.0	12
Tucson Blvd	3	143.8	280.5	0.4	7
6th Side Street	201	5.3	14.8	0.1	19
Wilson HAWK	20	6.9	16.2	0.1	16
	2	1.0	3.0	0.0	22
	1000	33.5	65.3	0.3	15
Campbell Ave	1	39.4	46.7	0.1	5
Total		694.1	1430.7	5.5	15

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	317	283	268	200	232	273	282	389	517	489	477	310
Average Queue (ft)	159	175	148	56	127	234	239	228	352	346	322	42
95th Queue (ft)	267	256	247	147	243	275	272	416	483	469	447	192
Link Distance (ft)		2992	2992				233	233		2020	2020	2020
Upstream Blk Time (%)					0	15	20					
Queuing Penalty (veh)					0	91	121					
Storage Bay Dist (ft)	250			50	180			290				150
Storage Blk Time (%)	2	1	28	3	1	31		10	18		39	
Queuing Penalty (veh)	6	2	32	7	6	60		47	30		30	

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	350	535	526	530
Average Queue (ft)	108	343	348	350
95th Queue (ft)	289	497	499	499
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	0	36		
Queuing Penalty (veh)	0	29		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	107	326	15	58	44	121
Average Queue (ft)	24	69	0	12	11	37
95th Queue (ft)	68	233	7	48	36	98
Link Distance (ft)		1396		55	442	447
Upstream Blk Time (%)				1		
Queuing Penalty (veh)				10		
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0	3		1		
Queuing Penalty (veh)	0	1		0		

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB	SB	SB
Directions Served	L	TR	L	TR	L	TR	L	TR
Maximum Queue (ft)	300	368	300	1878	236	536	400	1893
Average Queue (ft)	77	260	97	1571	41	301	233	1175
95th Queue (ft)	217	410	294	2333	153	480	485	2170
Link Distance (ft)		356		1877		1948		2192
Upstream Blk Time (%)		5		3				6
Queuing Penalty (veh)		28		35				0
Storage Bay Dist (ft)	200		100		300		300	
Storage Blk Time (%)		18	1	41		14	1	66
Queuing Penalty (veh)		10	12	27		2	6	44

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	B9922	NB	NB	NB	NB
Directions Served	L	T	T	L	T	T	R	T	L	T	T	R
Maximum Queue (ft)	265	340	277	190	516	488	260	1191	177	220	220	170
Average Queue (ft)	101	194	128	133	445	380	92	526	115	183	188	43
95th Queue (ft)	214	302	236	262	598	568	294	1330	191	213	219	146
Link Distance (ft)		486	486		423	423		1235		177	177	
Upstream Blk Time (%)		0	0		32	10		2	1	23	25	0
Queuing Penalty (veh)		0	0		165	52		25	0	120	129	0
Storage Bay Dist (ft)	180			90			160		100			70
Storage Blk Time (%)	1	14		2	72	33	0		16	41	47	0
Queuing Penalty (veh)	2	14		7	67	50	1		68	61	31	0

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB	SB
Directions Served	L	T	TR
Maximum Queue (ft)	174	550	551
Average Queue (ft)	102	280	287
95th Queue (ft)	201	458	459
Link Distance (ft)		2000	2000
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)	100		
Storage Blk Time (%)	5	42	
Queuing Penalty (veh)	18	40	

Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB
Directions Served	T	R	L	T	L	R
Maximum Queue (ft)	439	236	204	305	90	90
Average Queue (ft)	184	32	52	218	33	35
95th Queue (ft)	375	137	143	377	73	69
Link Distance (ft)	2072				292	2124
Upstream Blk Time (%)			0	2		
Queuing Penalty (veh)			0	14		
Storage Bay Dist (ft)	110		300		220	
Storage Blk Time (%)	13			0	2	
Queuing Penalty (veh)	8			0	2	

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	TR	L	T	TR
Maximum Queue (ft)	343	431	285	400	587	285	260	565	568	260	622	627
Average Queue (ft)	127	246	43	133	406	118	141	354	357	175	381	382
95th Queue (ft)	247	384	170	365	637	309	296	535	529	323	568	563
Link Distance (ft)	908				580		2113		2113	1973		1973
Upstream Blk Time (%)					4							
Queuing Penalty (veh)					32							
Storage Bay Dist (ft)	300			110	300	110		160			160	
Storage Blk Time (%)	0	29	0			51	1	4	38	12	39	
Queuing Penalty (veh)	0	44	0			104	4	20	39	69	53	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	15	54	160	399	34	59
Average Queue (ft)	1	5	26	102	12	17
95th Queue (ft)	9	28	99	309	36	44
Link Distance (ft)	56		687		311	327
Upstream Blk Time (%)	0					
Queuing Penalty (veh)	1					
Storage Bay Dist (ft)	100	100				
Storage Blk Time (%)	0		0	6		
Queuing Penalty (veh)	0		0	3		

Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	163	354	127	164	347	95	199	472	200	417
Average Queue (ft)	37	138	16	32	128	9	56	230	71	214
95th Queue (ft)	105	280	82	95	264	52	147	412	171	362
Link Distance (ft)		474			702			2075		1939
Upstream Blk Time (%)		0								
Queuing Penalty (veh)		0								
Storage Bay Dist (ft)	75		110	75		75	100		100	
Storage Blk Time (%)	2	14		1	16		1	35	4	33
Queuing Penalty (veh)	9	10		6	13		5	18	12	19

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	359	728	254	416	566	210	304	444	448	341	379	487
Average Queue (ft)	163	346	55	166	407	89	97	293	291	124	229	325
95th Queue (ft)	347	703	207	368	609	238	246	410	408	266	393	468
Link Distance (ft)		1812			558			1988	1988			2061
Upstream Blk Time (%)				0	2							
Queuing Penalty (veh)				0	15							
Storage Bay Dist (ft)	300		110	500		110	180			350	230	
Storage Blk Time (%)	6	36		0	50	0	0	30	3		21	21
Queuing Penalty (veh)	37	58		2	104	0	1	25	8		117	42

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	499	370
Average Queue (ft)	320	53
95th Queue (ft)	460	191
Link Distance (ft)	2061	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	10	
Queuing Penalty (veh)	9	

Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	44	69	194	337	165	108
Average Queue (ft)	11	17	72	80	70	32
95th Queue (ft)	37	54	147	246	129	75
Link Distance (ft)		54		582	279	312
Upstream Blk Time (%)	0	1		0		
Queuing Penalty (veh)	0	6		0		
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0	1	4	4		
Queuing Penalty (veh)	2	0	23	8		

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	134	557	180	214	562	192	244	461	236	497
Average Queue (ft)	88	247	69	55	260	28	115	202	55	267
95th Queue (ft)	152	455	185	163	476	122	237	375	174	435
Link Distance (ft)		604			1181			1974		2044
Upstream Blk Time (%)		0								
Queuing Penalty (veh)		1								
Storage Bay Dist (ft)	75		110	100		110	150		150	
Storage Blk Time (%)	28	27	0	1	21		18	19		33
Queuing Penalty (veh)	191	56	1	7	23		54	14		13

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Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	377	502	210	457	722	210	330	580	578	230	300	446
Average Queue (ft)	179	185	48	137	401	55	226	367	364	124	163	294
95th Queue (ft)	336	384	150	362	653	188	389	601	588	285	310	416
Link Distance (ft)		1293			1162			1958	1958			2072
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300		110	500		110	180			180	150	
Storage Blk Time (%)	7	22	0		45		38	33	33	0	15	33
Queuing Penalty (veh)	34	55	0		91		203	56	38	0	74	41

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	458	300
Average Queue (ft)	293	127
95th Queue (ft)	423	312
Link Distance (ft)	2072	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	33	0
Queuing Penalty (veh)	51	1

Intersection: 13: 6th/5th Street

Movement	EB	WB	WB	NB
Directions Served	TR	L	TR	LTR
Maximum Queue (ft)	182	32	52	61
Average Queue (ft)	31	3	4	20
95th Queue (ft)	118	18	26	49
Link Distance (ft)	1017		56	429
Upstream Blk Time (%)		0	0	
Queuing Penalty (veh)		0	1	
Storage Bay Dist (ft)		100		
Storage Blk Time (%)	1	0	0	
Queuing Penalty (veh)	0	0	0	

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	LTR	LTR	LTR	LTR
Maximum Queue (ft)	74	225	105	44
Average Queue (ft)	6	51	40	14
95th Queue (ft)	39	152	76	40
Link Distance (ft)	275	1189	622	706
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	150	177	144	155	359	562	534	349	230	444	433	417
Average Queue (ft)	59	96	56	74	266	290	269	191	122	290	283	252
95th Queue (ft)	116	151	115	135	387	575	538	306	258	406	401	375
Link Distance (ft)		1302		2255		1994	1994	1994		2040	2040	2040
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	145		145		240				130			
Storage Blk Time (%)	0	1	0		26	4			2	33		2
Queuing Penalty (veh)	0	3	1		113	13			12	41		9

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	306
Average Queue (ft)	112
95th Queue (ft)	219
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	0
Queuing Penalty (veh)	0

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	84	312
Average Queue (ft)	29	142
95th Queue (ft)	77	351
Link Distance (ft)	55	298
Upstream Blk Time (%)	6	2
Queuing Penalty (veh)	43	20
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 40: Country Club Rd

Movement	EB	NB	NB	SB
Directions Served	R	T	T	T
Maximum Queue (ft)	111	445	447	4
Average Queue (ft)	42	125	131	0
95th Queue (ft)	81	329	333	4
Link Distance (ft)	450	2095	2095	177
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	339	80
Average Queue (ft)	69	34
95th Queue (ft)	237	86
Link Distance (ft)	514	56
Upstream Blk Time (%)		8
Queuing Penalty (veh)		67
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

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Intersection: 72: 6th/5th Street

Movement	EB	SW	SW
Directions Served	L	R	R
Maximum Queue (ft)	12	540	524
Average Queue (ft)	0	256	75
95th Queue (ft)	12	627	362
Link Distance (ft)	100	486	486
Upstream Blk Time (%)	0	3	1
Queuing Penalty (veh)	0	17	5
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 100: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	370	74
Average Queue (ft)	117	31
95th Queue (ft)	308	77
Link Distance (ft)	528	54
Upstream Blk Time (%)		9
Queuing Penalty (veh)		56
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 111: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	118	104
Average Queue (ft)	48	42
95th Queue (ft)	95	85
Link Distance (ft)	340	283
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	69	240
Average Queue (ft)	25	69
95th Queue (ft)	69	195
Link Distance (ft)	56	275
Upstream Blk Time (%)	5	0
Queuing Penalty (veh)	22	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 201: 6th Side Street & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	269	247	298	190
Average Queue (ft)	35	30	122	79
95th Queue (ft)	162	141	264	164
Link Distance (ft)	298	356	433	422
Upstream Blk Time (%)	0	0	0	
Queuing Penalty (veh)	2	0	0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1000: 6th/5th Street

Movement	EB	EB	WB	NB
Directions Served	T	R	LTR	LTR
Maximum Queue (ft)	198	79	1000	85
Average Queue (ft)	18	3	510	33
95th Queue (ft)	117	38	1137	65
Link Distance (ft)	233	233	1396	446
Upstream Blk Time (%)	0	0	0	
Queuing Penalty (veh)	1	0	1	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

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Intersection: 1100: 6th/5th Street

Movement	EB	WB	NB
Directions Served	T	TR	LTR
Maximum Queue (ft)	32	35	32
Average Queue (ft)	1	2	11
95th Queue (ft)	21	37	35
Link Distance (ft)	582	604	226
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1200: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	148	100
Average Queue (ft)	60	43
95th Queue (ft)	116	77
Link Distance (ft)	214	281
Upstream Blk Time (%)	0	
Queuing Penalty (veh)	0	
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1400: 6th/5th Street

Movement	NB
Directions Served	LTR
Maximum Queue (ft)	58
Average Queue (ft)	22
95th Queue (ft)	49
Link Distance (ft)	307
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	
Storage Blk Time (%)	
Queuing Penalty (veh)	

Intersection: 3000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	T	LTR	LTR
Maximum Queue (ft)	25	116	43	136
Average Queue (ft)	1	59	12	40
95th Queue (ft)	15	142	37	113
Link Distance (ft)	1877	100	420	430
Upstream Blk Time (%)		4		
Queuing Penalty (veh)		23		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 4000: 6th/5th Street

Movement	B9922	WB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	140	481	224	272
Average Queue (ft)	5	64	77	90
95th Queue (ft)	82	342	202	253
Link Distance (ft)	423	2072	554	590
Upstream Blk Time (%)	0			1
Queuing Penalty (veh)	0			0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 5000: 6th/5th Street

Movement	WB	SB
Directions Served	TR	LTR
Maximum Queue (ft)	268	140
Average Queue (ft)	69	51
95th Queue (ft)	207	101
Link Distance (ft)	908	286
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 6000: 6th/5th Street

Movement	WB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	282	38	48
Average Queue (ft)	64	8	13
95th Queue (ft)	300	30	41
Link Distance (ft)	514	362	366
Upstream Blk Time (%)	0		
Queuing Penalty (veh)	4		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 7000: 6th/5th Street

Movement	EB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	14	33	95
Average Queue (ft)	0	4	43
95th Queue (ft)	14	20	77
Link Distance (ft)	687	267	311
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 8000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	70	68
Average Queue (ft)	33	28
95th Queue (ft)	61	57
Link Distance (ft)	326	341
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 9000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	67	185	59	130
Average Queue (ft)	2	25	18	51
95th Queue (ft)	66	122	49	99
Link Distance (ft)	558	528	210	316
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 3889

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	4:15	4:15	4:15	4:15	4:15	4:15	4:15
End Time	5:30	5:30	5:30	5:30	5:30	5:30	5:30
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	24255	24468	24426	24316	24210	24414	24344
Vehs Exited	23540	23581	23690	23621	23607	23727	23681
Starting Vehs	1237	1279	1235	1245	1385	1200	1296
Ending Vehs	1952	2166	1971	1940	1988	1887	1959
Travel Distance (mi)	25714	25819	25602	25540	25519	25667	25585
Travel Time (hr)	1835.7	1905.1	1759.2	1785.4	1852.3	1823.7	1845.0
Total Delay (hr)	1037.1	1101.2	964.1	992.3	1059.9	1027.5	1050.8
Total Stops	38969	42938	38891	37766	39454	37008	38289
Fuel Used (gal)	1028.9	1046.1	1008.9	1013.0	1027.3	1022.8	1026.9

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	4:15	4:15	4:15	4:15
End Time	5:30	5:30	5:30	5:30
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	24274	24510	24524	24375
Vehs Exited	23448	23734	23847	23645
Starting Vehs	1219	1319	1431	1281
Ending Vehs	2045	2095	2108	1998
Travel Distance (mi)	25444	25718	25805	25641
Travel Time (hr)	1946.3	1970.1	2019.4	1874.2
Total Delay (hr)	1156.3	1172.1	1218.6	1078.0
Total Stops	38797	41621	39964	39368
Fuel Used (gal)	1046.0	1061.6	1070.3	1035.2

Interval #0 Information Seeding

Start Time	4:15
End Time	4:30
Total Time (min)	15
No data recorded this interval.	

Interval #1 Information Recording

Start Time	4:30
End Time	5:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	24255	24468	24426	24316	24210	24414	24344
Vehs Exited	23540	23581	23690	23621	23607	23727	23681
Starting Vehs	1237	1279	1235	1245	1385	1200	1296
Ending Vehs	1952	2166	1971	1940	1988	1887	1959
Travel Distance (mi)	25714	25819	25602	25540	25519	25667	25585
Travel Time (hr)	1835.7	1905.1	1759.2	1785.4	1852.3	1823.7	1845.0
Total Delay (hr)	1037.1	1101.2	964.1	992.3	1059.9	1027.5	1050.8
Total Stops	38969	42938	38891	37766	39454	37008	38289
Fuel Used (gal)	1028.9	1046.1	1008.9	1013.0	1027.3	1022.8	1026.9

Interval #1 Information Recording

Start Time	4:30
End Time	5:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	24274	24510	24524	24375
Vehs Exited	23448	23734	23847	23645
Starting Vehs	1219	1319	1431	1281
Ending Vehs	2045	2095	2108	1998
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Travel Time (hr)	1946.3	1970.1	2019.4	1874.2
Total Delay (hr)	1156.3	1172.1	1218.6	1078.0
Total Stops	38797	41621	39964	39368
Fuel Used (gal)	1046.0	1061.6	1070.3	1035.2

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	101.2	100.6	101.8	0.0	0.0	0.0	1.2	0.2	1.1	11.4	9.6	9.7
Total Del/Veh (s)	426.8	487.8	433.5	70.3	32.8	29.9	66.5	43.9	11.0	271.1	294.3	295.2

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	27.8
Total Del/Veh (s)	223.0

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.1	15.3	41.7	0.0	0.0	0.0	0.1	0.1		0.1	0.1	8.2
Total Del/Veh (s)	84.1	98.9	92.8	29.2	0.7	0.1	199.4	274.1		18.1	14.2	54.2

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	3.0	5.7	1.4	6.2	4.9	6.3	1.8	0.5	0.6	32.9	33.6	33.2
Total Del/Veh (s)	62.3	38.0	34.1	102.5	86.5	85.4	333.6	68.4	67.0	423.4	190.1	181.8

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	10.3
Total Del/Veh (s)	99.2

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	1.6	0.2	0.3	0.1
Total Del/Veh (s)	50.8	31.8	51.5	30.0	3.6	44.6	34.2	6.4	57.8	41.8	34.2	35.1

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.3	1.4	0.1
Total Del/Veh (s)	21.9	10.1	15.8	11.0	51.5	19.3	17.7

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.3	0.2	0.3	1.4	0.2	0.3
Total Del/Veh (s)	129.5	41.3	24.1	52.7	53.2	32.3	90.5	80.6	78.8	108.4	51.4	47.4

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	63.5

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.1
Total Del/Veh (s)	6.2	0.9	0.1	11.9	6.0	4.1	54.2	40.4	14.3	21.5	12.8	20.3

7: 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	3.6

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.9	0.4	0.4	2.2	0.4	0.4
Total Del/Veh (s)	41.8	14.7	9.3	35.5	10.6	6.8	74.7	44.2	39.2	101.1	43.4	37.7

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	26.4

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.8	0.3	1.7	1.6	0.2	1.5
Total Del/Veh (s)	93.8	78.6	62.4	159.3	32.1	18.2	103.3	86.9	50.9	325.4	53.7	22.6

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	76.3

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.5	0.1	0.0	0.2	0.1	0.4	0.1	0.1	0.1
Total Del/Veh (s)	6.3	1.3	0.4	14.0	6.4	3.7	103.4	55.5	35.1	45.9	24.5	34.1

10: 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.1
Total Del/Veh (s)	6.0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.2	0.1	0.1	0.0	0.0	1.9	0.5	0.5	1.6	0.4	0.3
Total Del/Veh (s)	43.2	25.3	17.4	52.1	12.9	7.8	92.3	49.6	47.1	114.1	39.7	34.5

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	32.8

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.1	0.1	0.0	0.0	0.1	1.8	0.3	1.7	1.5	0.2	1.5
Total Del/Veh (s)	54.4	47.3	34.6	136.6	28.5	15.7	249.1	62.5	43.5	73.7	42.8	18.3

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	55.6

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	All
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	6.1	5.7	9.0	0.5	0.1	17.9	15.8	21.0	18.8	4.7

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	4.0	1.3	0.8	6.7	2.5	1.9	12.8	6.6	17.8	4.1	2.1

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.1	1.5	0.2	0.2	1.4	0.2	1.3
Total Del/Veh (s)	49.2	36.9	18.3	34.4	34.3	19.6	59.1	28.9	19.4	41.2	27.5	8.0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	30.2

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.1	0.1
Total Del/Veh (s)	5.3	5.7	5.5

40: Country Club Rd Performance by movement

Movement	EBT	EBR	NBT	SBT	All
Denied Del/Veh (s)	0.0	0.0	0.2	0.0	0.1
Total Del/Veh (s)	0.1	9.2	9.9	2.4	6.8

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	7.8	1.9	5.1

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.0
Total Del/Veh (s)	2.2	0.7	10.6	6.0

100: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.2	0.0	0.1
Total Del/Veh (s)	10.3	2.1	6.8

111: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.2	0.2	0.0
Total Del/Veh (s)	4.7	3.8	5.0	4.0	45.8	32.7	42.3	23.1	7.8

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.1	0.0
Total Del/Veh (s)	1.7	3.4	2.4

201: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	480.8	485.7	284.3	329.1	27.2
Total Del/Veh (s)	25.8	3.7	2.7	1094.7	1173.8	1385.2	1139.1	69.8

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBR	All
Denied Del/Veh (s)	0.1	0.0	0.0	0.2	0.1
Total Del/Veh (s)	9.6	3.3	6.6	14.0	8.1

1100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.1	0.0
Total Del/Veh (s)	3.2	4.2	3.1	31.2	67.0	7.3

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.2	0.0
Total Del/Veh (s)	7.0	5.8	2.0	36.7	9.8	7.3

1400: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.1	0.0
Total Del/Veh (s)	2.6	3.0	2.2	10.3	18.8	4.0

3000: 6th/5th Street Performance by movement

Movement	EBT	WBT	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	88.5	80.5	5.7
Total Del/Veh (s)	8.7	0.6	352.0	320.9	26.5

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	All
Denied Del/Veh (s)	0.1	0.1	0.1	0.0	0.1
Total Del/Veh (s)	6.9	5.4	6.6	5.8	6.6

5000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.2	0.1	0.1	0.1	0.2	0.2
Total Del/Veh (s)	3.3	6.7	131.2	222.8	29.3	9.1

6000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.1	0.0	5.8	5.5	0.8	0.5
Total Del/Veh (s)	3.0	8.1	98.4	98.2	65.3	14.1

7000: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	All
Denied Del/Veh (s)	0.0	0.1	0.1	0.1	0.1
Total Del/Veh (s)	3.2	4.5	3.1	14.3	3.8

8000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.1	0.1	0.1	0.1	0.1
Total Del/Veh (s)	2.9	5.9	16.9	11.8	4.9

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.1	0.1	0.0	81.0	3.4	5.2
Total Del/Veh (s)	7.0	5.3	3.3	170.4	26.9	16.0

Total Network Performance

Denied Del/Veh (s)	11.9
Total Del/Veh (s)	139.9

Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	487.8	653.1	0.6	4
	1000	11.5	19.8	0.1	12
	2	98.9	146.5	0.3	8
Wilson HAWK	20	5.3	7.8	0.0	9
6th Side Street	201	25.8	33.3	0.1	7
Tucson Blvd	3	37.7	52.5	0.1	6
	3000	9.5	52.7	0.4	25
6th/5th Street	72	2.3	7.3	0.0	15
Country Club Rd	4	31.8	47.5	0.1	9
	9922	6.8	18.8	0.1	18
	4000	6.9	36.0	0.2	25
Dodge Blvd	5	21.9	68.4	0.4	21
	5000	3.4	11.7	0.1	21
Alvernon Wy	6	41.2	62.2	0.2	11
	6000	3.5	18.3	0.1	25
	70	7.9	20.8	0.1	18
	7	0.9	2.9	0.0	24
	7000	3.2	20.1	0.1	25
Columbus Blvd	8	14.7	26.5	0.1	14
	8000	3.2	20.4	0.1	26
Swan Rd	9	77.7	118.9	0.4	11
	9000	8.4	23.5	0.1	19
	100	10.3	23.5	0.1	17
	10	1.3	3.3	0.0	21
	1100	3.3	17.6	0.1	25
Rosemont Blvd	11	24.3	39.4	0.1	12
	111	5.2	33.6	0.2	25
Craycroft Rd	12	47.2	77.5	0.3	12
	1200	8.4	36.8	0.2	23
	13	6.1	30.0	0.2	24
Chantilly Dr HAWK	130	1.7	4.2	0.0	16
Sahuara Ave	14	1.3	8.2	0.1	27
	1400	2.6	30.9	0.2	27
Wilmot Rd	15	37.5	65.0	0.3	15
Total		1059.7	1839.0	5.6	12

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	34.3	132.9	0.4	12
	1400	1.7	27.5	0.3	35
Sahuara Ave	14	2.5	29.7	0.2	29
	130	3.4	10.7	0.1	20
	13	0.5	2.5	0.0	27
	1200	2.1	25.9	0.2	28
Craycroft Rd	12	28.3	55.7	0.2	15
	111	6.2	37.4	0.3	25
Rosemont Blvd	11	12.9	40.8	0.2	21
	1100	5.0	20.3	0.1	22
	10	6.4	20.7	0.1	21
Arcadia Ave HAWK	100	2.1	4.7	0.0	15
	9000	3.3	15.9	0.1	25
Swan Rd	9	31.8	45.6	0.1	10
	8000	6.7	48.5	0.4	27
Columbus Blvd	8	10.4	27.4	0.1	19
	7000	4.9	17.1	0.1	22
	7	6.0	22.8	0.1	22
Irving Ave HAWK	70	1.9	4.5	0.0	15
	6000	8.1	20.6	0.1	19
Alvernon Wy	6	53.1	67.1	0.1	7
	5000	8.1	29.7	0.2	23
Dodge Blvd	5	12.4	20.2	0.1	12
	4000	6.9	54.9	0.4	27
	9922	3.6	32.9	0.2	27
Country Club Rd	4	30.0	40.7	0.1	8
6th/5th Street	72	13.7	30.0	0.1	14
	3000	0.6	10.7	0.0	10
Tucson Blvd	3	86.7	133.7	0.4	10
6th Side Street	201	4.1	13.6	0.1	21
Wilson HAWK	20	5.7	13.8	0.1	17
	2	0.7	2.7	0.0	25
	1000	6.6	39.1	0.3	25
Campbell Ave	1	32.8	39.5	0.1	6
Total		443.7	1139.7	5.5	17

Queuing and Blocking Report

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Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	425	3033	3037	200	232	272	267	362	410	412	391	229
Average Queue (ft)	407	2333	2325	170	143	212	215	142	281	283	258	43
95th Queue (ft)	530	3667	3653	280	244	282	277	277	369	375	349	145
Link Distance (ft)		2992	2992			233	233		2020	2020	2020	
Upstream Blk Time (%)		43	42		1	6	6					
Queuing Penalty (veh)		0	0		0	27	31					
Storage Bay Dist (ft)	250			50	180			290				150
Storage Blk Time (%)	3	87	55	6	8	14		0	6		30	
Queuing Penalty (veh)	15	248	94	31	26	25		0	9		40	

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	350	2128	2127	2097
Average Queue (ft)	282	1534	1510	1461
95th Queue (ft)	476	2466	2432	2365
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)		18	13	13
Queuing Penalty (veh)		0	0	0
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	2	69		
Queuing Penalty (veh)	9	126		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	69	1403	34	56	88	33
Average Queue (ft)	5	960	9	6	24	10
95th Queue (ft)	47	1757	30	33	73	32
Link Distance (ft)		1396		55	442	447
Upstream Blk Time (%)		2	0	0		
Queuing Penalty (veh)		23	0	3		
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)		46	0	0		
Queuing Penalty (veh)		2	0	0		

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB	SB	SB
Directions Served	L	TR	L	TR	L	TR	L	TR
Maximum Queue (ft)	300	374	300	1254	399	835	400	1682
Average Queue (ft)	171	360	91	808	129	443	260	1065
95th Queue (ft)	348	370	268	1310	338	730	499	2036
Link Distance (ft)		356		1877		1948		2192
Upstream Blk Time (%)		32						10
Queuing Penalty (veh)		400						0
Storage Bay Dist (ft)	200		100		300		300	
Storage Blk Time (%)	1	47	2	48	6	31	12	57
Queuing Penalty (veh)	16	73	13	22	35	12	70	42

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	B9922	NB	NB	NB	NB
Directions Served	L	T	T	L	T	T	R	T	L	T	T	R
Maximum Queue (ft)	280	441	391	190	416	368	119	13	177	218	222	170
Average Queue (ft)	168	269	184	131	241	142	5	0	111	183	189	49
95th Queue (ft)	317	402	315	228	378	286	59	9	189	210	214	157
Link Distance (ft)		486	486		423	423		1235		177	177	
Upstream Blk Time (%)		0	0		0	0			1	20	22	0
Queuing Penalty (veh)		0	0		1	0			0	109	120	0
Storage Bay Dist (ft)	180			90			160		100			70
Storage Blk Time (%)	2	24		13	39	4	0		11	43	47	0
Queuing Penalty (veh)	6	47		36	48	9	0		47	63	39	0

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB	SB
Directions Served	L	T	TR
Maximum Queue (ft)	175	416	401
Average Queue (ft)	138	265	262
95th Queue (ft)	216	390	376
Link Distance (ft)		2000	2000
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)	100		
Storage Blk Time (%)	17	41	
Queuing Penalty (veh)	64	61	

Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB
Directions Served	T	R	L	T	L	R
Maximum Queue (ft)	498	285	291	306	213	162
Average Queue (ft)	271	91	94	259	92	66
95th Queue (ft)	447	267	216	341	176	134
Link Distance (ft)	2072				292	2124
Upstream Blk Time (%)			0	2		
Queuing Penalty (veh)			0	19		
Storage Bay Dist (ft)	110		300		220	
Storage Blk Time (%)	21	0	0	2	0	1
Queuing Penalty (veh)	32	0	0	3	0	1

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	TR	L	T	TR
Maximum Queue (ft)	399	714	285	400	596	285	260	827	829	260	552	551
Average Queue (ft)	247	399	71	126	442	141	163	547	549	182	368	369
95th Queue (ft)	453	722	238	361	698	336	326	887	887	324	535	529
Link Distance (ft)	908				580		2113		2113	1973		1973
Upstream Blk Time (%)	2				8							
Queuing Penalty (veh)	16				68							
Storage Bay Dist (ft)	300	110		300	110		160			160		
Storage Blk Time (%)	15	39	0			53	1	2	53	15	41	
Queuing Penalty (veh)	105	104	0			118	9	9	51	78	51	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	30	56	37	327	45	40
Average Queue (ft)	4	7	1	73	16	13
95th Queue (ft)	19	37	22	236	43	36
Link Distance (ft)	56		687		311	327
Upstream Blk Time (%)	0					
Queuing Penalty (veh)	4					
Storage Bay Dist (ft)	100	100				
Storage Blk Time (%)	0		5			
Queuing Penalty (veh)	0		0			

Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	170	480	194	150	357	175	199	497	200	502
Average Queue (ft)	51	214	19	40	139	24	64	273	71	268
95th Queue (ft)	121	422	95	100	276	99	175	443	181	451
Link Distance (ft)		474			702			2075		1939
Upstream Blk Time (%)		1								
Queuing Penalty (veh)		6								
Storage Bay Dist (ft)	75		110	75		75	100		100	
Storage Blk Time (%)	5	22		4	13	0	1	41	8	40
Queuing Penalty (veh)	37	25		29	17	0	5	19	34	21

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	360	1285	254	404	559	210	305	922	929	450	376	725
Average Queue (ft)	169	727	84	195	329	76	158	556	554	226	270	447
95th Queue (ft)	393	1499	262	407	549	222	359	1004	999	526	447	867
Link Distance (ft)		1812			558			1988	1988			2061
Upstream Blk Time (%)		0		0	2							
Queuing Penalty (veh)		1		0	14							
Storage Bay Dist (ft)	300		110	500		110	180			350	230	
Storage Blk Time (%)	0	50	0	2	37		2	53	31		47	24
Queuing Penalty (veh)	0	100	1	11	85		10	54	57		244	33

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	717	370
Average Queue (ft)	432	67
95th Queue (ft)	837	252
Link Distance (ft)	2061	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	15	
Queuing Penalty (veh)	12	

Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	36	64	127	352	158	96
Average Queue (ft)	9	11	22	88	53	28
95th Queue (ft)	31	45	75	264	127	76
Link Distance (ft)		54		582	279	312
Upstream Blk Time (%)	0	1			1	
Queuing Penalty (veh)	0	5			0	
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0	1	0	6		
Queuing Penalty (veh)	0	0	0	2		

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	134	611	180	178	394	192	250	555	224	375
Average Queue (ft)	70	386	58	41	144	24	130	282	69	205
95th Queue (ft)	141	637	170	104	287	110	264	480	174	342
Link Distance (ft)		604			1181			1974		2044
Upstream Blk Time (%)		1								
Queuing Penalty (veh)		8								
Storage Bay Dist (ft)	75		110	100		110	150		150	
Storage Blk Time (%)	7	32		1	11		6	35	5	20
Queuing Penalty (veh)	68	67		6	9		22	34	17	11

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Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	400	874	210	415	496	210	330	857	858	230	299	507
Average Queue (ft)	158	395	111	211	206	52	218	486	483	137	94	339
95th Queue (ft)	371	767	253	409	392	164	387	987	969	294	257	481
Link Distance (ft)		1293			1162			1958	1958			2072
Upstream Blk Time (%)		0										
Queuing Penalty (veh)		0										
Storage Bay Dist (ft)	300		110	500		110	180			180	150	
Storage Blk Time (%)		46	1	1	26		36	37	38		1	38
Queuing Penalty (veh)		143	8	3	62		217	40	49		5	23

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	499	300
Average Queue (ft)	340	118
95th Queue (ft)	484	325
Link Distance (ft)	2072	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	38	0
Queuing Penalty (veh)	37	0

Intersection: 13: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	TR	L	TR	LTR	LTR
Maximum Queue (ft)	300	41	57	101	24
Average Queue (ft)	64	10	3	42	2
95th Queue (ft)	214	34	25	78	14
Link Distance (ft)	1017		56	429	480
Upstream Blk Time (%)		0	0		
Queuing Penalty (veh)		0	1		
Storage Bay Dist (ft)		100			
Storage Blk Time (%)	4	0	0		
Queuing Penalty (veh)	0	0	0		

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Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	LTR	LTR	LTR	LTR
Maximum Queue (ft)	108	96	33	40
Average Queue (ft)	15	10	9	15
95th Queue (ft)	62	51	31	40
Link Distance (ft)	275	1189	622	706
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	240	388	243	110	349	388	394	379	229	370	378	350
Average Queue (ft)	133	167	100	39	190	254	256	235	47	244	240	205
95th Queue (ft)	215	290	199	87	313	371	371	355	148	351	350	324
Link Distance (ft)		1302		2255		1994	1994	1994		2040	2040	2040
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	145		145		240				130			
Storage Blk Time (%)	7	16	1		4	10				30		1
Queuing Penalty (veh)	40	63	4		19	25				13		1

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	116
Average Queue (ft)	54
95th Queue (ft)	96
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	85	310
Average Queue (ft)	58	113
95th Queue (ft)	79	310
Link Distance (ft)	55	298
Upstream Blk Time (%)	32	1
Queuing Penalty (veh)	376	8
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 40: Country Club Rd

Movement	EB	NB	NB	SB
Directions Served	R	T	T	T
Maximum Queue (ft)	125	290	286	9
Average Queue (ft)	54	107	111	0
95th Queue (ft)	98	235	239	7
Link Distance (ft)	450	2095	2095	177
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	449	84
Average Queue (ft)	132	34
95th Queue (ft)	374	86
Link Distance (ft)	514	56
Upstream Blk Time (%)		8
Queuing Penalty (veh)		59
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

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Intersection: 72: 6th/5th Street

Movement	EB	SW	SW
Directions Served	L	R	R
Maximum Queue (ft)	26	485	5
Average Queue (ft)	1	83	0
95th Queue (ft)	18	259	5
Link Distance (ft)	100	486	486
Upstream Blk Time (%)	0	0	
Queuing Penalty (veh)	0	1	
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 100: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	480	79
Average Queue (ft)	156	31
95th Queue (ft)	415	79
Link Distance (ft)	528	54
Upstream Blk Time (%)	0	11
Queuing Penalty (veh)	1	73
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 111: 6th/5th Street

Movement	EB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	17	126	132
Average Queue (ft)	1	50	57
95th Queue (ft)	16	101	106
Link Distance (ft)	1181	340	283
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	73	221
Average Queue (ft)	29	49
95th Queue (ft)	77	151
Link Distance (ft)	56	275
Upstream Blk Time (%)	7	0
Queuing Penalty (veh)	51	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 201: 6th Side Street & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	317	144	448	445
Average Queue (ft)	299	15	350	329
95th Queue (ft)	356	77	573	540
Link Distance (ft)	298	356	433	422
Upstream Blk Time (%)	22		60	44
Queuing Penalty (veh)	264		0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1000: 6th/5th Street

Movement	EB	EB	WB	SB
Directions Served	T	R	LTR	LTR
Maximum Queue (ft)	277	268	334	111
Average Queue (ft)	121	71	69	39
95th Queue (ft)	309	248	232	82
Link Distance (ft)	233	233	1396	415
Upstream Blk Time (%)	3	1		
Queuing Penalty (veh)	20	8		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

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Intersection: 1100: 6th/5th Street

Movement	EB	NB	SB
Directions Served	T	LTR	LTR
Maximum Queue (ft)	203	168	146
Average Queue (ft)	20	63	52
95th Queue (ft)	107	129	116
Link Distance (ft)	582	226	197
Upstream Blk Time (%)		0	0
Queuing Penalty (veh)		0	0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1200: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	143	93
Average Queue (ft)	59	42
95th Queue (ft)	112	74
Link Distance (ft)	214	281
Upstream Blk Time (%)	0	
Queuing Penalty (veh)	0	
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1400: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	99	68
Average Queue (ft)	43	32
95th Queue (ft)	78	60
Link Distance (ft)	307	277
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

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Intersection: 3000: 6th/5th Street

Movement	EB	WB	SB
Directions Served	TR	T	LTR
Maximum Queue (ft)	68	5	432
Average Queue (ft)	4	0	292
95th Queue (ft)	39	5	523
Link Distance (ft)	1877	100	430
Upstream Blk Time (%)			33
Queuing Penalty (veh)			0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 4000: 6th/5th Street

Movement	B9922	B9922
Directions Served	T	
Maximum Queue (ft)	406	208
Average Queue (ft)	29	8
95th Queue (ft)	210	105
Link Distance (ft)	423	423
Upstream Blk Time (%)	0	0
Queuing Penalty (veh)	1	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 5000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	78	259	139	140
Average Queue (ft)	16	88	51	67
95th Queue (ft)	119	223	138	122
Link Distance (ft)	292	908	318	286
Upstream Blk Time (%)	1		0	
Queuing Penalty (veh)	7		0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

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Intersection: 6000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	15	402	263	199
Average Queue (ft)	0	100	109	67
95th Queue (ft)	9	336	264	175
Link Distance (ft)	580	514	362	366
Upstream Blk Time (%)		0	4	1
Queuing Penalty (veh)		0	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 7000: 6th/5th Street

Movement	EB	NB
Directions Served	TR	LTR
Maximum Queue (ft)	216	37
Average Queue (ft)	16	7
95th Queue (ft)	129	29
Link Distance (ft)	687	267
Upstream Blk Time (%)	0	
Queuing Penalty (veh)	0	
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 8000: 6th/5th Street

Movement	EB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	10	83	61
Average Queue (ft)	0	35	19
95th Queue (ft)	8	67	48
Link Distance (ft)	702	326	341
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 9000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	20	169	236	141
Average Queue (ft)	2	19	153	55
95th Queue (ft)	18	153	270	123
Link Distance (ft)	558	528	210	316
Upstream Blk Time (%)		0	34	1
Queuing Penalty (veh)		3	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 5401

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	4:15	4:15	4:15	4:15	4:15	4:15	4:15
End Time	5:30	5:30	5:30	5:30	5:30	5:30	5:30
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	24255	24468	24426	24316	24210	24414	24344
Vehs Exited	23540	23581	23690	23621	23607	23727	23681
Starting Vehs	1237	1279	1235	1245	1385	1200	1296
Ending Vehs	1952	2166	1971	1940	1988	1887	1959
Denied Entry After	297	271	151	224	333	314	326
Travel Distance (mi)	25714	25819	25602	25540	25519	25667	25585
Travel Time (hr)	1835.7	1905.1	1759.2	1785.4	1852.3	1823.7	1845.0
Total Delay (hr)	1037.1	1101.2	964.1	992.3	1059.9	1027.5	1050.8
Total Stops	38969	42938	38891	37766	39454	37008	38289
Fuel Used (gal)	1028.9	1046.1	1008.9	1013.0	1027.3	1022.8	1026.9

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	4:15	4:15	4:15	4:15
End Time	5:30	5:30	5:30	5:30
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	24274	24510	24524	24375
Vehs Exited	23448	23734	23847	23645
Starting Vehs	1219	1319	1431	1281
Ending Vehs	2045	2095	2108	1998
Denied Entry After	410	303	397	299
Travel Distance (mi)	25444	25718	25805	25641
Travel Time (hr)	1946.3	1970.1	2019.4	1874.2
Total Delay (hr)	1156.3	1172.1	1218.6	1078.0
Total Stops	38797	41621	39964	39368
Fuel Used (gal)	1046.0	1061.6	1070.3	1035.2

Interval #0 Information Seeding

Start Time	4:15
End Time	4:30
Total Time (min)	15

No data recorded this interval.

Interval #1 Information Recording

Start Time	4:30
End Time	5:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	24255	24468	24426	24316	24210	24414	24344
Vehs Exited	23540	23581	23690	23621	23607	23727	23681
Starting Vehs	1237	1279	1235	1245	1385	1200	1296
Ending Vehs	1952	2166	1971	1940	1988	1887	1959
Denied Entry After	297	271	151	224	333	314	326
Travel Distance (mi)	25714	25819	25602	25540	25519	25667	25585
Travel Time (hr)	1835.7	1905.1	1759.2	1785.4	1852.3	1823.7	1845.0
Total Delay (hr)	1037.1	1101.2	964.1	992.3	1059.9	1027.5	1050.8
Total Stops	38969	42938	38891	37766	39454	37008	38289
Fuel Used (gal)	1028.9	1046.1	1008.9	1013.0	1027.3	1022.8	1026.9

Interval #1 Information Recording

Start Time	4:30
End Time	5:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	24274	24510	24524	24375
Vehs Exited	23448	23734	23847	23645
Starting Vehs	1219	1319	1431	1281
Ending Vehs	2045	2095	2108	1998
Denied Entry After	410	303	397	299
Travel Distance (mi)	25444	25718	25805	25641
Travel Time (hr)	1946.3	1970.1	2019.4	1874.2
Total Delay (hr)	1156.3	1172.1	1218.6	1078.0
Total Stops	38797	41621	39964	39368
Fuel Used (gal)	1046.0	1061.6	1070.3	1035.2

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	34	120	21	0	0	0	0	0	0	4	24	3

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	206

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Entry After	0	11	0	0	0	0	0	0	0	0	0	11

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	4	0	0	0	0	2	10	1

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	17

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Entry After	0	0	0	0	0	0	0

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

7: 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

10: 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

40: Country Club Rd Performance by movement

Movement	EBT	EBR	NBT	SBT	All
Denied Entry After	0	0	0	0	0

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWR	All
Denied Entry After	0	0	0	0

100: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

111: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

201: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	8	22	7	7	44

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBR	All
Denied Entry After	0	0	0	0	0

1100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Entry After	0	0	0	0	0	0

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Entry After	0	0	0	0	0	0

1400: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Entry After	0	0	0	0	0	0

3000: 6th/5th Street Performance by movement

Movement	EBT	WBT	SBL	SBR	All
Denied Entry After	0	0	5	3	8

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	All
Denied Entry After	0	0	0	0	0

5000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0

6000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0

7000: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	All
Denied Entry After	0	0	0	0	0

8000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Entry After	0	0	0	0	0

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Entry After	0	0	0	11	2	13

Total Network Performance

Denied Entry After						299
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Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	487.8	653.1	0.6	4
	1000	11.5	19.8	0.1	12
	2	98.9	146.5	0.3	8
Wilson HAWK	20	5.3	7.8	0.0	9
6th Side Street	201	25.8	33.3	0.1	7
Tucson Blvd	3	37.7	52.5	0.1	6
	3000	9.5	52.7	0.4	25
6th/5th Street	72	2.3	7.3	0.0	15
Country Club Rd	4	31.8	47.5	0.1	9
	9922	6.8	18.8	0.1	18
	4000	6.9	36.0	0.2	25
Dodge Blvd	5	21.9	68.4	0.4	21
	5000	3.4	11.7	0.1	21
Alvernon Wy	6	41.2	62.2	0.2	11
	6000	3.5	18.3	0.1	25
	70	7.9	20.8	0.1	18
	7	0.9	2.9	0.0	24
	7000	3.2	20.1	0.1	25
Columbus Blvd	8	14.7	26.5	0.1	14
	8000	3.2	20.4	0.1	26
Swan Rd	9	77.7	118.9	0.4	11
	9000	8.4	23.5	0.1	19
	100	10.3	23.5	0.1	17
	10	1.3	3.3	0.0	21
	1100	3.3	17.6	0.1	25
Rosemont Blvd	11	24.3	39.4	0.1	12
	111	5.2	33.6	0.2	25
Craycroft Rd	12	47.2	77.5	0.3	12
	1200	8.4	36.8	0.2	23
	13	6.1	30.0	0.2	24
Chantilly Dr HAWK	130	1.7	4.2	0.0	16
Sahuara Ave	14	1.3	8.2	0.1	27
	1400	2.6	30.9	0.2	27
Wilmot Rd	15	37.5	65.0	0.3	15
Total		1059.7	1839.0	5.6	12

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	34.3	132.9	0.4	12
	1400	1.7	27.5	0.3	35
Sahuara Ave	14	2.5	29.7	0.2	29
	130	3.4	10.7	0.1	20
	13	0.5	2.5	0.0	27
	1200	2.1	25.9	0.2	28
Craycroft Rd	12	28.3	55.7	0.2	15
	111	6.2	37.4	0.3	25
Rosemont Blvd	11	12.9	40.8	0.2	21
	1100	5.0	20.3	0.1	22
	10	6.4	20.7	0.1	21
Arcadia Ave HAWK	100	2.1	4.7	0.0	15
	9000	3.3	15.9	0.1	25
Swan Rd	9	31.8	45.6	0.1	10
	8000	6.7	48.5	0.4	27
Columbus Blvd	8	10.4	27.4	0.1	19
	7000	4.9	17.1	0.1	22
	7	6.0	22.8	0.1	22
Irving Ave HAWK	70	1.9	4.5	0.0	15
	6000	8.1	20.6	0.1	19
Alvernon Wy	6	53.1	67.1	0.1	7
	5000	8.1	29.7	0.2	23
Dodge Blvd	5	12.4	20.2	0.1	12
	4000	6.9	54.9	0.4	27
	9922	3.6	32.9	0.2	27
Country Club Rd	4	30.0	40.7	0.1	8
6th/5th Street	72	13.7	30.0	0.1	14
	3000	0.6	10.7	0.0	10
Tucson Blvd	3	86.7	133.7	0.4	10
6th Side Street	201	4.1	13.6	0.1	21
Wilson HAWK	20	5.7	13.8	0.1	17
	2	0.7	2.7	0.0	25
	1000	6.6	39.1	0.3	25
Campbell Ave	1	32.8	39.5	0.1	6
Total		443.7	1139.7	5.5	17

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	425	3033	3037	200	232	272	267	362	410	412	391	229
Average Queue (ft)	407	2333	2325	170	143	212	215	142	281	283	258	43
95th Queue (ft)	530	3667	3653	280	244	282	277	277	369	375	349	145
Link Distance (ft)		2992	2992			233	233		2020	2020	2020	
Upstream Blk Time (%)		43	42		1	6	6					
Queuing Penalty (veh)		0	0		0	27	31					
Storage Bay Dist (ft)	250			50	180			290				150
Storage Blk Time (%)	3	87	55	6	8	14		0	6		30	
Queuing Penalty (veh)	15	248	94	31	26	25		0	9		40	

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	350	2128	2127	2097
Average Queue (ft)	282	1534	1510	1461
95th Queue (ft)	476	2466	2432	2365
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)		18	13	13
Queuing Penalty (veh)		0	0	0
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	2	69		
Queuing Penalty (veh)	9	126		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	69	1403	34	56	88	33
Average Queue (ft)	5	960	9	6	24	10
95th Queue (ft)	47	1757	30	33	73	32
Link Distance (ft)		1396		55	442	447
Upstream Blk Time (%)		2	0	0		
Queuing Penalty (veh)		23	0	3		
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)		46	0	0		
Queuing Penalty (veh)		2	0	0		

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB	SB	SB
Directions Served	L	TR	L	TR	L	TR	L	TR
Maximum Queue (ft)	300	374	300	1254	399	835	400	1682
Average Queue (ft)	171	360	91	808	129	443	260	1065
95th Queue (ft)	348	370	268	1310	338	730	499	2036
Link Distance (ft)		356		1877		1948		2192
Upstream Blk Time (%)		32						10
Queuing Penalty (veh)		400						0
Storage Bay Dist (ft)	200		100		300		300	
Storage Blk Time (%)	1	47	2	48	6	31	12	57
Queuing Penalty (veh)	16	73	13	22	35	12	70	42

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	B9922	NB	NB	NB	NB
Directions Served	L	T	T	L	T	T	R	T	L	T	T	R
Maximum Queue (ft)	280	441	391	190	416	368	119	13	177	218	222	170
Average Queue (ft)	168	269	184	131	241	142	5	0	111	183	189	49
95th Queue (ft)	317	402	315	228	378	286	59	9	189	210	214	157
Link Distance (ft)		486	486		423	423		1235		177	177	
Upstream Blk Time (%)		0	0		0	0			1	20	22	0
Queuing Penalty (veh)		0	0		1	0			0	109	120	0
Storage Bay Dist (ft)	180			90			160		100			70
Storage Blk Time (%)	2	24		13	39	4	0		11	43	47	0
Queuing Penalty (veh)	6	47		36	48	9	0		47	63	39	0

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB	SB
Directions Served	L	T	TR
Maximum Queue (ft)	175	416	401
Average Queue (ft)	138	265	262
95th Queue (ft)	216	390	376
Link Distance (ft)		2000	2000
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)	100		
Storage Blk Time (%)	17	41	
Queuing Penalty (veh)	64	61	

Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB
Directions Served	T	R	L	T	L	R
Maximum Queue (ft)	498	285	291	306	213	162
Average Queue (ft)	271	91	94	259	92	66
95th Queue (ft)	447	267	216	341	176	134
Link Distance (ft)	2072				292	2124
Upstream Blk Time (%)			0	2		
Queuing Penalty (veh)			0	19		
Storage Bay Dist (ft)	110		300		220	
Storage Blk Time (%)	21	0	0	2	0	1
Queuing Penalty (veh)	32	0	0	3	0	1

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	TR	L	T	TR
Maximum Queue (ft)	399	714	285	400	596	285	260	827	829	260	552	551
Average Queue (ft)	247	399	71	126	442	141	163	547	549	182	368	369
95th Queue (ft)	453	722	238	361	698	336	326	887	887	324	535	529
Link Distance (ft)	908				580		2113		2113	1973		1973
Upstream Blk Time (%)	2				8							
Queuing Penalty (veh)	16				68							
Storage Bay Dist (ft)	300	110		300	110		160			160		
Storage Blk Time (%)	15	39	0			53	1	2	53	15	41	
Queuing Penalty (veh)	105	104	0			118	9	9	51	78	51	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	30	56	37	327	45	40
Average Queue (ft)	4	7	1	73	16	13
95th Queue (ft)	19	37	22	236	43	36
Link Distance (ft)	56		687		311	327
Upstream Blk Time (%)	0					
Queuing Penalty (veh)	4					
Storage Bay Dist (ft)	100	100				
Storage Blk Time (%)	0		5			
Queuing Penalty (veh)	0		0			

Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	170	480	194	150	357	175	199	497	200	502
Average Queue (ft)	51	214	19	40	139	24	64	273	71	268
95th Queue (ft)	121	422	95	100	276	99	175	443	181	451
Link Distance (ft)		474			702			2075		1939
Upstream Blk Time (%)		1								
Queuing Penalty (veh)		6								
Storage Bay Dist (ft)	75		110	75		75	100		100	
Storage Blk Time (%)	5	22		4	13	0	1	41	8	40
Queuing Penalty (veh)	37	25		29	17	0	5	19	34	21

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	360	1285	254	404	559	210	305	922	929	450	376	725
Average Queue (ft)	169	727	84	195	329	76	158	556	554	226	270	447
95th Queue (ft)	393	1499	262	407	549	222	359	1004	999	526	447	867
Link Distance (ft)		1812			558			1988	1988			2061
Upstream Blk Time (%)		0		0	2							
Queuing Penalty (veh)		1		0	14							
Storage Bay Dist (ft)	300		110	500		110	180			350	230	
Storage Blk Time (%)	0	50	0	2	37		2	53	31		47	24
Queuing Penalty (veh)	0	100	1	11	85		10	54	57		244	33

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	717	370
Average Queue (ft)	432	67
95th Queue (ft)	837	252
Link Distance (ft)	2061	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	15	
Queuing Penalty (veh)	12	

Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	36	64	127	352	158	96
Average Queue (ft)	9	11	22	88	53	28
95th Queue (ft)	31	45	75	264	127	76
Link Distance (ft)		54		582	279	312
Upstream Blk Time (%)	0	1			1	
Queuing Penalty (veh)	0	5			0	
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0	1	0	6		
Queuing Penalty (veh)	0	0	0	2		

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	134	611	180	178	394	192	250	555	224	375
Average Queue (ft)	70	386	58	41	144	24	130	282	69	205
95th Queue (ft)	141	637	170	104	287	110	264	480	174	342
Link Distance (ft)		604			1181			1974		2044
Upstream Blk Time (%)		1								
Queuing Penalty (veh)		8								
Storage Bay Dist (ft)	75		110	100		110	150		150	
Storage Blk Time (%)	7	32		1	11		6	35	5	20
Queuing Penalty (veh)	68	67		6	9		22	34	17	11

Queuing and Blocking Report

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Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	400	874	210	415	496	210	330	857	858	230	299	507
Average Queue (ft)	158	395	111	211	206	52	218	486	483	137	94	339
95th Queue (ft)	371	767	253	409	392	164	387	987	969	294	257	481
Link Distance (ft)		1293			1162			1958	1958			2072
Upstream Blk Time (%)		0										
Queuing Penalty (veh)		0										
Storage Bay Dist (ft)	300		110	500		110	180			180	150	
Storage Blk Time (%)		46	1	1	26		36	37	38		1	38
Queuing Penalty (veh)		143	8	3	62		217	40	49		5	23

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	499	300
Average Queue (ft)	340	118
95th Queue (ft)	484	325
Link Distance (ft)	2072	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	38	0
Queuing Penalty (veh)	37	0

Intersection: 13: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	TR	L	TR	LTR	LTR
Maximum Queue (ft)	300	41	57	101	24
Average Queue (ft)	64	10	3	42	2
95th Queue (ft)	214	34	25	78	14
Link Distance (ft)	1017		56	429	480
Upstream Blk Time (%)		0	0		
Queuing Penalty (veh)		0	1		
Storage Bay Dist (ft)		100			
Storage Blk Time (%)	4	0	0		
Queuing Penalty (veh)	0	0	0		

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	LTR	LTR	LTR	LTR
Maximum Queue (ft)	108	96	33	40
Average Queue (ft)	15	10	9	15
95th Queue (ft)	62	51	31	40
Link Distance (ft)	275	1189	622	706
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	240	388	243	110	349	388	394	379	229	370	378	350
Average Queue (ft)	133	167	100	39	190	254	256	235	47	244	240	205
95th Queue (ft)	215	290	199	87	313	371	371	355	148	351	350	324
Link Distance (ft)		1302		2255		1994	1994	1994		2040	2040	2040
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	145		145		240				130			
Storage Blk Time (%)	7	16	1		4	10				30		1
Queuing Penalty (veh)	40	63	4		19	25				13		1

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	116
Average Queue (ft)	54
95th Queue (ft)	96
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	85	310
Average Queue (ft)	58	113
95th Queue (ft)	79	310
Link Distance (ft)	55	298
Upstream Blk Time (%)	32	1
Queuing Penalty (veh)	376	8
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 40: Country Club Rd

Movement	EB	NB	NB	SB
Directions Served	R	T	T	T
Maximum Queue (ft)	125	290	286	9
Average Queue (ft)	54	107	111	0
95th Queue (ft)	98	235	239	7
Link Distance (ft)	450	2095	2095	177
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	449	84
Average Queue (ft)	132	34
95th Queue (ft)	374	86
Link Distance (ft)	514	56
Upstream Blk Time (%)		8
Queuing Penalty (veh)		59
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

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Intersection: 72: 6th/5th Street

Movement	EB	SW	SW
Directions Served	L	R	R
Maximum Queue (ft)	26	485	5
Average Queue (ft)	1	83	0
95th Queue (ft)	18	259	5
Link Distance (ft)	100	486	486
Upstream Blk Time (%)	0	0	
Queuing Penalty (veh)	0	1	
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 100: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	480	79
Average Queue (ft)	156	31
95th Queue (ft)	415	79
Link Distance (ft)	528	54
Upstream Blk Time (%)	0	11
Queuing Penalty (veh)	1	73
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 111: 6th/5th Street

Movement	EB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	17	126	132
Average Queue (ft)	1	50	57
95th Queue (ft)	16	101	106
Link Distance (ft)	1181	340	283
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	73	221
Average Queue (ft)	29	49
95th Queue (ft)	77	151
Link Distance (ft)	56	275
Upstream Blk Time (%)	7	0
Queuing Penalty (veh)	51	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 201: 6th Side Street & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	317	144	448	445
Average Queue (ft)	299	15	350	329
95th Queue (ft)	356	77	573	540
Link Distance (ft)	298	356	433	422
Upstream Blk Time (%)	22		60	44
Queuing Penalty (veh)	264		0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1000: 6th/5th Street

Movement	EB	EB	WB	SB
Directions Served	T	R	LTR	LTR
Maximum Queue (ft)	277	268	334	111
Average Queue (ft)	121	71	69	39
95th Queue (ft)	309	248	232	82
Link Distance (ft)	233	233	1396	415
Upstream Blk Time (%)	3	1		
Queuing Penalty (veh)	20	8		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

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Intersection: 1100: 6th/5th Street

Movement	EB	NB	SB
Directions Served	T	LTR	LTR
Maximum Queue (ft)	203	168	146
Average Queue (ft)	20	63	52
95th Queue (ft)	107	129	116
Link Distance (ft)	582	226	197
Upstream Blk Time (%)		0	0
Queuing Penalty (veh)		0	0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1200: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	143	93
Average Queue (ft)	59	42
95th Queue (ft)	112	74
Link Distance (ft)	214	281
Upstream Blk Time (%)	0	
Queuing Penalty (veh)	0	
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1400: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	99	68
Average Queue (ft)	43	32
95th Queue (ft)	78	60
Link Distance (ft)	307	277
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 3000: 6th/5th Street

Movement	EB	WB	SB
Directions Served	TR	T	LTR
Maximum Queue (ft)	68	5	432
Average Queue (ft)	4	0	292
95th Queue (ft)	39	5	523
Link Distance (ft)	1877	100	430
Upstream Blk Time (%)			33
Queuing Penalty (veh)			0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 4000: 6th/5th Street

Movement	B9922	B9922
Directions Served	T	
Maximum Queue (ft)	406	208
Average Queue (ft)	29	8
95th Queue (ft)	210	105
Link Distance (ft)	423	423
Upstream Blk Time (%)	0	0
Queuing Penalty (veh)	1	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 5000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	78	259	139	140
Average Queue (ft)	16	88	51	67
95th Queue (ft)	119	223	138	122
Link Distance (ft)	292	908	318	286
Upstream Blk Time (%)	1		0	
Queuing Penalty (veh)	7		0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 6000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	15	402	263	199
Average Queue (ft)	0	100	109	67
95th Queue (ft)	9	336	264	175
Link Distance (ft)	580	514	362	366
Upstream Blk Time (%)		0	4	1
Queuing Penalty (veh)		0	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 7000: 6th/5th Street

Movement	EB	NB
Directions Served	TR	LTR
Maximum Queue (ft)	216	37
Average Queue (ft)	16	7
95th Queue (ft)	129	29
Link Distance (ft)	687	267
Upstream Blk Time (%)	0	
Queuing Penalty (veh)	0	
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 8000: 6th/5th Street

Movement	EB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	10	83	61
Average Queue (ft)	0	35	19
95th Queue (ft)	8	67	48
Link Distance (ft)	702	326	341
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 9000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	20	169	236	141
Average Queue (ft)	2	19	153	55
95th Queue (ft)	18	153	270	123
Link Distance (ft)	558	528	210	316
Upstream Blk Time (%)		0	34	1
Queuing Penalty (veh)		3	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 5401



Appendix K
2022 Road Diet Scenario B
Simtraffic Reports

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	7:15	7:15	7:15	7:15	7:15	7:15	7:15
End Time	8:30	8:30	8:30	8:30	8:30	8:30	8:30
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	20988	20817	20822	21011	21009	20878	20865
Vehs Exited	20715	20584	20583	20723	20676	20656	20659
Starting Vehs	885	902	928	931	1005	962	1019
Ending Vehs	1158	1135	1167	1219	1338	1184	1225
Travel Distance (mi)	22023	21957	22084	22223	22117	22013	22050
Travel Time (hr)	1155.7	1110.7	1181.5	1213.9	1224.1	1163.5	1229.2
Total Delay (hr)	469.7	426.6	491.9	521.4	534.6	477.2	540.8
Total Stops	26583	25674	27008	29184	28420	27436	28017
Fuel Used (gal)	786.8	777.0	792.9	805.2	804.4	788.9	802.9

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	7:15	7:15	7:15	7:15
End Time	8:30	8:30	8:30	8:30
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	20854	21162	21195	20965
Vehs Exited	20627	20796	20679	20669
Starting Vehs	1045	910	912	931
Ending Vehs	1272	1276	1428	1217
Travel Distance (mi)	22100	22219	22300	22108
Travel Time (hr)	1287.7	1188.7	1207.7	1196.3
Total Delay (hr)	598.6	497.2	512.9	507.1
Total Stops	29925	27780	28350	27836
Fuel Used (gal)	817.4	799.2	807.0	798.2

Interval #0 Information Seeding

Start Time	7:15
End Time	7:30
Total Time (min)	15
No data recorded this interval.	

Interval #1 Information Recording

Start Time	7:30
End Time	8:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	20988	20817	20822	21011	21009	20878	20865
Vehs Exited	20715	20584	20583	20723	20676	20656	20659
Starting Vehs	885	902	928	931	1005	962	1019
Ending Vehs	1158	1135	1167	1219	1338	1184	1225
Travel Distance (mi)	22023	21957	22084	22223	22117	22013	22050
Travel Time (hr)	1155.7	1110.7	1181.5	1213.9	1224.1	1163.5	1229.2
Total Delay (hr)	469.7	426.6	491.9	521.4	534.6	477.2	540.8
Total Stops	26583	25674	27008	29184	28420	27436	28017
Fuel Used (gal)	786.8	777.0	792.9	805.2	804.4	788.9	802.9

Interval #1 Information Recording

Start Time	7:30
End Time	8:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	20854	21162	21195	20965
Vehs Exited	20627	20796	20679	20669
Starting Vehs	1045	910	912	931
Ending Vehs	1272	1276	1428	1217
Travel Distance (mi)	22100	22219	22300	22108
Travel Time (hr)	1287.7	1188.7	1207.7	1196.3
Total Delay (hr)	598.6	497.2	512.9	507.1
Total Stops	29925	27780	28350	27836
Fuel Used (gal)	817.4	799.2	807.0	798.2

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.5	0.2	0.4	0.0	0.0	0.0	1.3	0.2	1.2	1.3	0.1	0.2
Total Del/Veh (s)	63.6	35.5	8.5	46.1	40.2	36.1	96.4	40.0	7.0	51.0	40.5	31.3

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	40.9

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.4	0.1	0.0		0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	38.8	6.3	4.6		1.3	0.2	89.1	16.4	185.8	96.7	85.8	5.5

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.9	0.4	0.4	1.6	0.4	0.4
Total Del/Veh (s)	70.5	23.1	20.1	106.5	98.6	102.9	281.8	55.8	49.3	103.7	58.5	56.6

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	74.7

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.5	0.0	0.1	0.0	0.0	0.1	1.7	0.2	0.2	0.1
Total Del/Veh (s)	51.5	18.4	28.0	21.7	3.7	51.0	38.9	4.8	50.5	41.6	31.7	30.0

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.3	1.3	0.1
Total Del/Veh (s)	14.4	7.8	11.2	9.8	51.8	12.2	13.4

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.4	0.2	0.2	1.5	0.2	0.3
Total Del/Veh (s)	183.8	35.2	16.0	56.4	63.9	38.8	115.3	45.7	40.5	68.5	44.7	42.9

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	54.1

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	20.4	1.0	0.3	10.9	10.0	7.0	38.7	5.3	114.1	47.8	101.8	7.6

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	2.0	0.4	0.4	2.1	0.4	0.4
Total Del/Veh (s)	33.0	13.1	5.8	23.6	13.4	7.5	69.8	40.6	33.1	66.3	39.0	32.2

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	25.4

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.8	0.3	1.9	1.8	0.2	1.6
Total Del/Veh (s)	68.1	30.3	12.7	54.4	40.0	22.0	49.4	34.9	13.9	58.0	32.2	14.2

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	35.1

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.3		0.1	0.1	0.0
Total Del/Veh (s)	6.0	2.4	1.0	20.5	6.3	6.2	13.4		47.9	22.0	7.1

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.7	0.3	0.6	0.5	0.2	0.3	1.9	0.4	0.4	1.7	0.4	0.4
Total Del/Veh (s)	53.5	19.4	10.4	31.3	20.2	11.1	93.2	33.9	28.6	50.4	39.6	36.6

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	29.4

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.7	0.3	1.7	1.7	0.2	1.6
Total Del/Veh (s)	54.5	34.5	10.6	41.4	42.8	22.6	48.1	32.8	13.5	65.5	28.1	14.3

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	34.1

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	3.7	2.4	4.3	0.5	12.9	8.9	2.1

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.2	0.1	0.1	0.1	0.1	0.1
Total Del/Veh (s)	8.1	1.8	0.9	8.5	5.0	4.0	18.6	17.2	7.9	17.8	16.0	5.9

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	4.4

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.2	1.6	0.2	0.3	1.4	0.2	1.3
Total Del/Veh (s)	49.9	48.8	12.1	44.6	43.6	26.5	52.6	23.8	15.0	35.5	24.0	10.5

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	26.5

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	1.6	1.0
Total Del/Veh (s)	1.5	9.2	6.4

40: Country Club Rd Performance by movement

Movement	EBR	NBT	SBT	All
Denied Del/Veh (s)	0.0	0.2	0.0	0.1
Total Del/Veh (s)	7.1	2.6	2.0	2.8

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	5.7	2.2	3.6

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWT	SWR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0
Total Del/Veh (s)	3.2	0.6	1.7	8.7	6.4

100: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.1	0.0
Total Del/Veh (s)	11.6	2.3	7.4

111: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	3.6	3.0	5.3	4.4	5.6	9.9	4.8

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.6	3.9	2.9

201: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.7	0.7	332.7	345.4	287.8	248.4	37.4
Total Del/Veh (s)	3.7	2.3	8.3	6.6	485.9	418.9	473.4	478.8	52.8

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	3.3	4.6	0.2
Total Del/Veh (s)	4.3	2.0	47.3	46.3	203.8	200.2	39.5

1100: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	2.1	1.7	3.5	2.8	28.6	14.3	3.7

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	4.3	3.3	2.1	15.5	6.7	3.3

1400: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.0
Total Del/Veh (s)	3.6	2.5	2.9	2.3	15.6	6.1	5.8	3.5

3000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	10.2	13.7	45.4	31.3	2.6
Total Del/Veh (s)	6.7	5.4	2.0	0.1	197.0	145.4	195.1	167.3	19.9

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.1	0.1	0.2	0.0
Total Del/Veh (s)	2.4	1.6	7.0	6.1	37.6	21.6	46.0	35.3	8.7

5000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.1	0.0	0.0	0.1	0.1	0.1	3.8	2.4	0.2
Total Del/Veh (s)	3.7	2.1	7.2	5.7	48.6	20.3	59.7	42.6	9.0

6000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	1.4	7.9	5.5	0.3
Total Del/Veh (s)	3.7	2.9	18.5	16.4	157.9	80.9	157.5	19.2

7000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.2	0.2	0.2	0.2	0.1	0.1
Total Del/Veh (s)	2.7	1.9	3.6	2.5	27.7	13.8	30.0	17.9	4.8

8000: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.1	0.1	0.1	0.1	0.1	0.1	0.1
Total Del/Veh (s)	2.0	7.3	6.0	24.7	10.4	12.4	5.5

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.1	0.1	0.1	0.2	0.1
Total Del/Veh (s)	3.8	2.7	2.9	1.9	30.3	17.9	32.1	16.4	5.2

Total Network Performance

Denied Del/Veh (s)	4.7
Total Del/Veh (s)	78.9

Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	35.5	102.3	0.6	20
	1000	5.8	14.0	0.1	16
	2	6.5	38.1	0.3	26
Wilson HAWK	20	1.5	4.1	0.0	17
6th Side Street	201	3.7	11.0	0.1	21
Tucson Blvd	3	23.6	32.7	0.1	9
	3000	7.3	49.8	0.4	27
6th/5th Street	72	3.3	6.9	0.0	16
Country Club Rd	4	18.4	31.5	0.1	13
	9922	3.8	15.8	0.1	22
	4000	2.4	31.7	0.2	28
Dodge Blvd	5	14.9	62.4	0.4	23
	5000	4.0	12.3	0.1	20
Alvernon Wy	6	35.5	56.9	0.2	12
	6000	4.5	19.7	0.1	23
	70	5.8	18.9	0.1	20
	7	1.0	3.0	0.0	23
	7000	2.7	19.7	0.1	26
Columbus Blvd	8	12.0	23.8	0.1	15
	8000	2.2	19.2	0.1	27
Swan Rd	9	30.1	71.3	0.4	18
	9000	4.7	19.7	0.1	23
	100	11.7	25.0	0.1	16
	10	2.4	4.4	0.0	16
	1100	2.4	16.5	0.1	26
Rosemont Blvd	11	19.4	34.0	0.1	13
	111	4.0	31.3	0.2	27
Craycroft Rd	12	33.9	64.1	0.3	15
	1200	5.6	34.1	0.2	25
	13	3.7	28.1	0.2	26
Chantilly Dr HAWK	130	1.6	4.2	0.0	16
Sahuara Ave	14	1.8	8.8	0.1	25
	1400	3.9	32.2	0.2	26
Wilmot Rd	15	49.0	79.3	0.3	12
Total		368.3	1026.7	5.6	20

Arterial Level of Service
Scenario B

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Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	43.6	95.2	0.4	17
	1400	4.0	31.9	0.3	30
Sahuara Ave	14	5.1	33.4	0.2	25
	130	3.9	11.4	0.1	19
	13	0.5	2.5	0.0	27
	1200	2.1	26.5	0.2	28
Craycroft Rd	12	42.9	70.5	0.2	12
	111	6.7	38.1	0.3	25
Rosemont Blvd	11	20.3	47.5	0.2	18
	1100	4.2	19.0	0.1	24
	10	6.5	20.6	0.1	21
Arcadia Ave HAWK	100	2.3	4.9	0.0	14
	9000	2.9	15.6	0.1	25
Swan Rd	9	39.8	53.6	0.1	8
	8000	8.5	51.0	0.4	26
Columbus Blvd	8	12.8	29.6	0.1	18
	7000	4.1	16.2	0.1	23
	7	10.0	26.9	0.1	19
Irving Ave HAWK	70	2.1	4.8	0.0	14
	6000	18.5	31.1	0.1	12
Alvernon Wy	6	64.1	78.3	0.1	6
	5000	9.1	31.4	0.2	21
Dodge Blvd	5	10.4	18.3	0.1	13
	4000	7.3	55.3	0.4	26
	9922	4.1	33.4	0.2	26
Country Club Rd	4	21.7	32.6	0.1	11
6th/5th Street	72	9.9	23.6	0.1	18
	3000	2.0	6.2	0.0	18
Tucson Blvd	3	98.8	140.3	0.4	9
6th Side Street	201	9.0	19.1	0.1	16
Wilson HAWK	20	9.5	19.0	0.1	14
	2	1.3	3.4	0.0	20
	1000	47.2	78.8	0.3	13
Campbell Ave	1	40.9	47.6	0.1	5
Total		576.2	1217.5	5.5	16

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	249	278	267	198	232	276	281	336	407	397	336	88
Average Queue (ft)	136	166	142	58	137	239	241	195	253	250	203	32
95th Queue (ft)	232	250	235	144	253	274	271	343	430	405	297	65
Link Distance (ft)		2992	2992			233	233		2020	2020	2020	
Upstream Blk Time (%)					1	18	23					
Queuing Penalty (veh)					0	115	144					
Storage Bay Dist (ft)	250			50	180			290				150
Storage Blk Time (%)	2	1	32	4	3	33		7	1		18	0
Queuing Penalty (veh)	4	1	39	9	15	67		26	2		28	0

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	216	333	350	343
Average Queue (ft)	70	228	232	230
95th Queue (ft)	145	310	320	317
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	0	14		
Queuing Penalty (veh)	0	11		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	100	332	6	60	52	118
Average Queue (ft)	23	73	0	17	14	45
95th Queue (ft)	66	241	5	58	40	119
Link Distance (ft)		1396		55	442	447
Upstream Blk Time (%)				1		
Queuing Penalty (veh)				19		
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)		4		1		
Queuing Penalty (veh)		1		0		

Queuing and Blocking Report
Scenario B

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Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB	SB	SB
Directions Served	L	TR	L	TR	L	TR	L	TR
Maximum Queue (ft)	283	367	275	1828	287	503	356	525
Average Queue (ft)	79	252	66	1148	159	264	103	296
95th Queue (ft)	202	400	230	2045	338	558	249	461
Link Distance (ft)		356		1877		1948		2192
Upstream Blk Time (%)		4		1				
Queuing Penalty (veh)		31		12				
Storage Bay Dist (ft)	200		100		300		300	
Storage Blk Time (%)		18	0	42	12	4	0	13
Queuing Penalty (veh)		12	4	21	37	3	1	9

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	B9922	NB	NB	NB	NB
Directions Served	L	T	T	L	T	T	R	T	L	T	T	R
Maximum Queue (ft)	207	268	201	190	411	359	180	104	176	196	198	154
Average Queue (ft)	98	137	74	67	207	135	11	5	89	139	140	21
95th Queue (ft)	180	247	169	177	360	301	92	76	158	203	205	90
Link Distance (ft)		486	486		423	423		1235		177	177	
Upstream Blk Time (%)					1	0			0	4	5	0
Queuing Penalty (veh)					3	2			0	13	14	0
Storage Bay Dist (ft)	180			90			160		100			70
Storage Blk Time (%)	1	4		0	34	4	0		7	23	33	0
Queuing Penalty (veh)	2	6		1	23	8	0		16	26	11	0

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB	SB
Directions Served	L	T	TR
Maximum Queue (ft)	174	336	348
Average Queue (ft)	99	171	198
95th Queue (ft)	183	282	303
Link Distance (ft)		2000	2000
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)	100		
Storage Blk Time (%)	10	26	
Queuing Penalty (veh)	19	32	

Queuing and Blocking Report
Scenario B

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Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB
Directions Served	T	R	L	T	L	R
Maximum Queue (ft)	397	171	192	302	151	102
Average Queue (ft)	133	31	48	229	68	36
95th Queue (ft)	343	126	116	346	125	81
Link Distance (ft)	2072				292	2124
Upstream Blk Time (%)			0	1		
Queuing Penalty (veh)			0	12		
Storage Bay Dist (ft)	110		300		220	
Storage Blk Time (%)	10			0	1	0
Queuing Penalty (veh)	11			0	1	0

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	TR	L	T	TR
Maximum Queue (ft)	333	576	285	400	597	285	260	618	613	260	508	523
Average Queue (ft)	189	287	66	132	515	133	205	353	354	147	329	333
95th Queue (ft)	376	576	210	384	715	338	321	538	534	297	481	485
Link Distance (ft)	908				580		2113		2113	1973		1973
Upstream Blk Time (%)	2				16							
Queuing Penalty (veh)	11				135							
Storage Bay Dist (ft)	300	110		300		110		160		160		
Storage Blk Time (%)	13	29	0			55	0	30	36	2	35	
Queuing Penalty (veh)	72	63	1			102	3	160	59	9	42	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	14	54	168	441	34	77
Average Queue (ft)	1	8	30	129	12	24
95th Queue (ft)	8	37	111	403	36	90
Link Distance (ft)	56		687		311	327
Upstream Blk Time (%)	0		0			
Queuing Penalty (veh)	2		3			
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0		9			
Queuing Penalty (veh)	0		5			

Queuing and Blocking Report
Scenario B

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Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	154	401	164	154	434	148	200	500	200	439
Average Queue (ft)	32	143	21	26	181	18	91	241	60	211
95th Queue (ft)	87	297	90	84	363	88	196	430	154	371
Link Distance (ft)		474			702			2075		1939
Upstream Blk Time (%)		0								
Queuing Penalty (veh)		1								
Storage Bay Dist (ft)	75		110	75		75	100		100	
Storage Blk Time (%)	1	20	0	0	21		6	33	2	31
Queuing Penalty (veh)	5	18	0	2	15		23	28	8	17

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	293	463	254	196	562	210	304	398	401	133	256	334
Average Queue (ft)	80	214	45	70	339	61	95	245	242	42	117	214
95th Queue (ft)	197	391	173	149	556	194	227	367	369	95	221	321
Link Distance (ft)		1812			558			1988	1988			2061
Upstream Blk Time (%)					1							
Queuing Penalty (veh)					6							
Storage Bay Dist (ft)	300		110	500		110	180			350	230	
Storage Blk Time (%)		29	0		43		1	21	1		1	7
Queuing Penalty (veh)		44	0		70		3	23	1		4	10

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	334	94
Average Queue (ft)	211	25
95th Queue (ft)	314	65
Link Distance (ft)	2061	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	1	
Queuing Penalty (veh)	1	

Queuing and Blocking Report
Scenario B

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Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	42	77	190	353	162	88
Average Queue (ft)	12	22	79	99	70	29
95th Queue (ft)	37	64	164	267	129	67
Link Distance (ft)		54		582	279	312
Upstream Blk Time (%)	0	1				
Queuing Penalty (veh)	0	11				
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0	1	5	5		
Queuing Penalty (veh)	1	0	33	10		

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	134	454	180	249	549	210	228	350	250	460
Average Queue (ft)	91	216	56	67	256	28	79	171	53	252
95th Queue (ft)	154	392	160	192	469	122	168	296	176	415
Link Distance (ft)		604			1181			1974		2044
Upstream Blk Time (%)		0								
Queuing Penalty (veh)		0								
Storage Bay Dist (ft)	75		110	100		110	150		150	
Storage Blk Time (%)	23	26	0	1	21		3	14		29
Queuing Penalty (veh)	131	57	0	4	22		11	11		11

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	250	344	181	236	591	210	329	477	466	230	267	344
Average Queue (ft)	96	148	34	59	296	76	126	280	282	92	96	192
95th Queue (ft)	183	285	121	155	505	219	287	435	439	251	201	304
Link Distance (ft)		1293			1162			1958	1958			2072
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300		110	500		110	180			180	150	
Storage Blk Time (%)	0	24	0		41	0	2	23	22		4	15
Queuing Penalty (veh)	0	49	0		68	0	9	34	22		15	17

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	336	262
Average Queue (ft)	189	47
95th Queue (ft)	302	145
Link Distance (ft)	2072	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	16	
Queuing Penalty (veh)	16	

Intersection: 13: 6th/5th Street

Movement	EB	WB	WB	NB
Directions Served	TR	L	TR	LTR
Maximum Queue (ft)	168	31	52	54
Average Queue (ft)	26	3	4	21
95th Queue (ft)	100	17	27	48
Link Distance (ft)	1017		56	429
Upstream Blk Time (%)		0	0	
Queuing Penalty (veh)		0	1	
Storage Bay Dist (ft)		100		
Storage Blk Time (%)	1	0	0	
Queuing Penalty (veh)	0	0	0	

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	LTR	LTR	LTR	LTR
Maximum Queue (ft)	87	287	91	39
Average Queue (ft)	4	56	40	14
95th Queue (ft)	40	183	71	38
Link Distance (ft)	275	1189	622	706
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	148	172	126	140	343	395	359	311	230	358	335	311
Average Queue (ft)	53	91	45	61	201	198	192	162	91	213	208	177
95th Queue (ft)	110	148	95	118	324	321	302	276	198	323	309	290
Link Distance (ft)		1302		2255		1994	1994	1994		2040	2040	2040
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	145		145		240				130			
Storage Blk Time (%)	0	1	0		8	3			2	24		0
Queuing Penalty (veh)	0	2	0		33	8			8	30		1

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	190
Average Queue (ft)	72
95th Queue (ft)	144
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Queuing and Blocking Report Scenario B

11/03/2022

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	77	315
Average Queue (ft)	30	175
95th Queue (ft)	77	392
Link Distance (ft)	55	298
Upstream Blk Time (%)	6	3
Queuing Penalty (veh)	45	45
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 40: Country Club Rd

Movement	EB	NB	NB
Directions Served	R	T	T
Maximum Queue (ft)	77	98	99
Average Queue (ft)	38	12	13
95th Queue (ft)	64	57	59
Link Distance (ft)	450	2095	2095
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	323	86
Average Queue (ft)	74	37
95th Queue (ft)	235	89
Link Distance (ft)	514	56
Upstream Blk Time (%)		10
Queuing Penalty (veh)		88
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report Scenario B

11/03/2022

Intersection: 72: 6th/5th Street

Movement	SW	SW
Directions Served	R	R
Maximum Queue (ft)	406	300
Average Queue (ft)	65	26
95th Queue (ft)	319	209
Link Distance (ft)	486	486
Upstream Blk Time (%)	1	0
Queuing Penalty (veh)	6	1
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 100: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	458	77
Average Queue (ft)	145	33
95th Queue (ft)	378	80
Link Distance (ft)	528	54
Upstream Blk Time (%)	0	11
Queuing Penalty (veh)	0	67
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 111: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	56	68
Average Queue (ft)	28	32
95th Queue (ft)	52	57
Link Distance (ft)	340	283
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	71	257
Average Queue (ft)	26	64
95th Queue (ft)	71	187
Link Distance (ft)	56	275
Upstream Blk Time (%)	4	0
Queuing Penalty (veh)	20	1
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 201: 6th Side Street & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	236	360	446	457
Average Queue (ft)	28	85	330	344
95th Queue (ft)	132	280	564	560
Link Distance (ft)	298	356	433	422
Upstream Blk Time (%)	0	0	53	53
Queuing Penalty (veh)	3	4	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1000: 6th/5th Street

Movement	EB	EB	WB	SB
Directions Served	T	R	LTR	LTR
Maximum Queue (ft)	167	80	1359	314
Average Queue (ft)	9	3	716	165
95th Queue (ft)	82	42	1440	355
Link Distance (ft)	233	233	1396	415
Upstream Blk Time (%)	0	0	0	5
Queuing Penalty (veh)	0	0	2	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

Scenario B

11/03/2022

Intersection: 1100: 6th/5th Street

Movement	EB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	22	70	82
Average Queue (ft)	1	24	35
95th Queue (ft)	19	57	67
Link Distance (ft)	582	226	197
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1200: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	52	40
Average Queue (ft)	17	13
95th Queue (ft)	44	38
Link Distance (ft)	214	281
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1400: 6th/5th Street

Movement	EB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	2	84	48
Average Queue (ft)	0	37	19
95th Queue (ft)	2	66	43
Link Distance (ft)	1189	307	277
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Queuing and Blocking Report
Scenario B

11/03/2022

Intersection: 3000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	T	LTR	LTR
Maximum Queue (ft)	20	82	267	338
Average Queue (ft)	1	19	107	151
95th Queue (ft)	12	85	284	361
Link Distance (ft)	1877	100	420	430
Upstream Blk Time (%)		1	5	9
Queuing Penalty (veh)		8	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 4000: 6th/5th Street

Movement	B9922	WB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	40	11	126	183
Average Queue (ft)	1	0	53	68
95th Queue (ft)	39	7	100	144
Link Distance (ft)	423	2072	554	590
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 5000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	52	262	104	169
Average Queue (ft)	10	64	38	72
95th Queue (ft)	95	201	81	151
Link Distance (ft)	292	908	318	286
Upstream Blk Time (%)	1			2
Queuing Penalty (veh)	7			0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

Scenario B

11/03/2022

Intersection: 6000: 6th/5th Street

Movement	WB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	513	174	175
Average Queue (ft)	224	68	57
95th Queue (ft)	559	197	172
Link Distance (ft)	514	362	366
Upstream Blk Time (%)	2	2	2
Queuing Penalty (veh)	20	0	0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 7000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	12	29	139	98
Average Queue (ft)	1	4	49	40
95th Queue (ft)	8	53	96	78
Link Distance (ft)	687	474	267	311
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 8000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	83	72
Average Queue (ft)	32	28
95th Queue (ft)	66	58
Link Distance (ft)	326	341
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario B

11/03/2022

Intersection: 9000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	34	130	109	146
Average Queue (ft)	1	9	40	57
95th Queue (ft)	20	76	82	109
Link Distance (ft)	558	528	210	316
Upstream Blk Time (%)			0	
Queuing Penalty (veh)			0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 2694

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	4:15	4:15	4:15	4:15	4:15	4:15	4:15
End Time	5:30	5:30	5:30	5:30	5:30	5:30	5:30
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	20317	20271	20501	20332	20271	20099	20467
Vehs Exited	20084	19990	20121	20022	19937	19774	20211
Starting Vehs	1023	1043	986	1003	1059	1027	1034
Ending Vehs	1256	1324	1366	1313	1393	1352	1290
Travel Distance (mi)	22748	22646	23053	22776	22425	22618	22896
Travel Time (hr)	1280.2	1215.6	1285.0	1290.4	1277.2	1259.2	1235.8
Total Delay (hr)	571.0	509.9	566.7	580.3	579.1	552.4	522.6
Total Stops	26707	27291	27774	28605	28852	28356	27949
Fuel Used (gal)	826.2	809.6	837.2	830.4	820.0	820.0	821.1

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	4:15	4:15	4:15	4:15
End Time	5:30	5:30	5:30	5:30
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	20493	20400	20293	20340
Vehs Exited	20168	20071	19989	20035
Starting Vehs	1135	1047	1015	1022
Ending Vehs	1460	1376	1319	1330
Travel Distance (mi)	22964	22853	22815	22779
Travel Time (hr)	1373.0	1318.7	1251.2	1278.6
Total Delay (hr)	658.3	606.6	538.8	568.6
Total Stops	30167	28219	28000	28191
Fuel Used (gal)	852.6	838.3	822.1	827.8

Interval #0 Information Seeding

Start Time	4:15
End Time	4:30
Total Time (min)	15
No data recorded this interval.	

Interval #1 Information Recording

Start Time	4:30
End Time	5:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	20317	20271	20501	20332	20271	20099	20467
Vehs Exited	20084	19990	20121	20022	19937	19774	20211
Starting Vehs	1023	1043	986	1003	1059	1027	1034
Ending Vehs	1256	1324	1366	1313	1393	1352	1290
Travel Distance (mi)	22748	22646	23053	22776	22425	22618	22896
Travel Time (hr)	1280.2	1215.6	1285.0	1290.4	1277.2	1259.2	1235.8
Total Delay (hr)	571.0	509.9	566.7	580.3	579.1	552.4	522.6
Total Stops	26707	27291	27774	28605	28852	28356	27949
Fuel Used (gal)	826.2	809.6	837.2	830.4	820.0	820.0	821.1

Interval #1 Information Recording

Start Time	4:30
End Time	5:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	20493	20400	20293	20340
Vehs Exited	20168	20071	19989	20035
Starting Vehs	1135	1047	1015	1022
Ending Vehs	1460	1376	1319	1330
Travel Distance (mi)	22964	22853	22815	22779
Travel Time (hr)	1373.0	1318.7	1251.2	1278.6
Total Delay (hr)	658.3	606.6	538.8	568.6
Total Stops	30167	28219	28000	28191
Fuel Used (gal)	852.6	838.3	822.1	827.8

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	21.2	21.0	18.3	0.0	0.0	0.0	1.5	0.1	1.4	1.2	0.2	0.2
Total Del/Veh (s)	330.6	393.8	349.9	67.8	30.8	24.3	53.9	38.1	8.1	62.1	53.4	48.1

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	5.9
Total Del/Veh (s)	136.3

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	6.7	6.1	0.2	0.0	0.0	0.0	0.1	0.1		0.1	0.1	3.2
Total Del/Veh (s)	62.1	69.2	63.4	26.5	0.8	0.1	105.2	354.4		97.7	37.0	38.8

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.8	0.1	0.0	0.0	0.0	1.9	0.3	0.3	1.6	0.4	0.5
Total Del/Veh (s)	48.4	34.6	29.7	60.6	38.4	36.8	61.4	41.8	38.9	71.4	42.0	38.5

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	40.0

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	1.7	0.2	0.2	0.1
Total Del/Veh (s)	35.1	17.2	41.1	18.4	3.0	47.8	38.6	5.4	53.1	44.9	33.8	29.6

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.4	1.3	0.1
Total Del/Veh (s)	30.2	18.8	17.8	12.0	55.8	20.2	23.8

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.4	0.2	0.3	1.5	0.2	0.2
Total Del/Veh (s)	84.1	68.1	43.5	59.0	40.9	20.1	89.8	44.2	39.6	65.9	42.6	37.1

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	50.3

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.1
Total Del/Veh (s)	6.6	0.8	0.2	14.8	5.6	4.8	58.4	15.1	11.4	32.5	39.4	15.7

7: 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	3.4

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.2	0.1	0.2	0.2	0.0	0.1	1.8	0.4	0.4	2.1	0.4	0.4
Total Del/Veh (s)	36.8	14.4	8.9	42.0	15.2	8.7	105.5	40.1	36.5	61.4	44.0	38.9

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	27.4

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.9	0.2	1.8	1.6	0.2	1.5
Total Del/Veh (s)	44.3	42.2	23.7	67.6	30.2	15.3	69.2	38.7	18.3	65.3	38.5	12.1

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	38.7

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.1	0.0	0.1		0.1	0.1	0.1	0.1
Total Del/Veh (s)	6.5	1.3	0.3	17.7	5.2	4.3	76.2		24.8	55.4	30.4	20.9

10: 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	4.8

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.1	0.3	0.1	0.0	0.1	1.9	0.4	0.4	1.5	0.3	0.3
Total Del/Veh (s)	31.4	19.1	10.5	41.7	16.6	8.8	48.6	32.7	27.2	44.4	31.4	25.2

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	23.9

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.8	0.3	1.8	1.6	0.2	1.5
Total Del/Veh (s)	44.3	38.7	22.3	55.8	31.7	13.3	63.9	31.3	15.9	63.4	31.6	11.9

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	33.4

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	All
Denied Del/Veh (s)	0.1	0.0	0.0	0.0	0.0	0.1	0.2		0.1	0.1
Total Del/Veh (s)	6.5	4.9	9.2	0.6	0.0	19.1	16.4		11.4	4.9

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	4.8	1.4	0.7	7.4	2.3	2.5	14.6	5.7	17.2	5.5	2.0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.2	1.6	0.2	0.2	1.4	0.2	1.3
Total Del/Veh (s)	49.9	29.2	17.1	39.8	41.2	20.2	52.8	23.3	13.3	35.7	24.0	6.0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	26.4

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	4.5	5.2	4.8

40: Country Club Rd Performance by movement

Movement	EBR	NBT	SBT	All
Denied Del/Veh (s)	0.0	0.2	0.0	0.1
Total Del/Veh (s)	4.7	4.1	2.3	3.5

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	7.2	1.7	4.6

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.0
Total Del/Veh (s)	2.2	0.7	8.2	4.8

100: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.3	0.0	0.2
Total Del/Veh (s)	10.4	1.8	6.7

111: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	4.1	3.4	3.8	6.9	4.0

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.8	3.7	2.5

201: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	22.6	3.1	378.6	17.8	18.9

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	SBR	All
Denied Del/Veh (s)	0.1	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	7.7	2.9	6.5	4.5	7.8	7.0

1100: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	2.5	1.9	3.5	2.7	8.7	3.2

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.2	0.1	0.0
Total Del/Veh (s)	5.1	4.1	1.9	21.1	12.9	6.5	4.9

1400: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.2	0.1	0.1	0.0
Total Del/Veh (s)	2.7	2.3	2.1	13.9	10.2	14.7	6.5	3.1

3000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	8.7	7.6	0.5	113.0	59.9	7.2

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	All
Denied Del/Veh (s)	0.1	0.1	0.1	0.2	0.1	0.1
Total Del/Veh (s)	4.4	3.4	7.1	6.3	16.3	6.1

5000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.4	0.0	0.2	0.1	0.1	0.2
Total Del/Veh (s)	6.5	5.4	114.8	101.1	19.9	9.6

6000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.0
Total Del/Veh (s)	4.1	3.1	3.7	3.0	37.9	18.2	16.5	5.0

7000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	3.0	2.0	3.5	2.7	11.4	3.5

8000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	2.6	5.4	11.3	7.2	3.9

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.1	0.1	0.0	0.1	0.1	0.1	0.0
Total Del/Veh (s)	4.6	3.6	1.3	18.6	31.0	12.6	4.0

Total Network Performance

Denied Del/Veh (s)	2.1
Total Del/Veh (s)	93.8

Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	393.8	474.0	0.6	5
	1000	9.9	18.1	0.1	13
	2	69.2	107.6	0.3	10
Wilson HAWK	20	4.5	7.0	0.0	10
6th Side Street	201	22.6	30.1	0.1	8
Tucson Blvd	3	34.5	44.6	0.1	7
	3000	9.6	53.3	0.4	25
6th/5th Street	72	2.2	7.3	0.0	15
Country Club Rd	4	17.2	33.1	0.1	12
	9922	4.3	16.3	0.1	21
	4000	4.4	33.3	0.2	27
Dodge Blvd	5	30.2	76.0	0.4	19
	5000	6.9	15.2	0.1	16
Alvernon Wy	6	68.2	89.5	0.2	7
	6000	4.7	19.9	0.1	23
	70	7.3	20.3	0.1	19
	7	0.8	2.8	0.0	24
Columbus Blvd	7000	3.0	20.0	0.1	26
	8	14.4	26.5	0.1	14
	8000	2.8	20.3	0.1	26
Swan Rd	9	42.2	83.6	0.4	16
	9000	5.5	20.7	0.1	21
	100	10.4	23.7	0.1	17
	10	1.3	3.3	0.0	21
	1100	2.6	16.9	0.1	26
Rosemont Blvd	11	19.1	34.1	0.1	13
	111	4.4	32.5	0.2	26
Craycroft Rd	12	38.7	68.7	0.3	14
	1200	6.2	34.7	0.2	25
	13	6.6	30.3	0.2	24
Chantilly Dr HAWK	130	1.7	4.3	0.0	16
Sahuara Ave	14	1.4	8.4	0.1	26
	1400	2.7	30.9	0.2	28
Wilmot Rd	15	29.1	54.1	0.3	18
Total		882.5	1561.7	5.6	13

Arterial Level of Service
Scenario B

11/04/2022

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	41.2	139.4	0.4	11
	1400	1.2	28.1	0.3	34
Sahuara Ave	14	2.5	30.4	0.2	28
	130	3.7	11.2	0.1	19
	13	0.6	2.5	0.0	27
	1200	2.0	25.8	0.2	28
Craycroft Rd	12	31.3	58.4	0.2	15
	111	4.6	36.0	0.3	26
Rosemont Blvd	11	16.5	44.1	0.2	19
	1100	4.0	19.2	0.1	24
	10	5.3	19.5	0.1	22
Arcadia Ave HAWK	100	1.8	4.4	0.0	16
	9000	1.3	13.9	0.1	28
Swan Rd	9	30.1	44.1	0.1	10
	8000	6.3	48.6	0.4	27
Columbus Blvd	8	15.0	32.2	0.1	16
	7000	4.1	16.4	0.1	22
	7	5.6	22.5	0.1	23
Irving Ave HAWK	70	1.8	4.4	0.0	16
	6000	3.7	16.3	0.1	23
Alvernon Wy	6	40.3	54.7	0.1	8
	5000	6.9	29.2	0.2	23
Dodge Blvd	5	12.4	20.1	0.1	12
	4000	7.5	54.4	0.4	27
	9922	3.2	32.6	0.2	27
Country Club Rd	4	18.4	29.1	0.1	12
6th/5th Street	72	10.1	26.1	0.1	16
	3000	0.5	10.6	0.0	10
Tucson Blvd	3	38.3	81.9	0.4	16
6th Side Street	201	3.4	13.0	0.1	22
Wilson HAWK	20	5.2	13.3	0.1	18
	2	0.8	2.7	0.0	25
	1000	6.5	39.1	0.3	25
Campbell Ave	1	30.6	37.3	0.1	6
Total		366.9	1061.9	5.5	19

Queuing and Blocking Report
Scenario B

11/04/2022

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	425	3011	3003	200	232	268	263	182	262	269	226	73
Average Queue (ft)	407	1920	1909	180	141	188	193	70	178	174	138	24
95th Queue (ft)	525	3373	3338	273	232	274	272	140	242	241	219	53
Link Distance (ft)		2992	2992			233	233		2020	2020	2020	
Upstream Blk Time (%)		18	17		1	3	4					
Queuing Penalty (veh)		0	0		0	13	16					
Storage Bay Dist (ft)	250			50	180			290				150
Storage Blk Time (%)	1	87	51	8	7	9			0		4	
Queuing Penalty (veh)	4	229	83	35	21	15			0		5	

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	350	511	500	494
Average Queue (ft)	199	324	325	315
95th Queue (ft)	358	507	498	489
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	7	32		
Queuing Penalty (veh)	28	66		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	114	1302	31	58	73	52
Average Queue (ft)	6	756	8	5	22	16
95th Queue (ft)	53	1544	28	31	73	51
Link Distance (ft)		1396		55	442	447
Upstream Blk Time (%)		1	0	0		
Queuing Penalty (veh)		6	0	1		
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)		37	0	0		
Queuing Penalty (veh)		1	0	0		

Queuing and Blocking Report
Scenario B

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Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB	SB	SB
Directions Served	L	TR	L	TR	L	TR	L	TR
Maximum Queue (ft)	300	374	300	791	165	385	304	412
Average Queue (ft)	130	358	84	410	45	219	83	234
95th Queue (ft)	312	381	241	729	112	339	191	361
Link Distance (ft)		356		1877		1948		2192
Upstream Blk Time (%)		28						
Queuing Penalty (veh)		328						
Storage Bay Dist (ft)	200		100		300		300	
Storage Blk Time (%)	1	43	2	39		3	0	4
Queuing Penalty (veh)	8	48	17	22		1	0	3

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	NB	NB	NB	NB	SB
Directions Served	L	T	T	L	T	T	R	L	T	T	R	L
Maximum Queue (ft)	279	376	255	188	336	222	26	176	204	209	170	174
Average Queue (ft)	103	176	106	61	162	89	1	105	157	157	34	131
95th Queue (ft)	221	312	220	150	278	190	29	176	212	217	128	205
Link Distance (ft)		486	486		423	423			177	177		
Upstream Blk Time (%)								1	7	7	0	
Queuing Penalty (veh)								0	28	28	0	
Storage Bay Dist (ft)	180			90			160	100			70	100
Storage Blk Time (%)	1	8		2	25	1		14	29	39	0	22
Queuing Penalty (veh)	5	16		6	14	2		37	44	18	0	50

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	TR
Maximum Queue (ft)	374	356
Average Queue (ft)	201	205
95th Queue (ft)	323	315
Link Distance (ft)	2000	2000
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)	32	
Queuing Penalty (veh)	56	

Queuing and Blocking Report
Scenario B

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Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB
Directions Served	T	R	L	T	L	R
Maximum Queue (ft)	751	285	250	307	273	208
Average Queue (ft)	336	96	82	249	146	68
95th Queue (ft)	880	269	184	341	239	145
Link Distance (ft)	2072				292	2124
Upstream Blk Time (%)			0	2		
Queuing Penalty (veh)			0	15		
Storage Bay Dist (ft)	110		300		220	
Storage Blk Time (%)	26	0	0	2	3	0
Queuing Penalty (veh)	60	1	0	3	4	0

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	TR	L	T	TR
Maximum Queue (ft)	400	896	285	375	587	285	260	541	536	260	455	459
Average Queue (ft)	230	624	136	115	341	96	171	335	335	132	293	300
95th Queue (ft)	481	1015	341	292	587	277	308	481	476	282	415	422
Link Distance (ft)	908				580		2113		2113	1973		1973
Upstream Blk Time (%)	5				1							
Queuing Penalty (veh)	50				11							
Storage Bay Dist (ft)	300	110		300	110		160			160		
Storage Blk Time (%)	0	55	0	0	45	0	12	37			2	32
Queuing Penalty (veh)	1	143	3	0	90	1	59	45			9	33

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	30	46	40	361	47	47
Average Queue (ft)	3	3	2	80	15	13
95th Queue (ft)	18	25	23	255	41	38
Link Distance (ft)	56		687		311	327
Upstream Blk Time (%)	0	0				
Queuing Penalty (veh)	0	2				
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0	0	5			
Queuing Penalty (veh)	0	0	0			

Queuing and Blocking Report
Scenario B

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Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	163	466	209	166	521	150	190	434	200	471
Average Queue (ft)	41	198	33	48	193	22	77	211	70	272
95th Queue (ft)	110	390	127	126	388	99	176	378	185	434
Link Distance (ft)		474			702			2075		1939
Upstream Blk Time (%)		0			0					
Queuing Penalty (veh)		2			0					
Storage Bay Dist (ft)	75		110	75		75	100		100	
Storage Blk Time (%)	2	23	0	4	18	0	11	32	1	42
Queuing Penalty (veh)	17	27	0	26	17	0	38	18	4	22

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	359	794	254	196	512	210	304	422	417	181	372	426
Average Queue (ft)	86	326	67	82	233	61	109	265	262	39	104	265
95th Queue (ft)	256	647	229	161	425	185	254	394	389	114	233	392
Link Distance (ft)		1812			558			1988	1988			2061
Upstream Blk Time (%)					0							
Queuing Penalty (veh)					1							
Storage Bay Dist (ft)	300		110	500		110	180			350	230	
Storage Blk Time (%)		44			32	0	3	23	2		1	14
Queuing Penalty (veh)		81			54	1	13	24	2		3	17

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	411	232
Average Queue (ft)	265	28
95th Queue (ft)	392	114
Link Distance (ft)	2061	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	6	
Queuing Penalty (veh)	4	

Queuing and Blocking Report
Scenario B

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Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	35	54	85	348	102	76
Average Queue (ft)	10	5	19	73	43	25
95th Queue (ft)	33	29	58	232	89	61
Link Distance (ft)		54		582	279	312
Upstream Blk Time (%)	0	0				
Queuing Penalty (veh)	0	3				
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0	0		5		
Queuing Penalty (veh)	1	0		1		

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	134	580	180	204	496	208	225	372	177	306
Average Queue (ft)	45	257	47	43	170	27	63	167	34	154
95th Queue (ft)	115	484	155	119	376	122	145	301	99	266
Link Distance (ft)		604			1181			1974		2044
Upstream Blk Time (%)		0								
Queuing Penalty (veh)		1								
Storage Bay Dist (ft)	75		110	100		110	150		150	
Storage Blk Time (%)	1	27	0	1	13		0	13		12
Queuing Penalty (veh)	10	37	0	7	11		1	10		5

Queuing and Blocking Report
Scenario B

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Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	398	632	210	199	414	210	329	418	425	230	299	433
Average Queue (ft)	97	287	96	89	202	60	91	261	260	76	84	256
95th Queue (ft)	265	530	235	167	355	187	228	391	392	224	205	391
Link Distance (ft)		1293			1162			1958	1958			2072
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300		110	500		110	180			180	150	
Storage Blk Time (%)		36	1		27	0	1	21	20		1	25
Queuing Penalty (veh)		93	4		56	0	8	19	17		6	20

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	435	299
Average Queue (ft)	253	46
95th Queue (ft)	389	180
Link Distance (ft)	2072	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	25	
Queuing Penalty (veh)	18	

Intersection: 13: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	TR	L	TR	LTR	LTR
Maximum Queue (ft)	312	38	38	99	27
Average Queue (ft)	65	10	2	40	2
95th Queue (ft)	220	34	18	78	15
Link Distance (ft)	1017		56	429	480
Upstream Blk Time (%)		0	0		
Queuing Penalty (veh)		0	1		
Storage Bay Dist (ft)		100			
Storage Blk Time (%)	4	0	0		
Queuing Penalty (veh)	0	1	0		

Queuing and Blocking Report
Scenario B

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Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	LTR	LTR	LTR	LTR
Maximum Queue (ft)	159	93	33	46
Average Queue (ft)	13	8	8	16
95th Queue (ft)	77	48	29	43
Link Distance (ft)	275	1189	622	706
Upstream Blk Time (%)	0			
Queuing Penalty (veh)	0			
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	214	297	236	119	323	356	344	314	216	395	390	350
Average Queue (ft)	105	133	80	44	149	201	200	170	44	226	221	187
95th Queue (ft)	187	229	174	96	262	318	315	296	140	345	340	316
Link Distance (ft)		1302		2255		1994	1994	1994		2040	2040	2040
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	145		145		240				130			
Storage Blk Time (%)	4	9	1		2	4				25		1
Queuing Penalty (veh)	21	29	5		9	8				11		1

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	132
Average Queue (ft)	38
95th Queue (ft)	90
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Queuing and Blocking Report
Scenario B

11/04/2022

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	84	308
Average Queue (ft)	56	112
95th Queue (ft)	84	300
Link Distance (ft)	55	298
Upstream Blk Time (%)	26	1
Queuing Penalty (veh)	314	5
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 40: Country Club Rd

Movement	EB	NB	NB	SB
Directions Served	R	T	T	T
Maximum Queue (ft)	81	173	157	2
Average Queue (ft)	39	29	23	0
95th Queue (ft)	64	108	95	2
Link Distance (ft)	450	2095	2095	177
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	404	80
Average Queue (ft)	119	35
95th Queue (ft)	343	86
Link Distance (ft)	514	56
Upstream Blk Time (%)		8
Queuing Penalty (veh)		56
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario B

11/04/2022

Intersection: 72: 6th/5th Street

Movement	SW	SW
Directions Served	R	R
Maximum Queue (ft)	278	58
Average Queue (ft)	47	2
95th Queue (ft)	163	50
Link Distance (ft)	486	486
Upstream Blk Time (%)	0	0
Queuing Penalty (veh)	0	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 100: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	487	72
Average Queue (ft)	149	30
95th Queue (ft)	426	77
Link Distance (ft)	528	54
Upstream Blk Time (%)	0	9
Queuing Penalty (veh)	2	62
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 111: 6th/5th Street

Movement	SB
Directions Served	LTR
Maximum Queue (ft)	61
Average Queue (ft)	21
95th Queue (ft)	50
Link Distance (ft)	283
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	
Storage Blk Time (%)	
Queuing Penalty (veh)	

Queuing and Blocking Report
Scenario B

11/04/2022

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	75	238
Average Queue (ft)	30	53
95th Queue (ft)	77	165
Link Distance (ft)	56	275
Upstream Blk Time (%)	7	0
Queuing Penalty (veh)	53	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 201: 6th Side Street & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	316	168	206	59
Average Queue (ft)	285	14	84	21
95th Queue (ft)	383	88	219	50
Link Distance (ft)	298	356	433	422
Upstream Blk Time (%)	19			
Queuing Penalty (veh)	216			
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1000: 6th/5th Street

Movement	EB	EB	WB	SB
Directions Served	T	R	LTR	LTR
Maximum Queue (ft)	285	269	295	64
Average Queue (ft)	125	55	44	26
95th Queue (ft)	321	218	170	53
Link Distance (ft)	233	233	1396	415
Upstream Blk Time (%)	2	1		
Queuing Penalty (veh)	15	4		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

Scenario B

11/04/2022

Intersection: 1100: 6th/5th Street

Movement	EB	SB
Directions Served	TR	LTR
Maximum Queue (ft)	58	93
Average Queue (ft)	2	42
95th Queue (ft)	32	75
Link Distance (ft)	582	197
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1200: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	121	68
Average Queue (ft)	49	32
95th Queue (ft)	90	56
Link Distance (ft)	214	281
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1400: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	54	74
Average Queue (ft)	19	33
95th Queue (ft)	46	59
Link Distance (ft)	307	277
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario B

11/04/2022

Intersection: 3000: 6th/5th Street

Movement	EB	SB
Directions Served	TR	LTR
Maximum Queue (ft)	19	129
Average Queue (ft)	1	47
95th Queue (ft)	19	113
Link Distance (ft)	1877	430
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 4000: 6th/5th Street

Movement	B9922	NB
Directions Served	T	LTR
Maximum Queue (ft)	296	91
Average Queue (ft)	10	38
95th Queue (ft)	115	74
Link Distance (ft)	423	554
Upstream Blk Time (%)	0	
Queuing Penalty (veh)	0	
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 5000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	211	233	184	30
Average Queue (ft)	65	64	72	5
95th Queue (ft)	255	187	180	23
Link Distance (ft)	292	908	318	286
Upstream Blk Time (%)	3		0	
Queuing Penalty (veh)	24		0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

Scenario B

11/04/2022

Intersection: 6000: 6th/5th Street

Movement	WB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	260	94	78
Average Queue (ft)	23	37	32
95th Queue (ft)	151	73	63
Link Distance (ft)	514	362	366
Upstream Blk Time (%)	0		
Queuing Penalty (veh)	0		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 7000: 6th/5th Street

Movement	EB	SB
Directions Served	TR	LTR
Maximum Queue (ft)	177	74
Average Queue (ft)	7	32
95th Queue (ft)	75	59
Link Distance (ft)	687	311
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 8000: 6th/5th Street

Movement	WB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	5	33	44
Average Queue (ft)	0	8	17
95th Queue (ft)	5	29	43
Link Distance (ft)	1812	326	341
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Queuing and Blocking Report

Scenario B

11/04/2022

Intersection: 9000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	92	45	60	73
Average Queue (ft)	6	2	25	30
95th Queue (ft)	68	25	56	61
Link Distance (ft)	558	528	210	316
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 3324

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	4:15	4:15	4:15	4:15	4:15	4:15	4:15
End Time	5:30	5:30	5:30	5:30	5:30	5:30	5:30
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	20317	20271	20501	20332	20271	20099	20467
Vehs Exited	20084	19990	20121	20022	19937	19774	20211
Starting Vehs	1023	1043	986	1003	1059	1027	1034
Ending Vehs	1256	1324	1366	1313	1393	1352	1290
Denied Entry After	96	45	65	103	85	14	7
Travel Distance (mi)	22748	22646	23053	22776	22425	22618	22896
Travel Time (hr)	1280.2	1215.6	1285.0	1290.4	1277.2	1259.2	1235.8
Total Delay (hr)	571.0	509.9	566.7	580.3	579.1	552.4	522.6
Total Stops	26707	27291	27774	28605	28852	28356	27949
Fuel Used (gal)	826.2	809.6	837.2	830.4	820.0	820.0	821.1

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	4:15	4:15	4:15	4:15
End Time	5:30	5:30	5:30	5:30
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	20493	20400	20293	20340
Vehs Exited	20168	20071	19989	20035
Starting Vehs	1135	1047	1015	1022
Ending Vehs	1460	1376	1319	1330
Denied Entry After	105	79	34	60
Travel Distance (mi)	22964	22853	22815	22779
Travel Time (hr)	1373.0	1318.7	1251.2	1278.6
Total Delay (hr)	658.3	606.6	538.8	568.6
Total Stops	30167	28219	28000	28191
Fuel Used (gal)	852.6	838.3	822.1	827.8

Interval #0 Information Seeding

Start Time	4:15
End Time	4:30
Total Time (min)	15

No data recorded this interval.

Interval #1 Information Recording

Start Time	4:30
End Time	5:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	20317	20271	20501	20332	20271	20099	20467
Vehs Exited	20084	19990	20121	20022	19937	19774	20211
Starting Vehs	1023	1043	986	1003	1059	1027	1034
Ending Vehs	1256	1324	1366	1313	1393	1352	1290
Denied Entry After	96	45	65	103	85	14	7
Travel Distance (mi)	22748	22646	23053	22776	22425	22618	22896
Travel Time (hr)	1280.2	1215.6	1285.0	1290.4	1277.2	1259.2	1235.8
Total Delay (hr)	571.0	509.9	566.7	580.3	579.1	552.4	522.6
Total Stops	26707	27291	27774	28605	28852	28356	27949
Fuel Used (gal)	826.2	809.6	837.2	830.4	820.0	820.0	821.1

Interval #1 Information Recording

Start Time	4:30
End Time	5:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	20493	20400	20293	20340
Vehs Exited	20168	20071	19989	20035
Starting Vehs	1135	1047	1015	1022
Ending Vehs	1460	1376	1319	1330
Denied Entry After	105	79	34	60
Travel Distance (mi)	22964	22853	22815	22779
Travel Time (hr)	1373.0	1318.7	1251.2	1278.6
Total Delay (hr)	658.3	606.6	538.8	568.6
Total Stops	30167	28219	28000	28191
Fuel Used (gal)	852.6	838.3	822.1	827.8

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	12	36	6	0	0	0	0	0	0	0	0	0

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	54

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Entry After	0	6	0	0	0	0	0	0	0	0	0	6

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Entry After	0	0	0	0	0	0	0

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

7: 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

10: 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

40: Country Club Rd Performance by movement

Movement	EBR	NBT	SBT	All
Denied Entry After	0	0	0	0

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWR	All
Denied Entry After	0	0	0	0

100: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

111: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBR	All
Denied Entry After	0	0	0	0	0

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

201: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Entry After	0	0	0	0	0

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	SBR	All
Denied Entry After	0	0	0	0	0	0

1100: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	SBR	All
Denied Entry After	0	0	0	0	0	0

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0	0

1400: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0

3000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	All
Denied Entry After	0	0	0	0	0	0

5000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0

6000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0

7000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	SBR	All
Denied Entry After	0	0	0	0	0	0

8000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Entry After	0	0	0	0	0

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0

Total Network Performance

Denied Entry After							60
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Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	393.8	474.0	0.6	5
	1000	9.9	18.1	0.1	13
	2	69.2	107.6	0.3	10
Wilson HAWK	20	4.5	7.0	0.0	10
6th Side Street	201	22.6	30.1	0.1	8
Tucson Blvd	3	34.5	44.6	0.1	7
	3000	9.6	53.3	0.4	25
6th/5th Street	72	2.2	7.3	0.0	15
Country Club Rd	4	17.2	33.1	0.1	12
	9922	4.3	16.3	0.1	21
	4000	4.4	33.3	0.2	27
Dodge Blvd	5	30.2	76.0	0.4	19
	5000	6.9	15.2	0.1	16
Alvernon Wy	6	68.2	89.5	0.2	7
	6000	4.7	19.9	0.1	23
	70	7.3	20.3	0.1	19
	7	0.8	2.8	0.0	24
Columbus Blvd	7000	3.0	20.0	0.1	26
	8	14.4	26.5	0.1	14
	8000	2.8	20.3	0.1	26
Swan Rd	9	42.2	83.6	0.4	16
	9000	5.5	20.7	0.1	21
	100	10.4	23.7	0.1	17
	10	1.3	3.3	0.0	21
	1100	2.6	16.9	0.1	26
Rosemont Blvd	11	19.1	34.1	0.1	13
	111	4.4	32.5	0.2	26
Craycroft Rd	12	38.7	68.7	0.3	14
	1200	6.2	34.7	0.2	25
	13	6.6	30.3	0.2	24
Chantilly Dr HAWK	130	1.7	4.3	0.0	16
Sahuara Ave	14	1.4	8.4	0.1	26
	1400	2.7	30.9	0.2	28
Wilmot Rd	15	29.1	54.1	0.3	18
Total		882.5	1561.7	5.6	13

Arterial Level of Service
Scenario B

11/07/2022

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	41.2	139.4	0.4	11
	1400	1.2	28.1	0.3	34
Sahuara Ave	14	2.5	30.4	0.2	28
	130	3.7	11.2	0.1	19
	13	0.6	2.5	0.0	27
	1200	2.0	25.8	0.2	28
Craycroft Rd	12	31.3	58.4	0.2	15
	111	4.6	36.0	0.3	26
Rosemont Blvd	11	16.5	44.1	0.2	19
	1100	4.0	19.2	0.1	24
	10	5.3	19.5	0.1	22
Arcadia Ave HAWK	100	1.8	4.4	0.0	16
	9000	1.3	13.9	0.1	28
Swan Rd	9	30.1	44.1	0.1	10
	8000	6.3	48.6	0.4	27
Columbus Blvd	8	15.0	32.2	0.1	16
	7000	4.1	16.4	0.1	22
	7	5.6	22.5	0.1	23
Irving Ave HAWK	70	1.8	4.4	0.0	16
	6000	3.7	16.3	0.1	23
Alvernon Wy	6	40.3	54.7	0.1	8
	5000	6.9	29.2	0.2	23
Dodge Blvd	5	12.4	20.1	0.1	12
	4000	7.5	54.4	0.4	27
	9922	3.2	32.6	0.2	27
Country Club Rd	4	18.4	29.1	0.1	12
6th/5th Street	72	10.1	26.1	0.1	16
	3000	0.5	10.6	0.0	10
Tucson Blvd	3	38.3	81.9	0.4	16
6th Side Street	201	3.4	13.0	0.1	22
Wilson HAWK	20	5.2	13.3	0.1	18
	2	0.8	2.7	0.0	25
	1000	6.5	39.1	0.3	25
Campbell Ave	1	30.6	37.3	0.1	6
Total		366.9	1061.9	5.5	19

Queuing and Blocking Report
Scenario B

11/07/2022

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	425	3011	3003	200	232	268	263	182	262	269	226	73
Average Queue (ft)	407	1920	1909	180	141	188	193	70	178	174	138	24
95th Queue (ft)	525	3373	3338	273	232	274	272	140	242	241	219	53
Link Distance (ft)		2992	2992			233	233		2020	2020	2020	
Upstream Blk Time (%)		18	17		1	3	4					
Queuing Penalty (veh)		0	0		0	13	16					
Storage Bay Dist (ft)	250			50	180			290				150
Storage Blk Time (%)	1	87	51	8	7	9			0		4	
Queuing Penalty (veh)	4	229	83	35	21	15			0		5	

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	350	511	500	494
Average Queue (ft)	199	324	325	315
95th Queue (ft)	358	507	498	489
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	7	32		
Queuing Penalty (veh)	28	66		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	114	1302	31	58	73	52
Average Queue (ft)	6	756	8	5	22	16
95th Queue (ft)	53	1544	28	31	73	51
Link Distance (ft)		1396		55	442	447
Upstream Blk Time (%)		1	0	0		
Queuing Penalty (veh)		6	0	1		
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)		37	0	0		
Queuing Penalty (veh)		1	0	0		

Queuing and Blocking Report
Scenario B

11/07/2022

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB	SB	SB
Directions Served	L	TR	L	TR	L	TR	L	TR
Maximum Queue (ft)	300	374	300	791	165	385	304	412
Average Queue (ft)	130	358	84	410	45	219	83	234
95th Queue (ft)	312	381	241	729	112	339	191	361
Link Distance (ft)		356		1877		1948		2192
Upstream Blk Time (%)		28						
Queuing Penalty (veh)		328						
Storage Bay Dist (ft)	200		100		300		300	
Storage Blk Time (%)	1	43	2	39		3	0	4
Queuing Penalty (veh)	8	48	17	22		1	0	3

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	NB	NB	NB	NB	SB
Directions Served	L	T	T	L	T	T	R	L	T	T	R	L
Maximum Queue (ft)	279	376	255	188	336	222	26	176	204	209	170	174
Average Queue (ft)	103	176	106	61	162	89	1	105	157	157	34	131
95th Queue (ft)	221	312	220	150	278	190	29	176	212	217	128	205
Link Distance (ft)		486	486		423	423			177	177		
Upstream Blk Time (%)								1	7	7	0	
Queuing Penalty (veh)								0	28	28	0	
Storage Bay Dist (ft)	180			90			160	100			70	100
Storage Blk Time (%)	1	8		2	25	1		14	29	39	0	22
Queuing Penalty (veh)	5	16		6	14	2		37	44	18	0	50

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	TR
Maximum Queue (ft)	374	356
Average Queue (ft)	201	205
95th Queue (ft)	323	315
Link Distance (ft)	2000	2000
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)	32	
Queuing Penalty (veh)	56	

Queuing and Blocking Report
Scenario B

11/07/2022

Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB
Directions Served	T	R	L	T	L	R
Maximum Queue (ft)	751	285	250	307	273	208
Average Queue (ft)	336	96	82	249	146	68
95th Queue (ft)	880	269	184	341	239	145
Link Distance (ft)	2072				292	2124
Upstream Blk Time (%)			0	2		
Queuing Penalty (veh)			0	15		
Storage Bay Dist (ft)	110		300		220	
Storage Blk Time (%)	26	0	0	2	3	0
Queuing Penalty (veh)	60	1	0	3	4	0

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	TR	L	T	TR
Maximum Queue (ft)	400	896	285	375	587	285	260	541	536	260	455	459
Average Queue (ft)	230	624	136	115	341	96	171	335	335	132	293	300
95th Queue (ft)	481	1015	341	292	587	277	308	481	476	282	415	422
Link Distance (ft)	908				580		2113		2113	1973		1973
Upstream Blk Time (%)	5				1							
Queuing Penalty (veh)	50				11							
Storage Bay Dist (ft)	300		110		300		110		160		160	
Storage Blk Time (%)	0	55	0	0	45	0	12	37		2	32	
Queuing Penalty (veh)	1	143	3	0	90	1	59	45		9	33	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	30	46	40	361	47	47
Average Queue (ft)	3	3	2	80	15	13
95th Queue (ft)	18	25	23	255	41	38
Link Distance (ft)	56		687		311	327
Upstream Blk Time (%)	0	0				
Queuing Penalty (veh)	0	2				
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0	0	5			
Queuing Penalty (veh)	0	0	0			

Queuing and Blocking Report
Scenario B

11/07/2022

Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	163	466	209	166	521	150	190	434	200	471
Average Queue (ft)	41	198	33	48	193	22	77	211	70	272
95th Queue (ft)	110	390	127	126	388	99	176	378	185	434
Link Distance (ft)		474			702			2075		1939
Upstream Blk Time (%)		0			0					
Queuing Penalty (veh)		2			0					
Storage Bay Dist (ft)	75		110	75		75	100		100	
Storage Blk Time (%)	2	23	0	4	18	0	11	32	1	42
Queuing Penalty (veh)	17	27	0	26	17	0	38	18	4	22

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	359	794	254	196	512	210	304	422	417	181	372	426
Average Queue (ft)	86	326	67	82	233	61	109	265	262	39	104	265
95th Queue (ft)	256	647	229	161	425	185	254	394	389	114	233	392
Link Distance (ft)		1812			558			1988	1988			2061
Upstream Blk Time (%)					0							
Queuing Penalty (veh)					1							
Storage Bay Dist (ft)	300		110	500		110	180			350	230	
Storage Blk Time (%)		44			32	0	3	23	2		1	14
Queuing Penalty (veh)		81			54	1	13	24	2		3	17

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	411	232
Average Queue (ft)	265	28
95th Queue (ft)	392	114
Link Distance (ft)	2061	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	6	
Queuing Penalty (veh)	4	

Queuing and Blocking Report
Scenario B

11/07/2022

Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	35	54	85	348	102	76
Average Queue (ft)	10	5	19	73	43	25
95th Queue (ft)	33	29	58	232	89	61
Link Distance (ft)		54		582	279	312
Upstream Blk Time (%)	0	0				
Queuing Penalty (veh)	0	3				
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0	0		5		
Queuing Penalty (veh)	1	0		1		

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	134	580	180	204	496	208	225	372	177	306
Average Queue (ft)	45	257	47	43	170	27	63	167	34	154
95th Queue (ft)	115	484	155	119	376	122	145	301	99	266
Link Distance (ft)		604			1181			1974		2044
Upstream Blk Time (%)		0								
Queuing Penalty (veh)		1								
Storage Bay Dist (ft)	75		110	100		110	150		150	
Storage Blk Time (%)	1	27	0	1	13		0	13		12
Queuing Penalty (veh)	10	37	0	7	11		1	10		5

Queuing and Blocking Report
Scenario B

11/07/2022

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	398	632	210	199	414	210	329	418	425	230	299	433
Average Queue (ft)	97	287	96	89	202	60	91	261	260	76	84	256
95th Queue (ft)	265	530	235	167	355	187	228	391	392	224	205	391
Link Distance (ft)		1293			1162			1958	1958			2072
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300		110	500		110	180			180	150	
Storage Blk Time (%)		36	1		27	0	1	21	20		1	25
Queuing Penalty (veh)		93	4		56	0	8	19	17		6	20

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	435	299
Average Queue (ft)	253	46
95th Queue (ft)	389	180
Link Distance (ft)	2072	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	25	
Queuing Penalty (veh)	18	

Intersection: 13: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	TR	L	TR	LTR	LTR
Maximum Queue (ft)	312	38	38	99	27
Average Queue (ft)	65	10	2	40	2
95th Queue (ft)	220	34	18	78	15
Link Distance (ft)	1017		56	429	480
Upstream Blk Time (%)		0	0		
Queuing Penalty (veh)		0	1		
Storage Bay Dist (ft)		100			
Storage Blk Time (%)	4	0	0		
Queuing Penalty (veh)	0	1	0		

Queuing and Blocking Report
Scenario B

11/07/2022

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	LTR	LTR	LTR	LTR
Maximum Queue (ft)	159	93	33	46
Average Queue (ft)	13	8	8	16
95th Queue (ft)	77	48	29	43
Link Distance (ft)	275	1189	622	706
Upstream Blk Time (%)	0			
Queuing Penalty (veh)	0			
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	214	297	236	119	323	356	344	314	216	395	390	350
Average Queue (ft)	105	133	80	44	149	201	200	170	44	226	221	187
95th Queue (ft)	187	229	174	96	262	318	315	296	140	345	340	316
Link Distance (ft)		1302		2255		1994	1994	1994		2040	2040	2040
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	145		145		240				130			
Storage Blk Time (%)	4	9	1		2	4				25		1
Queuing Penalty (veh)	21	29	5		9	8				11		1

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	132
Average Queue (ft)	38
95th Queue (ft)	90
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Queuing and Blocking Report Scenario B

11/07/2022

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	84	308
Average Queue (ft)	56	112
95th Queue (ft)	84	300
Link Distance (ft)	55	298
Upstream Blk Time (%)	26	1
Queuing Penalty (veh)	314	5
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 40: Country Club Rd

Movement	EB	NB	NB	SB
Directions Served	R	T	T	T
Maximum Queue (ft)	81	173	157	2
Average Queue (ft)	39	29	23	0
95th Queue (ft)	64	108	95	2
Link Distance (ft)	450	2095	2095	177
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	404	80
Average Queue (ft)	119	35
95th Queue (ft)	343	86
Link Distance (ft)	514	56
Upstream Blk Time (%)		8
Queuing Penalty (veh)		56
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario B

11/07/2022

Intersection: 72: 6th/5th Street

Movement	SW	SW
Directions Served	R	R
Maximum Queue (ft)	278	58
Average Queue (ft)	47	2
95th Queue (ft)	163	50
Link Distance (ft)	486	486
Upstream Blk Time (%)	0	0
Queuing Penalty (veh)	0	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 100: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	487	72
Average Queue (ft)	149	30
95th Queue (ft)	426	77
Link Distance (ft)	528	54
Upstream Blk Time (%)	0	9
Queuing Penalty (veh)	2	62
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 111: 6th/5th Street

Movement	SB
Directions Served	LTR
Maximum Queue (ft)	61
Average Queue (ft)	21
95th Queue (ft)	50
Link Distance (ft)	283
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	
Storage Blk Time (%)	
Queuing Penalty (veh)	

Queuing and Blocking Report
Scenario B

11/07/2022

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	75	238
Average Queue (ft)	30	53
95th Queue (ft)	77	165
Link Distance (ft)	56	275
Upstream Blk Time (%)	7	0
Queuing Penalty (veh)	53	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 201: 6th Side Street & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	316	168	206	59
Average Queue (ft)	285	14	84	21
95th Queue (ft)	383	88	219	50
Link Distance (ft)	298	356	433	422
Upstream Blk Time (%)	19			
Queuing Penalty (veh)	216			
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1000: 6th/5th Street

Movement	EB	EB	WB	SB
Directions Served	T	R	LTR	LTR
Maximum Queue (ft)	285	269	295	64
Average Queue (ft)	125	55	44	26
95th Queue (ft)	321	218	170	53
Link Distance (ft)	233	233	1396	415
Upstream Blk Time (%)	2	1		
Queuing Penalty (veh)	15	4		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

Scenario B

11/07/2022

Intersection: 1100: 6th/5th Street

Movement	EB	SB
Directions Served	TR	LTR
Maximum Queue (ft)	58	93
Average Queue (ft)	2	42
95th Queue (ft)	32	75
Link Distance (ft)	582	197
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1200: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	121	68
Average Queue (ft)	49	32
95th Queue (ft)	90	56
Link Distance (ft)	214	281
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1400: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	54	74
Average Queue (ft)	19	33
95th Queue (ft)	46	59
Link Distance (ft)	307	277
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario B

11/07/2022

Intersection: 3000: 6th/5th Street

Movement	EB	SB
Directions Served	TR	LTR
Maximum Queue (ft)	19	129
Average Queue (ft)	1	47
95th Queue (ft)	19	113
Link Distance (ft)	1877	430
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 4000: 6th/5th Street

Movement	B9922	NB
Directions Served	T	LTR
Maximum Queue (ft)	296	91
Average Queue (ft)	10	38
95th Queue (ft)	115	74
Link Distance (ft)	423	554
Upstream Blk Time (%)	0	
Queuing Penalty (veh)	0	
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 5000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	211	233	184	30
Average Queue (ft)	65	64	72	5
95th Queue (ft)	255	187	180	23
Link Distance (ft)	292	908	318	286
Upstream Blk Time (%)	3		0	
Queuing Penalty (veh)	24		0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

Scenario B

11/07/2022

Intersection: 6000: 6th/5th Street

Movement	WB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	260	94	78
Average Queue (ft)	23	37	32
95th Queue (ft)	151	73	63
Link Distance (ft)	514	362	366
Upstream Blk Time (%)	0		
Queuing Penalty (veh)	0		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 7000: 6th/5th Street

Movement	EB	SB
Directions Served	TR	LTR
Maximum Queue (ft)	177	74
Average Queue (ft)	7	32
95th Queue (ft)	75	59
Link Distance (ft)	687	311
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 8000: 6th/5th Street

Movement	WB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	5	33	44
Average Queue (ft)	0	8	17
95th Queue (ft)	5	29	43
Link Distance (ft)	1812	326	341
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Queuing and Blocking Report

Scenario B

11/07/2022

Intersection: 9000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	92	45	60	73
Average Queue (ft)	6	2	25	30
95th Queue (ft)	68	25	56	61
Link Distance (ft)	558	528	210	316
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 3324



Appendix L
Left-Turn Lane and
Left-Turn Arrow Warrants

L.1. LEFT TURN ARROW WARRANT FOR TRAFFIC VOLUMES

Left turn phasing may be considered when the cross-product during the peak hour exceeds the following values:

CROSS-PRODUCT (left turn volume x opposing through volume)

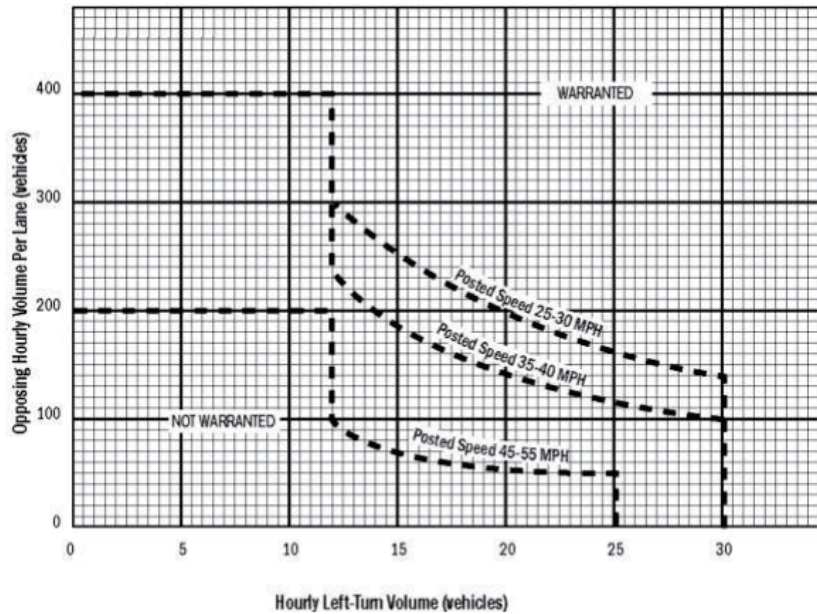
	2-LANE STREET	4-LANE STREET	6-LANE STREET
RURAL	50,000	100,000	150,000
URBAN	75,000	150,000	225,000

L.2. LEFT TURN ARROW WARRANT FOR CRASH EXPERIENCE

LEFT TURN CRASH EXPERIENCE

	ONE-YEAR PERIOD	TWO-YEAR PERIOD
ONE APPROACH	4	6
TWO OPPOSING APPROACHES	6	10

L.3. LEFT TURN LANE WARRANT

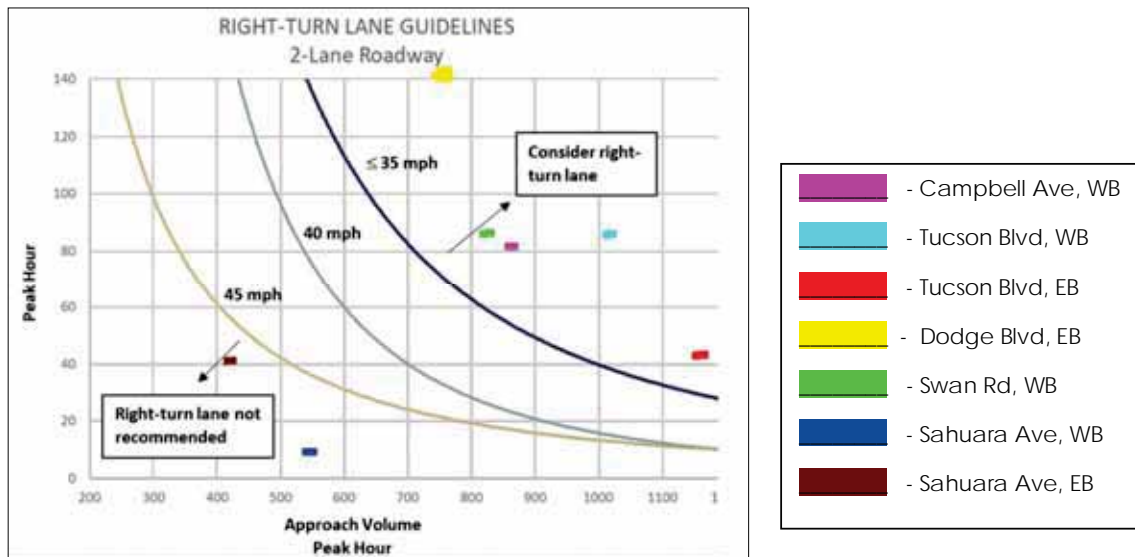




Appendix M

Right-Turn Lane Warrants

2021 Street Design Guide for the City of Tucson, Right Turn Lane Guidelines for Two-Lane Roadway, 2022 Road Diet Analysis





Appendix N
FHWA Road Diet
Feasibility Worksheets

FHWA Road Diet Feasibility Determinations for 2022 Road Diet Analysis

F.1. 2022 ROAD DIET ANALYSIS – SWAN RD TO ROSEMONT BLVD

The segment from Swan Road to Rosemond Boulevard had a daily volume of 15,511 vehicles for the conservative PAG volume collection in 2018.

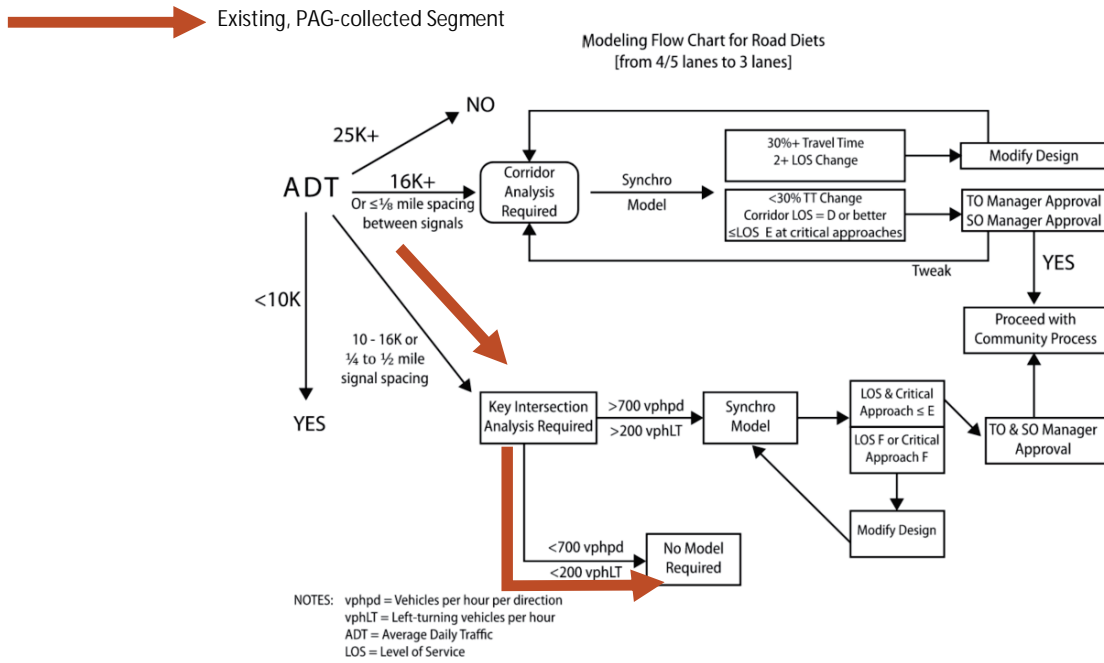


Figure 17. City of Seattle Modeling Flow Chart for Road Diet Feasibility Determination

Road Diet Feasibility Flow Chart – Swan Rd to Rosemont Blvd

Based on the Road Diet Feasibility flowchart analysis, a road diet is feasible on weekdays from Swan Road to Rosemont Boulevard along 5th Street based on the conservative PAG-collected daily volume segment.

F.2. 2022 ROAD DIET ANALYSIS – SAHUARA AVE TO WILMOT RD

The segment from Sahuara Avenue to Wilmot Road had a daily volume of 11,972 vehicles for the conservative PAG volume collection in 2018.

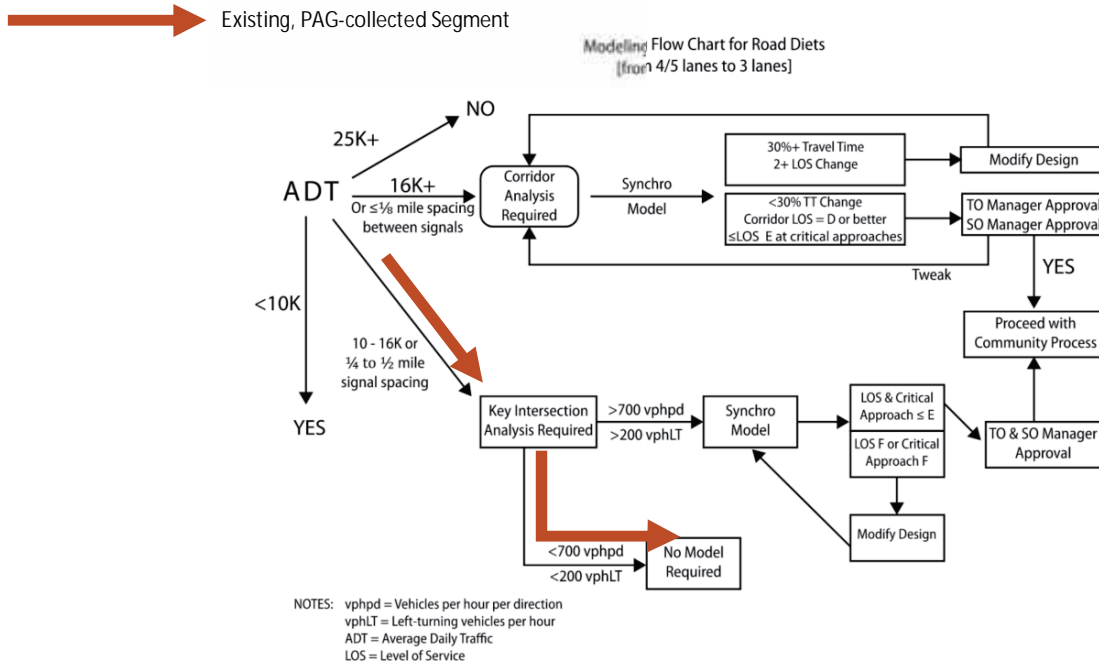


Figure 17. City of Seattle Modeling Flow Chart for Road Diet Feasibility Determination

Road Diet Feasibility Flow Chart – Sahuara Ave to Wilmot Rd

Based on the Road Diet Feasibility flowchart analysis, a road diet is feasible on weekdays from Sahuara Avenue to Wilmot Road along 5th Street based on the conservative PAG-collected daily volume segment.



Appendix O
2045 Traffic Analysis Worksheets

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	7:15	7:15	7:15	7:15	7:15	7:15	7:15
End Time	8:30	8:30	8:30	8:30	8:30	8:30	8:30
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	26919	26725	26993	26688	27008	26685	26923
Vehs Exited	26479	26092	26475	26058	26452	26206	26368
Starting Vehs	1215	1113	1215	1119	1199	1176	1234
Ending Vehs	1655	1746	1733	1749	1755	1655	1789
Travel Distance (mi)	27162	26898	27138	26942	27399	26903	27232
Travel Time (hr)	1562.7	1554.2	1562.5	1559.4	1539.4	1458.7	1597.8
Total Delay (hr)	716.8	714.6	717.0	719.1	685.0	620.9	748.6
Total Stops	38079	37423	37736	38927	38979	36439	38300
Fuel Used (gal)	1013.4	1008.4	1015.1	1008.6	1016.1	984.3	1024.8

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	7:15	7:15	7:15	7:15
End Time	8:30	8:30	8:30	8:30
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	26900	26911	27024	26882
Vehs Exited	26321	26338	26664	26349
Starting Vehs	1190	1170	1271	1173
Ending Vehs	1769	1743	1631	1713
Travel Distance (mi)	26976	27256	27293	27120
Travel Time (hr)	1566.2	1606.3	1500.1	1550.7
Total Delay (hr)	725.4	757.9	649.8	705.5
Total Stops	37259	38538	37882	37952
Fuel Used (gal)	1009.8	1028.2	1003.8	1011.2

Interval #0 Information Seeding

Start Time	7:15
End Time	7:30
Total Time (min)	15
Volumes adjusted by Growth Factors.	
No data recorded this interval.	

Interval #1 Information Recording

Start Time	7:30
End Time	8:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	26919	26725	26993	26688	27008	26685	26923
Vehs Exited	26479	26092	26475	26058	26452	26206	26368
Starting Vehs	1215	1113	1215	1119	1199	1176	1234
Ending Vehs	1655	1746	1733	1749	1755	1655	1789
Travel Distance (mi)	27162	26898	27138	26942	27399	26903	27232
Travel Time (hr)	1562.7	1554.2	1562.5	1559.4	1539.4	1458.7	1597.8
Total Delay (hr)	716.8	714.6	717.0	719.1	685.0	620.9	748.6
Total Stops	38079	37423	37736	38927	38979	36439	38300
Fuel Used (gal)	1013.4	1008.4	1015.1	1008.6	1016.1	984.3	1024.8

Interval #1 Information Recording

Start Time	7:30
End Time	8:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	26900	26911	27024	26882
Vehs Exited	26321	26338	26664	26349
Starting Vehs	1190	1170	1271	1173
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Travel Time (hr)	1566.2	1606.3	1500.1	1550.7
Total Delay (hr)	725.4	757.9	649.8	705.5
Total Stops	37259	38538	37882	37952
Fuel Used (gal)	1009.8	1028.2	1003.8	1011.2

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.5	0.2	0.4	9.1	9.5	6.1	7.9	7.1	6.9	3.8	2.9	2.7
Total Del/Veh (s)	281.2	40.3	15.7	78.1	92.0	96.6	343.8	221.3	144.0	184.6	241.9	246.8

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	5.3
Total Del/Veh (s)	182.8

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.2	0.2	0.0
Total Del/Veh (s)	16.5	5.0	4.3	6.5	0.7	0.2	35.5	13.0	46.8	50.6	22.4	3.8

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.8	0.3	0.2	1.6	0.5	0.5
Total Del/Veh (s)	41.0	21.1	17.3	29.2	23.4	23.7	41.8	24.1	19.7	38.9	29.0	24.0

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	24.8

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.1	0.0	0.1	0.0	0.1	0.0	0.1	0.2	1.4	0.2	0.2	0.1
Total Del/Veh (s)	51.6	31.0	41.0	37.3	7.0	45.2	26.5	6.2	46.5	33.3	29.5	31.7

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.3	0.0	0.2	1.0	0.1
Total Del/Veh (s)	5.6	5.1	10.2	8.8	46.1	9.3	8.5

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.3	0.2	0.3	1.4	0.2	0.3
Total Del/Veh (s)	35.6	19.6	18.7	30.8	33.6	32.9	72.6	40.4	38.0	59.5	49.2	46.5

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	40.9

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	10.7	0.6	0.3	7.6	3.2	1.3	18.7	4.3	15.8	16.1	8.8	2.5

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.0	0.0	0.1	0.0	0.0	1.6	0.4	0.4	1.9	0.4	0.3
Total Del/Veh (s)	24.2	9.1	7.6	24.5	13.7	12.7	38.6	20.5	16.1	35.1	20.6	16.4

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	16.3

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.5	0.3	1.5	1.6	0.3	1.4
Total Del/Veh (s)	35.9	30.0	30.5	40.7	37.6	36.5	40.6	30.0	12.1	61.3	30.2	8.9

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	31.9

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.3	0.1	0.1	0.1	0.0
Total Del/Veh (s)	7.6	1.6	0.9	26.1	7.2	3.8	8.1	95.1	70.1	29.9	7.9

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.3	0.0	0.0	0.4	0.0	0.0	1.6	0.4	0.3	1.2	0.3	0.4
Total Del/Veh (s)	52.7	15.8	13.4	35.8	17.3	15.4	45.9	26.2	20.4	35.6	28.7	24.0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	22.6

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.2	0.0	0.0	0.1	0.0	0.0	1.5	0.3	1.5	1.5	0.3	1.5
Total Del/Veh (s)	51.6	41.1	37.6	35.4	30.9	27.7	73.7	37.1	14.9	49.5	34.1	14.1

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	36.7

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.1	0.1	0.1	0.0
Total Del/Veh (s)	1.9	1.8	3.8	0.4	10.6	13.7	6.9	12.0	1.9

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.2	0.2	0.2	0.1	0.1	0.1
Total Del/Veh (s)	3.9	1.0	0.9	5.9	1.7	1.2	12.2	15.5	5.7	14.0	12.7	5.2

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	2.5

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.1	0.0	0.1	0.1	0.2	1.4	0.2	0.3	1.2	0.2	1.1
Total Del/Veh (s)	43.0	47.1	23.0	30.5	31.6	19.6	121.1	32.5	20.9	39.7	39.5	13.4

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	40.5

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.6	4.5	3.6

40: Country Club Rd Performance by movement

Movement	EBR	NBT	SBT	All
Denied Del/Veh (s)	0.0	0.2	0.0	0.1
Total Del/Veh (s)	11.1	9.5	2.5	6.6

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	3.4	2.0	2.6

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWT	SWR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0
Total Del/Veh (s)	2.9	0.4	2.6	2.9	2.7

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.6	2.6	2.2

200: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.2	0.3	0.0
Total Del/Veh (s)	1.6	1.7	3.0	29.2	32.5	25.0	6.3

1000: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.1	0.0
Total Del/Veh (s)	3.0	5.5	5.6	7.0	41.7	4.9

1050: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	6.5	2.2	4.6

1100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	1.0	3.4	3.6	5.8	2.5

1101: 5th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.2	0.1	0.0
Total Del/Veh (s)	2.8	3.0	4.0	4.7	16.5	7.3	20.3	4.6

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.2	0.0
Total Del/Veh (s)	3.3	4.1	1.3	12.7	5.8	3.3

1400: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	1.0	0.9	1.8	1.9	4.2	1.6

3000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.2	0.2	0.2	0.0
Total Del/Veh (s)	4.0	0.3	18.7	8.8	20.3	12.8	2.9

4000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.2	0.2	0.0
Total Del/Veh (s)	1.6	5.3	17.4	7.3	23.1	14.8	5.2

5000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	1.8	2.1	2.5	8.5	2.5

6000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.1	0.0
Total Del/Veh (s)	1.6	1.1	13.2	5.2	6.6	1.7

7000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.1	0.0
Total Del/Veh (s)	0.9	2.0	4.7	12.0	6.6	1.8

8000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.2	0.0
Total Del/Veh (s)	2.7	2.7	3.7	4.0	5.1	6.0	3.5

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.2	0.0
Total Del/Veh (s)	3.2	3.3	0.9	19.1	7.5	2.9

Total Network Performance

Denied Del/Veh (s)	1.6
Total Del/Veh (s)	89.0

Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	40.3	107.0	0.6	19
	1000	3.7	23.4	0.2	25
	2	5.4	26.3	0.2	24
Wilson HAWK	20	1.5	4.1	0.0	17
6th Side Street	200	1.6	9.1	0.1	25
Tucson Blvd	3	20.8	30.1	0.1	9
	3000	4.4	45.5	0.4	29
6th/5th Street	72	3.0	6.7	0.0	16
Country Club Rd	4	31.0	44.0	0.1	9
	9922	2.2	14.2	0.1	24
	4000	1.6	30.7	0.2	29
Dodge Blvd	5	5.5	52.2	0.4	28
	5000	1.9	17.0	0.1	27
Alvernon Wy	6	19.6	33.7	0.1	13
	6000	2.0	16.8	0.1	27
	70	3.4	16.6	0.1	23
	7	0.6	2.6	0.0	26
Columbus Blvd	7000	0.9	16.6	0.1	28
	8	9.0	22.3	0.1	18
	8000	3.0	33.4	0.3	28
Swan Rd	9	29.9	58.1	0.3	16
	9000	4.4	19.1	0.1	23
	1050	6.5	19.8	0.1	20
	10	1.6	3.7	0.0	19
Rosemont Blvd	1100	1.2	16.5	0.1	28
	11	15.8	29.5	0.1	15
5th Side Street	1101	3.2	32.3	0.3	28
Craycroft Rd	12	43.4	71.4	0.2	12
	1200	4.3	30.8	0.2	26
	13	1.9	28.0	0.2	28
Chantilly Dr HAWK	130	1.6	4.2	0.0	16
Sahuara Ave	14	1.0	8.1	0.1	27
	1400	1.1	27.7	0.3	33
Wilmot Rd	15	47.1	70.4	0.3	13
Total		324.4	971.8	5.6	21

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	31.6	131.5	0.4	12
	1400	1.3	29.0	0.3	31
Sahuara Ave	14	1.8	32.0	0.3	28
	130	2.6	10.1	0.1	22
	13	0.4	2.4	0.0	28
	1200	1.3	27.4	0.2	29
Craycroft Rd	12	29.9	55.3	0.2	15
5th Side Street	1101	5.3	34.6	0.2	26
Rosemont Blvd	11	17.4	46.2	0.3	20
	1100	4.0	18.0	0.1	24
	10	7.2	22.5	0.1	20
Arcadia Ave HAWK	1050	2.2	4.7	0.0	14
	9000	0.9	13.8	0.1	28
Swan Rd	9	38.5	52.1	0.1	9
	8000	4.3	33.9	0.3	27
Columbus Blvd	8	13.9	52.9	0.3	23
	7000	2.3	15.8	0.1	26
	7	3.2	18.8	0.1	25
Irving Ave HAWK	70	2.0	4.6	0.0	15
	6000	1.1	13.7	0.1	28
Alvernon Wy	6	33.4	47.5	0.1	9
	5000	3.1	17.8	0.1	25
Dodge Blvd	5	9.1	24.0	0.1	19
	4000	5.4	52.2	0.4	28
	9922	2.5	31.9	0.2	28
Country Club Rd	4	37.3	48.2	0.1	7
6th/5th Street	72	3.8	17.5	0.1	24
	3000	0.3	4.3	0.0	25
Tucson Blvd	3	23.4	65.5	0.4	20
6th Side Street	200	3.3	12.9	0.1	22
Wilson HAWK	20	4.7	12.8	0.1	18
	2	0.7	2.7	0.0	25
	1000	5.6	26.6	0.2	24
Campbell Ave	1	92.0	119.1	0.2	5
Total		395.8	1102.7	5.5	18

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	419	790	737	200	380	769	770	390	2049	2009	1966	370
Average Queue (ft)	331	416	310	104	306	551	567	382	1409	1377	1296	212
95th Queue (ft)	516	915	755	214	486	820	824	437	2318	2278	2184	492
Link Distance (ft)		2986	2986			757	757		2020	2020	2020	
Upstream Blk Time (%)						4	4		18	12	10	
Queuing Penalty (veh)						24	27		0	0	0	
Storage Bay Dist (ft)	250			50	180			290				150
Storage Blk Time (%)	60	1	53	13	18	58		61	55		68	
Queuing Penalty (veh)	161	1	113	36	90	156		364	172		73	

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	349	2033	2021	1962
Average Queue (ft)	156	1231	1209	1165
95th Queue (ft)	409	2208	2171	2094
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)		7	5	5
Queuing Penalty (veh)		0	0	0
Storage Bay Dist (ft)	200			
Storage Blk Time (%)		70		
Queuing Penalty (veh)		59		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	187	216	61	59	69	136
Average Queue (ft)	64	51	12	7	25	46
95th Queue (ft)	149	159	47	35	58	97
Link Distance (ft)	866	866	55	55	433	438
Upstream Blk Time (%)			1	0		
Queuing Penalty (veh)			8	2		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

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Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	112	234	254	257	432	443	40	362	196	422
Average Queue (ft)	43	105	120	57	228	248	10	185	54	236
95th Queue (ft)	88	202	220	173	390	411	34	309	128	369
Link Distance (ft)		356	356		1877	1877		1935		2179
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	200			100			300		300	
Storage Blk Time (%)		1		1	23			1		3
Queuing Penalty (veh)		1		5	15			0		2

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	NB	NB	NB	NB	SB
Directions Served	L	T	T	L	T	T	R	L	T	T	R	L
Maximum Queue (ft)	147	195	206	190	396	421	260	172	207	208	170	174
Average Queue (ft)	64	106	117	117	250	261	104	106	174	180	48	111
95th Queue (ft)	124	178	193	217	366	382	307	185	206	211	154	205
Link Distance (ft)		488	488		423	423			172	172		
Upstream Blk Time (%)					0	0		1	17	20	0	
Queuing Penalty (veh)					0	1		0	104	120	0	
Storage Bay Dist (ft)	180			90			160	100			70	100
Storage Blk Time (%)	0	1		12	41	32	0	12	37	43	0	7
Queuing Penalty (veh)	0	1		51	57	63	2	57	54	42	1	29

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	TR
Maximum Queue (ft)	413	427
Average Queue (ft)	248	254
95th Queue (ft)	375	379
Link Distance (ft)	2000	2000
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)	37	
Queuing Penalty (veh)	48	

Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	WB	NB	NB
Directions Served	T	TR	L	T	T	L	R
Maximum Queue (ft)	127	147	126	298	304	83	112
Average Queue (ft)	34	40	50	125	142	33	49
95th Queue (ft)	92	106	95	258	278	74	86
Link Distance (ft)	2071	2071		619	619	2122	
Upstream Blk Time (%)							
Queuing Penalty (veh)							
Storage Bay Dist (ft)			300			220	
Storage Blk Time (%)				0			
Queuing Penalty (veh)				0			

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	TR	L	T	TR
Maximum Queue (ft)	187	150	182	176	360	369	250	563	564	250	650	652
Average Queue (ft)	94	73	93	63	200	218	162	338	342	155	411	411
95th Queue (ft)	162	134	157	135	313	333	287	558	556	305	628	626
Link Distance (ft)		581	581		578	578		2106	2106		1977	1977
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300			300			150			150		
Storage Blk Time (%)					1		9	35		2	43	
Queuing Penalty (veh)					1		51	53		13	57	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	43	45	153	138	44	40
Average Queue (ft)	3	3	56	31	17	15
95th Queue (ft)	22	22	128	103	42	38
Link Distance (ft)	56	56	627	627	304	320
Upstream Blk Time (%)	0	0				
Queuing Penalty (veh)	1	0				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	96	161	179	131	248	249	192	318	151	294
Average Queue (ft)	33	66	84	35	127	138	57	139	43	143
95th Queue (ft)	74	124	146	83	215	230	125	246	108	238
Link Distance (ft)		533	533		1685	1685		2072		1936
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			75			250		250	
Storage Blk Time (%)	2	5		1	15			1		1
Queuing Penalty (veh)	5	2		4	7			1		0

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	149	238	259	165	289	314	303	408	392	213	314	388
Average Queue (ft)	58	123	149	70	173	198	76	241	237	71	161	247
95th Queue (ft)	114	205	233	137	261	283	193	360	355	145	275	353
Link Distance (ft)		1226	1226		560	560		1988	1988			2060
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300			500			180			350	230	
Storage Blk Time (%)								19	1		3	10
Queuing Penalty (veh)								15	3		15	20

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	395	210
Average Queue (ft)	247	39
95th Queue (ft)	359	124
Link Distance (ft)	2060	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	3	
Queuing Penalty (veh)	3	

Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	71	58	280	242	142	143
Average Queue (ft)	29	17	131	70	65	50
95th Queue (ft)	72	53	235	188	112	114
Link Distance (ft)	52	52	616	616	261	294
Upstream Blk Time (%)	5	1				
Queuing Penalty (veh)	20	3				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	170	279	290	205	347	355	226	312	201	391
Average Queue (ft)	95	130	144	53	173	185	77	144	39	204
95th Queue (ft)	170	235	246	140	310	329	155	251	129	332
Link Distance (ft)		570	570		1260	1260		1970		2041
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			100			150		150	
Storage Blk Time (%)	26	17		1	14		1	8	0	18
Queuing Penalty (veh)	76	20		6	9		4	9	0	7

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	210	225	262	165	248	271	330	524	501	230	299	418
Average Queue (ft)	103	128	162	81	148	164	195	310	307	114	121	268
95th Queue (ft)	179	208	246	145	227	240	344	467	462	272	267	383
Link Distance (ft)		1212	1212		1092	1092		1951	1951			2065
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300			500			180			180	150	
Storage Blk Time (%)	0						16	29	29	0	2	29
Queuing Penalty (veh)	0						93	59	33	0	10	35

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	430	300
Average Queue (ft)	268	107
95th Queue (ft)	387	277
Link Distance (ft)	2065	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	28	0
Queuing Penalty (veh)	52	1

Intersection: 13: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	74	104	59	45	68	60
Average Queue (ft)	8	13	7	3	32	21
95th Queue (ft)	39	58	33	21	57	50
Link Distance (ft)	1086	1086	56	56	422	473
Upstream Blk Time (%)			0	0		
Queuing Penalty (veh)			1	0		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	53	48	81	20	87	52
Average Queue (ft)	5	3	25	2	41	25
95th Queue (ft)	27	22	64	15	69	49
Link Distance (ft)	275	275	1271	1271	614	699
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	159	198	161	154	359	786	748	499	230	572	561	554
Average Queue (ft)	66	122	88	68	308	429	395	229	123	352	346	315
95th Queue (ft)	126	179	147	129	416	847	799	390	270	529	521	499
Link Distance (ft)		1219	1219	2255		1997	1997	1997		2032	2032	2032
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	150				240				130			
Storage Blk Time (%)	0	4			47	5			1	43		10
Queuing Penalty (veh)	0	4			234	18			5	53		36

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	399
Average Queue (ft)	136
95th Queue (ft)	332
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	0
Queuing Penalty (veh)	0

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	68	71	273	286
Average Queue (ft)	19	23	77	85
95th Queue (ft)	60	67	219	234
Link Distance (ft)	55	55	298	298
Upstream Blk Time (%)	2	4	0	0
Queuing Penalty (veh)	7	13	1	1
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 40: Country Club Rd

Movement	EB	NB	NB
Directions Served	R	T	T
Maximum Queue (ft)	92	317	340
Average Queue (ft)	41	100	114
95th Queue (ft)	74	259	277
Link Distance (ft)	451	2094	2094
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	155	171	75	83
Average Queue (ft)	30	41	30	35
95th Queue (ft)	107	129	76	87
Link Distance (ft)	513	513	56	56
Upstream Blk Time (%)			5	6
Queuing Penalty (veh)			21	27
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 72: 6th/5th Street

Movement	SW
Directions Served	R
Maximum Queue (ft)	3
Average Queue (ft)	0
95th Queue (ft)	3
Link Distance (ft)	488
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	
Storage Blk Time (%)	
Queuing Penalty (veh)	

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	63	71	107	115
Average Queue (ft)	14	23	26	32
95th Queue (ft)	52	67	80	91
Link Distance (ft)	56	56	275	275
Upstream Blk Time (%)	2	4		
Queuing Penalty (veh)	4	8		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 200: 6th Side Street & 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	TR	T	TR	LTR	LTR
Maximum Queue (ft)	11	32	59	133	224
Average Queue (ft)	0	1	2	60	93
95th Queue (ft)	9	18	25	108	175
Link Distance (ft)	298	356	356	419	408
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 1000: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	TR	T	TR	LTR	LTR
Maximum Queue (ft)	2	195	200	89	39
Average Queue (ft)	0	32	35	37	8
95th Queue (ft)	2	171	182	71	31
Link Distance (ft)	757	866	866	442	411
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 1050: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	200	257	61	70
Average Queue (ft)	52	73	24	27
95th Queue (ft)	148	206	67	71
Link Distance (ft)	526	526	52	52
Upstream Blk Time (%)			4	7
Queuing Penalty (veh)			15	24
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1100: 6th/5th Street

Movement	WB	WB	NB
Directions Served	T	TR	LTR
Maximum Queue (ft)	8	11	55
Average Queue (ft)	0	0	26
95th Queue (ft)	6	8	52
Link Distance (ft)	570	570	214
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1101: 5th Side Street & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	T	TR	T	TR	LTR	LTR
Maximum Queue (ft)	5	2	10	14	100	81
Average Queue (ft)	0	0	0	1	43	36
95th Queue (ft)	7	2	6	9	77	69
Link Distance (ft)	1260	1260	1212	1212	332	276
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 1200: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	10	23	89	82
Average Queue (ft)	0	1	38	41
95th Queue (ft)	8	13	70	68
Link Distance (ft)	1092	1092	211	276
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1400: 6th/5th Street

Movement	EB	WB	SB
Directions Served	T	T	LTR
Maximum Queue (ft)	8	4	67
Average Queue (ft)	0	0	28
95th Queue (ft)	8	3	52
Link Distance (ft)	1271	1219	274
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

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Intersection: 3000: 6th/5th Street

Movement	WB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	2	87	97
Average Queue (ft)	0	36	43
95th Queue (ft)	2	68	76
Link Distance (ft)	100	405	430
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 4000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	58	170
Average Queue (ft)	22	70
95th Queue (ft)	51	130
Link Distance (ft)	547	583
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 5000: 6th/5th Street

Movement	EB	EB	SB
Directions Served	T	TR	LTR
Maximum Queue (ft)	13	12	88
Average Queue (ft)	0	0	33
95th Queue (ft)	8	8	63
Link Distance (ft)	619	619	280
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 6000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	68	63
Average Queue (ft)	32	24
95th Queue (ft)	59	50
Link Distance (ft)	355	360
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 7000: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	61	52
Average Queue (ft)	25	23
95th Queue (ft)	51	49
Link Distance (ft)	260	304
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 8000: 6th/5th Street

Movement	EB	NB	SB
Directions Served	T	LTR	LTR
Maximum Queue (ft)	6	67	90
Average Queue (ft)	0	35	40
95th Queue (ft)	5	58	67
Link Distance (ft)	1287	319	333
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 9000: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	23	33	48	103
Average Queue (ft)	1	1	14	48
95th Queue (ft)	11	13	41	80
Link Distance (ft)	560	560	204	310
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 3103

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	4:15	4:15	4:15	4:15	4:15	4:15	4:15
End Time	5:30	5:30	5:30	5:30	5:30	5:30	5:30
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	28557	28710	28946	28410	28827	28601	28801
Vehs Exited	28122	28240	28383	27912	28298	28255	28379
Starting Vehs	1478	1461	1378	1377	1521	1570	1448
Ending Vehs	1913	1931	1941	1875	2050	1916	1870
Travel Distance (mi)	30128	30026	30230	29711	30265	30123	30124
Travel Time (hr)	1844.3	1911.3	1880.7	1912.7	1955.7	1987.2	1864.1
Total Delay (hr)	903.3	971.1	935.6	985.0	1007.6	1045.4	924.6
Total Stops	42897	44177	45114	42402	45164	45083	42370
Fuel Used (gal)	1153.7	1166.7	1165.5	1159.7	1182.6	1186.5	1158.0

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	4:15	4:15	4:15	4:15
End Time	5:30	5:30	5:30	5:30
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	28731	28569	28729	28691
Vehs Exited	28252	28257	28259	28232
Starting Vehs	1512	1498	1513	1466
Ending Vehs	1991	1810	1983	1912
Travel Distance (mi)	29941	29895	29964	30041
Travel Time (hr)	1924.8	1926.0	1954.5	1916.1
Total Delay (hr)	988.4	992.7	1020.4	977.4
Total Stops	43873	44466	43618	43916
Fuel Used (gal)	1168.4	1172.2	1174.6	1168.8

Interval #0 Information Seeding

Start Time	4:15
End Time	4:30
Total Time (min)	15
No data recorded this interval.	

Interval #1 Information Recording

Start Time	4:30
End Time	5:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	28557	28710	28946	28410	28827	28601	28801
Vehs Exited	28122	28240	28383	27912	28298	28255	28379
Starting Vehs	1478	1461	1378	1377	1521	1570	1448
Ending Vehs	1913	1931	1941	1875	2050	1916	1870
Travel Distance (mi)	30128	30026	30230	29711	30265	30123	30124
Travel Time (hr)	1844.3	1911.3	1880.7	1912.7	1955.7	1987.2	1864.1
Total Delay (hr)	903.3	971.1	935.6	985.0	1007.6	1045.4	924.6
Total Stops	42897	44177	45114	42402	45164	45083	42370
Fuel Used (gal)	1153.7	1166.7	1165.5	1159.7	1182.6	1186.5	1158.0

Interval #1 Information Recording

Start Time	4:30
End Time	5:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	28731	28569	28729	28691
Vehs Exited	28252	28257	28259	28232
Starting Vehs	1512	1498	1513	1466
Ending Vehs	1991	1810	1983	1912
Travel Distance (mi)	29941	29895	29964	30041
Travel Time (hr)	1924.8	1926.0	1954.5	1916.1
Total Delay (hr)	988.4	992.7	1020.4	977.4
Total Stops	43873	44466	43618	43916
Fuel Used (gal)	1168.4	1172.2	1174.6	1168.8

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.6	0.3	0.5	0.0	0.0	0.0	1.1	0.2	1.0	164.7	161.5	160.8
Total Del/Veh (s)	117.3	121.5	102.8	66.8	42.6	42.0	189.0	62.1	19.8	337.3	417.1	432.1

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	51.8
Total Del/Veh (s)	180.8

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	10.0	6.1	5.4	12.0	1.0	0.2	41.8	20.0	40.0	37.5	14.7	5.1

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.1	0.2	0.1	0.0	0.0	1.9	0.5	0.5	8.5	5.9	5.7
Total Del/Veh (s)	52.5	19.0	19.3	37.7	12.2	12.9	218.9	55.1	51.8	325.2	98.8	94.2

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	1.4
Total Del/Veh (s)	49.0

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.0	0.1	0.1	0.2	0.3	1.3	0.2	0.3	0.2
Total Del/Veh (s)	27.6	31.0	74.0	33.8	7.8	40.3	35.4	16.4	132.8	85.3	74.0	48.6

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.1	0.4	0.0	0.3	1.0	0.2
Total Del/Veh (s)	12.0	11.9	28.5	7.1	40.8	15.1	13.1

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.2	0.3	0.3	1.3	0.2	0.3
Total Del/Veh (s)	159.7	59.5	62.9	50.5	61.9	67.6	116.0	77.1	73.9	78.2	51.8	47.8

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	69.4

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.1
Total Del/Veh (s)	4.3	0.6	0.2	9.3	2.0	2.6	18.0	10.8	6.6	16.4	21.6	7.1

7: 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	1.5

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.5	0.4	0.4	1.8	0.4	0.4
Total Del/Veh (s)	55.2	15.0	15.2	41.4	17.4	18.9	36.0	17.6	14.5	29.9	18.1	13.5

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	18.7

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.0	0.0	0.0	0.0	0.0	1.6	0.3	1.6	1.6	0.3	1.4
Total Del/Veh (s)	61.2	42.6	41.0	93.3	48.3	46.6	56.1	42.9	18.5	108.9	34.1	9.8

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	44.9

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.2	0.1	0.2	0.1	0.1	0.1
Total Del/Veh (s)	5.4	1.1	0.4	18.7	3.9	2.6	30.4	17.5	10.0	23.6	30.8	11.6

10: 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	3.5

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.9	0.3	0.4	0.4	0.0	0.1	1.9	0.5	0.5	1.4	0.4	0.4
Total Del/Veh (s)	48.4	26.6	24.3	57.9	19.8	17.4	56.0	34.9	30.9	52.5	33.2	27.0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	29.8

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.5	0.3	1.4	1.6	0.2	1.5
Total Del/Veh (s)	47.5	35.9	37.8	43.5	25.2	21.7	68.8	63.2	38.4	49.5	48.0	17.0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	46.8

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.1	0.2	0.1	0.1	0.0
Total Del/Veh (s)	2.9	3.1	6.3	0.6	0.1	11.8	8.7	14.7	15.2	2.9

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	3.8	1.1	0.7	6.4	1.7	1.4	11.6	5.7	11.4	4.4	1.9

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.1	0.0	0.1	0.1	0.1	1.3	0.2	0.2	1.2	0.2	1.1
Total Del/Veh (s)	45.6	43.1	22.7	23.0	29.0	18.3	83.5	34.0	25.5	39.6	30.5	7.3

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	34.2

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.7	4.2	2.8

40: Country Club Rd Performance by movement

Movement	EBR	NBT	SBT	All
Denied Del/Veh (s)	0.0	0.3	0.0	0.1
Total Del/Veh (s)	10.6	36.5	2.7	20.8

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	4.0	1.4	2.8

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWT	SWR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0
Total Del/Veh (s)	2.0	0.5	2.7	5.0	3.1

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.8	2.9	2.2

200: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.2	0.2	0.2	0.0
Total Del/Veh (s)	1.6	1.1	1.6	1.5	34.3	22.4	38.0	22.0	4.6

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	5.5	5.7	1.5	1.3	29.2	6.3	3.9

1050: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	6.6	2.3	4.8

1100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.1	0.0
Total Del/Veh (s)	1.5	2.4	2.9	12.3	22.2	3.1

1101: 5th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.2	0.1	0.1	0.0
Total Del/Veh (s)	4.5	5.1	3.0	3.2	25.7	14.3	21.1	7.7	5.7

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.2	0.0
Total Del/Veh (s)	4.4	5.6	1.3	15.7	5.3	4.2

1400: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.1	0.0
Total Del/Veh (s)	1.7	1.3	1.3	5.7	12.1	2.1

3000: 6th/5th Street Performance by movement

Movement	EBT	WBT	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.2	0.2	0.0
Total Del/Veh (s)	6.8	0.4	46.7	32.4	6.9

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0
Total Del/Veh (s)	4.4	5.3	4.7	5.4	4.6

5000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.2	0.0
Total Del/Veh (s)	6.3	3.3	3.0	37.6	11.8	5.8

6000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.2	0.0
Total Del/Veh (s)	3.8	4.0	1.7	36.9	18.4	6.0

7000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBL	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.1	0.0
Total Del/Veh (s)	1.7	1.9	2.9	3.6	7.6	19.3	2.9

8000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	3.6	3.9	7.5	7.8	3.9

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.2	0.0
Total Del/Veh (s)	4.0	4.5	1.0	25.7	8.1	4.1

Total Network Performance

Denied Del/Veh (s)	12.3
Total Del/Veh (s)	104.9

Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	121.5	187.4	0.6	11
	1000	6.7	26.7	0.2	22
	2	6.1	27.0	0.2	23
Wilson HAWK	20	1.6	4.2	0.0	16
6th Side Street	200	1.6	9.0	0.1	26
Tucson Blvd	3	19.3	28.6	0.1	10
	3000	7.3	51.0	0.4	26
6th/5th Street	72	2.1	7.1	0.0	15
Country Club Rd	4	31.0	46.8	0.1	9
	9922	2.9	14.9	0.1	23
	4000	4.4	33.6	0.2	26
Dodge Blvd	5	12.0	57.4	0.4	25
	5000	6.7	21.5	0.1	22
Alvernon Wy	6	59.5	73.5	0.1	6
	6000	4.6	19.8	0.1	23
	70	4.0	17.1	0.1	22
	7	0.6	2.7	0.0	26
	7000	1.8	17.4	0.1	27
Columbus Blvd	8	14.9	27.9	0.1	15
	8000	3.9	33.9	0.3	27
Swan Rd	9	41.8	70.6	0.3	13
	9000	5.2	20.6	0.1	22
	1050	6.6	19.9	0.1	20
	10	1.1	3.2	0.0	21
	1100	1.6	16.9	0.1	27
Rosemont Blvd	11	27.3	40.9	0.1	11
5th Side Street	1101	5.0	33.7	0.3	27
Craycroft Rd	12	36.2	64.5	0.2	14
	1200	5.2	32.4	0.2	25
	13	2.9	28.9	0.2	27
Chantilly Dr HAWK	130	1.7	4.2	0.0	16
Sahuara Ave	14	1.1	8.3	0.1	26
	1400	1.7	31.8	0.3	28
Wilmot Rd	15	43.1	70.0	0.3	13
Total		493.0	1153.4	5.6	18

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	29.0	128.4	0.4	12
	1400	0.8	25.5	0.3	35
Sahuara Ave	14	1.7	32.0	0.3	28
	130	2.9	10.5	0.1	21
	13	0.6	2.6	0.0	26
	1200	1.3	27.3	0.2	29
Craycroft Rd	12	23.9	49.7	0.2	16
5th Side Street	1101	3.8	33.3	0.2	27
Rosemont Blvd	11	20.1	48.3	0.3	19
	1100	2.9	16.8	0.1	26
	10	3.9	19.3	0.1	24
Arcadia Ave HAWK	1050	2.2	4.8	0.0	14
	9000	1.0	13.9	0.1	28
Swan Rd	9	49.5	63.4	0.1	7
	8000	4.7	35.2	0.3	26
Columbus Blvd	8	17.3	55.1	0.3	22
	7000	3.2	16.5	0.1	25
	7	2.0	17.6	0.1	27
Irving Ave HAWK	70	1.4	4.0	0.0	17
	6000	1.7	14.4	0.1	27
Alvernon Wy	6	61.3	75.3	0.1	6
	5000	3.7	18.7	0.1	24
Dodge Blvd	5	7.2	21.7	0.1	21
	4000	5.0	50.6	0.4	29
	9922	2.1	31.6	0.2	28
Country Club Rd	4	33.8	44.7	0.1	8
6th/5th Street	72	6.5	22.7	0.1	18
	3000	0.4	9.9	0.0	11
Tucson Blvd	3	11.4	54.8	0.4	24
6th Side Street	200	1.7	11.3	0.1	25
Wilson HAWK	20	4.2	12.2	0.1	19
	2	1.0	3.0	0.0	23
	1000	1.5	22.5	0.2	28
Campbell Ave	1	42.7	60.8	0.2	10
Total		356.4	1058.0	5.5	19

Queuing and Blocking Report
2045 No Build PM

11/06/2022

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	425	1350	1350	200	355	381	382	390	724	690	564	370
Average Queue (ft)	353	811	834	179	173	227	246	348	523	497	371	151
95th Queue (ft)	543	1435	1439	255	292	337	353	463	819	778	549	391
Link Distance (ft)		2986	2986			757	757		2020	2020	2020	
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	250			50	180			290				150
Storage Blk Time (%)	14	54	68	34	13	19		53	18		47	0
Queuing Penalty (veh)	72	151	195	169	45	42		266	47		82	1

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	350	2214	2212	2210
Average Queue (ft)	251	1977	1963	1932
95th Queue (ft)	479	2616	2620	2631
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)		60	58	58
Queuing Penalty (veh)		0	0	0
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	0	74		
Queuing Penalty (veh)	0	132		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	257	278	71	56	98	90
Average Queue (ft)	75	78	33	6	37	38
95th Queue (ft)	206	224	75	31	77	75
Link Distance (ft)	866	866	55	55	433	438
Upstream Blk Time (%)			8	0		
Queuing Penalty (veh)			37	1		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	257	362	365	114	233	253	399	706	400	1176
Average Queue (ft)	108	207	224	37	79	97	114	400	228	677
95th Queue (ft)	207	359	375	86	178	195	306	676	460	1357
Link Distance (ft)		356	356		1877	1877		1935		2179
Upstream Blk Time (%)	0	1	1							2
Queuing Penalty (veh)	0	5	8							0
Storage Bay Dist (ft)	200			100			300		300	
Storage Blk Time (%)	3	9		2	7			25	10	41
Queuing Penalty (veh)	15	14		8	4			10	65	31

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	NB	NB	NB	NB	SB
Directions Served	L	T	T	L	T	T	R	L	T	T	R	L
Maximum Queue (ft)	252	338	347	190	349	353	260	172	219	223	170	174
Average Queue (ft)	89	186	194	143	191	191	78	103	182	188	86	164
95th Queue (ft)	191	297	306	219	309	310	254	189	203	207	202	211
Link Distance (ft)		488	488		423	423			172	172		
Upstream Blk Time (%)		0	0		0	0		1	42	41	1	
Queuing Penalty (veh)		0	0		0	0		0	276	269	0	
Storage Bay Dist (ft)	180			90			160	100			70	100
Storage Blk Time (%)	1	13		46	29	14	4	10	57	56	3	49
Queuing Penalty (veh)	3	25		139	54	37	11	52	88	74	16	227

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	TR
Maximum Queue (ft)	825	819
Average Queue (ft)	525	514
95th Queue (ft)	931	903
Link Distance (ft)	2000	2000
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)	51	
Queuing Penalty (veh)	106	

Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	WB	NB	NB
Directions Served	T	TR	L	T	T	L	R
Maximum Queue (ft)	249	263	246	302	295	178	168
Average Queue (ft)	99	110	122	110	120	90	79
95th Queue (ft)	202	220	211	245	260	153	137
Link Distance (ft)	2071	2071		619	619	2122	
Upstream Blk Time (%)							
Queuing Penalty (veh)							
Storage Bay Dist (ft)			300				220
Storage Blk Time (%)			0	0		0	0
Queuing Penalty (veh)			0	0		0	0

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	TR	L	T	TR
Maximum Queue (ft)	398	559	571	310	542	548	250	978	967	250	666	662
Average Queue (ft)	311	382	372	109	328	345	184	621	620	164	446	445
95th Queue (ft)	466	617	587	276	536	541	314	1039	1024	296	701	689
Link Distance (ft)		581	581		578	578		2106	2106		1977	1977
Upstream Blk Time (%)		9	2		1	2						
Queuing Penalty (veh)		51	11		7	9						
Storage Bay Dist (ft)	300			300			150			150		
Storage Blk Time (%)	34	12		0	18		17	50		5	43	
Queuing Penalty (veh)	125	29		0	18		107	72		30	55	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	62	40	137	139	54	39
Average Queue (ft)	8	3	20	18	23	13
95th Queue (ft)	37	22	82	79	48	36
Link Distance (ft)	56	56	627	627	304	320
Upstream Blk Time (%)	0	0				
Queuing Penalty (veh)	2	1				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	169	228	243	150	244	243	99	273	90	299
Average Queue (ft)	70	124	139	43	120	140	43	150	35	156
95th Queue (ft)	136	200	212	98	212	227	84	239	72	249
Link Distance (ft)		533	533		1685	1685		2072		1936
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			75			250		250	
Storage Blk Time (%)	18	20		4	18			1		1
Queuing Penalty (veh)	71	17		14	10			0		0

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	290	411	440	281	377	418	304	588	607	450	348	451
Average Queue (ft)	131	218	238	149	246	267	165	392	388	126	191	292
95th Queue (ft)	250	369	393	262	353	377	353	549	551	362	344	440
Link Distance (ft)		1226	1226		560	560		1988	1988			2060
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300			500			180			350	230	
Storage Blk Time (%)	0	3					0	37	12		15	15
Queuing Penalty (veh)	2	5					3	42	24		84	25

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	450	179
Average Queue (ft)	286	38
95th Queue (ft)	432	114
Link Distance (ft)	2060	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	4	
Queuing Penalty (veh)	4	

Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	72	55	164	171	109	79
Average Queue (ft)	28	7	56	37	41	34
95th Queue (ft)	71	35	129	119	79	66
Link Distance (ft)	52	52	616	616	261	294
Upstream Blk Time (%)	3	0				
Queuing Penalty (veh)	15	2				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	174	481	492	188	313	324	250	487	236	406
Average Queue (ft)	121	291	311	44	143	157	135	248	59	210
95th Queue (ft)	209	458	474	115	273	294	256	420	152	340
Link Distance (ft)		570	570		1260	1260		1970		2041
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			100			150		150	
Storage Blk Time (%)	24	29		1	16		5	24	0	21
Queuing Penalty (veh)	118	44		4	7		20	36	2	12

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	269	351	391	207	186	216	330	797	780	230	300	593
Average Queue (ft)	136	201	241	102	99	118	176	480	477	138	94	366
95th Queue (ft)	239	327	371	180	163	182	377	784	772	298	264	559
Link Distance (ft)		1212	1212		1092	1092		1951	1951			2065
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300			500			180			180	150	
Storage Blk Time (%)	0	2					1	48	47	0	0	44
Queuing Penalty (veh)	1	3					8	62	60	0	0	26

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	575	300
Average Queue (ft)	366	133
95th Queue (ft)	556	343
Link Distance (ft)	2065	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	43	
Queuing Penalty (veh)	49	

Intersection: 13: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	124	150	64	36	92	60
Average Queue (ft)	17	23	13	2	40	24
95th Queue (ft)	72	89	46	14	72	53
Link Distance (ft)	1086	1086	56	56	422	473
Upstream Blk Time (%)			1	0		
Queuing Penalty (veh)			3	0		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	74	52	75	55	49	59
Average Queue (ft)	12	4	17	3	18	27
95th Queue (ft)	46	28	55	22	45	52
Link Distance (ft)	275	275	1271	1271	614	699
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	249	328	217	107	359	542	523	446	230	433	422	396
Average Queue (ft)	146	178	127	39	245	325	318	276	53	282	276	242
95th Queue (ft)	228	265	199	85	397	494	477	409	171	388	379	350
Link Distance (ft)		1219	1219	2255		1997	1997	1997		2032	2032	2032
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	150				240				130			
Storage Blk Time (%)	9	23			19	16				37		2
Queuing Penalty (veh)	43	52			126	44				18		4

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	190
Average Queue (ft)	56
95th Queue (ft)	132
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	77	84	213	216
Average Queue (ft)	27	30	57	57
95th Queue (ft)	72	79	156	160
Link Distance (ft)	55	55	298	298
Upstream Blk Time (%)	5	7		0
Queuing Penalty (veh)	34	43		0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 40: Country Club Rd

Movement	EB	NB	NB
Directions Served	R	T	T
Maximum Queue (ft)	112	579	580
Average Queue (ft)	56	302	302
95th Queue (ft)	95	600	593
Link Distance (ft)	451	2094	2094
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	207	216	73	81
Average Queue (ft)	44	51	23	23
95th Queue (ft)	150	164	66	70
Link Distance (ft)	513	513	56	56
Upstream Blk Time (%)			3	3
Queuing Penalty (veh)			12	13
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 72: 6th/5th Street

Movement	EB	EB	SW	SW
Directions Served	L	L	R	R
Maximum Queue (ft)	10	22	43	25
Average Queue (ft)	1	1	2	1
95th Queue (ft)	9	20	20	12
Link Distance (ft)	100	100	488	488
Upstream Blk Time (%)		0		
Queuing Penalty (veh)		0		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	70	66	115	116
Average Queue (ft)	22	27	25	30
95th Queue (ft)	65	71	80	89
Link Distance (ft)	56	56	275	275
Upstream Blk Time (%)	3	4		
Queuing Penalty (veh)	12	15		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 200: 6th Side Street & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	T	TR	T	TR	LTR	LTR
Maximum Queue (ft)	64	84	9	11	148	197
Average Queue (ft)	4	5	0	0	61	76
95th Queue (ft)	27	37	6	8	120	153
Link Distance (ft)	298	298	356	356	419	408
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

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Intersection: 1000: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	T	TR	T	TR	LTR	LTR
Maximum Queue (ft)	11	6	4	3	25	68
Average Queue (ft)	0	0	0	0	3	34
95th Queue (ft)	8	7	4	3	16	57
Link Distance (ft)	757	757	866	866	442	411
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 1050: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	277	294	64	70
Average Queue (ft)	81	85	27	31
95th Queue (ft)	224	241	69	75
Link Distance (ft)	526	526	52	52
Upstream Blk Time (%)			6	7
Queuing Penalty (veh)			21	25
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1100: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	T	LTR	LTR
Maximum Queue (ft)	2	5	139	66
Average Queue (ft)	0	0	60	22
95th Queue (ft)	2	7	108	54
Link Distance (ft)	616	570	214	185
Upstream Blk Time (%)			0	
Queuing Penalty (veh)			0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1101: 5th Side Street & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	T	TR	T	TR	LTR	LTR
Maximum Queue (ft)	19	18	8	4	179	88
Average Queue (ft)	1	1	0	0	72	40
95th Queue (ft)	10	9	7	4	138	73
Link Distance (ft)	1260	1260	1212	1212	332	276
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 1200: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	28	16	114	78
Average Queue (ft)	1	1	42	40
95th Queue (ft)	17	11	84	66
Link Distance (ft)	1092	1092	211	276
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1400: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	T	LTR	LTR
Maximum Queue (ft)	6	6	94	63
Average Queue (ft)	0	0	42	20
95th Queue (ft)	4	5	72	54
Link Distance (ft)	1271	1219	305	274
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 3000: 6th/5th Street

Movement	EB	SB
Directions Served	T	LTR
Maximum Queue (ft)	9	216
Average Queue (ft)	0	94
95th Queue (ft)	9	177
Link Distance (ft)	1877	430
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 4000: 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	TR	T	TR
Maximum Queue (ft)	15	20	10	13
Average Queue (ft)	0	1	0	0
95th Queue (ft)	9	10	7	8
Link Distance (ft)	1235	1235	2071	2071
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 5000: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	173	174	100	130
Average Queue (ft)	42	37	39	54
95th Queue (ft)	228	217	105	97
Link Distance (ft)	619	619	310	280
Upstream Blk Time (%)	0	0		
Queuing Penalty (veh)	0	0		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 6000: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	T	TR	T	TR	LTR	LTR
Maximum Queue (ft)	13	4	52	65	156	147
Average Queue (ft)	0	0	6	8	67	65
95th Queue (ft)	8	5	47	54	139	159
Link Distance (ft)	578	578	513	513	355	360
Upstream Blk Time (%)						0
Queuing Penalty (veh)						0
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 7000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	T	LTR	LTR
Maximum Queue (ft)	17	8	17	83	76
Average Queue (ft)	1	0	1	41	23
95th Queue (ft)	10	6	9	68	56
Link Distance (ft)	627	627	533	260	304
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 8000: 6th/5th Street

Movement	EB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	2	66	71
Average Queue (ft)	0	29	27
95th Queue (ft)	2	54	54
Link Distance (ft)	1287	319	333
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 9000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	TR	LTR	LTR
Maximum Queue (ft)	17	18	2	117	84
Average Queue (ft)	1	1	0	53	43
95th Queue (ft)	10	11	2	95	71
Link Distance (ft)	560	560	526	204	310
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Network Summary

Network wide Queuing Penalty: 4525

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	4:15	4:15	4:15	4:15	4:15	4:15	4:15
End Time	5:30	5:30	5:30	5:30	5:30	5:30	5:30
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	28557	28710	28946	28410	28827	28601	28801
Vehs Exited	28122	28240	28383	27912	28298	28255	28379
Starting Vehs	1478	1461	1378	1377	1521	1570	1448
Ending Vehs	1913	1931	1941	1875	2050	1916	1870
Denied Entry After	234	256	143	436	259	369	306
Travel Distance (mi)	30128	30026	30230	29711	30265	30123	30124
Travel Time (hr)	1844.3	1911.3	1880.7	1912.7	1955.7	1987.2	1864.1
Total Delay (hr)	903.3	971.1	935.6	985.0	1007.6	1045.4	924.6
Total Stops	42897	44177	45114	42402	45164	45083	42370
Fuel Used (gal)	1153.7	1166.7	1165.5	1159.7	1182.6	1186.5	1158.0

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	4:15	4:15	4:15	4:15
End Time	5:30	5:30	5:30	5:30
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	28731	28569	28729	28691
Vehs Exited	28252	28257	28259	28232
Starting Vehs	1512	1498	1513	1466
Ending Vehs	1991	1810	1983	1912
Denied Entry After	295	326	350	296
Travel Distance (mi)	29941	29895	29964	30041
Travel Time (hr)	1924.8	1926.0	1954.5	1916.1
Total Delay (hr)	988.4	992.7	1020.4	977.4
Total Stops	43873	44466	43618	43916
Fuel Used (gal)	1168.4	1172.2	1174.6	1168.8

Interval #0 Information Seeding

Start Time	4:15
End Time	4:30
Total Time (min)	15

No data recorded this interval.

Interval #1 Information Recording

Start Time	4:30
End Time	5:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	28557	28710	28946	28410	28827	28601	28801
Vehs Exited	28122	28240	28383	27912	28298	28255	28379
Starting Vehs	1478	1461	1378	1377	1521	1570	1448
Ending Vehs	1913	1931	1941	1875	2050	1916	1870
Denied Entry After	234	256	143	436	259	369	306
Travel Distance (mi)	30128	30026	30230	29711	30265	30123	30124
Travel Time (hr)	1844.3	1911.3	1880.7	1912.7	1955.7	1987.2	1864.1
Total Delay (hr)	903.3	971.1	935.6	985.0	1007.6	1045.4	924.6
Total Stops	42897	44177	45114	42402	45164	45083	42370
Fuel Used (gal)	1153.7	1166.7	1165.5	1159.7	1182.6	1186.5	1158.0

Interval #1 Information Recording

Start Time	4:30
End Time	5:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	28731	28569	28729	28691
Vehs Exited	28252	28257	28259	28232
Starting Vehs	1512	1498	1513	1466
Ending Vehs	1991	1810	1983	1912
Denied Entry After	295	326	350	296
Travel Distance (mi)	29941	29895	29964	30041
Travel Time (hr)	1924.8	1926.0	1954.5	1916.1
Total Delay (hr)	988.4	992.7	1020.4	977.4
Total Stops	43873	44466	43618	43916
Fuel Used (gal)	1168.4	1172.2	1174.6	1168.8

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	24	234	27

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	285

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	2	8	1

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	11

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Entry After	0	0	0	0	0	0	0

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

7: 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

10: 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

40: Country Club Rd Performance by movement

Movement	EBR	NBT	SBT	All
Denied Entry After	0	0	0	0

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWT	SWR	All
Denied Entry After	0	0	0	0	0

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

200: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0

1050: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

1100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Entry After	0	0	0	0	0	0

1101: 5th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Entry After	0	0	0	0	0	0

1400: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Entry After	0	0	0	0	0	0

3000: 6th/5th Street Performance by movement

Movement	EBT	WBT	SBL	SBR	All
Denied Entry After	0	0	0	0	0

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	All
Denied Entry After	0	0	0	0	0

5000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0

6000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Entry After	0	0	0	0	0	0

7000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBL	All
Denied Entry After	0	0	0	0	0	0	0

8000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Entry After	0	0	0	0	0

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Entry After	0	0	0	0	0	0

Total Network Performance

Denied Entry After						296
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Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	121.5	187.4	0.6	11
	1000	6.7	26.7	0.2	22
	2	6.1	27.0	0.2	23
Wilson HAWK	20	1.6	4.2	0.0	16
6th Side Street	200	1.6	9.0	0.1	26
Tucson Blvd	3	19.3	28.6	0.1	10
	3000	7.3	51.0	0.4	26
6th/5th Street	72	2.1	7.1	0.0	15
Country Club Rd	4	31.0	46.8	0.1	9
	9922	2.9	14.9	0.1	23
	4000	4.4	33.6	0.2	26
Dodge Blvd	5	12.0	57.4	0.4	25
	5000	6.7	21.5	0.1	22
Alvernon Wy	6	59.5	73.5	0.1	6
	6000	4.6	19.8	0.1	23
	70	4.0	17.1	0.1	22
	7	0.6	2.7	0.0	26
	7000	1.8	17.4	0.1	27
Columbus Blvd	8	14.9	27.9	0.1	15
	8000	3.9	33.9	0.3	27
Swan Rd	9	41.8	70.6	0.3	13
	9000	5.2	20.6	0.1	22
	1050	6.6	19.9	0.1	20
	10	1.1	3.2	0.0	21
	1100	1.6	16.9	0.1	27
Rosemont Blvd	11	27.3	40.9	0.1	11
5th Side Street	1101	5.0	33.7	0.3	27
Craycroft Rd	12	36.2	64.5	0.2	14
	1200	5.2	32.4	0.2	25
	13	2.9	28.9	0.2	27
Chantilly Dr HAWK	130	1.7	4.2	0.0	16
Sahuara Ave	14	1.1	8.3	0.1	26
	1400	1.7	31.8	0.3	28
Wilmot Rd	15	43.1	70.0	0.3	13
Total		493.0	1153.4	5.6	18

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	29.0	128.4	0.4	12
	1400	0.8	25.5	0.3	35
Sahuara Ave	14	1.7	32.0	0.3	28
	130	2.9	10.5	0.1	21
	13	0.6	2.6	0.0	26
	1200	1.3	27.3	0.2	29
Craycroft Rd	12	23.9	49.7	0.2	16
5th Side Street	1101	3.8	33.3	0.2	27
Rosemont Blvd	11	20.1	48.3	0.3	19
	1100	2.9	16.8	0.1	26
	10	3.9	19.3	0.1	24
Arcadia Ave HAWK	1050	2.2	4.8	0.0	14
	9000	1.0	13.9	0.1	28
Swan Rd	9	49.5	63.4	0.1	7
	8000	4.7	35.2	0.3	26
Columbus Blvd	8	17.3	55.1	0.3	22
	7000	3.2	16.5	0.1	25
	7	2.0	17.6	0.1	27
Irving Ave HAWK	70	1.4	4.0	0.0	17
	6000	1.7	14.4	0.1	27
Alvernon Wy	6	61.3	75.3	0.1	6
	5000	3.7	18.7	0.1	24
Dodge Blvd	5	7.2	21.7	0.1	21
	4000	5.0	50.6	0.4	29
	9922	2.1	31.6	0.2	28
Country Club Rd	4	33.8	44.7	0.1	8
6th/5th Street	72	6.5	22.7	0.1	18
	3000	0.4	9.9	0.0	11
Tucson Blvd	3	11.4	54.8	0.4	24
6th Side Street	200	1.7	11.3	0.1	25
Wilson HAWK	20	4.2	12.2	0.1	19
	2	1.0	3.0	0.0	23
	1000	1.5	22.5	0.2	28
Campbell Ave	1	42.7	60.8	0.2	10
Total		356.4	1058.0	5.5	19

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	425	1350	1350	200	355	381	382	390	724	690	564	370
Average Queue (ft)	353	811	834	179	173	227	246	348	523	497	371	151
95th Queue (ft)	543	1435	1439	255	292	337	353	463	819	778	549	391
Link Distance (ft)		2986	2986			757	757		2020	2020	2020	
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	250			50	180			290				150
Storage Blk Time (%)	14	54	68	34	13	19		53	18		47	0
Queuing Penalty (veh)	72	151	195	169	45	42		266	47		82	1

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	350	2214	2212	2210
Average Queue (ft)	251	1977	1963	1932
95th Queue (ft)	479	2616	2620	2631
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)		60	58	58
Queuing Penalty (veh)		0	0	0
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	0	74		
Queuing Penalty (veh)	0	132		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	257	278	71	56	98	90
Average Queue (ft)	75	78	33	6	37	38
95th Queue (ft)	206	224	75	31	77	75
Link Distance (ft)	866	866	55	55	433	438
Upstream Blk Time (%)			8	0		
Queuing Penalty (veh)			37	1		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	257	362	365	114	233	253	399	706	400	1176
Average Queue (ft)	108	207	224	37	79	97	114	400	228	677
95th Queue (ft)	207	359	375	86	178	195	306	676	460	1357
Link Distance (ft)		356	356		1877	1877		1935		2179
Upstream Blk Time (%)	0	1	1							2
Queuing Penalty (veh)	0	5	8							0
Storage Bay Dist (ft)	200			100			300		300	
Storage Blk Time (%)	3	9		2	7			25	10	41
Queuing Penalty (veh)	15	14		8	4			10	65	31

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	NB	NB	NB	NB	SB
Directions Served	L	T	T	L	T	T	R	L	T	T	R	L
Maximum Queue (ft)	252	338	347	190	349	353	260	172	219	223	170	174
Average Queue (ft)	89	186	194	143	191	191	78	103	182	188	86	164
95th Queue (ft)	191	297	306	219	309	310	254	189	203	207	202	211
Link Distance (ft)		488	488		423	423			172	172		
Upstream Blk Time (%)		0	0		0	0		1	42	41	1	
Queuing Penalty (veh)		0	0		0	0		0	276	269	0	
Storage Bay Dist (ft)	180			90			160	100			70	100
Storage Blk Time (%)	1	13		46	29	14	4	10	57	56	3	49
Queuing Penalty (veh)	3	25		139	54	37	11	52	88	74	16	227

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	TR
Maximum Queue (ft)	825	819
Average Queue (ft)	525	514
95th Queue (ft)	931	903
Link Distance (ft)	2000	2000
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)	51	
Queuing Penalty (veh)	106	

Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	WB	NB	NB
Directions Served	T	TR	L	T	T	L	R
Maximum Queue (ft)	249	263	246	302	295	178	168
Average Queue (ft)	99	110	122	110	120	90	79
95th Queue (ft)	202	220	211	245	260	153	137
Link Distance (ft)	2071	2071		619	619	2122	
Upstream Blk Time (%)							
Queuing Penalty (veh)							
Storage Bay Dist (ft)			300				220
Storage Blk Time (%)			0	0		0	0
Queuing Penalty (veh)			0	0		0	0

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	TR	L	T	TR
Maximum Queue (ft)	398	559	571	310	542	548	250	978	967	250	666	662
Average Queue (ft)	311	382	372	109	328	345	184	621	620	164	446	445
95th Queue (ft)	466	617	587	276	536	541	314	1039	1024	296	701	689
Link Distance (ft)		581	581		578	578		2106	2106		1977	1977
Upstream Blk Time (%)		9	2		1	2						
Queuing Penalty (veh)		51	11		7	9						
Storage Bay Dist (ft)	300			300			150			150		
Storage Blk Time (%)	34	12		0	18		17	50		5	43	
Queuing Penalty (veh)	125	29		0	18		107	72		30	55	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	62	40	137	139	54	39
Average Queue (ft)	8	3	20	18	23	13
95th Queue (ft)	37	22	82	79	48	36
Link Distance (ft)	56	56	627	627	304	320
Upstream Blk Time (%)	0	0				
Queuing Penalty (veh)	2	1				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	169	228	243	150	244	243	99	273	90	299
Average Queue (ft)	70	124	139	43	120	140	43	150	35	156
95th Queue (ft)	136	200	212	98	212	227	84	239	72	249
Link Distance (ft)		533	533		1685	1685		2072		1936
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			75			250		250	
Storage Blk Time (%)	18	20		4	18			1		1
Queuing Penalty (veh)	71	17		14	10			0		0

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	290	411	440	281	377	418	304	588	607	450	348	451
Average Queue (ft)	131	218	238	149	246	267	165	392	388	126	191	292
95th Queue (ft)	250	369	393	262	353	377	353	549	551	362	344	440
Link Distance (ft)		1226	1226		560	560		1988	1988			2060
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300			500			180			350	230	
Storage Blk Time (%)	0	3					0	37	12		15	15
Queuing Penalty (veh)	2	5					3	42	24		84	25

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	450	179
Average Queue (ft)	286	38
95th Queue (ft)	432	114
Link Distance (ft)	2060	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	4	
Queuing Penalty (veh)	4	

Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	72	55	164	171	109	79
Average Queue (ft)	28	7	56	37	41	34
95th Queue (ft)	71	35	129	119	79	66
Link Distance (ft)	52	52	616	616	261	294
Upstream Blk Time (%)	3	0				
Queuing Penalty (veh)	15	2				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	174	481	492	188	313	324	250	487	236	406
Average Queue (ft)	121	291	311	44	143	157	135	248	59	210
95th Queue (ft)	209	458	474	115	273	294	256	420	152	340
Link Distance (ft)		570	570		1260	1260		1970		2041
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			100			150		150	
Storage Blk Time (%)	24	29		1	16		5	24	0	21
Queuing Penalty (veh)	118	44		4	7		20	36	2	12

Queuing and Blocking Report
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Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	269	351	391	207	186	216	330	797	780	230	300	593
Average Queue (ft)	136	201	241	102	99	118	176	480	477	138	94	366
95th Queue (ft)	239	327	371	180	163	182	377	784	772	298	264	559
Link Distance (ft)		1212	1212		1092	1092		1951	1951			2065
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300			500			180			180	150	
Storage Blk Time (%)	0	2					1	48	47	0	0	44
Queuing Penalty (veh)	1	3					8	62	60	0	0	26

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	575	300
Average Queue (ft)	366	133
95th Queue (ft)	556	343
Link Distance (ft)	2065	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	43	
Queuing Penalty (veh)	49	

Intersection: 13: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	124	150	64	36	92	60
Average Queue (ft)	17	23	13	2	40	24
95th Queue (ft)	72	89	46	14	72	53
Link Distance (ft)	1086	1086	56	56	422	473
Upstream Blk Time (%)			1	0		
Queuing Penalty (veh)			3	0		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	74	52	75	55	49	59
Average Queue (ft)	12	4	17	3	18	27
95th Queue (ft)	46	28	55	22	45	52
Link Distance (ft)	275	275	1271	1271	614	699
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	249	328	217	107	359	542	523	446	230	433	422	396
Average Queue (ft)	146	178	127	39	245	325	318	276	53	282	276	242
95th Queue (ft)	228	265	199	85	397	494	477	409	171	388	379	350
Link Distance (ft)		1219	1219	2255		1997	1997	1997		2032	2032	2032
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	150				240				130			
Storage Blk Time (%)	9	23			19	16				37		2
Queuing Penalty (veh)	43	52			126	44				18		4

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	190
Average Queue (ft)	56
95th Queue (ft)	132
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	77	84	213	216
Average Queue (ft)	27	30	57	57
95th Queue (ft)	72	79	156	160
Link Distance (ft)	55	55	298	298
Upstream Blk Time (%)	5	7		0
Queuing Penalty (veh)	34	43		0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 40: Country Club Rd

Movement	EB	NB	NB
Directions Served	R	T	T
Maximum Queue (ft)	112	579	580
Average Queue (ft)	56	302	302
95th Queue (ft)	95	600	593
Link Distance (ft)	451	2094	2094
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	207	216	73	81
Average Queue (ft)	44	51	23	23
95th Queue (ft)	150	164	66	70
Link Distance (ft)	513	513	56	56
Upstream Blk Time (%)			3	3
Queuing Penalty (veh)			12	13
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 72: 6th/5th Street

Movement	EB	EB	SW	SW
Directions Served	L	L	R	R
Maximum Queue (ft)	10	22	43	25
Average Queue (ft)	1	1	2	1
95th Queue (ft)	9	20	20	12
Link Distance (ft)	100	100	488	488
Upstream Blk Time (%)		0		
Queuing Penalty (veh)		0		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	70	66	115	116
Average Queue (ft)	22	27	25	30
95th Queue (ft)	65	71	80	89
Link Distance (ft)	56	56	275	275
Upstream Blk Time (%)	3	4		
Queuing Penalty (veh)	12	15		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 200: 6th Side Street & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	T	TR	T	TR	LTR	LTR
Maximum Queue (ft)	64	84	9	11	148	197
Average Queue (ft)	4	5	0	0	61	76
95th Queue (ft)	27	37	6	8	120	153
Link Distance (ft)	298	298	356	356	419	408
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Queuing and Blocking Report
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Intersection: 1000: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	T	TR	T	TR	LTR	LTR
Maximum Queue (ft)	11	6	4	3	25	68
Average Queue (ft)	0	0	0	0	3	34
95th Queue (ft)	8	7	4	3	16	57
Link Distance (ft)	757	757	866	866	442	411
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 1050: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	277	294	64	70
Average Queue (ft)	81	85	27	31
95th Queue (ft)	224	241	69	75
Link Distance (ft)	526	526	52	52
Upstream Blk Time (%)			6	7
Queuing Penalty (veh)			21	25
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1100: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	T	LTR	LTR
Maximum Queue (ft)	2	5	139	66
Average Queue (ft)	0	0	60	22
95th Queue (ft)	2	7	108	54
Link Distance (ft)	616	570	214	185
Upstream Blk Time (%)			0	
Queuing Penalty (veh)			0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1101: 5th Side Street & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	T	TR	T	TR	LTR	LTR
Maximum Queue (ft)	19	18	8	4	179	88
Average Queue (ft)	1	1	0	0	72	40
95th Queue (ft)	10	9	7	4	138	73
Link Distance (ft)	1260	1260	1212	1212	332	276
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 1200: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	28	16	114	78
Average Queue (ft)	1	1	42	40
95th Queue (ft)	17	11	84	66
Link Distance (ft)	1092	1092	211	276
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1400: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	T	LTR	LTR
Maximum Queue (ft)	6	6	94	63
Average Queue (ft)	0	0	42	20
95th Queue (ft)	4	5	72	54
Link Distance (ft)	1271	1219	305	274
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 3000: 6th/5th Street

Movement	EB	SB
Directions Served	T	LTR
Maximum Queue (ft)	9	216
Average Queue (ft)	0	94
95th Queue (ft)	9	177
Link Distance (ft)	1877	430
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 4000: 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	TR	T	TR
Maximum Queue (ft)	15	20	10	13
Average Queue (ft)	0	1	0	0
95th Queue (ft)	9	10	7	8
Link Distance (ft)	1235	1235	2071	2071
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 5000: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	173	174	100	130
Average Queue (ft)	42	37	39	54
95th Queue (ft)	228	217	105	97
Link Distance (ft)	619	619	310	280
Upstream Blk Time (%)	0	0		
Queuing Penalty (veh)	0	0		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 6000: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	T	TR	T	TR	LTR	LTR
Maximum Queue (ft)	13	4	52	65	156	147
Average Queue (ft)	0	0	6	8	67	65
95th Queue (ft)	8	5	47	54	139	159
Link Distance (ft)	578	578	513	513	355	360
Upstream Blk Time (%)						0
Queuing Penalty (veh)						0
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 7000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	T	LTR	LTR
Maximum Queue (ft)	17	8	17	83	76
Average Queue (ft)	1	0	1	41	23
95th Queue (ft)	10	6	9	68	56
Link Distance (ft)	627	627	533	260	304
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 8000: 6th/5th Street

Movement	EB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	2	66	71
Average Queue (ft)	0	29	27
95th Queue (ft)	2	54	54
Link Distance (ft)	1287	319	333
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 9000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	TR	LTR	LTR
Maximum Queue (ft)	17	18	2	117	84
Average Queue (ft)	1	1	0	53	43
95th Queue (ft)	10	11	2	95	71
Link Distance (ft)	560	560	526	204	310
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Network Summary

Network wide Queuing Penalty: 4525

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	6:45	6:45	6:45	6:45	6:45	6:45	6:45
End Time	8:00	8:00	8:00	8:00	8:00	8:00	8:00
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	26591	27187	27277	27394	27309	27085	27166
Vehs Exited	25007	26015	25638	26206	25385	25654	25848
Starting Vehs	1229	1175	1201	1221	1221	1241	1195
Ending Vehs	2813	2347	2840	2409	3145	2672	2513
Travel Distance (mi)	25508	26486	26120	26621	25843	26182	26397
Travel Time (hr)	2295.9	2036.5	2309.6	2057.8	2213.8	2183.3	2053.8
Total Delay (hr)	1505.9	1215.9	1501.6	1232.9	1414.0	1372.8	1235.5
Total Stops	46504	48353	47909	46641	47429	50053	49517
Fuel Used (gal)	1136.7	1102.6	1153.8	1111.5	1124.7	1127.6	1104.4

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	6:45	6:45	6:45	6:45
End Time	8:00	8:00	8:00	8:00
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	26649	26799	27281	27071
Vehs Exited	25232	25378	25836	25623
Starting Vehs	1112	1187	1195	1186
Ending Vehs	2529	2608	2640	2642
Travel Distance (mi)	25767	26004	26340	26127
Travel Time (hr)	2131.7	2215.6	2179.3	2167.7
Total Delay (hr)	1333.9	1410.7	1362.5	1358.6
Total Stops	45538	47018	49809	47878
Fuel Used (gal)	1105.6	1134.9	1130.6	1123.2

Interval #0 Information Seeding

Start Time	6:45
End Time	7:00
Total Time (min)	15

No data recorded this interval.

Interval #1 Information Recording

Start Time	7:00
End Time	8:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	26591	27187	27277	27394	27309	27085	27166
Vehs Exited	25007	26015	25638	26206	25385	25654	25848
Starting Vehs	1229	1175	1201	1221	1221	1241	1195
Ending Vehs	2813	2347	2840	2409	3145	2672	2513
Travel Distance (mi)	25508	26486	26120	26621	25843	26182	26397
Travel Time (hr)	2295.9	2036.5	2309.6	2057.8	2213.8	2183.3	2053.8
Total Delay (hr)	1505.9	1215.9	1501.6	1232.9	1414.0	1372.8	1235.5
Total Stops	46504	48353	47909	46641	47429	50053	49517
Fuel Used (gal)	1136.7	1102.6	1153.8	1111.5	1124.7	1127.6	1104.4

Interval #1 Information Recording

Start Time	7:00
End Time	8:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	26649	26799	27281	27071
Vehs Exited	25232	25378	25836	25623
Starting Vehs	1112	1187	1195	1186
Ending Vehs	2529	2608	2640	2642
Travel Distance (mi)	25767	26004	26340	26127
Travel Time (hr)	2131.7	2215.6	2179.3	2167.7
Total Delay (hr)	1333.9	1410.7	1362.5	1358.6
Total Stops	45538	47018	49809	47878
Fuel Used (gal)	1105.6	1134.9	1130.6	1123.2

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.5	0.2	0.4	0.4	0.2	0.4	1.2	0.2	1.0	1.1	0.1	0.2
Total Del/Veh (s)	82.8	36.8	11.5	52.7	40.1	38.8	294.8	129.7	60.3	112.7	156.3	160.1

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	114.2

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.4	0.1	0.1	0.0	0.0	0.0	0.1	0.1	0.2	0.2	0.8	0.1
Total Del/Veh (s)	39.1	7.2	5.7	7.4	1.1	0.2	106.4	68.0	281.6	236.6	198.4	13.4

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.6	0.6	95.2	114.1	114.7	1.7	0.4	0.3	36.2	38.1	31.4
Total Del/Veh (s)	65.2	26.9	24.0	156.0	158.7	153.8	271.7	66.0	60.6	541.8	250.4	244.2

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	52.9
Total Del/Veh (s)	143.7

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.2	0.1	2.5	1.2	1.4	0.0	0.0	0.0	1.4	0.2	0.2	0.5
Total Del/Veh (s)	109.7	38.4	66.2	76.7	15.2	57.0	33.2	6.7	70.6	53.2	49.7	50.7

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	2.6	0.4	0.1	0.5	0.2	1.3	1.2
Total Del/Veh (s)	50.6	30.6	14.3	10.5	85.9	97.5	31.4

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	19.2	16.1	19.5	3.2	2.7	2.7
Total Del/Veh (s)	348.5	78.2	54.4	56.7	69.5	44.8	388.2	153.3	152.8	265.0	146.0	140.3

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	6.4
Total Del/Veh (s)	142.9

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	9.8	2.7	0.8	12.4	12.1	9.8	81.6	44.8	77.0	44.2	59.6	9.9

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	4.2	4.1	0.8	0.1	0.1	0.3	5.8	5.8	7.9	2.2	0.4	0.4
Total Del/Veh (s)	73.8	45.0	33.5	21.9	11.5	6.3	163.7	111.1	123.5	226.9	44.5	40.3

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	2.3
Total Del/Veh (s)	53.1

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	37.3	43.2	30.4	8.0	11.1	13.6	1.6	0.3	1.6	15.1	12.7	12.1
Total Del/Veh (s)	385.2	278.3	242.6	68.4	64.9	47.9	56.7	49.4	87.3	489.5	78.1	44.1

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	12.3
Total Del/Veh (s)	115.6

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	212.1	0.1	0.2	0.2	28.6
Total Del/Veh (s)	7.8	7.7	2.6	19.7	6.8	5.9	128.6	88.6	67.3	41.2	23.0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	109.8	110.8	87.5	1.0	0.5	0.6	2.0	0.4	0.4	1.5	0.4	0.3
Total Del/Veh (s)	298.2	103.7	87.8	33.4	22.9	13.8	98.5	39.4	30.2	48.1	38.7	36.3

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	28.2
Total Del/Veh (s)	57.1

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.2	0.0	0.3	0.8	4.2	2.9	3.3	1.7	2.9	1.5	0.3	1.5
Total Del/Veh (s)	108.6	27.9	14.2	43.4	68.1	46.4	265.4	92.7	57.4	72.7	48.2	32.0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	1.6
Total Del/Veh (s)	73.3

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	4.5	2.7	2.9	0.5	15.8	14.8	9.2	11.6	2.9

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.2	0.2	0.2	0.1	0.1	0.1
Total Del/Veh (s)	4.7	1.6	0.9	6.7	2.8	2.3	16.4	18.5	8.3	16.6	14.6	7.4

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	3.7

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.1	0.1	0.2	0.2	0.2	1.5	0.3	0.2	1.2	0.2	1.1
Total Del/Veh (s)	45.9	44.5	20.2	39.9	41.7	24.9	260.9	37.1	22.3	43.4	32.2	17.2

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	50.6

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	1.0	0.6
Total Del/Veh (s)	1.7	6.7	4.8

40: Country Club Rd Performance by movement

Movement	EBR	NBT	SBT	All
Denied Del/Veh (s)	0.0	0.3	0.0	0.1
Total Del/Veh (s)	15.0	37.2	2.8	22.5

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	20.8	2.8	9.8

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWT	SWR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0
Total Del/Veh (s)	3.5	0.6	2.3	21.4	14.8

100: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	67.5	2.1	34.8

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.7	3.7	2.9

200: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	30.8	559.8	587.0	64.1
Total Del/Veh (s)	4.6	3.0	4.9	322.5	440.4	420.9	52.3

1000: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.1	0.0
Total Del/Veh (s)	8.2	6.0	5.0	16.0	85.0	7.5

1100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	All
Denied Del/Veh (s)	26.2	0.0	0.0	187.9	13.4
Total Del/Veh (s)	90.8	5.5	4.3	504.8	44.5

1101: 5th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.2	0.2	0.0
Total Del/Veh (s)	3.7	3.2	8.2	7.0	38.2	19.0	43.1	9.2

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	19.7	1.7
Total Del/Veh (s)	4.7	3.6	7.0	35.5	51.5	11.3

1400: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	1.9	1.9	3.2	2.7	4.1	5.9	2.9

3000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	11.7	21.1	56.4	76.6	3.5
Total Del/Veh (s)	9.0	7.1	5.1	0.1	280.1	231.6	212.3	293.1	26.1

4000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	5.4	5.2	0.6
Total Del/Veh (s)	5.6	17.6	62.0	60.6	103.9	90.9	21.8

5000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	20.7	19.4	8.2	24.7	14.0

6000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.8	0.0	37.6	53.8	11.9	2.5
Total Del/Veh (s)	13.7	24.0	405.3	265.7	381.0	41.3

7000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	1.5	0.1	0.1	0.0
Total Del/Veh (s)	31.9	3.0	97.9	123.3	67.4	17.3

8000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	70.5	0.2	4.6
Total Del/Veh (s)	22.7	20.2	8.4	7.7	128.5	14.9	20.6

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1	71.1	6.3
Total Del/Veh (s)	29.6	28.5	14.2	95.3	151.3	34.4

Total Network Performance

Denied Del/Veh (s)	20.6
Total Del/Veh (s)	153.0

Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	36.8	102.9	0.6	20
	1000	10.1	22.7	0.1	16
	2	7.4	34.9	0.2	25
Wilson HAWK	20	1.7	4.2	0.0	16
6th Side Street	200	4.6	11.8	0.1	20
Tucson Blvd	3	27.1	36.6	0.1	8
	3000	9.6	51.7	0.4	26
6th/5th Street	72	3.4	7.0	0.0	16
Country Club Rd	4	38.4	51.2	0.1	8
	9922	7.9	19.9	0.1	17
	4000	5.6	34.5	0.2	26
Dodge Blvd	5	51.2	99.5	0.4	15
	5000	21.5	31.3	0.1	10
Alvernon Wy	6	78.2	97.5	0.2	6
	6000	15.6	31.5	0.1	15
	70	21.0	33.9	0.1	11
	7	2.7	4.7	0.0	14
Columbus Blvd	7000	31.5	46.9	0.1	10
	8	45.0	62.5	0.1	7
Swan Rd	8000	23.3	31.7	0.1	8
	9	271.7	366.9	0.4	5
	9000	24.9	37.4	0.1	10
	100	67.5	82.4	0.1	5
	10	7.7	9.7	0.0	7
	1100	89.5	141.8	0.1	4
Rosemont Blvd	11	102.5	240.7	0.1	4
5th Side Street	1101	4.4	23.7	0.2	25
Craycroft Rd	12	26.3	63.7	0.3	19
	1200	6.2	24.3	0.2	23
	13	4.5	39.0	0.3	27
Chantilly Dr HAWK	130	1.7	4.2	0.0	16
Sahuara Ave	14	1.6	8.7	0.1	25
	1400	2.0	28.9	0.3	31
Wilmot Rd	15	43.1	68.2	0.3	13
Total		1096.1	1956.4	5.6	12

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	41.7	142.3	0.4	11
	1400	2.8	30.9	0.3	29
Sahuara Ave	14	2.9	33.1	0.3	27
	130	3.7	11.2	0.1	19
	13	0.5	2.5	0.0	27
	1200	7.0	41.6	0.3	25
Craycroft Rd	12	67.1	89.1	0.2	7
5th Side Street	1101	10.1	48.9	0.3	24
Rosemont Blvd	11	22.0	41.4	0.2	15
	1100	6.5	21.3	0.1	21
	10	6.8	20.9	0.1	21
Arcadia Ave HAWK	100	2.1	4.6	0.0	15
	9000	14.2	28.8	0.1	16
Swan Rd	9	64.7	89.4	0.1	5
	8000	9.7	60.6	0.4	26
Columbus Blvd	8	9.9	21.2	0.1	16
	7000	3.3	16.9	0.1	24
	7	12.1	27.6	0.1	17
Irving Ave HAWK	70	2.8	5.4	0.0	13
	6000	24.0	36.5	0.1	11
Alvernon Wy	6	69.6	83.8	0.1	5
	5000	10.1	30.4	0.2	20
Dodge Blvd	5	11.0	19.2	0.1	13
	4000	17.6	64.5	0.4	23
	9922	49.6	78.4	0.2	11
Country Club Rd	4	76.7	88.4	0.1	4
6th/5th Street	72	25.7	39.3	0.1	10
	3000	5.1	9.3	0.0	12
Tucson Blvd	3	158.4	324.4	0.4	7
6th Side Street	200	5.4	14.8	0.1	19
Wilson HAWK	20	6.9	15.7	0.1	16
	2	1.1	3.1	0.0	22
	1000	6.1	33.9	0.2	25
Campbell Ave	1	40.1	51.0	0.1	7
Total		797.2	1630.7	5.5	13

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	305	310	290	200	315	385	390	390	1527	1479	1345	370
Average Queue (ft)	173	173	155	87	148	259	275	374	963	935	818	175
95th Queue (ft)	297	279	249	185	264	355	363	456	1650	1598	1412	457
Link Distance (ft)		2986	2986			418	418		2020	2020	2020	
Upstream Blk Time (%)						0	0		1	1	0	
Queuing Penalty (veh)						1	1		0	0	0	
Storage Bay Dist (ft)	250			50	180			290				150
Storage Blk Time (%)	7	1	43	12	8	23		62	44		64	0
Queuing Penalty (veh)	17	1	88	30	40	59		353	134		66	0

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	350	1301	1267	1224
Average Queue (ft)	181	816	804	789
95th Queue (ft)	439	1381	1348	1313
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)	200			
Storage Blk Time (%)		66		
Queuing Penalty (veh)		53		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	154	357	30	63	110	324
Average Queue (ft)	49	90	3	12	39	141
95th Queue (ft)	110	273	19	48	93	316
Link Distance (ft)		1205		55	442	447
Upstream Blk Time (%)				1		0
Queuing Penalty (veh)				12		0
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	2	5		1		
Queuing Penalty (veh)	15	3		0		

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB	SB	SB
Directions Served	L	TR	L	TR	L	TR	L	TR
Maximum Queue (ft)	300	371	300	1886	212	623	400	2136
Average Queue (ft)	68	268	94	1598	32	371	277	1293
95th Queue (ft)	196	407	291	2286	123	600	512	2356
Link Distance (ft)		356		1877		1948		2192
Upstream Blk Time (%)		4		4				15
Queuing Penalty (veh)		22		51				0
Storage Bay Dist (ft)	200		100		300		300	
Storage Blk Time (%)		19	0	44		23	19	59
Queuing Penalty (veh)		11	3	32		3	111	42

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	B9922	NB	NB	NB	NB
Directions Served	L	T	T	L	T	T	R	T	L	T	T	R
Maximum Queue (ft)	233	296	261	190	518	495	260	1137	177	227	224	170
Average Queue (ft)	105	163	132	150	452	389	140	525	114	188	191	55
95th Queue (ft)	204	269	227	263	578	553	349	1331	196	216	212	165
Link Distance (ft)		486	486		423	423		1235		177	177	
Upstream Blk Time (%)		1			30	9		3	3	35	35	0
Queuing Penalty (veh)		3			175	55		40	0	219	221	0
Storage Bay Dist (ft)	180			90			160		100			70
Storage Blk Time (%)	5	7		3	76	36	0		17	48	52	0
Queuing Penalty (veh)	11	7		13	106	73	2		84	71	53	1

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB	SB
Directions Served	L	T	TR
Maximum Queue (ft)	174	697	701
Average Queue (ft)	129	354	362
95th Queue (ft)	216	616	617
Link Distance (ft)		2000	2000
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)	100		
Storage Blk Time (%)	15	47	
Queuing Penalty (veh)	63	61	

Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB
Directions Served	T	R	L	T	L	R
Maximum Queue (ft)	985	285	268	299	409	241
Average Queue (ft)	384	61	63	241	97	87
95th Queue (ft)	1030	232	166	383	409	237
Link Distance (ft)	2072			288	2124	
Upstream Blk Time (%)	1		0	3		
Queuing Penalty (veh)	8		0	34		
Storage Bay Dist (ft)		110	300			220
Storage Blk Time (%)	29		0	3	2	11
Queuing Penalty (veh)	17		0	4	2	4

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	TR	L	T	TR
Maximum Queue (ft)	396	737	261	400	597	285	250	1683	1677	250	1843	1822
Average Queue (ft)	286	465	50	173	545	130	228	1008	1000	205	1000	982
95th Queue (ft)	484	930	183	446	704	332	302	2060	2041	314	1934	1899
Link Distance (ft)		825			580			2113	2113		1973	1973
Upstream Blk Time (%)		17			22			13	12		5	4
Queuing Penalty (veh)		99			210			0	0		0	0
Storage Bay Dist (ft)	300		110	300		110	150			150		
Storage Blk Time (%)	47	31	0		59	0	69	47		39	53	
Queuing Penalty (veh)	206	61	0		126	3	420	72		251	73	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	16	58	199	487	81	69
Average Queue (ft)	1	13	40	166	21	21
95th Queue (ft)	8	51	133	455	61	58
Link Distance (ft)		56		628	311	327
Upstream Blk Time (%)	0	10		0		
Queuing Penalty (veh)	0	62		2		
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0	10		13		
Queuing Penalty (veh)	0	0		10		

Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	174	518	184	155	395	150	350	1375	354	550
Average Queue (ft)	57	256	30	32	171	16	106	475	105	268
95th Queue (ft)	151	543	130	97	324	80	271	1304	281	445
Link Distance (ft)		533			436			2075		1939
Upstream Blk Time (%)		14			0			3		
Queuing Penalty (veh)		94			3			0		
Storage Bay Dist (ft)	75		100	75		75	250		350	
Storage Blk Time (%)	7	37		0	22	0	0	27	5	4
Queuing Penalty (veh)	42	37		3	18	0	1	21	23	2

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	360	2225	254	466	483	200	305	775	804	450	380	1752
Average Queue (ft)	212	1248	65	139	455	89	135	440	446	247	349	919
95th Queue (ft)	436	2650	230	375	547	233	326	741	780	500	459	1822
Link Distance (ft)		2213			466			1988	1988			2061
Upstream Blk Time (%)		19		0	22							6
Queuing Penalty (veh)		138		0	180							0
Storage Bay Dist (ft)	300		110	500		100	180			350	230	
Storage Blk Time (%)	15	64	0	0	60	0		38	12	16	73	30
Queuing Penalty (veh)	103	117	0	1	132	4		35	34	93	452	66

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	1726	400
Average Queue (ft)	896	137
95th Queue (ft)	1783	405
Link Distance (ft)	2061	
Upstream Blk Time (%)	5	
Queuing Penalty (veh)	0	
Storage Bay Dist (ft)		300
Storage Blk Time (%)	26	
Queuing Penalty (veh)	28	

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Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	52	79	197	351	318	141
Average Queue (ft)	17	45	71	103	204	50
95th Queue (ft)	47	84	163	277	372	127
Link Distance (ft)		54		579	279	312
Upstream Blk Time (%)	1	30			48	0
Queuing Penalty (veh)	0	256			0	0
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	1	30	6	6		
Queuing Penalty (veh)	5	13	39	12		

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	134	621	180	232	586	193	246	450	248	453
Average Queue (ft)	124	561	105	54	314	35	116	188	44	248
95th Queue (ft)	158	778	235	170	585	145	220	412	147	390
Link Distance (ft)		607			810			1974		2044
Upstream Blk Time (%)		43			0					
Queuing Penalty (veh)		346			0					
Storage Bay Dist (ft)	75		110	100		110	150		150	
Storage Blk Time (%)	85	32	0	1	28		11	12		28
Queuing Penalty (veh)	633	87	3	5	31		32	14		11

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Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	316	391	210	599	705	210	330	1244	1209	230	300	572
Average Queue (ft)	153	147	47	220	513	54	292	707	693	126	172	361
95th Queue (ft)	299	308	149	599	777	189	409	1437	1410	293	342	528
Link Distance (ft)	1664			711			1958		1958		2072	
Upstream Blk Time (%)					4		3		3			
Queuing Penalty (veh)					37		0		0			
Storage Bay Dist (ft)	300		110		500		110		180		150	
Storage Blk Time (%)	5	18	0	56		63	43	46	0	4	41	
Queuing Penalty (veh)	25	54	0	114		370	89	53	0	24	51	

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	565	300
Average Queue (ft)	362	183
95th Queue (ft)	531	371
Link Distance (ft)	2072	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)	150	
Storage Blk Time (%)	41	1
Queuing Penalty (veh)	77	8

Intersection: 13: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	TR	L	TR	LTR	LTR
Maximum Queue (ft)	192	33	34	76	63
Average Queue (ft)	32	3	3	33	20
95th Queue (ft)	119	19	20	62	51
Link Distance (ft)	1468		56	429	480
Upstream Blk Time (%)			0		
Queuing Penalty (veh)			0		
Storage Bay Dist (ft)	100				
Storage Blk Time (%)	2		0		
Queuing Penalty (veh)	0		0		

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	28	47	58	56	117	55
Average Queue (ft)	2	3	17	4	46	26
95th Queue (ft)	13	24	47	28	83	52
Link Distance (ft)		275		1271	622	706
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)		0	0	0		
Queuing Penalty (veh)		0	0	0		

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	148	217	184	189	360	1419	1391	1158	230	524	511	463
Average Queue (ft)	61	117	73	80	344	858	810	310	123	339	330	300
95th Queue (ft)	124	189	144	152	420	1567	1521	759	256	473	464	429
Link Distance (ft)		1220		2255		1994	1994	1994		2040	2040	2040
Upstream Blk Time (%)						1	0	0				
Queuing Penalty (veh)						0	0	0				
Storage Bay Dist (ft)	145		145		240				130			
Storage Blk Time (%)	0	4	0		73	3			2	36		6
Queuing Penalty (veh)	1	9	1		360	11			15	45		23

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	400
Average Queue (ft)	139
95th Queue (ft)	294
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	0
Queuing Penalty (veh)	1

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	81	312
Average Queue (ft)	28	144
95th Queue (ft)	77	353
Link Distance (ft)	55	298
Upstream Blk Time (%)	6	2
Queuing Penalty (veh)	48	22
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 40: Country Club Rd

Movement	EB	NB	NB	SB
Directions Served	R	T	T	T
Maximum Queue (ft)	116	674	666	90
Average Queue (ft)	41	300	309	7
95th Queue (ft)	83	781	777	81
Link Distance (ft)	450	2095	2095	177
Upstream Blk Time (%)				1
Queuing Penalty (veh)				3
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	436	90
Average Queue (ft)	128	43
95th Queue (ft)	408	94
Link Distance (ft)	514	56
Upstream Blk Time (%)	7	13
Queuing Penalty (veh)	43	112
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

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Intersection: 72: 6th/5th Street

Movement	EB	SW	SW
Directions Served	L	R	R
Maximum Queue (ft)	11	543	461
Average Queue (ft)	1	275	89
95th Queue (ft)	18	645	397
Link Distance (ft)	100	486	486
Upstream Blk Time (%)	1	5	2
Queuing Penalty (veh)	3	32	15
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 100: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	637	77
Average Queue (ft)	395	33
95th Queue (ft)	801	80
Link Distance (ft)	620	54
Upstream Blk Time (%)	23	10
Queuing Penalty (veh)	199	71
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	73	251
Average Queue (ft)	25	65
95th Queue (ft)	71	189
Link Distance (ft)	56	275
Upstream Blk Time (%)	5	0
Queuing Penalty (veh)	22	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 200: 6th Side Street & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	236	233	403	459
Average Queue (ft)	32	31	252	416
95th Queue (ft)	146	140	469	526
Link Distance (ft)	298	356	433	422
Upstream Blk Time (%)	0	0	15	84
Queuing Penalty (veh)	1	0	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	R	LTR	LTR	LTR
Maximum Queue (ft)	338	259	31	108	50
Average Queue (ft)	37	12	1	43	10
95th Queue (ft)	198	108	18	83	37
Link Distance (ft)	418	418	1205	446	415
Upstream Blk Time (%)	0				
Queuing Penalty (veh)	0				
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 1100: 6th/5th Street

Movement	EB	NB
Directions Served	TR	LTR
Maximum Queue (ft)	597	215
Average Queue (ft)	434	123
95th Queue (ft)	822	265
Link Distance (ft)	579	226
Upstream Blk Time (%)	30	31
Queuing Penalty (veh)	218	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1101: 5th Side Street & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	11	26	127	114
Average Queue (ft)	0	1	57	47
95th Queue (ft)	8	23	105	93
Link Distance (ft)	810	1664	341	283
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1200: 6th/5th Street

Movement	WB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	230	126	162
Average Queue (ft)	48	53	75
95th Queue (ft)	222	106	200
Link Distance (ft)	1468	214	281
Upstream Blk Time (%)		0	5
Queuing Penalty (veh)		0	0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1400: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	54	58
Average Queue (ft)	27	27
95th Queue (ft)	50	48
Link Distance (ft)	307	277
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 3000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	T	LTR	LTR
Maximum Queue (ft)	190	117	338	342
Average Queue (ft)	11	61	136	145
95th Queue (ft)	217	146	346	375
Link Distance (ft)	1877	100	420	430
Upstream Blk Time (%)		6	7	11
Queuing Penalty (veh)		33	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 4000: 6th/5th Street

Movement	EB	B9922	B9922	WB	NB	SB
Directions Served	TR	T		TR	LTR	LTR
Maximum Queue (ft)	130	216	97	734	89	496
Average Queue (ft)	13	16	3	107	31	168
95th Queue (ft)	209	131	58	620	73	418
Link Distance (ft)	1235	423	423	2072	554	590
Upstream Blk Time (%)	0	0	0	0		4
Queuing Penalty (veh)	3	1	0	5		0
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 5000: 6th/5th Street

Movement	EB	WB	SB
Directions Served	TR	TR	LTR
Maximum Queue (ft)	270	333	109
Average Queue (ft)	105	109	48
95th Queue (ft)	378	283	91
Link Distance (ft)	375	825	286
Upstream Blk Time (%)	12		
Queuing Penalty (veh)	92		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

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Intersection: 6000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	267	510	304	278
Average Queue (ft)	47	285	144	112
95th Queue (ft)	296	625	353	290
Link Distance (ft)	580	514	362	366
Upstream Blk Time (%)	4	4	11	5
Queuing Penalty (veh)	21	34	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 7000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	434	51	154	152
Average Queue (ft)	117	3	42	44
95th Queue (ft)	506	46	131	128
Link Distance (ft)	628	533	267	311
Upstream Blk Time (%)	9		2	
Queuing Penalty (veh)	57		0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 8000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	292	61	273	111
Average Queue (ft)	91	2	105	46
95th Queue (ft)	315	38	287	85
Link Distance (ft)	302	2213	326	342
Upstream Blk Time (%)	15		15	
Queuing Penalty (veh)	106		0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 9000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	482	440	72	300
Average Queue (ft)	200	164	22	177
95th Queue (ft)	573	397	61	372
Link Distance (ft)	466	620	210	316
Upstream Blk Time (%)	17	0		24
Queuing Penalty (veh)	188	1		0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 9879

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	4:15	4:15	4:15	4:15	4:15	4:15	4:15
End Time	5:30	5:30	5:30	5:30	5:30	5:30	5:30
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	26997	26878	26955	27145	26502	26810	27358
Vehs Exited	25957	25789	25870	25954	25568	25742	26109
Starting Vehs	1502	1582	1492	1550	1543	1516	1531
Ending Vehs	2542	2671	2577	2741	2477	2584	2780
Travel Distance (mi)	27608	27207	26855	27391	26418	26957	27326
Travel Time (hr)	2712.1	2823.6	2660.8	2723.4	2898.8	2580.0	2768.3
Total Delay (hr)	1855.1	1980.4	1830.5	1875.6	2082.4	1743.5	1920.7
Total Stops	48907	49714	47523	49938	45595	46691	51411
Fuel Used (gal)	1282.3	1299.9	1255.5	1280.1	1301.3	1235.3	1293.3

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	4:15	4:15	4:15	4:15
End Time	5:30	5:30	5:30	5:30
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	27014	27145	27078	26986
Vehs Exited	25965	25899	25807	25869
Starting Vehs	1637	1484	1626	1533
Ending Vehs	2686	2730	2897	2657
Travel Distance (mi)	27719	27532	27512	27252
Travel Time (hr)	2718.1	2737.5	2946.5	2756.9
Total Delay (hr)	1858.4	1882.7	2093.1	1912.2
Total Stops	49004	49677	52280	49076
Fuel Used (gal)	1287.6	1287.4	1333.8	1285.7

Interval #0 Information Seeding

Start Time	4:15
End Time	4:30
Total Time (min)	15
No data recorded this interval.	

Interval #1 Information Recording

Start Time	4:30
End Time	5:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	26997	26878	26955	27145	26502	26810	27358
Vehs Exited	25957	25789	25870	25954	25568	25742	26109
Starting Vehs	1502	1582	1492	1550	1543	1516	1531
Ending Vehs	2542	2671	2577	2741	2477	2584	2780
Travel Distance (mi)	27608	27207	26855	27391	26418	26957	27326
Travel Time (hr)	2712.1	2823.6	2660.8	2723.4	2898.8	2580.0	2768.3
Total Delay (hr)	1855.1	1980.4	1830.5	1875.6	2082.4	1743.5	1920.7
Total Stops	48907	49714	47523	49938	45595	46691	51411
Fuel Used (gal)	1282.3	1299.9	1255.5	1280.1	1301.3	1235.3	1293.3

Interval #1 Information Recording

Start Time	4:30
End Time	5:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	27014	27145	27078	26986
Vehs Exited	25965	25899	25807	25869
Starting Vehs	1637	1484	1626	1533
Ending Vehs	2686	2730	2897	2657
Travel Distance (mi)	27719	27532	27512	27252
Travel Time (hr)	2718.1	2737.5	2946.5	2756.9
Total Delay (hr)	1858.4	1882.7	2093.1	1912.2
Total Stops	49004	49677	52280	49076
Fuel Used (gal)	1287.6	1287.4	1333.8	1285.7

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	141.3	144.7	142.5	0.0	0.0	0.0	5.1	3.3	4.4	96.9	97.6	100.2
Total Del/Veh (s)	405.0	466.6	431.9	77.7	34.9	29.3	396.8	63.3	142.1	448.5	356.0	365.4

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	68.1
Total Del/Veh (s)	259.8

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	19.0	26.1	49.3	0.0	0.0	0.0	71.8	100.5	47.0	19.8	21.2	16.5
Total Del/Veh (s)	88.0	95.5	91.1	28.2	1.1	0.5	774.7	741.8	662.5	283.5	212.2	75.8

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.8	2.7	2.4	0.4	0.1	0.1	6.1	4.1	4.6	457.0	448.0	473.2
Total Del/Veh (s)	62.0	33.2	29.5	74.6	56.4	52.5	390.1	172.5	173.1	1423.3	519.5	491.2

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	105.3
Total Del/Veh (s)	165.2

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	1.5	0.2	0.3	0.1
Total Del/Veh (s)	55.8	29.7	51.9	37.1	4.2	51.2	30.5	8.8	80.6	44.7	38.9	37.0

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.4	1.2	0.1
Total Del/Veh (s)	24.4	12.4	19.3	10.3	55.3	30.3	19.8

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	102.6	102.6	101.4	31.5	32.2	26.8
Total Del/Veh (s)	154.3	51.7	30.9	55.4	63.0	41.5	605.2	312.0	288.6	495.0	232.2	207.8

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	41.1
Total Del/Veh (s)	201.8

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.1
Total Del/Veh (s)	5.4	0.9	0.2	11.3	6.0	3.9	67.8	30.5	13.5	25.7	41.1	16.1

7: 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	3.9

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.8	0.4	0.4	2.2	0.4	0.4
Total Del/Veh (s)	68.1	17.8	11.7	37.7	11.0	6.9	256.5	47.4	44.2	134.2	51.8	46.8

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	35.6

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	7.6	5.3	6.4	1.6	0.2	1.4
Total Del/Veh (s)	118.2	95.3	75.7	137.9	35.8	23.2	173.7	164.3	122.8	465.7	56.8	24.8

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	2.0
Total Del/Veh (s)	111.7

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.1	0.0	0.1	0.3	0.2	0.1	0.2	0.1
Total Del/Veh (s)	8.6	1.6	0.5	17.0	6.6	5.7	126.5	47.3	38.9	53.9	40.1	27.9

10: 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.1
Total Del/Veh (s)	7.2

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.4	0.6	0.4	0.1	0.0	0.0	1.8	0.5	0.5	1.7	0.3	0.3
Total Del/Veh (s)	55.5	31.7	23.2	56.4	11.3	6.4	114.0	48.0	45.0	65.8	40.6	34.6

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	35.7

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.0	0.1	0.7	0.1	0.1	6.5	6.4	7.4	1.6	0.2	1.4
Total Del/Veh (s)	70.3	52.9	38.5	140.7	30.4	15.3	425.6	136.1	110.6	83.8	53.9	29.4

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	2.4
Total Del/Veh (s)	89.3

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	All
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.2	0.1	0.1	0.1	0.0
Total Del/Veh (s)	7.0	5.7	8.3	0.7	0.1	22.3	16.8	25.2	18.2	5.9

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	4.4	1.1	0.5	7.1	2.8	2.0	13.9	7.8	15.3	6.5	2.4

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.1	1.4	0.2	0.2	1.2	0.2	1.2
Total Del/Veh (s)	50.4	37.1	21.4	33.0	38.4	20.1	68.2	30.9	24.2	46.7	27.9	8.5

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	31.9

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.2	0.1
Total Del/Veh (s)	4.6	7.1	5.8

40: Country Club Rd Performance by movement

Movement	EBT	EBR	NBT	SBT	All
Denied Del/Veh (s)	0.0	0.0	0.3	0.0	0.1
Total Del/Veh (s)	0.2	12.4	16.1	2.7	10.3

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	7.1	1.8	4.5

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.1
Total Del/Veh (s)	2.2	0.6	11.3	6.3

100: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.3	0.0	0.1
Total Del/Veh (s)	12.7	2.0	8.1

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.1	0.0
Total Del/Veh (s)	1.8	3.9	2.7

200: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	685.9	672.1	1121.0	1108.6	127.6
Total Del/Veh (s)	19.3	16.3	5.6	4.3	808.8	791.7	818.0	743.0	77.2

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	4.3	7.4	0.0	0.1	0.1	2.4
Total Del/Veh (s)	47.2	22.9	3.1	145.9	5.8	26.1

1100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Del/Veh (s)	0.0	0.0	0.0	33.3	0.1	2.8
Total Del/Veh (s)	5.1	5.2	4.0	77.2	76.5	12.1

1101: 5th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	2.5	3.1	0.2	0.2	0.3
Total Del/Veh (s)	4.5	3.9	6.5	5.8	113.5	93.8	101.2	77.5	16.8

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.2	0.0
Total Del/Veh (s)	7.2	5.8	3.7	41.6	13.2	8.2

1400: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.1	0.0
Total Del/Veh (s)	2.6	2.2	1.6	11.0	17.7	3.7

3000: 6th/5th Street Performance by movement

Movement	EBT	WBT	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	216.9	227.3	19.0
Total Del/Veh (s)	9.6	0.6	396.8	361.4	35.4

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	All
Denied Del/Veh (s)	0.1	0.2	0.1	0.1	0.1
Total Del/Veh (s)	8.3	6.9	7.5	6.7	7.8

5000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.9	0.1	0.2	5.8	0.2	0.6
Total Del/Veh (s)	7.6	6.9	7.2	104.3	33.7	11.5

6000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.2	0.2	0.0	186.2	330.3	39.7
Total Del/Veh (s)	5.6	4.7	10.4	388.8	322.3	51.0

7000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBL	All
Denied Del/Veh (s)	0.0	0.0	0.2	0.1	0.2	0.1	0.1
Total Del/Veh (s)	3.4	2.5	6.7	5.1	17.0	50.9	6.3

8000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.2	0.1	0.1	0.2	0.1
Total Del/Veh (s)	3.2	8.3	25.9	20.4	7.1

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Del/Veh (s)	0.2	0.2	0.0	10.1	0.2	0.6
Total Del/Veh (s)	6.8	5.2	3.7	147.3	33.0	13.2

Total Network Performance

Denied Del/Veh (s)	49.7
Total Del/Veh (s)	192.0

Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	466.6	692.9	0.6	4
	1000	52.2	70.4	0.1	6
	2	95.5	149.5	0.2	7
Wilson HAWK	20	4.6	7.1	0.0	10
6th Side Street	200	19.3	26.6	0.1	9
Tucson Blvd	3	33.3	45.1	0.1	7
	3000	10.4	53.6	0.4	25
6th/5th Street	72	2.2	7.2	0.0	15
Country Club Rd	4	29.7	45.5	0.1	9
	9922	12.4	24.4	0.1	14
	4000	8.3	37.4	0.2	24
Dodge Blvd	5	24.4	71.1	0.4	20
	5000	8.2	19.2	0.1	16
Alvernon Wy	6	51.5	70.6	0.2	9
	6000	6.7	21.5	0.1	21
	70	7.1	20.1	0.1	19
	7	0.9	3.0	0.0	23
	7000	3.4	19.0	0.1	25
Columbus Blvd	8	16.9	30.1	0.1	14
	8000	3.5	12.0	0.1	21
Swan Rd	9	94.8	144.0	0.4	11
	9000	8.2	21.4	0.1	18
	100	12.7	27.9	0.1	16
	10	1.6	3.6	0.0	19
	1100	5.2	19.5	0.1	22
Rosemont Blvd	11	30.9	46.5	0.1	10
5th Side Street	1101	4.9	24.3	0.2	25
Craycroft Rd	12	51.8	90.0	0.3	13
	1200	8.5	27.2	0.2	20
	13	7.0	40.6	0.3	26
Chantilly Dr HAWK	130	1.7	4.3	0.0	16
Sahuara Ave	14	1.1	7.9	0.1	27
	1400	2.7	32.5	0.3	28
Wilmot Rd	15	37.7	62.1	0.3	15
Total		1126.0	1977.9	5.6	11

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	38.4	137.4	0.4	12
	1400	1.1	24.6	0.3	37
Sahuara Ave	14	2.8	32.8	0.3	28
	130	4.0	11.3	0.1	19
	13	0.7	2.7	0.0	25
	1200	3.8	37.9	0.3	27
Craycroft Rd	12	29.1	46.6	0.2	12
5th Side Street	1101	8.0	47.0	0.3	26
Rosemont Blvd	11	10.8	30.4	0.2	20
	1100	6.5	21.9	0.1	21
	10	6.6	20.9	0.1	21
Arcadia Ave HAWK	100	2.0	4.5	0.0	15
	9000	3.7	18.4	0.1	25
Swan Rd	9	35.0	46.7	0.1	8
	8000	9.4	60.0	0.4	26
Columbus Blvd	8	9.7	20.5	0.1	17
	7000	7.6	21.2	0.1	19
	7	6.0	21.5	0.1	22
Irving Ave HAWK	70	1.8	4.4	0.0	16
	6000	10.4	23.0	0.1	17
Alvernon Wy	6	64.8	78.6	0.1	6
	5000	8.6	28.5	0.2	22
Dodge Blvd	5	11.5	19.2	0.1	12
	4000	7.9	55.5	0.4	26
	9922	3.6	32.9	0.2	27
Country Club Rd	4	37.1	47.7	0.1	7
6th/5th Street	72	14.8	31.1	0.1	13
	3000	0.6	10.7	0.0	10
Tucson Blvd	3	56.5	98.5	0.4	13
6th Side Street	200	5.9	15.4	0.1	18
Wilson HAWK	20	7.2	15.4	0.1	15
	2	1.1	3.1	0.0	22
	1000	3.2	31.3	0.2	27
Campbell Ave	1	35.0	45.8	0.1	8
Total		455.0	1147.5	5.5	17

Queuing and Blocking Report

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Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	350	3030	3024	200	298	307	320	390	1310	1289	1246	370
Average Queue (ft)	275	2230	2223	153	165	185	206	374	843	823	561	151
95th Queue (ft)	499	3612	3592	283	286	293	300	438	1678	1671	1362	382
Link Distance (ft)		2986	2986			418	418		2020	2020	2020	
Upstream Blk Time (%)		38	37			1	0		3	1	2	
Queuing Penalty (veh)		0	0			3	0		0	0	0	
Storage Bay Dist (ft)	250			50	180			290				150
Storage Blk Time (%)	3	77	77	16	16	8		77	8		43	14
Queuing Penalty (veh)	15	221	222	79	57	18		389	21		76	72

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	350	2215	2213	2214
Average Queue (ft)	279	1894	1879	1855
95th Queue (ft)	474	2654	2656	2654
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)		56	51	51
Queuing Penalty (veh)		0	0	0
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	14	67		
Queuing Penalty (veh)	83	122		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	182	1219	46	74	430	281
Average Queue (ft)	20	921	19	18	252	110
95th Queue (ft)	104	1650	47	65	494	334
Link Distance (ft)		1205		55	442	447
Upstream Blk Time (%)		4	1	2	20	7
Queuing Penalty (veh)		46	0	25	0	0
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)		36	1	2		
Queuing Penalty (veh)		7	9	1		

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB	SB	SB
Directions Served	L	TR	L	TR	L	TR	L	TR
Maximum Queue (ft)	300	372	300	883	400	1461	400	2243
Average Queue (ft)	156	337	86	603	182	963	353	1805
95th Queue (ft)	334	451	255	818	442	1679	505	2779
Link Distance (ft)		356		1877		1948		2192
Upstream Blk Time (%)		23				5		58
Queuing Penalty (veh)		289				0		0
Storage Bay Dist (ft)	200		100		300		300	
Storage Blk Time (%)	1	38	2	46	7	61	71	33
Queuing Penalty (veh)	9	59	15	24	40	25	462	26

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	B9922	NB	NB	NB	NB
Directions Served	L	T	T	L	T	T	R	T	L	T	T	R
Maximum Queue (ft)	278	318	288	190	421	388	172	55	177	222	222	170
Average Queue (ft)	121	219	185	159	267	155	10	3	116	188	191	71
95th Queue (ft)	235	307	272	235	389	295	87	40	195	212	212	186
Link Distance (ft)		486	486		423	423		1235		177	177	
Upstream Blk Time (%)					1	0			2	29	29	0
Queuing Penalty (veh)					3	1			0	191	189	0
Storage Bay Dist (ft)	180			90			160		100			70
Storage Blk Time (%)	2	15		24	49	5	0		16	46	47	0
Queuing Penalty (veh)	9	29		68	90	12	1		82	68	60	2

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB	SB
Directions Served	L	T	TR
Maximum Queue (ft)	174	502	497
Average Queue (ft)	160	326	322
95th Queue (ft)	207	472	460
Link Distance (ft)		2000	2000
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)	100		
Storage Blk Time (%)	39	43	
Queuing Penalty (veh)	173	86	

Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB
Directions Served	T	R	L	T	L	R
Maximum Queue (ft)	604	285	286	299	258	190
Average Queue (ft)	342	107	101	260	102	86
95th Queue (ft)	572	301	225	342	240	176
Link Distance (ft)	2072				288	2124
Upstream Blk Time (%)			0	2		
Queuing Penalty (veh)			0	28		
Storage Bay Dist (ft)	110		300		220	
Storage Blk Time (%)	24	0	0	2	1	2
Queuing Penalty (veh)	36	0	0	5	1	2

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	TR	L	T	TR
Maximum Queue (ft)	400	811	285	400	597	285	250	2159	2164	250	1935	1936
Average Queue (ft)	303	518	108	178	566	168	220	1760	1746	230	1335	1321
95th Queue (ft)	470	895	301	455	664	368	319	2639	2629	315	2262	2243
Link Distance (ft)	825				580		2113		2113	1973		1973
Upstream Blk Time (%)	4				16		52		49	25		23
Queuing Penalty (veh)	41				164		0		0	0		0
Storage Bay Dist (ft)	300		110		300		110		150	150		
Storage Blk Time (%)	20	47	0		58	1	64	54		76	53	
Queuing Penalty (veh)	165	162	2		135	11	402	76		447	67	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	30	57	80	338	61	45
Average Queue (ft)	3	7	6	84	25	12
95th Queue (ft)	17	35	42	256	53	37
Link Distance (ft)	56		628		311	327
Upstream Blk Time (%)	0					
Queuing Penalty (veh)	3					
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0		5			
Queuing Penalty (veh)	0		0			

Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	174	519	185	147	392	174	315	600	307	584
Average Queue (ft)	77	259	34	44	181	26	149	314	83	343
95th Queue (ft)	162	453	133	108	330	105	317	509	228	546
Link Distance (ft)		533			436			2075		1939
Upstream Blk Time (%)		0			0					
Queuing Penalty (veh)		5			2					
Storage Bay Dist (ft)	75		100	75		75	250		350	
Storage Blk Time (%)	15	29	0	3	20	0	13	17		12
Queuing Penalty (veh)	133	44	0	26	26	1	68	11		6

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	360	1446	254	421	483	200	305	1678	1662	450	380	1097
Average Queue (ft)	198	820	90	187	352	77	177	1067	1058	337	339	618
95th Queue (ft)	416	1744	273	365	532	216	380	1939	1924	620	456	1134
Link Distance (ft)		2213			466			1988	1988			2061
Upstream Blk Time (%)		0		0	4			9	10			
Queuing Penalty (veh)		5		0	33			0	0			
Storage Bay Dist (ft)	300		110	500		100	180			350	230	
Storage Blk Time (%)		56		0	42	0	1	63	56		78	20
Queuing Penalty (veh)		127		0	102	0	6	68	102		406	32

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	1066	371
Average Queue (ft)	597	70
95th Queue (ft)	1099	241
Link Distance (ft)	2061	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	16	
Queuing Penalty (veh)	15	

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Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	46	72	147	352	175	116
Average Queue (ft)	15	14	26	93	63	42
95th Queue (ft)	42	55	90	269	145	88
Link Distance (ft)		54		579	279	312
Upstream Blk Time (%)	1	1		0	0	
Queuing Penalty (veh)	0	11		0	0	
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	1	1		6		
Queuing Penalty (veh)	7	0		3		

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	134	618	180	172	328	148	250	591	249	416
Average Queue (ft)	88	461	89	40	149	18	160	284	64	213
95th Queue (ft)	154	695	217	101	257	85	281	549	178	357
Link Distance (ft)		607			810			1974		2044
Upstream Blk Time (%)		2								
Queuing Penalty (veh)		29								
Storage Bay Dist (ft)	75		110	100		110	150		150	
Storage Blk Time (%)	16	36	0	1	10	0	23	27	0	23
Queuing Penalty (veh)	168	100	1	5	9	0	93	37	0	12

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	400	900	210	438	495	210	330	1760	1742	230	299	664
Average Queue (ft)	197	436	107	200	247	56	284	1025	1014	133	105	432
95th Queue (ft)	423	833	246	419	466	180	418	1911	1888	296	287	642
Link Distance (ft)		1664			711			1958	1958			2072
Upstream Blk Time (%)					1			9	9			
Queuing Penalty (veh)					6			0	0			
Storage Bay Dist (ft)	300		110	500		110	180			180	150	
Storage Blk Time (%)	2	50	2	4	29	0	67	48	50	0	1	47
Queuing Penalty (veh)	18	189	13	21	69	0	440	63	65	0	9	28

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	647	300
Average Queue (ft)	432	140
95th Queue (ft)	638	351
Link Distance (ft)	2072	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	46	0
Queuing Penalty (veh)	54	0

Intersection: 13: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	TR	L	TR	LTR	LTR
Maximum Queue (ft)	310	37	55	122	65
Average Queue (ft)	70	10	4	49	26
95th Queue (ft)	222	34	28	96	55
Link Distance (ft)	1468		56	429	480
Upstream Blk Time (%)		0	0		
Queuing Penalty (veh)		0	2		
Storage Bay Dist (ft)		100			
Storage Blk Time (%)	4	0	0		
Queuing Penalty (veh)	0	1	0		

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	41	67	47	49	49	64
Average Queue (ft)	10	4	11	3	19	27
95th Queue (ft)	35	30	37	26	45	56
Link Distance (ft)		275		1271	622	706
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)		0		0		
Queuing Penalty (veh)		0		0		

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	241	420	245	101	359	506	474	433	230	437	441	389
Average Queue (ft)	137	175	118	36	226	307	298	268	57	279	275	237
95th Queue (ft)	222	304	224	79	375	449	428	396	171	390	390	352
Link Distance (ft)		1220		2255		1994	1994	1994		2040	2040	2040
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	145		145		240				130			
Storage Blk Time (%)	8	17	2		10	14			0	32		2
Queuing Penalty (veh)	48	71	11		61	37			0	14		4

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	204
Average Queue (ft)	59
95th Queue (ft)	131
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	89	307
Average Queue (ft)	54	123
95th Queue (ft)	89	313
Link Distance (ft)	55	298
Upstream Blk Time (%)	22	1
Queuing Penalty (veh)	292	12
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 40: Country Club Rd

Movement	EB	NB	NB	SB
Directions Served	R	T	T	T
Maximum Queue (ft)	139	391	399	8
Average Queue (ft)	56	181	185	0
95th Queue (ft)	104	346	355	5
Link Distance (ft)	450	2095	2095	177
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	403	88
Average Queue (ft)	106	35
95th Queue (ft)	310	87
Link Distance (ft)	514	56
Upstream Blk Time (%)		8
Queuing Penalty (veh)		59
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 72: 6th/5th Street

Movement	SW	SW
Directions Served	R	R
Maximum Queue (ft)	481	149
Average Queue (ft)	79	5
95th Queue (ft)	285	82
Link Distance (ft)	486	486
Upstream Blk Time (%)	0	0
Queuing Penalty (veh)	1	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 100: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	534	74
Average Queue (ft)	188	33
95th Queue (ft)	468	79
Link Distance (ft)	620	54
Upstream Blk Time (%)	0	11
Queuing Penalty (veh)	1	79
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	74	248
Average Queue (ft)	32	58
95th Queue (ft)	78	174
Link Distance (ft)	56	275
Upstream Blk Time (%)	7	0
Queuing Penalty (veh)	54	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 200: 6th Side Street & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	317	211	461	460
Average Queue (ft)	254	19	394	423
95th Queue (ft)	429	114	572	482
Link Distance (ft)	298	356	433	422
Upstream Blk Time (%)	14	0	75	93
Queuing Penalty (veh)	178	3	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	R	LTR	LTR	LTR
Maximum Queue (ft)	446	453	20	28	61
Average Queue (ft)	371	328	1	3	25
95th Queue (ft)	546	563	22	18	49
Link Distance (ft)	418	418	1205	446	415
Upstream Blk Time (%)	19	17			
Queuing Penalty (veh)	128	116			
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 1100: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	315	6	238	101
Average Queue (ft)	46	0	124	33
95th Queue (ft)	207	5	252	86
Link Distance (ft)	579	607	226	197
Upstream Blk Time (%)	0		19	
Queuing Penalty (veh)	0		0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

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Intersection: 1101: 5th Side Street & 6th/5th Street

Movement	EB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	2	330	179
Average Queue (ft)	0	155	70
95th Queue (ft)	2	312	170
Link Distance (ft)	810	341	283
Upstream Blk Time (%)		6	0
Queuing Penalty (veh)		0	0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1200: 6th/5th Street

Movement	WB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	40	148	122
Average Queue (ft)	5	59	49
95th Queue (ft)	70	116	98
Link Distance (ft)	1468	214	281
Upstream Blk Time (%)		0	
Queuing Penalty (veh)		0	
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1400: 6th/5th Street

Movement	WB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	2	118	62
Average Queue (ft)	0	50	19
95th Queue (ft)	2	92	48
Link Distance (ft)	1220	307	277
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 3000: 6th/5th Street

Movement	EB	WB	SB
Directions Served	TR	T	LTR
Maximum Queue (ft)	49	22	463
Average Queue (ft)	3	1	373
95th Queue (ft)	35	13	561
Link Distance (ft)	1877	100	430
Upstream Blk Time (%)		0	58
Queuing Penalty (veh)		0	0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 4000: 6th/5th Street

Movement	B9922	B9922
Directions Served	T	
Maximum Queue (ft)	413	339
Average Queue (ft)	157	42
95th Queue (ft)	444	215
Link Distance (ft)	423	423
Upstream Blk Time (%)	0	0
Queuing Penalty (veh)	1	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 5000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	189	253	147	193
Average Queue (ft)	56	98	58	82
95th Queue (ft)	253	227	184	151
Link Distance (ft)	375	825	318	286
Upstream Blk Time (%)	1		4	
Queuing Penalty (veh)	15		0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

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Intersection: 6000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	12	392	363	392
Average Queue (ft)	0	139	267	299
95th Queue (ft)	9	332	469	508
Link Distance (ft)	580	514	362	366
Upstream Blk Time (%)		0	42	63
Queuing Penalty (veh)		0	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 7000: 6th/5th Street

Movement	EB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	180	130	81
Average Queue (ft)	12	51	26
95th Queue (ft)	100	98	63
Link Distance (ft)	628	267	311
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 8000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	31	99	100	123
Average Queue (ft)	7	7	39	49
95th Queue (ft)	77	69	86	93
Link Distance (ft)	302	2213	326	342
Upstream Blk Time (%)	0			
Queuing Penalty (veh)	1			
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 9000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	54	210	211	157
Average Queue (ft)	2	36	114	67
95th Queue (ft)	32	158	221	134
Link Distance (ft)	466	620	210	316
Upstream Blk Time (%)			10	
Queuing Penalty (veh)			0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 9081

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	4:15	4:15	4:15	4:15	4:15	4:15	4:15
End Time	5:30	5:30	5:30	5:30	5:30	5:30	5:30
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	26997	26878	26955	27145	26502	26810	27358
Vehs Exited	25957	25789	25870	25954	25568	25742	26109
Starting Vehs	1502	1582	1492	1550	1543	1516	1531
Ending Vehs	2542	2671	2577	2741	2477	2584	2780
Denied Entry After	1156	1281	1417	1236	1875	1185	1192
Travel Distance (mi)	27608	27207	26855	27391	26418	26957	27326
Travel Time (hr)	2712.1	2823.6	2660.8	2723.4	2898.8	2580.0	2768.3
Total Delay (hr)	1855.1	1980.4	1830.5	1875.6	2082.4	1743.5	1920.7
Total Stops	48907	49714	47523	49938	45595	46691	51411
Fuel Used (gal)	1282.3	1299.9	1255.5	1280.1	1301.3	1235.3	1293.3

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	4:15	4:15	4:15	4:15
End Time	5:30	5:30	5:30	5:30
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	27014	27145	27078	26986
Vehs Exited	25965	25899	25807	25869
Starting Vehs	1637	1484	1626	1533
Ending Vehs	2686	2730	2897	2657
Denied Entry After	1184	1234	1407	1313
Travel Distance (mi)	27719	27532	27512	27252
Travel Time (hr)	2718.1	2737.5	2946.5	2756.9
Total Delay (hr)	1858.4	1882.7	2093.1	1912.2
Total Stops	49004	49677	52280	49076
Fuel Used (gal)	1287.6	1287.4	1333.8	1285.7

Interval #0 Information Seeding

Start Time	4:15
End Time	4:30
Total Time (min)	15

No data recorded this interval.

Interval #1 Information Recording

Start Time	4:30
End Time	5:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	26997	26878	26955	27145	26502	26810	27358
Vehs Exited	25957	25789	25870	25954	25568	25742	26109
Starting Vehs	1502	1582	1492	1550	1543	1516	1531
Ending Vehs	2542	2671	2577	2741	2477	2584	2780
Denied Entry After	1156	1281	1417	1236	1875	1185	1192
Travel Distance (mi)	27608	27207	26855	27391	26418	26957	27326
Travel Time (hr)	2712.1	2823.6	2660.8	2723.4	2898.8	2580.0	2768.3
Total Delay (hr)	1855.1	1980.4	1830.5	1875.6	2082.4	1743.5	1920.7
Total Stops	48907	49714	47523	49938	45595	46691	51411
Fuel Used (gal)	1282.3	1299.9	1255.5	1280.1	1301.3	1235.3	1293.3

Interval #1 Information Recording

Start Time	4:30
End Time	5:30
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	27014	27145	27078	26986
Vehs Exited	25965	25899	25807	25869
Starting Vehs	1637	1484	1626	1533
Ending Vehs	2686	2730	2897	2657
Denied Entry After	1184	1234	1407	1313
Travel Distance (mi)	27719	27532	27512	27252
Travel Time (hr)	2718.1	2737.5	2946.5	2756.9
Total Delay (hr)	1858.4	1882.7	2093.1	1912.2
Total Stops	49004	49677	52280	49076
Fuel Used (gal)	1287.6	1287.4	1333.8	1285.7

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	57	198	56	0	0	0	4	20	3	16	170	19

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	543

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Entry After	0	12	1	0	0	0	2	5	0	0	0	20

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	2	0	31	230	32

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	295

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Entry After	0	0	0	0	0	0	0

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	14	132	10	4	50	4

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	214

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

7: 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	11	2	0	0	0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	13

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

10: 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	1	19	2	0	0	0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	22

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

40: Country Club Rd Performance by movement

Movement	EBT	EBR	NBT	SBT	All
Denied Entry After	0	0	0	0	0

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWR	All
Denied Entry After	0	0	0	0

100: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

200: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	18	18	29	63	128

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Entry After	5	0	0	0	0	5

1100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Entry After	0	0	0	4	0	4

1101: 5th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Entry After	0	0	0	0	0	0

1400: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBL	All
Denied Entry After	0	0	0	0	0	0

3000: 6th/5th Street Performance by movement

Movement	EBT	WBT	SBL	SBR	All
Denied Entry After	0	0	15	6	21

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	All
Denied Entry After	0	0	0	0	0

5000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0

6000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Entry After	0	0	0	13	34	47

7000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBL	All
Denied Entry After	0	0	0	0	0	0	0

8000: 6th/5th Street Performance by movement

Movement	EBT	WBT	NBR	SBR	All
Denied Entry After	0	0	0	0	0

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBR	All
Denied Entry After	0	0	0	1	0	1

Total Network Performance

Denied Entry After						1313
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Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	466.6	692.9	0.6	4
	1000	52.2	70.4	0.1	6
	2	95.5	149.5	0.2	7
Wilson HAWK	20	4.6	7.1	0.0	10
6th Side Street	200	19.3	26.6	0.1	9
Tucson Blvd	3	33.3	45.1	0.1	7
	3000	10.4	53.6	0.4	25
6th/5th Street	72	2.2	7.2	0.0	15
Country Club Rd	4	29.7	45.5	0.1	9
	9922	12.4	24.4	0.1	14
	4000	8.3	37.4	0.2	24
Dodge Blvd	5	24.4	71.1	0.4	20
	5000	8.2	19.2	0.1	16
Alvernon Wy	6	51.5	70.6	0.2	9
	6000	6.7	21.5	0.1	21
	70	7.1	20.1	0.1	19
	7	0.9	3.0	0.0	23
	7000	3.4	19.0	0.1	25
Columbus Blvd	8	16.9	30.1	0.1	14
	8000	3.5	12.0	0.1	21
Swan Rd	9	94.8	144.0	0.4	11
	9000	8.2	21.4	0.1	18
	100	12.7	27.9	0.1	16
	10	1.6	3.6	0.0	19
	1100	5.2	19.5	0.1	22
Rosemont Blvd	11	30.9	46.5	0.1	10
5th Side Street	1101	4.9	24.3	0.2	25
Craycroft Rd	12	51.8	90.0	0.3	13
	1200	8.5	27.2	0.2	20
	13	7.0	40.6	0.3	26
Chantilly Dr HAWK	130	1.7	4.3	0.0	16
Sahuara Ave	14	1.1	7.9	0.1	27
	1400	2.7	32.5	0.3	28
Wilmot Rd	15	37.7	62.1	0.3	15
Total		1126.0	1977.9	5.6	11

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	38.4	137.4	0.4	12
	1400	1.1	24.6	0.3	37
Sahuara Ave	14	2.8	32.8	0.3	28
	130	4.0	11.3	0.1	19
	13	0.7	2.7	0.0	25
	1200	3.8	37.9	0.3	27
Craycroft Rd	12	29.1	46.6	0.2	12
5th Side Street	1101	8.0	47.0	0.3	26
Rosemont Blvd	11	10.8	30.4	0.2	20
	1100	6.5	21.9	0.1	21
	10	6.6	20.9	0.1	21
Arcadia Ave HAWK	100	2.0	4.5	0.0	15
	9000	3.7	18.4	0.1	25
Swan Rd	9	35.0	46.7	0.1	8
	8000	9.4	60.0	0.4	26
Columbus Blvd	8	9.7	20.5	0.1	17
	7000	7.6	21.2	0.1	19
	7	6.0	21.5	0.1	22
Irving Ave HAWK	70	1.8	4.4	0.0	16
	6000	10.4	23.0	0.1	17
Alvernon Wy	6	64.8	78.6	0.1	6
	5000	8.6	28.5	0.2	22
Dodge Blvd	5	11.5	19.2	0.1	12
	4000	7.9	55.5	0.4	26
	9922	3.6	32.9	0.2	27
Country Club Rd	4	37.1	47.7	0.1	7
6th/5th Street	72	14.8	31.1	0.1	13
	3000	0.6	10.7	0.0	10
Tucson Blvd	3	56.5	98.5	0.4	13
6th Side Street	200	5.9	15.4	0.1	18
Wilson HAWK	20	7.2	15.4	0.1	15
	2	1.1	3.1	0.0	22
	1000	3.2	31.3	0.2	27
Campbell Ave	1	35.0	45.8	0.1	8
Total		455.0	1147.5	5.5	17

Queuing and Blocking Report

11/07/2022

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	350	3030	3024	200	298	307	320	390	1310	1289	1246	370
Average Queue (ft)	275	2230	2223	153	165	185	206	374	843	823	561	151
95th Queue (ft)	499	3612	3592	283	286	293	300	438	1678	1671	1362	382
Link Distance (ft)		2986	2986			418	418		2020	2020	2020	
Upstream Blk Time (%)		38	37			1	0		3	1	2	
Queuing Penalty (veh)		0	0			3	0		0	0	0	
Storage Bay Dist (ft)	250			50	180			290				150
Storage Blk Time (%)	3	77	77	16	16	8		77	8		43	14
Queuing Penalty (veh)	15	221	222	79	57	18		389	21		76	72

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	350	2215	2213	2214
Average Queue (ft)	279	1894	1879	1855
95th Queue (ft)	474	2654	2656	2654
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)		56	51	51
Queuing Penalty (veh)		0	0	0
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	14	67		
Queuing Penalty (veh)	83	122		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	182	1219	46	74	430	281
Average Queue (ft)	20	921	19	18	252	110
95th Queue (ft)	104	1650	47	65	494	334
Link Distance (ft)		1205		55	442	447
Upstream Blk Time (%)		4	1	2	20	7
Queuing Penalty (veh)		46	0	25	0	0
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)		36	1	2		
Queuing Penalty (veh)		7	9	1		

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB	SB	SB
Directions Served	L	TR	L	TR	L	TR	L	TR
Maximum Queue (ft)	300	372	300	883	400	1461	400	2243
Average Queue (ft)	156	337	86	603	182	963	353	1805
95th Queue (ft)	334	451	255	818	442	1679	505	2779
Link Distance (ft)		356		1877		1948		2192
Upstream Blk Time (%)		23				5		58
Queuing Penalty (veh)		289				0		0
Storage Bay Dist (ft)	200		100		300		300	
Storage Blk Time (%)	1	38	2	46	7	61	71	33
Queuing Penalty (veh)	9	59	15	24	40	25	462	26

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	B9922	NB	NB	NB	NB
Directions Served	L	T	T	L	T	T	R	T	L	T	T	R
Maximum Queue (ft)	278	318	288	190	421	388	172	55	177	222	222	170
Average Queue (ft)	121	219	185	159	267	155	10	3	116	188	191	71
95th Queue (ft)	235	307	272	235	389	295	87	40	195	212	212	186
Link Distance (ft)		486	486		423	423		1235		177	177	
Upstream Blk Time (%)					1	0			2	29	29	0
Queuing Penalty (veh)					3	1			0	191	189	0
Storage Bay Dist (ft)	180			90			160		100			70
Storage Blk Time (%)	2	15		24	49	5	0		16	46	47	0
Queuing Penalty (veh)	9	29		68	90	12	1		82	68	60	2

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB	SB
Directions Served	L	T	TR
Maximum Queue (ft)	174	502	497
Average Queue (ft)	160	326	322
95th Queue (ft)	207	472	460
Link Distance (ft)		2000	2000
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)	100		
Storage Blk Time (%)	39	43	
Queuing Penalty (veh)	173	86	

Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB
Directions Served	T	R	L	T	L	R
Maximum Queue (ft)	604	285	286	299	258	190
Average Queue (ft)	342	107	101	260	102	86
95th Queue (ft)	572	301	225	342	240	176
Link Distance (ft)	2072				288	2124
Upstream Blk Time (%)			0	2		
Queuing Penalty (veh)			0	28		
Storage Bay Dist (ft)	110		300		220	
Storage Blk Time (%)	24	0	0	2	1	2
Queuing Penalty (veh)	36	0	0	5	1	2

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	TR	L	T	TR
Maximum Queue (ft)	400	811	285	400	597	285	250	2159	2164	250	1935	1936
Average Queue (ft)	303	518	108	178	566	168	220	1760	1746	230	1335	1321
95th Queue (ft)	470	895	301	455	664	368	319	2639	2629	315	2262	2243
Link Distance (ft)	825				580		2113		2113	1973		1973
Upstream Blk Time (%)	4				16		52		49	25		23
Queuing Penalty (veh)	41				164		0		0	0		0
Storage Bay Dist (ft)	300		110		300		110		150	150		
Storage Blk Time (%)	20	47	0		58	1	64	54		76	53	
Queuing Penalty (veh)	165	162	2		135	11	402	76		447	67	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	30	57	80	338	61	45
Average Queue (ft)	3	7	6	84	25	12
95th Queue (ft)	17	35	42	256	53	37
Link Distance (ft)	56		628		311	327
Upstream Blk Time (%)	0					
Queuing Penalty (veh)	3					
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0		5			
Queuing Penalty (veh)	0		0			

Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	174	519	185	147	392	174	315	600	307	584
Average Queue (ft)	77	259	34	44	181	26	149	314	83	343
95th Queue (ft)	162	453	133	108	330	105	317	509	228	546
Link Distance (ft)		533			436			2075		1939
Upstream Blk Time (%)		0			0					
Queuing Penalty (veh)		5			2					
Storage Bay Dist (ft)	75		100	75		75	250		350	
Storage Blk Time (%)	15	29	0	3	20	0	13	17		12
Queuing Penalty (veh)	133	44	0	26	26	1	68	11		6

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	360	1446	254	421	483	200	305	1678	1662	450	380	1097
Average Queue (ft)	198	820	90	187	352	77	177	1067	1058	337	339	618
95th Queue (ft)	416	1744	273	365	532	216	380	1939	1924	620	456	1134
Link Distance (ft)		2213			466			1988	1988			2061
Upstream Blk Time (%)		0		0	4			9	10			
Queuing Penalty (veh)		5		0	33			0	0			
Storage Bay Dist (ft)	300		110	500		100	180			350	230	
Storage Blk Time (%)		56		0	42	0	1	63	56		78	20
Queuing Penalty (veh)		127		0	102	0	6	68	102		406	32

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	1066	371
Average Queue (ft)	597	70
95th Queue (ft)	1099	241
Link Distance (ft)	2061	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	16	
Queuing Penalty (veh)	15	

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Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	46	72	147	352	175	116
Average Queue (ft)	15	14	26	93	63	42
95th Queue (ft)	42	55	90	269	145	88
Link Distance (ft)		54		579	279	312
Upstream Blk Time (%)	1	1		0	0	
Queuing Penalty (veh)	0	11		0	0	
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	1	1		6		
Queuing Penalty (veh)	7	0		3		

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	134	618	180	172	328	148	250	591	249	416
Average Queue (ft)	88	461	89	40	149	18	160	284	64	213
95th Queue (ft)	154	695	217	101	257	85	281	549	178	357
Link Distance (ft)		607			810			1974		2044
Upstream Blk Time (%)		2								
Queuing Penalty (veh)		29								
Storage Bay Dist (ft)	75		110	100		110	150		150	
Storage Blk Time (%)	16	36	0	1	10	0	23	27	0	23
Queuing Penalty (veh)	168	100	1	5	9	0	93	37	0	12

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	400	900	210	438	495	210	330	1760	1742	230	299	664
Average Queue (ft)	197	436	107	200	247	56	284	1025	1014	133	105	432
95th Queue (ft)	423	833	246	419	466	180	418	1911	1888	296	287	642
Link Distance (ft)		1664			711			1958	1958			2072
Upstream Blk Time (%)					1			9	9			
Queuing Penalty (veh)					6			0	0			
Storage Bay Dist (ft)	300		110	500		110	180			180	150	
Storage Blk Time (%)	2	50	2	4	29	0	67	48	50	0	1	47
Queuing Penalty (veh)	18	189	13	21	69	0	440	63	65	0	9	28

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	647	300
Average Queue (ft)	432	140
95th Queue (ft)	638	351
Link Distance (ft)	2072	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	46	0
Queuing Penalty (veh)	54	0

Intersection: 13: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	TR	L	TR	LTR	LTR
Maximum Queue (ft)	310	37	55	122	65
Average Queue (ft)	70	10	4	49	26
95th Queue (ft)	222	34	28	96	55
Link Distance (ft)	1468		56	429	480
Upstream Blk Time (%)		0	0		
Queuing Penalty (veh)		0	2		
Storage Bay Dist (ft)		100			
Storage Blk Time (%)	4	0	0		
Queuing Penalty (veh)	0	1	0		

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	41	67	47	49	49	64
Average Queue (ft)	10	4	11	3	19	27
95th Queue (ft)	35	30	37	26	45	56
Link Distance (ft)		275		1271	622	706
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)		0		0		
Queuing Penalty (veh)		0		0		

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	241	420	245	101	359	506	474	433	230	437	441	389
Average Queue (ft)	137	175	118	36	226	307	298	268	57	279	275	237
95th Queue (ft)	222	304	224	79	375	449	428	396	171	390	390	352
Link Distance (ft)		1220		2255		1994	1994	1994		2040	2040	2040
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	145		145		240				130			
Storage Blk Time (%)	8	17	2		10	14			0	32		2
Queuing Penalty (veh)	48	71	11		61	37			0	14		4

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	204
Average Queue (ft)	59
95th Queue (ft)	131
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	89	307
Average Queue (ft)	54	123
95th Queue (ft)	89	313
Link Distance (ft)	55	298
Upstream Blk Time (%)	22	1
Queuing Penalty (veh)	292	12
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 40: Country Club Rd

Movement	EB	NB	NB	SB
Directions Served	R	T	T	T
Maximum Queue (ft)	139	391	399	8
Average Queue (ft)	56	181	185	0
95th Queue (ft)	104	346	355	5
Link Distance (ft)	450	2095	2095	177
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	403	88
Average Queue (ft)	106	35
95th Queue (ft)	310	87
Link Distance (ft)	514	56
Upstream Blk Time (%)		8
Queuing Penalty (veh)		59
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

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Intersection: 72: 6th/5th Street

Movement	SW	SW
Directions Served	R	R
Maximum Queue (ft)	481	149
Average Queue (ft)	79	5
95th Queue (ft)	285	82
Link Distance (ft)	486	486
Upstream Blk Time (%)	0	0
Queuing Penalty (veh)	1	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 100: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	534	74
Average Queue (ft)	188	33
95th Queue (ft)	468	79
Link Distance (ft)	620	54
Upstream Blk Time (%)	0	11
Queuing Penalty (veh)	1	79
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	74	248
Average Queue (ft)	32	58
95th Queue (ft)	78	174
Link Distance (ft)	56	275
Upstream Blk Time (%)	7	0
Queuing Penalty (veh)	54	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 200: 6th Side Street & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	317	211	461	460
Average Queue (ft)	254	19	394	423
95th Queue (ft)	429	114	572	482
Link Distance (ft)	298	356	433	422
Upstream Blk Time (%)	14	0	75	93
Queuing Penalty (veh)	178	3	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	R	LTR	LTR	LTR
Maximum Queue (ft)	446	453	20	28	61
Average Queue (ft)	371	328	1	3	25
95th Queue (ft)	546	563	22	18	49
Link Distance (ft)	418	418	1205	446	415
Upstream Blk Time (%)	19	17			
Queuing Penalty (veh)	128	116			
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 1100: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	315	6	238	101
Average Queue (ft)	46	0	124	33
95th Queue (ft)	207	5	252	86
Link Distance (ft)	579	607	226	197
Upstream Blk Time (%)	0		19	
Queuing Penalty (veh)	0		0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1101: 5th Side Street & 6th/5th Street

Movement	EB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	2	330	179
Average Queue (ft)	0	155	70
95th Queue (ft)	2	312	170
Link Distance (ft)	810	341	283
Upstream Blk Time (%)		6	0
Queuing Penalty (veh)		0	0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1200: 6th/5th Street

Movement	WB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	40	148	122
Average Queue (ft)	5	59	49
95th Queue (ft)	70	116	98
Link Distance (ft)	1468	214	281
Upstream Blk Time (%)		0	
Queuing Penalty (veh)		0	
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1400: 6th/5th Street

Movement	WB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	2	118	62
Average Queue (ft)	0	50	19
95th Queue (ft)	2	92	48
Link Distance (ft)	1220	307	277
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 3000: 6th/5th Street

Movement	EB	WB	SB
Directions Served	TR	T	LTR
Maximum Queue (ft)	49	22	463
Average Queue (ft)	3	1	373
95th Queue (ft)	35	13	561
Link Distance (ft)	1877	100	430
Upstream Blk Time (%)		0	58
Queuing Penalty (veh)		0	0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 4000: 6th/5th Street

Movement	B9922	B9922
Directions Served	T	
Maximum Queue (ft)	413	339
Average Queue (ft)	157	42
95th Queue (ft)	444	215
Link Distance (ft)	423	423
Upstream Blk Time (%)	0	0
Queuing Penalty (veh)	1	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 5000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	189	253	147	193
Average Queue (ft)	56	98	58	82
95th Queue (ft)	253	227	184	151
Link Distance (ft)	375	825	318	286
Upstream Blk Time (%)	1		4	
Queuing Penalty (veh)	15		0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

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Intersection: 6000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	12	392	363	392
Average Queue (ft)	0	139	267	299
95th Queue (ft)	9	332	469	508
Link Distance (ft)	580	514	362	366
Upstream Blk Time (%)		0	42	63
Queuing Penalty (veh)		0	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 7000: 6th/5th Street

Movement	EB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	180	130	81
Average Queue (ft)	12	51	26
95th Queue (ft)	100	98	63
Link Distance (ft)	628	267	311
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 8000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	31	99	100	123
Average Queue (ft)	7	7	39	49
95th Queue (ft)	77	69	86	93
Link Distance (ft)	302	2213	326	342
Upstream Blk Time (%)	0			
Queuing Penalty (veh)	1			
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 9000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	54	210	211	157
Average Queue (ft)	2	36	114	67
95th Queue (ft)	32	158	221	134
Link Distance (ft)	466	620	210	316
Upstream Blk Time (%)			10	
Queuing Penalty (veh)			0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 9081

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	6:45	6:45	6:45	6:45	6:45	6:45	6:45
End Time	8:00	8:00	8:00	8:00	8:00	8:00	8:00
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	24308	23951	24139	24336	24266	24061	24007
Vehs Exited	24128	23824	23924	24067	24041	23908	23794
Starting Vehs	1039	1018	938	999	1047	1046	991
Ending Vehs	1219	1145	1153	1268	1272	1199	1204
Travel Distance (mi)	25541	25145	25072	25361	25103	25328	25075
Travel Time (hr)	1230.1	1185.7	1182.6	1224.7	1175.4	1197.4	1170.9
Total Delay (hr)	426.7	395.7	397.6	428.3	387.8	401.1	381.5
Total Stops	31248	29892	29522	30942	29308	30025	29502
Fuel Used (gal)	900.6	877.7	877.0	891.5	875.5	885.4	873.7

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	6:45	6:45	6:45	6:45
End Time	8:00	8:00	8:00	8:00
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	24153	24036	24492	24173
Vehs Exited	23972	23838	24254	23975
Starting Vehs	1041	1019	1012	992
Ending Vehs	1222	1217	1250	1191
Travel Distance (mi)	25143	24978	25481	25223
Travel Time (hr)	1172.2	1166.9	1228.8	1193.5
Total Delay (hr)	382.9	383.2	428.3	401.3
Total Stops	28784	29136	31250	29955
Fuel Used (gal)	874.2	868.8	896.9	882.1

Interval #0 Information Seeding

Start Time	6:45
End Time	7:00
Total Time (min)	15
No data recorded this interval.	

Interval #1 Information Recording

Start Time	7:00
End Time	8:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	24308	23951	24139	24336	24266	24061	24007
Vehs Exited	24128	23824	23924	24067	24041	23908	23794
Starting Vehs	1039	1018	938	999	1047	1046	991
Ending Vehs	1219	1145	1153	1268	1272	1199	1204
Travel Distance (mi)	25541	25145	25072	25361	25103	25328	25075
Travel Time (hr)	1230.1	1185.7	1182.6	1224.7	1175.4	1197.4	1170.9
Total Delay (hr)	426.7	395.7	397.6	428.3	387.8	401.1	381.5
Total Stops	31248	29892	29522	30942	29308	30025	29502
Fuel Used (gal)	900.6	877.7	877.0	891.5	875.5	885.4	873.7

Interval #1 Information Recording

Start Time	7:00
End Time	8:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	24153	24036	24492	24173
Vehs Exited	23972	23838	24254	23975
Starting Vehs	1041	1019	1012	992
Ending Vehs	1222	1217	1250	1191
Travel Distance (mi)	25143	24978	25481	25223
Travel Time (hr)	1172.2	1166.9	1228.8	1193.5
Total Delay (hr)	382.9	383.2	428.3	401.3
Total Stops	28784	29136	31250	29955
Fuel Used (gal)	874.2	868.8	896.9	882.1

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.5	0.2	0.5	0.0	0.0	0.0	1.2	0.2	1.0	1.3	0.1	0.2
Total Del/Veh (s)	113.8	37.6	12.5	80.3	56.0	56.3	124.4	53.3	11.3	46.3	54.9	49.6

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	57.1

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.2	0.2	0.1	0.2	0.0
Total Del/Veh (s)	14.7	4.2	3.3	5.5	0.8	0.2	34.7	14.6	56.3	46.7	24.0	3.8

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.2	0.0	0.0	0.2	0.0	0.1	1.8	0.4	0.4	1.6	0.5	0.5
Total Del/Veh (s)	55.0	15.6	15.1	23.5	15.0	13.8	78.3	29.3	25.9	57.3	35.6	30.7

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	24.3

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.1	0.0	0.0	0.0	0.1	0.0	0.0	0.0	1.5	0.2	0.2	0.1
Total Del/Veh (s)	29.6	17.2	36.1	30.5	6.0	41.1	30.0	6.2	44.2	34.2	27.8	28.0

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.9	0.1
Total Del/Veh (s)	8.0	7.6	11.3	10.4	40.3	9.0	10.7

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.3	0.3	0.3	1.4	0.2	0.3
Total Del/Veh (s)	39.1	21.7	22.0	37.9	32.1	32.0	103.3	63.7	55.4	55.7	52.3	49.2

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	48.5

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.2	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	10.2	0.7	0.4	8.4	3.9	3.0	12.7	4.0	19.1	27.3	13.2	3.0

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.5	0.5	0.4	1.8	0.3	0.4
Total Del/Veh (s)	26.1	9.4	8.8	24.3	14.4	14.4	35.5	20.2	15.1	29.2	17.2	12.9

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	16.0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.6	0.2	1.5	1.6	0.2	1.5
Total Del/Veh (s)	40.7	30.2	26.9	38.6	33.9	33.5	38.6	29.0	9.0	61.9	26.8	8.5

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	30.7

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.3	0.1	0.1	0.1	0.0
Total Del/Veh (s)	6.6	1.6	1.0	19.2	6.4	3.7	7.3	98.7	46.7	24.3	6.4

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.3	0.3	0.4	0.7	0.3	0.3
Total Del/Veh (s)	23.4	8.4	7.6	19.3	9.0	7.6	53.3	20.3	14.5	32.9	27.2	21.8

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.1
Total Del/Veh (s)	15.7

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.5	0.3	1.5	1.6	0.2	1.6
Total Del/Veh (s)	42.9	30.2	25.0	33.0	32.4	30.5	42.5	30.5	11.4	48.3	24.9	8.1

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	29.7

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	2.0	2.2	4.6	0.5	12.5	14.3	7.3	14.8	2.0

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.2	0.2	0.1	0.1	0.1
Total Del/Veh (s)	4.5	1.0	0.8	6.8	1.8	1.3	12.5	15.0	6.2	13.7	15.7	6.1

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	2.6

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.1	1.4	0.2	0.2	1.3	0.2	1.2
Total Del/Veh (s)	49.3	48.7	16.1	37.0	35.3	22.6	81.8	21.6	12.0	33.8	20.5	6.6

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	26.4

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.6	4.0	3.1

40: Country Club Rd Performance by movement

Movement	EBR	NBT	SBT	All
Denied Del/Veh (s)	0.0	0.2	0.0	0.1
Total Del/Veh (s)	7.3	2.9	2.2	2.9

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	3.9	2.0	2.8

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWT	SWR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0
Total Del/Veh (s)	2.9	0.6	2.2	2.7	2.7

100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.2	0.0
Total Del/Veh (s)	0.9	2.1	2.0	5.3	5.8	1.9

110: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.2	0.2	0.0
Total Del/Veh (s)	2.5	2.6	3.3	3.5	4.1	13.9	7.0	3.4

120: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.0
Total Del/Veh (s)	2.6	3.0	1.6	1.7	10.6	4.8	4.5	2.5

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.6	2.7	2.2

140: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.2	0.1	0.0
Total Del/Veh (s)	1.6	1.8	1.6	1.6	9.7	3.6	4.6	2.0

200: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.2	0.2	0.0
Total Del/Veh (s)	1.2	0.9	2.6	2.5	30.9	31.9	19.9	4.2

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0
Total Del/Veh (s)	2.9	2.6	1.8	2.1	2.2

1050: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.1	0.0
Total Del/Veh (s)	8.1	3.4	5.9

3000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.2	0.1	0.1	0.0
Total Del/Veh (s)	5.0	4.9	0.5	0.2	19.6	8.3	21.6	10.3	3.2

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.2	0.2	0.1	0.0
Total Del/Veh (s)	1.8	1.8	6.3	7.3	20.1	9.1	23.2	13.8	5.8

5000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	1.8	2.0	2.6	5.3	9.9	2.5

6000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.1	0.2	0.0
Total Del/Veh (s)	2.7	2.9	1.5	1.3	16.2	6.2	6.7	2.5

7000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.2	0.2	0.2	0.0
Total Del/Veh (s)	1.5	1.8	2.2	18.1	9.1	16.6	10.1	3.5

8000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.2	0.1	0.0
Total Del/Veh (s)	2.5	2.7	3.8	4.1	14.7	6.1	5.3	3.6

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.2	0.0
Total Del/Veh (s)	2.8	3.0	1.7	1.4	19.4	6.2	3.0

Total Network Performance

Denied Del/Veh (s)	0.4
Total Del/Veh (s)	57.0

Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	37.6	103.7	0.6	20
	1000	4.0	24.2	0.2	24
	2	4.2	24.5	0.2	26
Wilson HAWK	20	1.5	4.0	0.0	17
6th Side Street	200	1.2	8.7	0.1	27
Tucson Blvd	3	15.2	24.1	0.1	12
	3000	5.6	48.1	0.4	27
6th/5th Street	72	3.1	6.8	0.0	16
Country Club Rd	4	17.2	30.3	0.1	14
	9922	1.5	13.6	0.1	25
	4000	1.8	30.8	0.2	29
Dodge Blvd	5	7.7	55.3	0.4	26
	5000	2.0	17.6	0.1	26
Alvernon Wy	6	21.3	35.3	0.1	13
	6000	3.1	18.1	0.1	25
	70	4.0	17.1	0.1	22
	7	0.7	2.8	0.0	24
Columbus Blvd	7000	1.5	17.1	0.1	27
	8	9.5	22.5	0.1	18
	8000	2.8	32.7	0.3	28
Swan Rd	9	30.0	58.6	0.3	15
	9000	3.7	18.7	0.1	24
	1050	8.1	21.4	0.1	18
	10	1.6	3.6	0.0	19
	100	1.1	15.9	0.1	29
Rosemont Blvd	11	8.4	22.1	0.1	19
	110	2.6	31.9	0.3	28
Craycroft Rd	12	30.3	59.0	0.2	15
	120	3.3	30.5	0.2	27
	13	2.1	28.0	0.2	28
Chantilly Dr HAWK	130	1.6	4.2	0.0	16
Sahuara Ave	14	1.0	8.1	0.1	27
	140	1.6	31.9	0.3	28
Wilmot Rd	15	50.2	78.8	0.3	11
Total		291.3	950.0	5.6	21

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	35.3	137.4	0.4	12
	140	1.4	34.0	0.3	27
Sahuara Ave	14	1.9	32.1	0.3	28
	130	2.7	10.3	0.1	21
	13	0.5	2.5	0.0	27
	120	1.7	27.7	0.2	28
Craycroft Rd	12	32.3	58.5	0.2	14
	110	4.2	34.1	0.2	26
Rosemont Blvd	11	8.9	37.9	0.3	24
	100	2.4	16.3	0.1	27
	10	6.5	21.5	0.1	21
Arcadia Ave HAWK	1050	3.5	6.0	0.0	12
	9000	1.7	14.5	0.1	27
Swan Rd	9	34.0	48.0	0.1	9
	8000	4.5	34.4	0.3	26
Columbus Blvd	8	14.7	52.6	0.3	23
	7000	2.5	15.8	0.1	26
	7	3.9	19.5	0.1	24
Irving Ave HAWK	70	2.0	4.6	0.0	15
	6000	1.5	14.1	0.1	27
Alvernon Wy	6	31.8	45.9	0.1	10
	5000	3.2	18.0	0.1	25
Dodge Blvd	5	10.6	25.9	0.1	18
	4000	6.7	54.8	0.4	27
	9922	2.4	31.8	0.2	28
Country Club Rd	4	30.5	41.4	0.1	8
6th/5th Street	72	3.5	17.4	0.1	24
	3000	0.5	4.5	0.0	24
Tucson Blvd	3	14.8	56.9	0.4	23
6th Side Street	200	2.9	12.4	0.1	23
Wilson HAWK	20	4.0	12.2	0.1	19
	2	0.8	2.7	0.0	25
	1000	1.8	22.3	0.2	28
Campbell Ave	1	56.0	74.4	0.2	8
Total		335.7	1042.3	5.5	19

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	199	387	392	200	380	626	641	390	742	710	566	340
Average Queue (ft)	148	208	187	95	258	375	388	333	436	408	285	65
95th Queue (ft)	229	381	325	199	439	579	583	449	763	720	436	187
Link Distance (ft)		2986	2986			757	757		2020	2020	2020	
Upstream Blk Time (%)						0	0					
Queuing Penalty (veh)						0	0					
Storage Bay Dist (ft)	100			50	180			290				150
Storage Blk Time (%)	47	23	50	12	31	40		39	7		36	0
Queuing Penalty (veh)	116	38	107	28	148	106		164	24		73	0

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	349	441	429	443
Average Queue (ft)	93	306	307	304
95th Queue (ft)	250	423	420	423
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	0	32		
Queuing Penalty (veh)	0	26		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	165	171	68	56	116	130
Average Queue (ft)	63	37	19	7	48	49
95th Queue (ft)	142	123	58	34	91	100
Link Distance (ft)	866	866	55	55	433	438
Upstream Blk Time (%)			1	0		
Queuing Penalty (veh)			10	1		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	155	248	268	130	313	327	247	370	253	478
Average Queue (ft)	65	125	137	39	150	161	87	201	84	248
95th Queue (ft)	129	218	233	91	261	275	183	330	200	406
Link Distance (ft)		356	356		1877	1877		1935		2179
Upstream Blk Time (%)			0							
Queuing Penalty (veh)			0							
Storage Bay Dist (ft)	100			100			300		300	
Storage Blk Time (%)	8	12		0	15		0	2		6
Queuing Penalty (veh)	25	9		2	9		0	1		5

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	B9922	B9922	NB	NB	NB
Directions Served	L	T	T	L	T	T	R	T	T	L	T	T
Maximum Queue (ft)	174	165	175	190	369	409	260	4	2	170	197	205
Average Queue (ft)	73	69	86	80	213	228	82	0	0	79	140	144
95th Queue (ft)	141	134	147	176	323	349	276	4	2	150	203	209
Link Distance (ft)		488	488		423	423		1235	1235		172	172
Upstream Blk Time (%)					0	0				0	4	5
Queuing Penalty (veh)					1	2				0	15	20
Storage Bay Dist (ft)	180			90			160			100		
Storage Blk Time (%)	0	0		4	33	24	0			6	23	32
Queuing Penalty (veh)	1	0		16	33	59	1			18	28	18

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	NB	SB	SB	SB
Directions Served	R	L	T	TR
Maximum Queue (ft)	169	174	312	351
Average Queue (ft)	26	120	180	199
95th Queue (ft)	107	197	284	305
Link Distance (ft)			2000	2000
Upstream Blk Time (%)	0			
Queuing Penalty (veh)	0			
Storage Bay Dist (ft)	70	100		
Storage Blk Time (%)	0	15	27	
Queuing Penalty (veh)	0	36	45	

Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	WB	NB	NB
Directions Served	T	TR	L	T	T	L	R
Maximum Queue (ft)	176	215	122	268	294	153	88
Average Queue (ft)	66	85	58	143	165	67	44
95th Queue (ft)	141	169	106	247	272	125	76
Link Distance (ft)	2071	2071		619	619	2122	
Upstream Blk Time (%)							
Queuing Penalty (veh)							
Storage Bay Dist (ft)	300			220			
Storage Blk Time (%)				0		0	
Queuing Penalty (veh)				0		0	

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB			
Directions Served	L	T	TR	L	T	TR	L	T	TR	L	T	TR			
Maximum Queue (ft)	210	211	259	145	346	369	250	786	770	250	619	610			
Average Queue (ft)	97	112	147	64	211	228	207	480	474	149	387	387			
95th Queue (ft)	175	188	225	119	317	337	315	862	844	299	591	588			
Link Distance (ft)		581	581		578	578		2106	2106		1977	1977			
Upstream Blk Time (%)															
Queuing Penalty (veh)															
Storage Bay Dist (ft)	300			300			150			150					
Storage Blk Time (%)							1		25		45		1		47
Queuing Penalty (veh)							1		148		98		5		55

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	54	57	175	182	38	47
Average Queue (ft)	7	7	61	47	14	15
95th Queue (ft)	34	32	133	141	39	41
Link Distance (ft)	56	56	627	627	304	320
Upstream Blk Time (%)	0	0				
Queuing Penalty (veh)	1	1				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	99	152	178	111	254	274	199	304	100	238
Average Queue (ft)	36	67	87	29	127	144	74	148	36	122
95th Queue (ft)	75	123	148	77	221	243	152	246	76	202
Link Distance (ft)		533	533		1685	1685		2072		1936
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			75			100		100	
Storage Blk Time (%)	2	5		1	15		4	19	0	13
Queuing Penalty (veh)	4	3		4	6		19	21	0	7

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	133	240	259	155	284	314	304	384	376	139	296	365
Average Queue (ft)	56	126	151	61	167	190	86	239	237	43	154	212
95th Queue (ft)	108	204	234	119	261	285	210	351	344	100	272	327
Link Distance (ft)		1226	1226		560	560		1988	1988			2060
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300			500			180			350	230	
Storage Blk Time (%)							0	19	1		5	4
Queuing Penalty (veh)							1	18	1		26	9

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	348	81
Average Queue (ft)	207	30
95th Queue (ft)	311	64
Link Distance (ft)	2060	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	1	
Queuing Penalty (veh)	1	

Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	69	66	226	199	132	108
Average Queue (ft)	31	16	107	70	62	41
95th Queue (ft)	71	54	196	170	107	88
Link Distance (ft)	52	52	616	616	261	294
Upstream Blk Time (%)	4	1				
Queuing Penalty (veh)	15	3				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	114	146	163	85	151	174	185	290	175	374
Average Queue (ft)	58	67	88	34	72	83	64	114	35	165
95th Queue (ft)	100	123	147	70	132	146	141	216	121	294
Link Distance (ft)		570	570		1260	1260		1970		2041
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			100			150		150	
Storage Blk Time (%)	7	4		0	3		2	3		13
Queuing Penalty (veh)	16	5		1	2		5	3		4

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Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	197	190	229	132	257	272	329	495	484	230	184	292
Average Queue (ft)	96	86	114	53	122	140	138	279	278	78	80	183
95th Queue (ft)	170	161	195	108	213	228	289	435	429	220	148	270
Link Distance (ft)		1212	1212		1092	1092		1951	1951			2065
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300			500			180			180	150	
Storage Blk Time (%)							2	22	22	0	1	13
Queuing Penalty (veh)							13	41	21	0	5	14

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	306	219
Average Queue (ft)	182	46
95th Queue (ft)	274	127
Link Distance (ft)	2065	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	13	
Queuing Penalty (veh)	16	

Intersection: 13: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	77	126	57	47	71	65
Average Queue (ft)	8	16	6	3	33	20
95th Queue (ft)	44	71	33	21	59	52
Link Distance (ft)	1086	1086	56	56	422	473
Upstream Blk Time (%)			0	0		
Queuing Penalty (veh)			1	0		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	39	26	85	30	86	59
Average Queue (ft)	3	1	26	2	42	26
95th Queue (ft)	20	12	68	17	71	51
Link Distance (ft)	275	275	1271	1271	614	699
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	123	160	124	150	351	572	531	308	224	327	332	299
Average Queue (ft)	54	91	57	66	251	261	231	165	83	203	199	167
95th Queue (ft)	102	143	105	123	391	505	448	272	186	293	290	271
Link Distance (ft)		1219	1219	2255		1997	1997	1997		2032	2032	2032
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	150				240				130			
Storage Blk Time (%)	0	1			26	2			1	21		0
Queuing Penalty (veh)	0	1			126	5			7	26		0

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	123
Average Queue (ft)	55
95th Queue (ft)	99
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	66	81	246	249
Average Queue (ft)	22	29	65	68
95th Queue (ft)	64	77	191	197
Link Distance (ft)	55	55	298	298
Upstream Blk Time (%)	3	5	0	0
Queuing Penalty (veh)	12	21	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 40: Country Club Rd

Movement	EB	NB	NB
Directions Served	R	T	T
Maximum Queue (ft)	79	122	125
Average Queue (ft)	39	16	18
95th Queue (ft)	63	71	77
Link Distance (ft)	451	2094	2094
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	159	202	76	86
Average Queue (ft)	33	49	31	34
95th Queue (ft)	112	149	79	87
Link Distance (ft)	513	513	56	56
Upstream Blk Time (%)			5	7
Queuing Penalty (veh)			25	32
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 72: 6th/5th Street

Movement

Directions Served
 Maximum Queue (ft)
 Average Queue (ft)
 95th Queue (ft)
 Link Distance (ft)
 Upstream Blk Time (%)
 Queuing Penalty (veh)
 Storage Bay Dist (ft)
 Storage Blk Time (%)
 Queuing Penalty (veh)

Intersection: 100: 6th/5th Street

Movement	WB	NB	SB
Directions Served	T	LTR	LTR
Maximum Queue (ft)	6	36	80
Average Queue (ft)	0	14	38
95th Queue (ft)	6	40	66
Link Distance (ft)	570	214	185
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 110: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	TR	LTR	LTR
Maximum Queue (ft)	9	6	2	56	98
Average Queue (ft)	0	0	0	29	40
95th Queue (ft)	7	6	3	51	73
Link Distance (ft)	1260	1260	1212	332	276
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

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Intersection: 120: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	T	T	TR	LTR	LTR
Maximum Queue (ft)	15	7	9	63	61
Average Queue (ft)	0	0	0	33	29
95th Queue (ft)	9	5	6	58	53
Link Distance (ft)	1092	1086	1086	211	276
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	63	71	116	137
Average Queue (ft)	15	23	30	36
95th Queue (ft)	54	66	92	108
Link Distance (ft)	56	56	275	275
Upstream Blk Time (%)	2	4		
Queuing Penalty (veh)	5	10		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 140: 6th/5th Street

Movement	EB	NB	SB
Directions Served	T	LTR	LTR
Maximum Queue (ft)	5	68	57
Average Queue (ft)	0	34	31
95th Queue (ft)	5	58	52
Link Distance (ft)	1271	305	274
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 200: 6th Side Street & 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	T	T	TR	LTR	LTR
Maximum Queue (ft)	5	9	18	98	155
Average Queue (ft)	0	0	1	38	67
95th Queue (ft)	5	7	10	74	125
Link Distance (ft)	298	356	356	419	408
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 1000: 6th/5th Street

Movement	EB	WB	WB
Directions Served	T	T	TR
Maximum Queue (ft)	6	3	5
Average Queue (ft)	0	0	0
95th Queue (ft)	5	3	5
Link Distance (ft)	757	866	866
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1050: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	205	277	70	82
Average Queue (ft)	66	95	34	42
95th Queue (ft)	172	236	75	88
Link Distance (ft)	526	526	52	52
Upstream Blk Time (%)			8	12
Queuing Penalty (veh)			27	44
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 3000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	T	LTR	LTR
Maximum Queue (ft)	25	52	19	102	84
Average Queue (ft)	1	3	1	45	35
95th Queue (ft)	15	23	9	81	65
Link Distance (ft)	1877	1877	100	405	430
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 4000: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	T	TR	T	TR	LTR	LTR
Maximum Queue (ft)	17	2	6	10	129	118
Average Queue (ft)	1	0	0	0	56	54
95th Queue (ft)	10	2	6	7	99	99
Link Distance (ft)	1235	1235	2071	2071	547	583
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 5000: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	3	4	58	70
Average Queue (ft)	0	0	24	25
95th Queue (ft)	3	4	49	54
Link Distance (ft)	619	619	310	280
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 6000: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	T	TR	T	TR	LTR	LTR
Maximum Queue (ft)	12	1	8	8	92	80
Average Queue (ft)	0	0	0	0	38	34
95th Queue (ft)	9	1	6	5	69	65
Link Distance (ft)	578	578	513	513	355	360
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 7000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	TR	LTR	LTR
Maximum Queue (ft)	5	11	2	140	104
Average Queue (ft)	0	0	0	58	51
95th Queue (ft)	5	8	2	104	88
Link Distance (ft)	627	627	533	260	304
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 8000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	TR	LTR	LTR
Maximum Queue (ft)	8	5	4	82	61
Average Queue (ft)	0	0	0	40	28
95th Queue (ft)	6	5	4	67	53
Link Distance (ft)	1287	1287	1226	319	333
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 9000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	4	6	81	82
Average Queue (ft)	0	0	34	40
95th Queue (ft)	4	4	67	66
Link Distance (ft)	560	526	204	310
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 2123

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	4:45	4:45	4:45	4:45	4:45	4:45	4:45
End Time	6:00	6:00	6:00	6:00	6:00	6:00	6:00
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	24058	23819	23886	24130	24199	23789	23813
Vehs Exited	23893	23642	23696	23861	23891	23643	23651
Starting Vehs	1146	1077	1155	1143	1075	1138	1158
Ending Vehs	1311	1254	1345	1412	1383	1284	1320
Denied Entry After	2	0	2	1	3	3	2
Travel Distance (mi)	26389	26066	26226	26324	26544	26049	26314
Travel Time (hr)	1300.5	1225.1	1239.7	1255.3	1286.5	1249.0	1245.5
Total Delay (hr)	473.9	408.5	416.7	431.1	453.6	433.5	421.7
Total Stops	32277	29879	30472	30747	31527	30277	30413
Fuel Used (gal)	935.1	910.6	918.1	923.2	935.6	913.8	924.0

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	4:45	4:45	4:45	4:45
End Time	6:00	6:00	6:00	6:00
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	24103	23962	24073	23985
Vehs Exited	23945	23706	23869	23777
Starting Vehs	1123	1169	1185	1117
Ending Vehs	1281	1425	1389	1322
Denied Entry After	1	4	2	0
Travel Distance (mi)	26386	26213	26443	26295
Travel Time (hr)	1239.1	1300.9	1283.7	1262.5
Total Delay (hr)	411.7	479.2	455.1	438.5
Total Stops	30360	32315	32183	31048
Fuel Used (gal)	920.8	929.6	931.9	924.2

Interval #0 Information Seeding

Start Time	4:45
End Time	5:00
Total Time (min)	15

No data recorded this interval.

Interval #1 Information Recording

Start Time	5:00
End Time	6:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	24058	23819	23886	24130	24199	23789	23813
Vehs Exited	23893	23642	23696	23861	23891	23643	23651
Starting Vehs	1146	1077	1155	1143	1075	1138	1158
Ending Vehs	1311	1254	1345	1412	1383	1284	1320
Denied Entry After	2	0	2	1	3	3	2
Travel Distance (mi)	26389	26066	26226	26324	26544	26049	26314
Travel Time (hr)	1300.5	1225.1	1239.7	1255.3	1286.5	1249.0	1245.5
Total Delay (hr)	473.9	408.5	416.7	431.1	453.6	433.5	421.7
Total Stops	32277	29879	30472	30747	31527	30277	30413
Fuel Used (gal)	935.1	910.6	918.1	923.2	935.6	913.8	924.0

Interval #1 Information Recording

Start Time	5:00
End Time	6:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	24103	23962	24073	23985
Vehs Exited	23945	23706	23869	23777
Starting Vehs	1123	1169	1185	1117
Ending Vehs	1281	1425	1389	1322
Denied Entry After	1	4	2	0
Travel Distance (mi)	26386	26213	26443	26295
Travel Time (hr)	1239.1	1300.9	1283.7	1262.5
Total Delay (hr)	411.7	479.2	455.1	438.5
Total Stops	30360	32315	32183	31048
Fuel Used (gal)	920.8	929.6	931.9	924.2

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Entry After	0	0	0	0	0	0	0

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

7: 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

10: 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

40: Country Club Rd Performance by movement

Movement	EBR	NBT	SBT	All
Denied Entry After	0	0	0	0

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWR	All
Denied Entry After	0	0	0	0

100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	All
Denied Entry After	0	0	0	0	0

110: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0

120: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

140: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0

200: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0

1050: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

3000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0	0

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	All
Denied Entry After	0	0	0	0	0	0	0	0

5000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0

6000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0

7000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0

8000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBR	All
Denied Entry After	0	0	0	0	0

Total Network Performance

Denied Entry After	0
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Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	86.5	152.8	0.6	14
	1000	5.7	26.0	0.2	23
	2	4.9	25.6	0.2	25
Wilson HAWK	20	1.4	3.9	0.0	17
6th Side Street	200	1.7	9.3	0.1	25
Tucson Blvd	3	16.2	25.6	0.1	11
	3000	7.4	51.3	0.4	26
6th/5th Street	72	2.0	7.0	0.0	16
Country Club Rd	4	23.7	39.5	0.1	10
	9922	2.3	14.3	0.1	24
	4000	3.0	31.9	0.2	28
Dodge Blvd	5	11.0	57.2	0.4	25
	5000	2.6	17.7	0.1	26
Alvernon Wy	6	36.1	50.4	0.1	9
	6000	4.3	19.6	0.1	23
	70	4.4	17.5	0.1	22
	7	0.6	2.6	0.0	26
	7000	1.9	17.5	0.1	27
Columbus Blvd	8	12.4	25.6	0.1	16
	8000	4.2	34.6	0.3	27
Swan Rd	9	34.3	63.2	0.3	14
	9000	4.2	19.4	0.1	23
	1050	6.6	19.9	0.1	20
	10	1.0	3.1	0.0	22
	100	1.4	16.4	0.1	28
Rosemont Blvd	11	11.7	25.8	0.1	17
	110	3.8	33.5	0.3	27
Craycroft Rd	12	31.3	59.8	0.2	15
	120	3.4	30.5	0.2	27
	13	2.9	28.1	0.2	28
Chantilly Dr HAWK	130	1.5	4.0	0.0	17
Sahuara Ave	14	0.9	7.9	0.1	27
	140	1.7	31.5	0.3	29
Wilmot Rd	15	32.2	56.0	0.3	16
Total		369.3	1029.4	5.6	20

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	28.6	128.1	0.4	12
	140	0.8	28.7	0.3	31
Sahuara Ave	14	1.4	31.1	0.3	29
	130	2.4	9.9	0.1	22
	13	0.5	2.5	0.0	28
	120	1.2	26.6	0.2	29
Craycroft Rd	12	24.6	50.3	0.2	16
	110	3.3	33.0	0.2	27
Rosemont Blvd	11	8.6	38.1	0.3	24
	100	2.1	16.5	0.1	26
	10	3.1	18.1	0.1	25
Arcadia Ave HAWK	1050	2.4	4.9	0.0	14
	9000	1.0	13.8	0.1	28
Swan Rd	9	30.6	44.7	0.1	10
	8000	4.1	34.0	0.3	27
Columbus Blvd	8	13.6	52.3	0.3	23
	7000	2.6	16.2	0.1	25
	7	2.3	17.8	0.1	26
Irving Ave HAWK	70	1.7	4.3	0.0	16
	6000	1.5	14.1	0.1	27
Alvernon Wy	6	29.5	43.8	0.1	10
	5000	3.5	18.6	0.1	24
Dodge Blvd	5	11.8	26.6	0.1	17
	4000	6.1	52.2	0.4	28
	9922	1.9	31.3	0.2	28
Country Club Rd	4	31.0	41.7	0.1	8
6th/5th Street	72	5.5	21.6	0.1	19
	3000	0.4	10.0	0.0	11
Tucson Blvd	3	10.8	54.4	0.4	24
6th Side Street	200	1.5	11.1	0.1	26
Wilson HAWK	20	4.4	12.5	0.1	19
	2	1.0	3.0	0.0	23
	1000	1.5	22.4	0.2	28
Campbell Ave	1	40.3	58.7	0.2	10
Total		285.6	992.7	5.5	20

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	200	960	975	200	370	450	459	300	312	308	267	141
Average Queue (ft)	180	600	619	170	244	251	237	154	203	202	171	50
95th Queue (ft)	247	1148	1162	259	414	474	398	270	279	274	240	103
Link Distance (ft)		2986	2986			757	757		2020	2020	2020	
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	100			50	180			290				150
Storage Blk Time (%)	39	53	62	32	41	12		2	0		9	0
Queuing Penalty (veh)	187	140	175	150	118	26		7	0		13	1

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	350	573	558	528
Average Queue (ft)	242	396	394	377
95th Queue (ft)	417	571	554	538
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	9	40		
Queuing Penalty (veh)	44	86		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	246	278	81	43	90	90
Average Queue (ft)	56	61	38	2	36	36
95th Queue (ft)	182	203	81	19	72	71
Link Distance (ft)	866	866	55	55	433	438
Upstream Blk Time (%)			9	0		
Queuing Penalty (veh)			41	0		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	253	352	354	115	204	214	189	397	267	432
Average Queue (ft)	77	182	198	46	88	99	39	202	84	227
95th Queue (ft)	173	341	353	95	179	188	113	328	180	365
Link Distance (ft)		356	356		1877	1877		1935		2179
Upstream Blk Time (%)		0	0							
Queuing Penalty (veh)		1	2							
Storage Bay Dist (ft)	100			100			300		300	
Storage Blk Time (%)	5	17		2	6			2		3
Queuing Penalty (veh)	26	19		8	4			1		3

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	NB	NB	NB	NB	SB
Directions Served	L	T	T	L	T	T	R	L	T	T	R	L
Maximum Queue (ft)	220	288	301	184	260	261	233	170	213	210	170	175
Average Queue (ft)	85	160	171	67	151	159	32	96	158	161	52	151
95th Queue (ft)	171	262	266	138	229	233	147	170	212	217	160	208
Link Distance (ft)		488	488		423	423			172	172		
Upstream Blk Time (%)								1	10	10	0	
Queuing Penalty (veh)								0	46	48	0	
Storage Bay Dist (ft)	180			90			160	100			70	100
Storage Blk Time (%)	1	6		6	31	13	1	11	28	37	1	36
Queuing Penalty (veh)	3	12		17	26	34	3	37	44	28	2	102

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	TR
Maximum Queue (ft)	383	361
Average Queue (ft)	223	212
95th Queue (ft)	352	323
Link Distance (ft)	2000	2000
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)	28	
Queuing Penalty (veh)	68	

Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	WB	NB	NB
Directions Served	T	TR	L	T	T	L	R
Maximum Queue (ft)	246	280	175	242	258	256	153
Average Queue (ft)	97	124	83	138	162	127	66
95th Queue (ft)	196	229	148	221	242	214	117
Link Distance (ft)	2071	2071		619	619	2122	
Upstream Blk Time (%)							
Queuing Penalty (veh)							
Storage Bay Dist (ft)			300				220
Storage Blk Time (%)				0		1	
Queuing Penalty (veh)				0		2	

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	TR	L	T	TR
Maximum Queue (ft)	258	395	429	168	299	333	250	830	826	250	685	680
Average Queue (ft)	121	224	255	79	179	197	199	537	534	140	419	420
95th Queue (ft)	220	358	393	146	272	293	316	964	950	299	662	657
Link Distance (ft)		581	581		578	578		2106	2106		1977	1977
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300			300			150			150		
Storage Blk Time (%)	0	2			0		16	53		0	50	
Queuing Penalty (veh)	1	4			0		94	90		2	51	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	58	20	124	137	64	37
Average Queue (ft)	7	1	22	25	24	11
95th Queue (ft)	34	10	82	92	53	34
Link Distance (ft)	56	56	627	627	304	320
Upstream Blk Time (%)	0	0				
Queuing Penalty (veh)	3	0				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	158	260	284	154	271	284	161	364	187	568
Average Queue (ft)	30	132	155	43	119	138	49	158	60	237
95th Queue (ft)	88	225	248	105	218	240	118	297	159	488
Link Distance (ft)		533	533		1685	1685		2072		1936
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			75			100		100	
Storage Blk Time (%)	1	16		4	13		4	22	1	36
Queuing Penalty (veh)	4	6		14	6		15	11	5	19

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	143	306	330	151	250	275	301	370	375	85	336	414
Average Queue (ft)	63	195	220	60	132	158	87	242	244	33	122	249
95th Queue (ft)	121	286	312	119	223	247	206	346	349	65	227	363
Link Distance (ft)		1226	1226		560	560		1988	1988			2060
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300			500			180			350	230	
Storage Blk Time (%)		0					0	20	1		0	12
Queuing Penalty (veh)		1					1	18	1		2	19

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	420	175
Average Queue (ft)	249	33
95th Queue (ft)	368	110
Link Distance (ft)	2060	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	3	
Queuing Penalty (veh)	3	

Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	70	54	150	151	109	94
Average Queue (ft)	23	5	43	28	43	36
95th Queue (ft)	63	31	108	98	83	70
Link Distance (ft)	52	52	616	616	261	294
Upstream Blk Time (%)	2	0				
Queuing Penalty (veh)	8	1				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	165	245	263	77	139	155	139	208	92	220
Average Queue (ft)	51	120	140	27	59	74	54	106	29	108
95th Queue (ft)	117	211	230	63	109	126	106	178	75	184
Link Distance (ft)		570	570		1260	1260		1970		2041
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			100			150		150	
Storage Blk Time (%)	2	13		0	1		0	2		2
Queuing Penalty (veh)	8	9		1	0		0	2		1

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	170	293	317	174	188	213	315	471	469	230	276	384
Average Queue (ft)	76	148	182	67	96	116	100	271	272	87	72	246
95th Queue (ft)	144	243	274	133	163	184	237	401	402	240	173	349
Link Distance (ft)		1212	1212		1092	1092		1951	1951			2065
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300			500			180			180	150	
Storage Blk Time (%)		0					0	23	23		0	25
Queuing Penalty (veh)		0					1	26	19		1	20

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	398	300
Average Queue (ft)	245	58
95th Queue (ft)	354	202
Link Distance (ft)	2065	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	24	
Queuing Penalty (veh)	21	

Intersection: 13: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	137	152	60	3	103	64
Average Queue (ft)	15	20	13	0	44	25
95th Queue (ft)	73	87	45	3	82	55
Link Distance (ft)	1086	1086	56	56	422	473
Upstream Blk Time (%)	1					
Queuing Penalty (veh)	2					
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	62	11	67	2	48	64
Average Queue (ft)	12	0	12	0	19	26
95th Queue (ft)	45	6	43	2	45	54
Link Distance (ft)	275	275	1271	1271	614	699
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	202	224	169	110	267	316	321	277	212	352	344	320
Average Queue (ft)	112	136	93	40	138	194	194	162	36	216	214	179
95th Queue (ft)	174	201	156	86	234	284	286	264	116	313	312	289
Link Distance (ft)		1219	1219	2255		1997	1997	1997		2032	2032	2032
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	150				240				130			
Storage Blk Time (%)	3	10			1	2				24		0
Queuing Penalty (veh)	10	16			6	5				10		0

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	86
Average Queue (ft)	36
95th Queue (ft)	68
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	74	84	208	215
Average Queue (ft)	24	28	60	50
95th Queue (ft)	69	77	157	153
Link Distance (ft)	55	55	298	298
Upstream Blk Time (%)	4	6	0	0
Queuing Penalty (veh)	29	39	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 40: Country Club Rd

Movement	EB	NB	NB
Directions Served	R	T	T
Maximum Queue (ft)	88	208	191
Average Queue (ft)	48	44	42
95th Queue (ft)	78	139	133
Link Distance (ft)	451	2094	2094
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	215	251	74	85
Average Queue (ft)	51	64	27	32
95th Queue (ft)	166	194	74	83
Link Distance (ft)	513	513	56	56
Upstream Blk Time (%)			4	5
Queuing Penalty (veh)			18	22
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 72: 6th/5th Street

Movement	SW	SW
Directions Served	R	R
Maximum Queue (ft)	17	31
Average Queue (ft)	1	1
95th Queue (ft)	11	14
Link Distance (ft)	488	488
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 100: 6th/5th Street

Movement	WB	NB
Directions Served	T	LTR
Maximum Queue (ft)	8	63
Average Queue (ft)	0	32
95th Queue (ft)	5	54
Link Distance (ft)	570	214
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 110: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	10	8	63	56
Average Queue (ft)	0	0	28	23
95th Queue (ft)	6	6	53	50
Link Distance (ft)	1260	1260	332	276
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report
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Intersection: 120: 6th/5th Street

Movement	WB	NB	SB
Directions Served	T	LTR	LTR
Maximum Queue (ft)	5	102	68
Average Queue (ft)	0	49	35
95th Queue (ft)	5	85	57
Link Distance (ft)	1086	211	276
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	72	72	95	119
Average Queue (ft)	23	25	20	26
95th Queue (ft)	67	71	68	83
Link Distance (ft)	56	56	275	275
Upstream Blk Time (%)	4	4		
Queuing Penalty (veh)	14	17		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 140: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	27	70
Average Queue (ft)	2	33
95th Queue (ft)	14	59
Link Distance (ft)	305	274
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 200: 6th Side Street & 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	TR	LTR	LTR
Maximum Queue (ft)	22	45	3	94	112
Average Queue (ft)	1	3	0	37	42
95th Queue (ft)	13	24	3	76	86
Link Distance (ft)	298	298	356	419	408
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 1000: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	14	10	68	54
Average Queue (ft)	0	0	21	26
95th Queue (ft)	8	7	51	50
Link Distance (ft)	757	757	442	411
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1050: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	276	282	69	82
Average Queue (ft)	75	82	26	31
95th Queue (ft)	215	236	68	78
Link Distance (ft)	526	526	52	52
Upstream Blk Time (%)			5	8
Queuing Penalty (veh)			18	27
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 3000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	TR	LTR	LTR
Maximum Queue (ft)	8	39	2	112	54
Average Queue (ft)	0	3	0	47	21
95th Queue (ft)	8	21	2	86	48
Link Distance (ft)	1877	1877	100	405	430
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 4000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	TR	LTR	LTR
Maximum Queue (ft)	6	6	18	77	71
Average Queue (ft)	0	0	1	34	24
95th Queue (ft)	5	6	9	63	59
Link Distance (ft)	1235	1235	2071	547	583
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 5000: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	T	TR	T	TR	LTR	LTR
Maximum Queue (ft)	9	11	3	3	174	72
Average Queue (ft)	0	0	0	0	65	30
95th Queue (ft)	6	8	3	3	126	55
Link Distance (ft)	619	619	581	581	310	280
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 6000: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	T	T	TR	LTR	LTR
Maximum Queue (ft)	10	6	8	187	57
Average Queue (ft)	0	0	0	91	22
95th Queue (ft)	7	4	5	161	47
Link Distance (ft)	578	513	513	355	360
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 7000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	T	LTR	LTR
Maximum Queue (ft)	23	2	3	82	108
Average Queue (ft)	1	0	0	39	42
95th Queue (ft)	12	2	3	65	78
Link Distance (ft)	627	627	533	260	304
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 8000: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	5	22	60	96
Average Queue (ft)	0	1	22	46
95th Queue (ft)	5	11	51	78
Link Distance (ft)	1287	1287	319	333
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 9000: 6th/5th Street

Movement	EB	EB	WB	SB
Directions Served	T	TR	TR	LTR
Maximum Queue (ft)	6	4	6	64
Average Queue (ft)	0	0	0	33
95th Queue (ft)	6	4	5	55
Link Distance (ft)	560	560	526	310
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 2240

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	4:45	4:45	4:45	4:45	4:45	4:45	4:45
End Time	6:00	6:00	6:00	6:00	6:00	6:00	6:00
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	24058	23819	23886	24130	24199	23789	23813
Vehs Exited	23893	23642	23696	23861	23891	23643	23651
Starting Vehs	1146	1077	1155	1143	1075	1138	1158
Ending Vehs	1311	1254	1345	1412	1383	1284	1320
Travel Distance (mi)	26389	26066	26226	26324	26544	26049	26314
Travel Time (hr)	1300.5	1225.1	1239.7	1255.3	1286.5	1249.0	1245.5
Total Delay (hr)	473.9	408.5	416.7	431.1	453.6	433.5	421.7
Total Stops	32277	29879	30472	30747	31527	30277	30413
Fuel Used (gal)	935.1	910.6	918.1	923.2	935.6	913.8	924.0

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	4:45	4:45	4:45	4:45
End Time	6:00	6:00	6:00	6:00
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	24103	23962	24073	23985
Vehs Exited	23945	23706	23869	23777
Starting Vehs	1123	1169	1185	1117
Ending Vehs	1281	1425	1389	1322
Travel Distance (mi)	26386	26213	26443	26295
Travel Time (hr)	1239.1	1300.9	1283.7	1262.5
Total Delay (hr)	411.7	479.2	455.1	438.5
Total Stops	30360	32315	32183	31048
Fuel Used (gal)	920.8	929.6	931.9	924.2

Interval #0 Information Seeding

Start Time	4:45
End Time	5:00
Total Time (min)	15
No data recorded this interval.	

Interval #1 Information Recording

Start Time	5:00
End Time	6:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	24058	23819	23886	24130	24199	23789	23813
Vehs Exited	23893	23642	23696	23861	23891	23643	23651
Starting Vehs	1146	1077	1155	1143	1075	1138	1158
Ending Vehs	1311	1254	1345	1412	1383	1284	1320
Travel Distance (mi)	26389	26066	26226	26324	26544	26049	26314
Travel Time (hr)	1300.5	1225.1	1239.7	1255.3	1286.5	1249.0	1245.5
Total Delay (hr)	473.9	408.5	416.7	431.1	453.6	433.5	421.7
Total Stops	32277	29879	30472	30747	31527	30277	30413
Fuel Used (gal)	935.1	910.6	918.1	923.2	935.6	913.8	924.0

Interval #1 Information Recording

Start Time	5:00
End Time	6:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	24103	23962	24073	23985
Vehs Exited	23945	23706	23869	23777
Starting Vehs	1123	1169	1185	1117
Ending Vehs	1281	1425	1389	1322
Travel Distance (mi)	26386	26213	26443	26295
Travel Time (hr)	1239.1	1300.9	1283.7	1262.5
Total Delay (hr)	411.7	479.2	455.1	438.5
Total Stops	30360	32315	32183	31048
Fuel Used (gal)	920.8	929.6	931.9	924.2

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.6	0.2	0.4	0.0	0.0	0.0	1.3	0.1	1.3	1.2	0.2	0.2
Total Del/Veh (s)	99.2	86.5	67.4	122.9	39.7	37.0	79.9	36.1	16.7	69.6	56.6	51.9

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	61.4

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.2	0.1	0.2	0.0
Total Del/Veh (s)	9.6	4.9	4.4	11.2	1.0	0.2	34.0	19.2	35.7	34.0	12.1	4.4

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.0	0.0	0.0	0.0	0.0	1.9	0.4	0.4	1.6	0.4	0.3
Total Del/Veh (s)	31.3	16.3	15.6	35.3	11.0	9.8	52.7	33.4	28.6	66.0	35.7	29.3

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	22.5

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	1.6	0.2	0.2	0.1
Total Del/Veh (s)	29.3	23.7	47.1	31.0	5.2	41.0	30.5	10.5	56.7	36.6	28.6	30.2

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.4	0.9	0.1
Total Del/Veh (s)	11.4	12.2	17.1	11.1	43.0	12.7	14.1

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.3	0.2	0.2	1.3	0.2	0.2
Total Del/Veh (s)	42.5	36.4	38.5	45.9	29.3	27.5	105.6	78.6	71.9	60.0	60.3	57.2

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	55.3

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.1
Total Del/Veh (s)	6.3	0.6	0.2	10.3	2.2	2.1	21.5	16.6	8.9	14.5	24.3	7.8

7: 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	1.6

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.0	0.0	0.1	0.0	0.0	1.6	0.3	0.3	1.8	0.4	0.3
Total Del/Veh (s)	25.2	12.4	12.2	37.8	13.3	12.4	72.5	26.1	20.4	52.9	39.0	33.9

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	21.0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.6	0.2	1.5	1.6	0.2	1.6
Total Del/Veh (s)	41.3	34.3	33.9	41.5	30.9	28.1	43.5	30.4	9.5	51.4	31.2	8.3

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	32.1

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.2	0.1	0.1	0.1
Total Del/Veh (s)	3.9	1.0	0.5	10.2	3.1	2.4	40.8	27.0	10.1	22.9	29.0	12.3

10: 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	3.2

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.1	0.0	0.0	0.1	0.0	0.0	1.3	0.3	0.4	0.6	0.3	0.3
Total Del/Veh (s)	22.1	11.7	11.3	26.9	8.7	8.3	29.8	16.4	11.7	26.8	17.9	13.3

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.1
Total Del/Veh (s)	13.6

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.5	0.2	1.5	1.4	0.2	1.5
Total Del/Veh (s)	35.6	31.3	30.9	37.9	25.6	21.6	45.8	30.9	12.2	45.5	28.1	8.2

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	29.5

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.2	0.2	0.1	0.1	0.0
Total Del/Veh (s)	2.9	2.7	5.9	0.5	0.1	13.8	9.0	14.9	16.2	3.1

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	3.7	0.9	0.5	5.8	1.3	1.0	12.0	5.1	11.4	4.2	1.6

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.1	1.3	0.1	0.2	1.3	0.1	1.2
Total Del/Veh (s)	49.2	31.3	20.0	29.1	28.6	14.7	48.0	20.5	12.1	34.8	21.7	4.8

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	24.1

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.4	4.4	2.7

40: Country Club Rd Performance by movement

Movement	EBR	NBT	SBT	All
Denied Del/Veh (s)	0.0	0.2	0.0	0.1
Total Del/Veh (s)	6.7	5.1	2.3	4.2

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	4.3	1.7	3.1

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0
Total Del/Veh (s)	1.9	0.6	4.3	2.8

100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	1.3	1.9	1.8	6.4	1.7

110: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	3.5	3.7	2.6	5.3	5.0	3.3

120: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.1	0.2	0.0
Total Del/Veh (s)	2.8	3.1	1.2	0.8	12.9	8.5	4.7	3.2

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.5	2.4	1.9

140: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.1	0.0
Total Del/Veh (s)	1.6	1.6	1.3	3.3	10.7	4.9	1.8

200: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.2	0.0
Total Del/Veh (s)	1.7	1.4	1.4	31.0	33.8	16.8	3.2

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.0
Total Del/Veh (s)	4.5	4.9	1.5	1.6	30.4	10.2	6.6	3.7

1050: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	6.6	2.4	4.8

3000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.1	0.0
Total Del/Veh (s)	6.7	6.7	0.4	0.3	13.4	4.8	4.6

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	All
Denied Del/Veh (s)	0.0	0.1	0.0	0.1	0.2	0.1	0.1	0.0
Total Del/Veh (s)	3.0	3.8	5.5	6.3	25.4	10.2	29.9	4.7

5000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.1	0.0	0.0	0.2	0.2	0.1	0.0
Total Del/Veh (s)	2.4	2.7	2.7	2.7	29.5	18.1	8.7	3.9

6000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.3	0.2	0.1	0.0
Total Del/Veh (s)	3.8	4.1	1.5	1.4	31.5	18.9	6.4	4.9

7000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.2	0.1	0.0
Total Del/Veh (s)	1.9	2.1	2.4	2.4	7.3	17.6	7.2	2.8

8000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.2	0.0
Total Del/Veh (s)	3.9	4.7	3.5	3.7	16.1	5.8	6.5	4.1

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	3.4	3.6	1.0	5.1	2.6

Total Network Performance

Denied Del/Veh (s)	0.4
Total Del/Veh (s)	62.6

Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	86.5	152.8	0.6	14
	1000	5.7	26.0	0.2	23
	2	4.9	25.6	0.2	25
Wilson HAWK	20	1.4	3.9	0.0	17
6th Side Street	200	1.7	9.3	0.1	25
Tucson Blvd	3	16.2	25.6	0.1	11
	3000	7.4	51.3	0.4	26
6th/5th Street	72	2.0	7.0	0.0	16
Country Club Rd	4	23.7	39.5	0.1	10
	9922	2.3	14.3	0.1	24
	4000	3.0	31.9	0.2	28
Dodge Blvd	5	11.0	57.2	0.4	25
	5000	2.6	17.7	0.1	26
Alvernon Wy	6	36.1	50.4	0.1	9
	6000	4.3	19.6	0.1	23
	70	4.4	17.5	0.1	22
	7	0.6	2.6	0.0	26
Columbus Blvd	7000	1.9	17.5	0.1	27
	8	12.4	25.6	0.1	16
	8000	4.2	34.6	0.3	27
Swan Rd	9	34.3	63.2	0.3	14
	9000	4.2	19.4	0.1	23
	1050	6.6	19.9	0.1	20
	10	1.0	3.1	0.0	22
Rosemont Blvd	100	1.4	16.4	0.1	28
	11	11.7	25.8	0.1	17
Craycroft Rd	110	3.8	33.5	0.3	27
	12	31.3	59.8	0.2	15
Chantilly Dr HAWK	120	3.4	30.5	0.2	27
	13	2.9	28.1	0.2	28
Sahuara Ave	130	1.5	4.0	0.0	17
Wilmot Rd	14	0.9	7.9	0.1	27
	140	1.7	31.5	0.3	29
Total	15	32.2	56.0	0.3	16
		369.3	1029.4	5.6	20

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	28.6	128.1	0.4	12
	140	0.8	28.7	0.3	31
Sahuara Ave	14	1.4	31.1	0.3	29
	130	2.4	9.9	0.1	22
	13	0.5	2.5	0.0	28
	120	1.2	26.6	0.2	29
Craycroft Rd	12	24.6	50.3	0.2	16
	110	3.3	33.0	0.2	27
Rosemont Blvd	11	8.6	38.1	0.3	24
	100	2.1	16.5	0.1	26
	10	3.1	18.1	0.1	25
Arcadia Ave HAWK	1050	2.4	4.9	0.0	14
	9000	1.0	13.8	0.1	28
Swan Rd	9	30.6	44.7	0.1	10
	8000	4.1	34.0	0.3	27
Columbus Blvd	8	13.6	52.3	0.3	23
	7000	2.6	16.2	0.1	25
	7	2.3	17.8	0.1	26
Irving Ave HAWK	70	1.7	4.3	0.0	16
	6000	1.5	14.1	0.1	27
Alvernon Wy	6	29.5	43.8	0.1	10
	5000	3.5	18.6	0.1	24
Dodge Blvd	5	11.8	26.6	0.1	17
	4000	6.1	52.2	0.4	28
	9922	1.9	31.3	0.2	28
Country Club Rd	4	31.0	41.7	0.1	8
6th/5th Street	72	5.5	21.6	0.1	19
	3000	0.4	10.0	0.0	11
Tucson Blvd	3	10.8	54.4	0.4	24
6th Side Street	200	1.5	11.1	0.1	26
Wilson HAWK	20	4.4	12.5	0.1	19
	2	1.0	3.0	0.0	23
	1000	1.5	22.4	0.2	28
Campbell Ave	1	40.3	58.7	0.2	10
Total		285.6	992.7	5.5	20

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	200	960	975	200	370	450	459	300	312	308	267	141
Average Queue (ft)	180	600	619	170	244	251	237	154	203	202	171	50
95th Queue (ft)	247	1148	1162	259	414	474	398	270	279	274	240	103
Link Distance (ft)		2986	2986			757	757		2020	2020	2020	
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	100			50	180			290				150
Storage Blk Time (%)	39	53	62	32	41	12		2	0		9	0
Queuing Penalty (veh)	187	140	175	150	118	26		7	0		13	1

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	350	573	558	528
Average Queue (ft)	242	396	394	377
95th Queue (ft)	417	571	554	538
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	9	40		
Queuing Penalty (veh)	44	86		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	246	278	81	43	90	90
Average Queue (ft)	56	61	38	2	36	36
95th Queue (ft)	182	203	81	19	72	71
Link Distance (ft)	866	866	55	55	433	438
Upstream Blk Time (%)			9	0		
Queuing Penalty (veh)			41	0		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

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Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	253	352	354	115	204	214	189	397	267	432
Average Queue (ft)	77	182	198	46	88	99	39	202	84	227
95th Queue (ft)	173	341	353	95	179	188	113	328	180	365
Link Distance (ft)		356	356		1877	1877		1935		2179
Upstream Blk Time (%)		0	0							
Queuing Penalty (veh)		1	2							
Storage Bay Dist (ft)	100			100			300		300	
Storage Blk Time (%)	5	17		2	6			2		3
Queuing Penalty (veh)	26	19		8	4			1		3

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	NB	NB	NB	NB	SB
Directions Served	L	T	T	L	T	T	R	L	T	T	R	L
Maximum Queue (ft)	220	288	301	184	260	261	233	170	213	210	170	175
Average Queue (ft)	85	160	171	67	151	159	32	96	158	161	52	151
95th Queue (ft)	171	262	266	138	229	233	147	170	212	217	160	208
Link Distance (ft)		488	488		423	423			172	172		
Upstream Blk Time (%)								1	10	10	0	
Queuing Penalty (veh)								0	46	48	0	
Storage Bay Dist (ft)	180			90			160	100			70	100
Storage Blk Time (%)	1	6		6	31	13	1	11	28	37	1	36
Queuing Penalty (veh)	3	12		17	26	34	3	37	44	28	2	102

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	TR
Maximum Queue (ft)	383	361
Average Queue (ft)	223	212
95th Queue (ft)	352	323
Link Distance (ft)	2000	2000
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)	28	
Queuing Penalty (veh)	68	

Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	WB	NB	NB
Directions Served	T	TR	L	T	T	L	R
Maximum Queue (ft)	246	280	175	242	258	256	153
Average Queue (ft)	97	124	83	138	162	127	66
95th Queue (ft)	196	229	148	221	242	214	117
Link Distance (ft)	2071	2071		619	619	2122	
Upstream Blk Time (%)							
Queuing Penalty (veh)							
Storage Bay Dist (ft)			300				220
Storage Blk Time (%)				0		1	
Queuing Penalty (veh)				0		2	

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	TR	L	T	TR
Maximum Queue (ft)	258	395	429	168	299	333	250	830	826	250	685	680
Average Queue (ft)	121	224	255	79	179	197	199	537	534	140	419	420
95th Queue (ft)	220	358	393	146	272	293	316	964	950	299	662	657
Link Distance (ft)		581	581		578	578		2106	2106		1977	1977
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300			300			150			150		
Storage Blk Time (%)	0	2			0		16	53		0	50	
Queuing Penalty (veh)	1	4			0		94	90		2	51	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	58	20	124	137	64	37
Average Queue (ft)	7	1	22	25	24	11
95th Queue (ft)	34	10	82	92	53	34
Link Distance (ft)	56	56	627	627	304	320
Upstream Blk Time (%)	0	0				
Queuing Penalty (veh)	3	0				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	158	260	284	154	271	284	161	364	187	568
Average Queue (ft)	30	132	155	43	119	138	49	158	60	237
95th Queue (ft)	88	225	248	105	218	240	118	297	159	488
Link Distance (ft)		533	533		1685	1685		2072		1936
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			75			100		100	
Storage Blk Time (%)	1	16		4	13		4	22	1	36
Queuing Penalty (veh)	4	6		14	6		15	11	5	19

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	143	306	330	151	250	275	301	370	375	85	336	414
Average Queue (ft)	63	195	220	60	132	158	87	242	244	33	122	249
95th Queue (ft)	121	286	312	119	223	247	206	346	349	65	227	363
Link Distance (ft)		1226	1226		560	560		1988	1988			2060
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300			500			180		350	230		
Storage Blk Time (%)		0					0	20	1		0	12
Queuing Penalty (veh)		1					1	18	1		2	19

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	420	175
Average Queue (ft)	249	33
95th Queue (ft)	368	110
Link Distance (ft)	2060	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	3	
Queuing Penalty (veh)	3	

Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	70	54	150	151	109	94
Average Queue (ft)	23	5	43	28	43	36
95th Queue (ft)	63	31	108	98	83	70
Link Distance (ft)	52	52	616	616	261	294
Upstream Blk Time (%)	2	0				
Queuing Penalty (veh)	8	1				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	165	245	263	77	139	155	139	208	92	220
Average Queue (ft)	51	120	140	27	59	74	54	106	29	108
95th Queue (ft)	117	211	230	63	109	126	106	178	75	184
Link Distance (ft)		570	570		1260	1260		1970		2041
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	75			100			150		150	
Storage Blk Time (%)	2	13		0	1		0	2		2
Queuing Penalty (veh)	8	9		1	0		0	2		1

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	T	T	R	L	T
Maximum Queue (ft)	170	293	317	174	188	213	315	471	469	230	276	384
Average Queue (ft)	76	148	182	67	96	116	100	271	272	87	72	246
95th Queue (ft)	144	243	274	133	163	184	237	401	402	240	173	349
Link Distance (ft)		1212	1212		1092	1092		1951	1951			2065
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300			500			180			180	150	
Storage Blk Time (%)		0					0	23	23		0	25
Queuing Penalty (veh)		0					1	26	19		1	20

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	398	300
Average Queue (ft)	245	58
95th Queue (ft)	354	202
Link Distance (ft)	2065	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	24	
Queuing Penalty (veh)	21	

Intersection: 13: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	137	152	60	3	103	64
Average Queue (ft)	15	20	13	0	44	25
95th Queue (ft)	73	87	45	3	82	55
Link Distance (ft)	1086	1086	56	56	422	473
Upstream Blk Time (%)	1					
Queuing Penalty (veh)	2					
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	62	11	67	2	48	64
Average Queue (ft)	12	0	12	0	19	26
95th Queue (ft)	45	6	43	2	45	54
Link Distance (ft)	275	275	1271	1271	614	699
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	202	224	169	110	267	316	321	277	212	352	344	320
Average Queue (ft)	112	136	93	40	138	194	194	162	36	216	214	179
95th Queue (ft)	174	201	156	86	234	284	286	264	116	313	312	289
Link Distance (ft)		1219	1219	2255		1997	1997	1997		2032	2032	2032
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	150				240				130			
Storage Blk Time (%)	3	10			1	2				24		0
Queuing Penalty (veh)	10	16			6	5				10		0

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	86
Average Queue (ft)	36
95th Queue (ft)	68
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	74	84	208	215
Average Queue (ft)	24	28	60	50
95th Queue (ft)	69	77	157	153
Link Distance (ft)	55	55	298	298
Upstream Blk Time (%)	4	6	0	0
Queuing Penalty (veh)	29	39	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 40: Country Club Rd

Movement	EB	NB	NB
Directions Served	R	T	T
Maximum Queue (ft)	88	208	191
Average Queue (ft)	48	44	42
95th Queue (ft)	78	139	133
Link Distance (ft)	451	2094	2094
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	215	251	74	85
Average Queue (ft)	51	64	27	32
95th Queue (ft)	166	194	74	83
Link Distance (ft)	513	513	56	56
Upstream Blk Time (%)			4	5
Queuing Penalty (veh)			18	22
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 72: 6th/5th Street

Movement	SW	SW
Directions Served	R	R
Maximum Queue (ft)	17	31
Average Queue (ft)	1	1
95th Queue (ft)	11	14
Link Distance (ft)	488	488
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 100: 6th/5th Street

Movement	WB	NB
Directions Served	T	LTR
Maximum Queue (ft)	8	63
Average Queue (ft)	0	32
95th Queue (ft)	5	54
Link Distance (ft)	570	214
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 110: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	10	8	63	56
Average Queue (ft)	0	0	28	23
95th Queue (ft)	6	6	53	50
Link Distance (ft)	1260	1260	332	276
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

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Intersection: 120: 6th/5th Street

Movement	WB	NB	SB
Directions Served	T	LTR	LTR
Maximum Queue (ft)	5	102	68
Average Queue (ft)	0	49	35
95th Queue (ft)	5	85	57
Link Distance (ft)	1086	211	276
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	72	72	95	119
Average Queue (ft)	23	25	20	26
95th Queue (ft)	67	71	68	83
Link Distance (ft)	56	56	275	275
Upstream Blk Time (%)	4	4		
Queuing Penalty (veh)	14	17		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 140: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	27	70
Average Queue (ft)	2	33
95th Queue (ft)	14	59
Link Distance (ft)	305	274
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 200: 6th Side Street & 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	TR	LTR	LTR
Maximum Queue (ft)	22	45	3	94	112
Average Queue (ft)	1	3	0	37	42
95th Queue (ft)	13	24	3	76	86
Link Distance (ft)	298	298	356	419	408
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 1000: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	14	10	68	54
Average Queue (ft)	0	0	21	26
95th Queue (ft)	8	7	51	50
Link Distance (ft)	757	757	442	411
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1050: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	276	282	69	82
Average Queue (ft)	75	82	26	31
95th Queue (ft)	215	236	68	78
Link Distance (ft)	526	526	52	52
Upstream Blk Time (%)			5	8
Queuing Penalty (veh)			18	27
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 3000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	TR	LTR	LTR
Maximum Queue (ft)	8	39	2	112	54
Average Queue (ft)	0	3	0	47	21
95th Queue (ft)	8	21	2	86	48
Link Distance (ft)	1877	1877	100	405	430
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 4000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	TR	LTR	LTR
Maximum Queue (ft)	6	6	18	77	71
Average Queue (ft)	0	0	1	34	24
95th Queue (ft)	5	6	9	63	59
Link Distance (ft)	1235	1235	2071	547	583
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 5000: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	T	TR	T	TR	LTR	LTR
Maximum Queue (ft)	9	11	3	3	174	72
Average Queue (ft)	0	0	0	0	65	30
95th Queue (ft)	6	8	3	3	126	55
Link Distance (ft)	619	619	581	581	310	280
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 6000: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	T	T	TR	LTR	LTR
Maximum Queue (ft)	10	6	8	187	57
Average Queue (ft)	0	0	0	91	22
95th Queue (ft)	7	4	5	161	47
Link Distance (ft)	578	513	513	355	360
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 7000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	TR	T	LTR	LTR
Maximum Queue (ft)	23	2	3	82	108
Average Queue (ft)	1	0	0	39	42
95th Queue (ft)	12	2	3	65	78
Link Distance (ft)	627	627	533	260	304
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 8000: 6th/5th Street

Movement	EB	EB	NB	SB
Directions Served	T	TR	LTR	LTR
Maximum Queue (ft)	5	22	60	96
Average Queue (ft)	0	1	22	46
95th Queue (ft)	5	11	51	78
Link Distance (ft)	1287	1287	319	333
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 9000: 6th/5th Street

Movement	EB	EB	WB	SB
Directions Served	T	TR	TR	LTR
Maximum Queue (ft)	6	4	6	64
Average Queue (ft)	0	0	0	33
95th Queue (ft)	6	4	5	55
Link Distance (ft)	560	560	526	310
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 2240

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	6:45	6:45	6:45	6:45	6:45	6:45	6:45
End Time	8:00	8:00	8:00	8:00	8:00	8:00	8:00
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	23563	23894	23647	24132	24026	23704	23986
Vehs Exited	22983	23332	23120	23530	23409	23010	23284
Starting Vehs	1109	1176	1094	1165	1134	1148	1113
Ending Vehs	1689	1738	1621	1767	1751	1842	1815
Travel Distance (mi)	23916	24363	24312	24594	24400	24298	24478
Travel Time (hr)	1589.6	1562.4	1724.8	1634.3	1694.5	1742.2	1645.6
Total Delay (hr)	843.2	800.2	965.0	866.9	931.2	982.3	878.4
Total Stops	33746	35522	34398	36712	35581	37567	37290
Fuel Used (gal)	937.0	941.4	976.2	964.8	974.0	980.0	964.3

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	6:45	6:45	6:45	6:45
End Time	8:00	8:00	8:00	8:00
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	23724	23792	23883	23829
Vehs Exited	23220	23392	23338	23260
Starting Vehs	1165	1166	1146	1121
Ending Vehs	1669	1566	1691	1701
Travel Distance (mi)	24261	24420	24315	24336
Travel Time (hr)	1575.5	1611.6	1562.0	1634.3
Total Delay (hr)	816.3	848.1	802.6	873.4
Total Stops	34578	34437	33755	35362
Fuel Used (gal)	942.1	956.9	941.6	957.8

Interval #0 Information Seeding

Start Time	6:45
End Time	7:00
Total Time (min)	15
No data recorded this interval.	

Interval #1 Information Recording

Start Time	7:00
End Time	8:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	23563	23894	23647	24132	24026	23704	23986
Vehs Exited	22983	23332	23120	23530	23409	23010	23284
Starting Vehs	1109	1176	1094	1165	1134	1148	1113
Ending Vehs	1689	1738	1621	1767	1751	1842	1815
Travel Distance (mi)	23916	24363	24312	24594	24400	24298	24478
Travel Time (hr)	1589.6	1562.4	1724.8	1634.3	1694.5	1742.2	1645.6
Total Delay (hr)	843.2	800.2	965.0	866.9	931.2	982.3	878.4
Total Stops	33746	35522	34398	36712	35581	37567	37290
Fuel Used (gal)	937.0	941.4	976.2	964.8	974.0	980.0	964.3

Interval #1 Information Recording

Start Time	7:00
End Time	8:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	23724	23792	23883	23829
Vehs Exited	23220	23392	23338	23260
Starting Vehs	1165	1166	1146	1121
Ending Vehs	1669	1566	1691	1701
Travel Distance (mi)	24261	24420	24315	24336
Travel Time (hr)	1575.5	1611.6	1562.0	1634.3
Total Delay (hr)	816.3	848.1	802.6	873.4
Total Stops	34578	34437	33755	35362
Fuel Used (gal)	942.1	956.9	941.6	957.8

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.5	0.2	0.4	0.0	0.0	0.0	5.1	4.7	5.2	1.2	0.1	0.2
Total Del/Veh (s)	74.4	36.8	10.9	51.8	38.4	35.4	430.8	55.3	11.4	49.6	44.4	38.7

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	1.8
Total Del/Veh (s)	70.2

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.5	0.1	0.1	0.4	0.2	0.1	0.2	0.2	2.9	3.5	2.7	0.3
Total Del/Veh (s)	30.2	6.9	5.5	9.8	1.3	0.4	109.8	61.7	206.2	144.8	154.9	12.5

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.2	0.9	0.9	65.8	64.4	61.6	181.1	185.8	174.4	1.6	0.5	0.5
Total Del/Veh (s)	63.5	26.2	22.5	152.8	148.0	146.6	763.9	271.6	279.8	212.3	108.3	106.7

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	55.2
Total Del/Veh (s)	142.2

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.1	0.0	0.1	0.1	0.1	0.0	0.1	0.1	1.5	0.2	0.2	0.2
Total Del/Veh (s)	61.8	21.3	38.5	39.4	6.0	49.2	37.2	5.6	54.9	44.3	37.0	36.3

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.3	1.3	0.1
Total Del/Veh (s)	19.9	10.8	14.4	10.8	53.2	21.1	16.7

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	11.8	11.2	12.0	1.5	0.2	0.2
Total Del/Veh (s)	197.1	42.8	26.7	67.5	78.0	52.5	462.6	192.6	169.2	74.1	44.4	41.8

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	3.7
Total Del/Veh (s)	115.5

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	22.8	1.1	0.3	28.4	32.8	29.5	369.8	102.3	237.0	131.9	267.2	24.4

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.8	0.5	0.5	2.2	0.3	0.3
Total Del/Veh (s)	44.4	12.4	5.7	23.0	13.9	7.9	113.3	45.2	38.5	115.6	41.1	36.8

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	30.7

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.8	0.2	1.7	1.6	0.2	1.6
Total Del/Veh (s)	76.6	32.7	14.4	57.3	51.9	33.9	48.4	40.9	17.3	94.8	38.6	16.1

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	43.3

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.3	0.1	0.1	0.1	0.0
Total Del/Veh (s)	9.5	2.7	1.2	22.2	9.6	7.3	14.8	31.5	68.0	49.2	9.8

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	2.0	0.4	0.3	1.4	0.3	0.3
Total Del/Veh (s)	58.5	17.5	10.1	25.1	13.6	7.4	73.2	35.4	30.1	49.1	40.7	34.6

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	26.1

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.7	0.3	1.7	1.7	0.2	1.7
Total Del/Veh (s)	54.2	30.6	11.5	38.5	40.7	20.9	57.8	43.0	20.5	66.4	32.2	15.8

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	38.1

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	4.7	4.7	4.4	0.6	16.2	17.3	10.5	14.9	3.3

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.2	0.2	0.2	0.1	0.1	0.1
Total Del/Veh (s)	6.9	1.8	0.9	8.3	3.0	2.3	19.8	21.2	8.8	12.7	17.0	9.5

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	3.9

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.2	1.5	0.2	0.2	1.2	0.2	1.2
Total Del/Veh (s)	48.3	45.5	15.0	42.8	42.0	27.5	66.4	27.2	20.8	37.4	26.7	11.1

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	29.7

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.5	6.2	4.2

40: Country Club Rd Performance by movement

Movement	EBR	NBT	SBT	All
Denied Del/Veh (s)	0.0	0.2	0.0	0.1
Total Del/Veh (s)	8.0	4.5	2.3	3.9

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	7.0	4.2	5.4

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWT	SWR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0
Total Del/Veh (s)	3.2	0.7	2.2	18.1	11.8

100: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.2	0.1
Total Del/Veh (s)	17.5	2.9	10.5

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.6	4.0	3.0

200: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	314.5	315.3	22.9
Total Del/Veh (s)	5.1	3.1	5.7	4.4	113.2	463.8	453.7	35.9

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0
Total Del/Veh (s)	7.2	3.0	4.2	3.6	5.3

1100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.2	0.2	0.1	0.2	0.1
Total Del/Veh (s)	1.8	4.5	3.5	6.4	16.7	4.1

1101: 5th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.1	0.0	0.0	0.0	0.1	0.2	0.2	0.0
Total Del/Veh (s)	4.0	3.1	6.5	5.8	7.4	25.8	15.3	6.2

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.0
Total Del/Veh (s)	3.9	3.0	4.3	3.3	17.0	7.6	6.8	4.5

1400: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.0
Total Del/Veh (s)	4.1	3.1	3.0	2.5	15.5	6.0	7.2	3.8

3000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	57.1	74.2	7.6	21.8	5.5
Total Del/Veh (s)	8.1	6.7	3.9	0.1	229.8	190.4	214.1	196.7	24.4

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.2	0.2	0.1	0.0
Total Del/Veh (s)	4.4	3.3	9.3	8.1	55.3	32.3	67.0	51.6	12.5

5000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.1	0.0	0.2	0.1	0.1	0.1
Total Del/Veh (s)	7.2	4.4	7.7	33.1	25.1	8.3

6000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	580.7	572.5	618.2	53.1
Total Del/Veh (s)	5.6	4.4	38.6	34.5	1042.7	819.7	1201.0	74.9

7000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	2.3	155.1	153.9	144.6	154.3	24.7
Total Del/Veh (s)	3.6	2.4	10.6	160.0	130.7	156.2	179.4	27.7

8000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.1	0.1	0.1	0.1	2.2	2.9	0.1	0.3
Total Del/Veh (s)	2.4	1.6	10.5	8.5	44.8	21.3	26.0	9.0

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	SBR	All
Denied Del/Veh (s)	0.1	0.0	0.0	0.0	16.8	11.8	1.3
Total Del/Veh (s)	4.2	3.1	10.1	8.3	134.9	101.1	16.8

Total Network Performance

Denied Del/Veh (s)	16.3
Total Del/Veh (s)	110.2

Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	36.8	103.0	0.6	20
	1000	9.9	22.5	0.1	16
	2	6.9	34.7	0.2	25
Wilson HAWK	20	1.5	4.1	0.0	17
6th Side Street	200	5.1	12.7	0.1	18
Tucson Blvd	3	26.1	36.1	0.1	8
	3000	8.9	51.6	0.4	26
6th/5th Street	72	3.4	7.0	0.0	16
Country Club Rd	4	21.3	34.4	0.1	12
	9922	6.0	18.0	0.1	19
	4000	4.4	33.6	0.2	26
Dodge Blvd	5	20.6	67.8	0.4	21
	5000	7.4	17.6	0.1	17
Alvernon Wy	6	42.5	61.8	0.2	10
	6000	6.7	21.5	0.1	21
	70	7.0	20.1	0.1	19
	7	1.1	3.2	0.0	22
Columbus Blvd	7000	3.7	19.4	0.1	24
	8	11.5	24.4	0.1	17
	8000	2.7	10.9	0.1	23
Swan Rd	9	31.2	81.7	0.4	19
	9000	5.3	18.3	0.1	21
	100	17.5	32.9	0.1	14
	10	2.7	4.6	0.0	15
	1100	2.3	16.3	0.1	27
Rosemont Blvd	11	17.5	31.9	0.1	14
5th Side Street	1101	4.4	23.8	0.2	25
Craycroft Rd	12	29.7	68.2	0.3	18
	1200	5.1	23.7	0.2	23
	13	4.9	39.3	0.3	26
Chantilly Dr HAWK	130	1.6	4.2	0.0	16
Sahuara Ave	14	1.8	8.9	0.1	25
	1400	4.4	34.5	0.3	26
Wilmot Rd	15	46.6	74.8	0.3	12
Total		408.5	1067.6	5.6	19

Arterial Level of Service
Scenario B

11/04/2022

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	42.0	142.6	0.4	11
	1400	2.2	30.7	0.3	29
Sahuara Ave	14	3.1	33.2	0.3	27
	130	4.0	11.6	0.1	19
	13	0.6	2.6	0.0	26
	1200	4.4	38.8	0.3	27
Craycroft Rd	12	39.8	57.1	0.2	10
5th Side Street	1101	8.3	47.4	0.3	25
Rosemont Blvd	11	13.1	32.2	0.2	19
	1100	5.3	20.2	0.1	23
	10	9.7	23.8	0.1	18
Arcadia Ave HAWK	100	2.9	5.5	0.0	13
	9000	10.1	25.1	0.1	18
Swan Rd	9	51.6	63.3	0.1	6
	8000	11.6	63.1	0.4	25
Columbus Blvd	8	12.6	23.6	0.1	14
	7000	11.0	27.0	0.1	17
	7	33.3	48.7	0.1	10
Irving Ave HAWK	70	4.2	6.9	0.0	10
	6000	38.6	51.1	0.1	8
Alvernon Wy	6	78.1	92.1	0.1	5
	5000	9.8	29.7	0.2	21
Dodge Blvd	5	11.0	18.8	0.1	13
	4000	9.5	57.6	0.4	25
	9922	11.5	40.6	0.2	22
Country Club Rd	4	39.4	50.2	0.1	7
6th/5th Street	72	21.0	34.5	0.1	12
	3000	3.9	8.1	0.0	13
Tucson Blvd	3	147.8	259.4	0.4	7
6th Side Street	200	6.4	15.8	0.1	18
Wilson HAWK	20	6.4	14.4	0.1	16
	2	1.3	3.4	0.0	21
	1000	4.3	32.1	0.2	27
Campbell Ave	1	38.4	49.3	0.1	7
Total		697.2	1460.4	5.5	14

Queuing and Blocking Report
Scenario B

11/04/2022

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	271	282	284	200	301	370	372	390	1866	1840	1634	344
Average Queue (ft)	152	158	146	80	152	251	269	384	1199	1155	554	66
95th Queue (ft)	256	244	239	170	255	345	358	427	2046	1995	1428	212
Link Distance (ft)		2986	2986			418	418		2020	2020	2020	
Upstream Blk Time (%)							0		8	2	0	
Queuing Penalty (veh)							0		0	0	0	
Storage Bay Dist (ft)	250			50	180			290				150
Storage Blk Time (%)	3	0	42	11	9	21		84	1		34	0
Queuing Penalty (veh)	7	1	90	27	41	57		355	3		68	0

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	332	412	407	408
Average Queue (ft)	79	278	279	274
95th Queue (ft)	196	365	367	369
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	0	25		
Queuing Penalty (veh)	0	20		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	154	326	38	88	220	240
Average Queue (ft)	38	80	7	33	83	110
95th Queue (ft)	94	262	28	80	183	278
Link Distance (ft)		1205		55	442	447
Upstream Blk Time (%)			0	1		3
Queuing Penalty (veh)			0	16		0
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0	4	0	1		
Queuing Penalty (veh)	3	2	1	0		

Queuing and Blocking Report
Scenario B

11/04/2022

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB	SB	SB
Directions Served	L	TR	L	TR	L	TR	L	TR
Maximum Queue (ft)	299	371	300	1886	395	1677	399	989
Average Queue (ft)	99	294	83	1515	285	935	205	577
95th Queue (ft)	247	424	263	2289	491	2199	453	1293
Link Distance (ft)		356		1877		1948		2192
Upstream Blk Time (%)		7		4		29		0
Queuing Penalty (veh)		48		46		0		0
Storage Bay Dist (ft)	200		100		300		300	
Storage Blk Time (%)		24	1	45	52	10	9	34
Queuing Penalty (veh)		18	13	28	206	9	45	30

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	B9922	NB	NB	NB	NB
Directions Served	L	T	T	L	T	T	R	T	L	T	T	R
Maximum Queue (ft)	217	233	198	190	461	431	234	493	176	202	213	170
Average Queue (ft)	101	133	99	108	281	202	41	86	94	159	165	40
95th Queue (ft)	184	217	178	229	474	425	194	527	174	216	220	138
Link Distance (ft)		486	486		423	423		1235		177	177	
Upstream Blk Time (%)					6	3		0	0	9	10	0
Queuing Penalty (veh)					36	14		5	0	35	40	0
Storage Bay Dist (ft)	180			90			160		100			70
Storage Blk Time (%)	3	3		2	52	12	0		9	32	40	0
Queuing Penalty (veh)	7	4		6	51	31	1		26	40	22	0

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB	SB
Directions Served	L	T	TR
Maximum Queue (ft)	174	436	439
Average Queue (ft)	137	228	241
95th Queue (ft)	209	380	381
Link Distance (ft)		2000	2000
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)	100		
Storage Blk Time (%)	20	34	
Queuing Penalty (veh)	50	58	

Queuing and Blocking Report
Scenario B

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Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB
Directions Served	T	R	L	T	L	R
Maximum Queue (ft)	659	262	287	298	199	138
Average Queue (ft)	232	52	72	260	79	52
95th Queue (ft)	566	188	187	351	167	116
Link Distance (ft)	2072				288	2124
Upstream Blk Time (%)			0	2		
Queuing Penalty (veh)			0	28		
Storage Bay Dist (ft)	110		300		220	
Storage Blk Time (%)	19	0	0	2	0	1
Queuing Penalty (veh)	21	0	0	3	0	1

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	TR	L	T	TR
Maximum Queue (ft)	384	650	285	400	598	285	250	1999	2005	250	527	523
Average Queue (ft)	212	386	105	170	587	137	249	1305	1278	160	353	354
95th Queue (ft)	422	719	282	448	627	344	261	2334	2297	307	496	489
Link Distance (ft)	825				580		2113		2113	1973		1973
Upstream Blk Time (%)	4				31		17		13			
Queuing Penalty (veh)	33				304		0		0			
Storage Bay Dist (ft)	300	110		300	110		150			150		
Storage Blk Time (%)	21	40	1			62	0	90	42	5	40	
Queuing Penalty (veh)	134	104	4			119	4	522	91	25	47	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	22	59	200	636	95	124
Average Queue (ft)	2	8	81	408	32	41
95th Queue (ft)	13	39	223	770	102	121
Link Distance (ft)	56		628		311	327
Upstream Blk Time (%)	0		4			
Queuing Penalty (veh)	3		48			
Storage Bay Dist (ft)	100	100				
Storage Blk Time (%)	0		0	32		
Queuing Penalty (veh)	0		0	23		

Queuing and Blocking Report
Scenario B

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Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	167	386	170	151	436	148	349	516	216	434
Average Queue (ft)	50	147	27	29	186	18	143	275	67	232
95th Queue (ft)	124	297	108	97	376	90	295	464	163	380
Link Distance (ft)		533			436			2075		1939
Upstream Blk Time (%)					1					
Queuing Penalty (veh)					10					
Storage Bay Dist (ft)	75		100	75		75	250		350	
Storage Blk Time (%)	6	19		1	25		4	13		2
Queuing Penalty (veh)	34	23		4	17		20	15		1

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	305	500	232	465	483	200	304	478	468	203	354	436
Average Queue (ft)	92	230	40	96	419	80	99	304	300	52	201	277
95th Queue (ft)	213	414	158	283	568	221	250	437	433	154	341	434
Link Distance (ft)		2213			466			1988	1988			2061
Upstream Blk Time (%)				0	12							
Queuing Penalty (veh)				0	99							
Storage Bay Dist (ft)	300		110	500		100	180			350	230	
Storage Blk Time (%)		31	0	0	53	0	0	30	4		13	14
Queuing Penalty (veh)		47	0	0	99	1	0	29	5		63	28

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	442	207
Average Queue (ft)	280	33
95th Queue (ft)	432	125
Link Distance (ft)	2061	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	5	
Queuing Penalty (veh)	4	

Queuing and Blocking Report
Scenario B

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Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	44	78	199	463	192	122
Average Queue (ft)	19	25	95	154	83	47
95th Queue (ft)	45	70	188	370	152	104
Link Distance (ft)		54		579	279	312
Upstream Blk Time (%)	1	2		0		
Queuing Penalty (veh)	0	13		1		
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	1	2	7	10		
Queuing Penalty (veh)	7	1	48	20		

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	134	472	180	232	451	158	241	370	249	466
Average Queue (ft)	88	207	56	43	187	19	83	159	42	232
95th Queue (ft)	158	391	161	129	360	96	181	303	148	395
Link Distance (ft)		607			810			1974		2044
Upstream Blk Time (%)		0								
Queuing Penalty (veh)		2								
Storage Bay Dist (ft)	75		110	100		110	150		150	
Storage Blk Time (%)	23	26	0	0	17		4	12		25
Queuing Penalty (veh)	135	58	0	1	15		10	9		8

Queuing and Blocking Report
Scenario B

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Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	229	326	210	234	564	210	330	606	595	230	274	384
Average Queue (ft)	114	141	44	61	288	54	178	367	361	102	95	225
95th Queue (ft)	198	265	140	154	483	180	358	574	562	271	204	340
Link Distance (ft)	1664			711			1958		1958		2072	
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300		110	500		110	180			180	150	
Storage Blk Time (%)	0	19	0		42	0	3	34	33		4	22
Queuing Penalty (veh)	0	46	0		69	0	21	62	33		16	24

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	371	299
Average Queue (ft)	220	65
95th Queue (ft)	339	189
Link Distance (ft)	2072	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)	150	
Storage Blk Time (%)	21	0
Queuing Penalty (veh)	27	0

Intersection: 13: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	TR	L	TR	LTR	LTR
Maximum Queue (ft)	211	33	50	82	59
Average Queue (ft)	35	4	5	33	22
95th Queue (ft)	136	20	28	63	50
Link Distance (ft)	1468		56	429	480
Upstream Blk Time (%)	0				
Queuing Penalty (veh)	1				
Storage Bay Dist (ft)	100				
Storage Blk Time (%)	2		0		
Queuing Penalty (veh)	0		0		

Queuing and Blocking Report
Scenario B

11/04/2022

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	31	40	66	27	106	57
Average Queue (ft)	3	2	22	2	48	24
95th Queue (ft)	18	22	56	19	84	52
Link Distance (ft)		275		1271	622	706
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)		0	0	0		
Queuing Penalty (veh)		0	0	0		

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	144	174	118	173	356	468	442	345	230	422	396	375
Average Queue (ft)	51	89	50	70	243	253	240	206	108	258	250	219
95th Queue (ft)	104	146	99	134	363	412	384	318	239	370	357	331
Link Distance (ft)		1220		2255		1994	1994	1994		2040	2040	2040
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	145		145		240				130			
Storage Blk Time (%)	0	1	0		16	6			2	29		1
Queuing Penalty (veh)	0	2	0		76	21			12	37		2

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	202
Average Queue (ft)	75
95th Queue (ft)	147
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Queuing and Blocking Report Scenario B

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Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	81	310
Average Queue (ft)	28	122
95th Queue (ft)	77	330
Link Distance (ft)	55	298
Upstream Blk Time (%)	6	1
Queuing Penalty (veh)	54	16
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 40: Country Club Rd

Movement	EB	NB	NB	SB
Directions Served	R	T	T	T
Maximum Queue (ft)	86	158	182	3
Average Queue (ft)	38	32	37	0
95th Queue (ft)	64	114	127	3
Link Distance (ft)	450	2095	2095	177
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	369	83
Average Queue (ft)	85	57
95th Queue (ft)	263	88
Link Distance (ft)	514	56
Upstream Blk Time (%)		21
Queuing Penalty (veh)		204
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report Scenario B

11/04/2022

Intersection: 72: 6th/5th Street

Movement	SW	SW
Directions Served	R	R
Maximum Queue (ft)	503	424
Average Queue (ft)	186	74
95th Queue (ft)	563	368
Link Distance (ft)	486	486
Upstream Blk Time (%)	5	2
Queuing Penalty (veh)	29	9
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 100: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	555	77
Average Queue (ft)	210	44
95th Queue (ft)	495	87
Link Distance (ft)	620	54
Upstream Blk Time (%)	0	16
Queuing Penalty (veh)	1	114
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	71	236
Average Queue (ft)	26	71
95th Queue (ft)	70	199
Link Distance (ft)	56	275
Upstream Blk Time (%)	5	0
Queuing Penalty (veh)	24	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario B

11/04/2022

Intersection: 200: 6th Side Street & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	274	257	168	461
Average Queue (ft)	57	31	70	384
95th Queue (ft)	201	143	154	559
Link Distance (ft)	298	356	433	422
Upstream Blk Time (%)	1	0		71
Queuing Penalty (veh)	6	0		0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1000: 6th/5th Street

Movement	EB	EB
Directions Served	T	R
Maximum Queue (ft)	313	76
Average Queue (ft)	30	3
95th Queue (ft)	175	44
Link Distance (ft)	418	418
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1100: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	44	51	33	119
Average Queue (ft)	3	2	13	48
95th Queue (ft)	42	33	37	90
Link Distance (ft)	579	607	226	197
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

Scenario B

11/04/2022

Intersection: 1101: 5th Side Street & 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	67	112
Average Queue (ft)	29	47
95th Queue (ft)	56	86
Link Distance (ft)	341	283
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1200: 6th/5th Street

Movement	WB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	3	89	71
Average Queue (ft)	0	37	31
95th Queue (ft)	3	69	56
Link Distance (ft)	1468	214	281
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1400: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	75	66
Average Queue (ft)	34	30
95th Queue (ft)	61	53
Link Distance (ft)	307	277
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report
Scenario B

11/04/2022

Intersection: 3000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	T	LTR	LTR
Maximum Queue (ft)	26	117	423	288
Average Queue (ft)	1	46	193	107
95th Queue (ft)	13	131	444	292
Link Distance (ft)	1877	100	420	430
Upstream Blk Time (%)		5	20	4
Queuing Penalty (veh)		30	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 4000: 6th/5th Street

Movement	EB	B9922	B9922	WB	NB	SB
Directions Served	TR	T		TR	LTR	LTR
Maximum Queue (ft)	6	91	28	165	244	194
Average Queue (ft)	0	5	1	14	85	89
95th Queue (ft)	5	63	27	191	189	180
Link Distance (ft)	1235	423	423	2072	554	590
Upstream Blk Time (%)					0	
Queuing Penalty (veh)					0	
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 5000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	118	291	82	86
Average Queue (ft)	36	111	30	30
95th Queue (ft)	211	262	77	66
Link Distance (ft)	375	825	318	286
Upstream Blk Time (%)	2			
Queuing Penalty (veh)	18			
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report
Scenario B

11/04/2022

Intersection: 6000: 6th/5th Street

Movement	WB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	531	386	395
Average Queue (ft)	449	279	306
95th Queue (ft)	657	478	491
Link Distance (ft)	514	362	366
Upstream Blk Time (%)	10	56	66
Queuing Penalty (veh)	100	0	0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 7000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	6	421	282	277
Average Queue (ft)	0	105	167	149
95th Queue (ft)	5	389	324	335
Link Distance (ft)	628	533	267	311
Upstream Blk Time (%)		0	31	23
Queuing Penalty (veh)		2	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 8000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	6	171	148	102
Average Queue (ft)	0	16	57	37
95th Queue (ft)	5	148	136	92
Link Distance (ft)	302	2213	326	342
Upstream Blk Time (%)			1	
Queuing Penalty (veh)			0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report Scenario B

11/04/2022

Intersection: 9000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	56	411	169	226
Average Queue (ft)	3	118	76	110
95th Queue (ft)	38	353	174	268
Link Distance (ft)	466	620	210	316
Upstream Blk Time (%)		0	7	9
Queuing Penalty (veh)		0	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 4960

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	4:45	4:45	4:45	4:45	4:45	4:45	4:45
End Time	6:00	6:00	6:00	6:00	6:00	6:00	6:00
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	23675	23853	23742	23615	23860	23449	23768
Vehs Exited	23035	22901	22964	22961	23076	22788	22860
Starting Vehs	1200	1116	1195	1185	1143	1195	1204
Ending Vehs	1840	2068	1973	1839	1927	1856	2112
Denied Entry After	294	288	344	397	191	252	398
Travel Distance (mi)	25094	25064	24973	24939	25178	24742	25070
Travel Time (hr)	1729.0	1744.9	1784.8	1792.7	1719.4	1680.7	1897.0
Total Delay (hr)	948.2	963.4	1006.6	1016.1	933.8	909.8	1117.0
Total Stops	38140	39408	38879	37807	38405	37359	41375
Fuel Used (gal)	994.9	994.8	1000.6	1002.3	990.1	972.9	1031.1

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	4:45	4:45	4:45	4:45
End Time	6:00	6:00	6:00	6:00
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	23421	23600	23299	23624
Vehs Exited	22740	22771	22841	22896
Starting Vehs	1181	1160	1198	1160
Ending Vehs	1862	1989	1656	1900
Denied Entry After	280	491	789	371
Travel Distance (mi)	24765	24937	24166	24893
Travel Time (hr)	1754.1	1939.2	1764.1	1780.6
Total Delay (hr)	982.4	1162.0	1014.1	1005.3
Total Stops	39558	40378	33846	38515
Fuel Used (gal)	991.2	1033.3	979.4	999.1

Interval #0 Information Seeding

Start Time	4:45
End Time	5:00
Total Time (min)	15
No data recorded this interval.	

Interval #1 Information Recording

Start Time	5:00
End Time	6:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	23675	23853	23742	23615	23860	23449	23768
Vehs Exited	23035	22901	22964	22961	23076	22788	22860
Starting Vehs	1200	1116	1195	1185	1143	1195	1204
Ending Vehs	1840	2068	1973	1839	1927	1856	2112
Denied Entry After	294	288	344	397	191	252	398
Travel Distance (mi)	25094	25064	24973	24939	25178	24742	25070
Travel Time (hr)	1729.0	1744.9	1784.8	1792.7	1719.4	1680.7	1897.0
Total Delay (hr)	948.2	963.4	1006.6	1016.1	933.8	909.8	1117.0
Total Stops	38140	39408	38879	37807	38405	37359	41375
Fuel Used (gal)	994.9	994.8	1000.6	1002.3	990.1	972.9	1031.1

Interval #1 Information Recording

Start Time	5:00
End Time	6:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	23421	23600	23299	23624
Vehs Exited	22740	22771	22841	22896
Starting Vehs	1181	1160	1198	1160
Ending Vehs	1862	1989	1656	1900
Denied Entry After	280	491	789	371
Travel Distance (mi)	24765	24937	24166	24893
Travel Time (hr)	1754.1	1939.2	1764.1	1780.6
Total Delay (hr)	982.4	1162.0	1014.1	1005.3
Total Stops	39558	40378	33846	38515
Fuel Used (gal)	991.2	1033.3	979.4	999.1

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	20	76	20	0	0	0	0	0	0	1	6	0

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	123

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Entry After	0	21	1	0	0	0	3	14	0	0	0	39

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Entry After	0	0	0	0	0	0	0

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	12	80	4	0	0	0

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	96

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

7: 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

10: 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Entry After	0	0	0	0	0	0	0	0	0	0	0	0

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Entry After	0

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

40: Country Club Rd Performance by movement

Movement	EBT	EBR	NBT	SBT	All
Denied Entry After	0	0	0	0	0

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWR	All
Denied Entry After	0	0	0	0

100: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Entry After	0	0	0

200: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBL	SBR	All
Denied Entry After	0	0	0	0	21	10	31

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0

1100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	All
Denied Entry After	0	0	0	0	0

1101: 5th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0

1400: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0

3000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0	0

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	All
Denied Entry After	6	0	0	0	0	0	0	6

5000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	25	39	0	64

6000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	7	5	0	12

7000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBL	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0

8000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Entry After	0	0	0	0	0	0	0	0

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBR	All
Denied Entry After	0	0	0	0	0

Total Network Performance

Denied Entry After	371
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Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	353.1	460.8	0.6	5
	1000	41.8	55.1	0.1	7
	2	101.9	164.1	0.2	7
Wilson HAWK	20	4.6	7.2	0.0	9
6th Side Street	200	20.9	28.5	0.1	8
Tucson Blvd	3	33.8	43.6	0.1	7
	3000	11.1	54.5	0.4	24
6th/5th Street	72	2.3	7.3	0.0	15
Country Club Rd	4	27.0	42.9	0.1	10
	9922	14.7	26.7	0.1	13
	4000	33.7	69.3	0.2	14
Dodge Blvd	5	143.9	188.6	0.4	8
	5000	28.3	41.4	0.1	8
Alvernon Wy	6	94.4	113.8	0.2	5
	6000	7.7	22.9	0.1	20
	70	7.8	20.9	0.1	18
	7	0.8	2.9	0.0	24
	7000	4.0	19.6	0.1	24
Columbus Blvd	8	18.3	31.7	0.1	13
	8000	5.0	13.4	0.1	19
Swan Rd	9	44.9	94.9	0.4	17
	9000	5.6	18.7	0.1	20
	100	10.2	25.6	0.1	18
	10	1.5	3.6	0.0	20
	1100	2.3	16.6	0.1	26
Rosemont Blvd	11	16.7	31.7	0.1	14
5th Side Street	1101	5.6	25.4	0.2	23
Craycroft Rd	12	35.5	73.8	0.3	16
	1200	4.4	23.0	0.2	24
	13	8.4	42.1	0.3	25
Chantilly Dr HAWK	130	1.8	4.3	0.0	16
Sahuara Ave	14	1.2	8.2	0.1	27
	1400	3.2	33.0	0.3	27
Wilmot Rd	15	29.2	52.6	0.3	17
Total		1125.7	1868.4	5.6	11

Arterial Level of Service
Scenario B

11/07/2022

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	39.2	141.5	0.4	11
	1400	1.1	27.1	0.3	33
Sahuara Ave	14	2.5	32.5	0.3	28
	130	3.6	11.1	0.1	20
	13	0.7	2.6	0.0	26
	1200	3.1	36.9	0.3	28
Craycroft Rd	12	30.8	48.0	0.2	11
5th Side Street	1101	6.1	45.1	0.3	27
Rosemont Blvd	11	9.9	29.5	0.2	20
	1100	3.7	19.0	0.1	24
	10	5.9	20.1	0.1	22
Arcadia Ave HAWK	100	1.8	4.3	0.0	16
	9000	2.2	17.1	0.1	26
Swan Rd	9	28.7	40.5	0.1	9
	8000	13.4	64.9	0.4	24
Columbus Blvd	8	13.6	25.0	0.1	14
	7000	5.5	19.1	0.1	21
	7	7.6	23.1	0.1	20
Irving Ave HAWK	70	2.0	4.6	0.0	15
	6000	9.1	21.7	0.1	18
Alvernon Wy	6	54.8	69.1	0.1	7
	5000	8.4	28.9	0.2	21
Dodge Blvd	5	11.3	19.0	0.1	13
	4000	8.1	54.7	0.4	27
	9922	3.2	32.4	0.2	27
Country Club Rd	4	25.7	36.4	0.1	9
6th/5th Street	72	11.4	27.5	0.1	15
	3000	0.5	10.6	0.0	10
Tucson Blvd	3	48.5	91.6	0.4	14
6th Side Street	200	5.3	14.9	0.1	19
Wilson HAWK	20	8.4	16.5	0.1	14
	2	1.4	3.4	0.0	20
	1000	4.7	33.0	0.2	26
Campbell Ave	1	35.8	46.6	0.1	8
Total		417.9	1118.2	5.5	18

Queuing and Blocking Report
Scenario B

11/07/2022

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	350	3021	3022	200	312	359	351	280	315	404	382	140
Average Queue (ft)	287	1796	1798	179	187	202	217	131	208	227	203	52
95th Queue (ft)	480	3438	3412	272	323	327	318	235	285	439	458	111
Link Distance (ft)		2986	2986			418	418		2020	2020	2020	
Upstream Blk Time (%)		22	22			1	0					
Queuing Penalty (veh)		0	0			6	0					
Storage Bay Dist (ft)	250			50	180			290				150
Storage Blk Time (%)	4	72	74	19	25	9		0	0		14	0
Queuing Penalty (veh)	17	190	211	91	72	19		1	1		20	1

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	350	1243	1235	1167
Average Queue (ft)	282	725	711	658
95th Queue (ft)	456	1404	1367	1250
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)		1	0	
Queuing Penalty (veh)		0	0	
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	15	55		
Queuing Penalty (veh)	76	118		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	132	1218	49	78	460	348
Average Queue (ft)	12	1066	28	30	318	141
95th Queue (ft)	84	1585	52	85	559	383
Link Distance (ft)		1205		55	442	447
Upstream Blk Time (%)		5	3	5	43	9
Queuing Penalty (veh)		59	0	46	0	0
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)		40	3	5		
Queuing Penalty (veh)		4	23	2		

Queuing and Blocking Report
Scenario B

11/07/2022

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB	SB	SB
Directions Served	L	TR	L	TR	L	TR	L	TR
Maximum Queue (ft)	300	374	299	873	251	440	399	555
Average Queue (ft)	129	349	103	494	67	254	138	291
95th Queue (ft)	313	432	278	801	171	390	305	480
Link Distance (ft)		356		1877		1948		2192
Upstream Blk Time (%)		23						
Queuing Penalty (veh)		283						
Storage Bay Dist (ft)	200		100		300		300	
Storage Blk Time (%)	0	39	3	45		5	5	9
Queuing Penalty (veh)	2	45	21	29		2	21	8

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	NB	NB	NB	NB	SB
Directions Served	L	T	T	L	T	T	R	L	T	T	R	L
Maximum Queue (ft)	279	353	331	190	359	301	143	176	213	216	170	174
Average Queue (ft)	115	194	162	85	187	124	7	111	174	176	58	158
95th Queue (ft)	230	325	299	186	311	244	76	191	217	222	168	205
Link Distance (ft)		486	486		423	423			177	177		
Upstream Blk Time (%)		0	0		0	0		1	13	14	0	
Queuing Penalty (veh)		1	1		0	0		0	61	68	0	
Storage Bay Dist (ft)	180			90			160	100			70	100
Storage Blk Time (%)	1	14		7	33	3	0	13	35	43	0	33
Queuing Penalty (veh)	4	26		18	28	8	0	46	55	33	1	93

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	TR
Maximum Queue (ft)	428	412
Average Queue (ft)	246	236
95th Queue (ft)	386	360
Link Distance (ft)	2000	2000
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)	34	
Queuing Penalty (veh)	83	

Queuing and Blocking Report
Scenario B

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Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB
Directions Served	T	R	L	T	L	R
Maximum Queue (ft)	2080	285	257	297	336	265
Average Queue (ft)	1396	209	87	240	152	119
95th Queue (ft)	2568	404	183	332	270	223
Link Distance (ft)	2072			288	2124	
Upstream Blk Time (%)	4		0	1		
Queuing Penalty (veh)	51		0	14		
Storage Bay Dist (ft)		110	300			220
Storage Blk Time (%)	55	0	0	1	3	2
Queuing Penalty (veh)	125	0	0	2	6	4

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	TR	L	T	TR
Maximum Queue (ft)	400	842	285	400	595	285	250	2156	2159	250	764	761
Average Queue (ft)	246	809	166	178	483	130	246	1535	1519	179	494	492
95th Queue (ft)	500	955	370	422	690	334	280	2629	2613	314	967	954
Link Distance (ft)		825			580			2113	2113		1973	1973
Upstream Blk Time (%)		19			6			40	35			
Queuing Penalty (veh)		236			58			0	0			
Storage Bay Dist (ft)	300		110	300		110	150			150		
Storage Blk Time (%)	0	60	1		53	0	94	46		27	49	
Queuing Penalty (veh)	2	199	8		113	1	534	77		141	51	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	32	52	80	412	78	49
Average Queue (ft)	4	3	6	113	27	13
95th Queue (ft)	20	24	42	341	62	38
Link Distance (ft)		56		628	311	327
Upstream Blk Time (%)	0	0		0		
Queuing Penalty (veh)	0	1		0		
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0	0		7		
Queuing Penalty (veh)	0	0		1		

Queuing and Blocking Report
Scenario B

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Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	164	532	200	173	400	175	178	391	315	527
Average Queue (ft)	33	293	36	55	221	24	67	216	60	309
95th Queue (ft)	99	491	136	130	386	103	164	345	190	485
Link Distance (ft)		533			436			2075		1939
Upstream Blk Time (%)		0			2					
Queuing Penalty (veh)		3			12					
Storage Bay Dist (ft)	75		100	75		75	250		350	
Storage Blk Time (%)	1	31		10	24	0	1	5		7
Queuing Penalty (veh)	12	31		81	23	1	5	3		4

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	340	715	254	277	475	200	304	470	474	175	368	537
Average Queue (ft)	93	335	61	84	255	79	121	309	302	38	194	326
95th Queue (ft)	259	608	212	180	451	214	289	446	438	108	367	488
Link Distance (ft)		2213			466			1988	1988			2061
Upstream Blk Time (%)				0	1							
Queuing Penalty (veh)				0	8							
Storage Bay Dist (ft)	300		110	500		100	180			350	230	
Storage Blk Time (%)		49		0	33	0	1	31	4		12	22
Queuing Penalty (veh)		89		0	67	1	5	29	4		63	35

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	522	312
Average Queue (ft)	318	51
95th Queue (ft)	481	200
Link Distance (ft)	2061	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	11	
Queuing Penalty (veh)	9	

Queuing and Blocking Report
Scenario B

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Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	46	73	162	328	170	124
Average Queue (ft)	13	20	25	84	60	44
95th Queue (ft)	40	62	86	252	150	97
Link Distance (ft)		54		579	279	312
Upstream Blk Time (%)	0	1			2	
Queuing Penalty (veh)	0	7			0	
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0	1	0	5		
Queuing Penalty (veh)	1	0	1	2		

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	134	567	180	123	375	162	250	389	159	369
Average Queue (ft)	45	259	46	35	124	15	93	198	39	186
95th Queue (ft)	108	497	155	89	267	84	205	332	109	321
Link Distance (ft)		607			810			1974		2044
Upstream Blk Time (%)		0								
Queuing Penalty (veh)		1								
Storage Bay Dist (ft)	75		110	100		110	150		150	
Storage Blk Time (%)	2	26		0	9		2	19	0	18
Queuing Penalty (veh)	15	42		2	7		6	18	0	7

Queuing and Blocking Report
Scenario B

11/07/2022

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	375	549	210	187	423	210	329	470	474	230	299	466
Average Queue (ft)	101	256	104	87	218	75	125	306	306	89	105	290
95th Queue (ft)	246	453	240	156	367	209	281	444	446	249	257	423
Link Distance (ft)		1664			711			1958	1958			2072
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300		110	500		110	180			180	150	
Storage Blk Time (%)	0	36	1		29	0	1	27	26	0	0	31
Queuing Penalty (veh)	0	115	6		59	0	9	30	22	0	3	25

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	482	300
Average Queue (ft)	290	78
95th Queue (ft)	425	253
Link Distance (ft)	2072	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	30	
Queuing Penalty (veh)	27	

Intersection: 13: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	TR	L	TR	LTR	LTR
Maximum Queue (ft)	423	37	43	136	67
Average Queue (ft)	86	10	3	54	25
95th Queue (ft)	289	34	20	104	54
Link Distance (ft)	1468		56	429	480
Upstream Blk Time (%)		0	0		
Queuing Penalty (veh)		0	1		
Storage Bay Dist (ft)		100			
Storage Blk Time (%)	5	0	0		
Queuing Penalty (veh)	0	2	0		

Queuing and Blocking Report
Scenario B

11/07/2022

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	41	6	50	4	46	61
Average Queue (ft)	11	0	12	0	19	27
95th Queue (ft)	36	4	39	3	45	53
Link Distance (ft)		275		1271	622	706
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	223	291	222	102	308	374	357	339	198	416	422	401
Average Queue (ft)	110	136	86	43	159	224	216	187	43	259	252	220
95th Queue (ft)	183	222	169	86	274	339	328	302	135	382	382	352
Link Distance (ft)		1220		2255		1994	1994	1994		2040	2040	2040
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	145		145		240				130			
Storage Blk Time (%)	3	10	1		3	5				29		1
Queuing Penalty (veh)	17	33	6		15	12				12		2

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	126
Average Queue (ft)	39
95th Queue (ft)	90
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Queuing and Blocking Report

Scenario B

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Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	92	311
Average Queue (ft)	59	141
95th Queue (ft)	85	335
Link Distance (ft)	55	298
Upstream Blk Time (%)	25	2
Queuing Penalty (veh)	328	18
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 40: Country Club Rd

Movement	EB	NB	NB
Directions Served	R	T	T
Maximum Queue (ft)	88	200	194
Average Queue (ft)	43	56	57
95th Queue (ft)	73	159	157
Link Distance (ft)	450	2095	2095
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	416	82
Average Queue (ft)	125	39
95th Queue (ft)	342	89
Link Distance (ft)	514	56
Upstream Blk Time (%)	0	10
Queuing Penalty (veh)	0	81
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report Scenario B

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Intersection: 72: 6th/5th Street

Movement	EB	SW	SW
Directions Served	L	R	R
Maximum Queue (ft)	12	364	150
Average Queue (ft)	1	50	5
95th Queue (ft)	20	217	85
Link Distance (ft)	100	486	486
Upstream Blk Time (%)	0	0	0
Queuing Penalty (veh)	1	0	0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 100: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	474	71
Average Queue (ft)	134	31
95th Queue (ft)	397	77
Link Distance (ft)	620	54
Upstream Blk Time (%)	0	9
Queuing Penalty (veh)	0	65
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	77	211
Average Queue (ft)	31	48
95th Queue (ft)	79	147
Link Distance (ft)	56	275
Upstream Blk Time (%)	8	
Queuing Penalty (veh)	62	
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report
Scenario B

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Intersection: 200: 6th Side Street & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	318	228	128	440
Average Queue (ft)	276	29	51	374
95th Queue (ft)	408	140	105	544
Link Distance (ft)	298	356	433	422
Upstream Blk Time (%)	15	0		68
Queuing Penalty (veh)	192	0		0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	R	LTR	LTR	LTR
Maximum Queue (ft)	453	455	80	98	68
Average Queue (ft)	388	340	11	29	27
95th Queue (ft)	539	564	136	90	53
Link Distance (ft)	418	418	1205	446	415
Upstream Blk Time (%)	12	8			
Queuing Penalty (veh)	78	55			
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 1100: 6th/5th Street

Movement	EB	NB
Directions Served	TR	LTR
Maximum Queue (ft)	73	71
Average Queue (ft)	3	32
95th Queue (ft)	39	58
Link Distance (ft)	579	226
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario B

11/07/2022

Intersection: 1101: 5th Side Street & 6th/5th Street

Movement	EB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	8	75	66
Average Queue (ft)	0	32	26
95th Queue (ft)	8	60	54
Link Distance (ft)	810	341	283
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1200: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	145	95
Average Queue (ft)	63	39
95th Queue (ft)	116	69
Link Distance (ft)	214	281
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1400: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	26	76
Average Queue (ft)	2	32
95th Queue (ft)	13	60
Link Distance (ft)	307	277
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report Scenario B

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Intersection: 3000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	T	LTR	LTR
Maximum Queue (ft)	104	7	155	57
Average Queue (ft)	6	0	65	19
95th Queue (ft)	73	7	121	45
Link Distance (ft)	1877	100	420	430
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 4000: 6th/5th Street

Movement	EB	B9922	B9922	NB	SB
Directions Served	TR	T		LTR	LTR
Maximum Queue (ft)	1022	416	333	276	166
Average Queue (ft)	305	131	65	92	56
95th Queue (ft)	1098	423	297	247	157
Link Distance (ft)	1235	423	423	554	590
Upstream Blk Time (%)	9	2	1		
Queuing Penalty (veh)	104	12	4		
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 5000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	388	224	352	92
Average Queue (ft)	331	58	311	35
95th Queue (ft)	518	169	400	70
Link Distance (ft)	375	825	318	286
Upstream Blk Time (%)	10		81	
Queuing Penalty (veh)	118		0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report
Scenario B

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Intersection: 6000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	9	371	376	91
Average Queue (ft)	0	93	241	32
95th Queue (ft)	10	324	434	94
Link Distance (ft)	580	514	362	366
Upstream Blk Time (%)		1	29	
Queuing Penalty (veh)		4	0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 7000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	147	5	125	159
Average Queue (ft)	7	0	51	60
95th Queue (ft)	71	5	97	116
Link Distance (ft)	628	533	267	311
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 8000: 6th/5th Street

Movement	WB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	240	82	137
Average Queue (ft)	28	31	68
95th Queue (ft)	271	78	184
Link Distance (ft)	2213	326	342
Upstream Blk Time (%)			5
Queuing Penalty (veh)			0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 9000: 6th/5th Street

Movement	EB	WB	SB
Directions Served	TR	TR	LTR
Maximum Queue (ft)	12	134	84
Average Queue (ft)	0	9	38
95th Queue (ft)	12	70	67
Link Distance (ft)	466	620	316
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Network Summary

Network wide Queuing Penalty: 5633

Summary of All Intervals

Run Number	1	2	3	4	5	6	7
Start Time	4:45	4:45	4:45	4:45	4:45	4:45	4:45
End Time	6:00	6:00	6:00	6:00	6:00	6:00	6:00
Total Time (min)	75	75	75	75	75	75	75
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1	1
Vehs Entered	23675	23853	23742	23615	23860	23449	23768
Vehs Exited	23035	22901	22964	22961	23076	22788	22860
Starting Vehs	1200	1116	1195	1185	1143	1195	1204
Ending Vehs	1840	2068	1973	1839	1927	1856	2112
Travel Distance (mi)	25094	25064	24973	24939	25178	24742	25070
Travel Time (hr)	1729.0	1744.9	1784.8	1792.7	1719.4	1680.7	1897.0
Total Delay (hr)	948.2	963.4	1006.6	1016.1	933.8	909.8	1117.0
Total Stops	38140	39408	38879	37807	38405	37359	41375
Fuel Used (gal)	994.9	994.8	1000.6	1002.3	990.1	972.9	1031.1

Summary of All Intervals

Run Number	8	9	10	Avg
Start Time	4:45	4:45	4:45	4:45
End Time	6:00	6:00	6:00	6:00
Total Time (min)	75	75	75	75
Time Recorded (min)	60	60	60	60
# of Intervals	2	2	2	2
# of Recorded Intervals	1	1	1	1
Vehs Entered	23421	23600	23299	23624
Vehs Exited	22740	22771	22841	22896
Starting Vehs	1181	1160	1198	1160
Ending Vehs	1862	1989	1656	1900
Travel Distance (mi)	24765	24937	24166	24893
Travel Time (hr)	1754.1	1939.2	1764.1	1780.6
Total Delay (hr)	982.4	1162.0	1014.1	1005.3
Total Stops	39558	40378	33846	38515
Fuel Used (gal)	991.2	1033.3	979.4	999.1

Interval #0 Information Seeding

Start Time	4:45
End Time	5:00
Total Time (min)	15
No data recorded this interval.	

Interval #1 Information Recording

Start Time	5:00
End Time	6:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	1	2	3	4	5	6	7
Vehs Entered	23675	23853	23742	23615	23860	23449	23768
Vehs Exited	23035	22901	22964	22961	23076	22788	22860
Starting Vehs	1200	1116	1195	1185	1143	1195	1204
Ending Vehs	1840	2068	1973	1839	1927	1856	2112
Travel Distance (mi)	25094	25064	24973	24939	25178	24742	25070
Travel Time (hr)	1729.0	1744.9	1784.8	1792.7	1719.4	1680.7	1897.0
Total Delay (hr)	948.2	963.4	1006.6	1016.1	933.8	909.8	1117.0
Total Stops	38140	39408	38879	37807	38405	37359	41375
Fuel Used (gal)	994.9	994.8	1000.6	1002.3	990.1	972.9	1031.1

Interval #1 Information Recording

Start Time	5:00
End Time	6:00
Total Time (min)	60

Volumes adjusted by PHF.

Run Number	8	9	10	Avg
Vehs Entered	23421	23600	23299	23624
Vehs Exited	22740	22771	22841	22896
Starting Vehs	1181	1160	1198	1160
Ending Vehs	1862	1989	1656	1900
Travel Distance (mi)	24765	24937	24166	24893
Travel Time (hr)	1754.1	1939.2	1764.1	1780.6
Total Delay (hr)	982.4	1162.0	1014.1	1005.3
Total Stops	39558	40378	33846	38515
Fuel Used (gal)	991.2	1033.3	979.4	999.1

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	46.5	45.4	46.2	0.0	0.0	0.0	1.3	0.1	1.3	1.5	0.4	0.4
Total Del/Veh (s)	300.2	353.1	311.5	91.2	35.7	33.0	62.8	38.0	57.9	160.6	118.0	118.1

1: Campbell Ave & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	12.6
Total Del/Veh (s)	146.9

2: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	31.3	34.1	32.4	0.0	0.1	0.0	338.6	333.6	51.9	29.4	33.1	27.0
Total Del/Veh (s)	93.2	101.9	98.2	30.6	1.4	0.4	1193.9	1152.5	528.3	331.8	298.8	85.8

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.5	0.7	0.1	0.0	0.0	1.7	0.4	0.4	1.5	0.4	0.4
Total Del/Veh (s)	50.7	34.0	30.4	66.7	48.3	47.0	115.6	42.0	38.3	139.7	47.4	42.5

3: Tucson Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.4
Total Del/Veh (s)	47.2

4: Country Club Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	1.5	0.2	0.2	0.1
Total Del/Veh (s)	45.1	27.0	47.3	25.7	3.4	44.2	36.2	8.2	55.8	42.2	34.4	33.2

5: Dodge Blvd & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.4	1.3	0.1
Total Del/Veh (s)	144.4	121.5	23.9	10.6	57.5	44.1	75.5

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	55.0	50.2	52.0	1.4	0.2	0.2
Total Del/Veh (s)	98.1	94.6	67.4	58.5	53.7	29.8	740.2	250.6	232.6	185.6	74.5	70.6

6: Alvernon Wy & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	15.7
Total Del/Veh (s)	145.2

7: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.1	0.0	0.1	0.0	0.1	0.1	0.1	0.1	0.1	0.1
Total Del/Veh (s)	13.2	0.8	0.2	12.2	7.5	2.9	62.3	32.7	16.2	36.7	36.2	27.7

7: 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.0
Total Del/Veh (s)	4.8

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.2	0.1	0.2	0.1	0.2	0.2	1.9	0.3	0.3	1.9	0.4	0.4
Total Del/Veh (s)	44.5	19.4	13.4	69.3	15.4	8.9	142.4	37.2	30.8	67.4	47.7	41.9

8: Columbus Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	30.0

9: Swan Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.7	0.2	1.8	1.6	0.2	1.4
Total Del/Veh (s)	51.1	45.0	26.0	59.2	29.7	16.5	70.0	42.9	18.3	119.9	43.9	16.1

9: Swan Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	43.8

10: 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.1	0.1	0.3	0.1	0.1	1.1	7.7	2.6	0.2	0.2	0.2
Total Del/Veh (s)	5.9	1.5	0.4	12.7	5.9	3.7	100.1	46.6	37.3	43.0	48.9	35.0

10: 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	6.8

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.4	0.1	0.2	0.1	0.0	0.1	1.9	0.4	0.4	1.6	0.3	0.3
Total Del/Veh (s)	29.8	16.8	10.6	37.7	10.1	4.8	61.9	40.0	34.3	58.6	39.8	35.2

11: Rosemont Blvd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	24.0

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	1.7	0.2	1.6	1.5	0.2	1.4
Total Del/Veh (s)	45.9	36.0	21.6	56.0	32.0	16.1	76.4	35.2	18.7	66.0	34.4	13.4

12: Craycroft Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.3
Total Del/Veh (s)	35.5

13: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	All
Denied Del/Veh (s)	0.1	0.2	0.0	0.0	0.0	0.2	0.2	0.1	0.1	0.1
Total Del/Veh (s)	8.3	6.9	11.8	0.7	0.1	23.3	21.1	26.7	18.3	7.3

14: Sahuara Ave & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1	0.0
Total Del/Veh (s)	3.9	1.2	0.6	10.6	2.4	2.1	16.1	6.5	16.9	5.5	2.3

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.1	1.5	0.2	0.2	1.2	0.1	1.3
Total Del/Veh (s)	51.3	29.1	19.5	35.6	39.2	16.2	55.5	23.4	13.3	38.1	25.2	6.3

15: Wilmot Rd & 6th/5th Street Performance by movement

Movement	All
Denied Del/Veh (s)	0.2
Total Del/Veh (s)	27.0

20: Wilson HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	4.7	8.3	6.4

40: Country Club Rd Performance by movement

Movement	EBT	EBR	NBT	SBT	All
Denied Del/Veh (s)	0.0	0.0	0.2	0.0	0.1
Total Del/Veh (s)	0.1	5.9	6.1	2.4	4.7

70: 6th/5th Street & Irving Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	7.6	2.0	4.8

72: 6th/5th Street Performance by movement

Movement	EBL	EBT	SWR	All
Denied Del/Veh (s)	0.0	0.0	0.1	0.0
Total Del/Veh (s)	2.2	0.6	8.8	5.1

100: 6th/5th Street & Arcadia Ave HAWK Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	10.2	1.8	6.4

130: Chantilly Dr HAWK & 6th/5th Street Performance by movement

Movement	EBT	WBT	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	1.8	3.6	2.5

200: 6th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBL	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	559.9	644.9	22.1
Total Del/Veh (s)	20.9	17.7	4.8	54.2	1232.1	1077.1	45.0

1000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.6	3.7	0.0	0.0	0.1	0.2	0.1	0.4
Total Del/Veh (s)	38.1	17.4	4.6	3.7	93.3	164.1	7.6	23.5

1100: 6th/5th Street Performance by movement

Movement	EBT	WBT	WBR	NBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.0
Total Del/Veh (s)	2.2	3.2	2.7	11.2	3.0

1101: 5th Side Street & 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.1	0.0
Total Del/Veh (s)	5.1	4.3	4.9	11.7	6.7	5.3

1200: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.2	0.1	0.0
Total Del/Veh (s)	3.6	3.3	3.0	2.2	26.4	16.1	7.6	5.5

1400: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.1	0.2	0.1	0.0
Total Del/Veh (s)	3.2	2.4	2.0	4.6	14.9	6.3	3.1

3000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.1	0.0
Total Del/Veh (s)	9.9	8.4	0.5	0.1	31.0	10.4	7.1

4000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBL	All
Denied Del/Veh (s)	6.9	8.4	0.1	0.1	0.2	0.2	0.1	3.5
Total Del/Veh (s)	33.7	30.8	7.8	6.7	211.6	162.9	210.8	27.9

5000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	2.9	1.7	0.0	0.0	640.5	647.4	0.1	45.0
Total Del/Veh (s)	25.4	21.4	6.8	5.7	581.5	591.8	22.3	39.1

6000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	73.2	70.2	0.1	7.2
Total Del/Veh (s)	6.6	5.4	9.1	6.9	173.0	150.3	66.9	23.4

7000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBR	SBL	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.2	0.2	0.2	0.0
Total Del/Veh (s)	3.9	2.6	5.0	3.8	19.2	41.9	24.0	6.4

8000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	WBR	NBL	NBR	SBR	All
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.1	0.1	24.0	1.7
Total Del/Veh (s)	4.6	3.1	12.0	10.0	58.5	23.0	35.7	10.5

9000: 6th/5th Street Performance by movement

Movement	EBT	EBR	WBT	SBR	All
Denied Del/Veh (s)	0.1	0.0	0.0	0.1	0.0
Total Del/Veh (s)	4.7	3.7	2.2	11.0	4.0

Total Network Performance

Denied Del/Veh (s)	15.2
Total Del/Veh (s)	131.3

Arterial Level of Service: EB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Campbell Ave	1	353.1	460.8	0.6	5
	1000	41.8	55.1	0.1	7
	2	101.9	164.1	0.2	7
Wilson HAWK	20	4.6	7.2	0.0	9
6th Side Street	200	20.9	28.5	0.1	8
Tucson Blvd	3	33.8	43.6	0.1	7
	3000	11.1	54.5	0.4	24
6th/5th Street	72	2.3	7.3	0.0	15
Country Club Rd	4	27.0	42.9	0.1	10
	9922	14.7	26.7	0.1	13
	4000	33.7	69.3	0.2	14
Dodge Blvd	5	143.9	188.6	0.4	8
	5000	28.3	41.4	0.1	8
Alvernon Wy	6	94.4	113.8	0.2	5
	6000	7.7	22.9	0.1	20
	70	7.8	20.9	0.1	18
	7	0.8	2.9	0.0	24
	7000	4.0	19.6	0.1	24
Columbus Blvd	8	18.3	31.7	0.1	13
	8000	5.0	13.4	0.1	19
Swan Rd	9	44.9	94.9	0.4	17
	9000	5.6	18.7	0.1	20
	100	10.2	25.6	0.1	18
	10	1.5	3.6	0.0	20
	1100	2.3	16.6	0.1	26
Rosemont Blvd	11	16.7	31.7	0.1	14
5th Side Street	1101	5.6	25.4	0.2	23
Craycroft Rd	12	35.5	73.8	0.3	16
	1200	4.4	23.0	0.2	24
	13	8.4	42.1	0.3	25
Chantilly Dr HAWK	130	1.8	4.3	0.0	16
Sahuara Ave	14	1.2	8.2	0.1	27
	1400	3.2	33.0	0.3	27
Wilmot Rd	15	29.2	52.6	0.3	17
Total		1125.7	1868.4	5.6	11

Arterial Level of Service
Scenario B

11/05/2022

Arterial Level of Service: WB 6th/5th Street

Cross Street	Node	Delay (s/veh)	Travel time (s)	Dist (mi)	Arterial Speed
Wilmot Rd	15	39.2	141.5	0.4	11
	1400	1.1	27.1	0.3	33
Sahuara Ave	14	2.5	32.5	0.3	28
	130	3.6	11.1	0.1	20
	13	0.7	2.6	0.0	26
	1200	3.1	36.9	0.3	28
Craycroft Rd	12	30.8	48.0	0.2	11
5th Side Street	1101	6.1	45.1	0.3	27
Rosemont Blvd	11	9.9	29.5	0.2	20
	1100	3.7	19.0	0.1	24
	10	5.9	20.1	0.1	22
Arcadia Ave HAWK	100	1.8	4.3	0.0	16
	9000	2.2	17.1	0.1	26
Swan Rd	9	28.7	40.5	0.1	9
	8000	13.4	64.9	0.4	24
Columbus Blvd	8	13.6	25.0	0.1	14
	7000	5.5	19.1	0.1	21
	7	7.6	23.1	0.1	20
Irving Ave HAWK	70	2.0	4.6	0.0	15
	6000	9.1	21.7	0.1	18
Alvernon Wy	6	54.8	69.1	0.1	7
	5000	8.4	28.9	0.2	21
Dodge Blvd	5	11.3	19.0	0.1	13
	4000	8.1	54.7	0.4	27
	9922	3.2	32.4	0.2	27
Country Club Rd	4	25.7	36.4	0.1	9
6th/5th Street	72	11.4	27.5	0.1	15
	3000	0.5	10.6	0.0	10
Tucson Blvd	3	48.5	91.6	0.4	14
6th Side Street	200	5.3	14.9	0.1	19
Wilson HAWK	20	8.4	16.5	0.1	14
	2	1.4	3.4	0.0	20
	1000	4.7	33.0	0.2	26
Campbell Ave	1	35.8	46.6	0.1	8
Total		417.9	1118.2	5.5	18

Queuing and Blocking Report
Scenario B

11/05/2022

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	L	T	T	T	R
Maximum Queue (ft)	350	3021	3022	200	312	359	351	280	315	404	382	140
Average Queue (ft)	287	1796	1798	179	187	202	217	131	208	227	203	52
95th Queue (ft)	480	3438	3412	272	323	327	318	235	285	439	458	111
Link Distance (ft)		2986	2986			418	418		2020	2020	2020	
Upstream Blk Time (%)		22	22			1	0					
Queuing Penalty (veh)		0	0			6	0					
Storage Bay Dist (ft)	250			50	180			290				150
Storage Blk Time (%)	4	72	74	19	25	9		0	0		14	0
Queuing Penalty (veh)	17	190	211	91	72	19		1	1		20	1

Intersection: 1: Campbell Ave & 6th/5th Street

Movement	SB	SB	SB	SB
Directions Served	L	T	T	TR
Maximum Queue (ft)	350	1243	1235	1167
Average Queue (ft)	282	725	711	658
95th Queue (ft)	456	1404	1367	1250
Link Distance (ft)		2162	2162	2162
Upstream Blk Time (%)		1	0	
Queuing Penalty (veh)		0	0	
Storage Bay Dist (ft)	200			
Storage Blk Time (%)	15	55		
Queuing Penalty (veh)	76	118		

Intersection: 2: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	132	1218	49	78	460	348
Average Queue (ft)	12	1066	28	30	318	141
95th Queue (ft)	84	1585	52	85	559	383
Link Distance (ft)		1205		55	442	447
Upstream Blk Time (%)		5	3	5	43	9
Queuing Penalty (veh)		59	0	46	0	0
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)		40	3	5		
Queuing Penalty (veh)		4	23	2		

Queuing and Blocking Report
Scenario B

11/05/2022

Intersection: 3: Tucson Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB	SB	SB
Directions Served	L	TR	L	TR	L	TR	L	TR
Maximum Queue (ft)	300	374	299	873	251	440	399	555
Average Queue (ft)	129	349	103	494	67	254	138	291
95th Queue (ft)	313	432	278	801	171	390	305	480
Link Distance (ft)		356		1877		1948		2192
Upstream Blk Time (%)		23						
Queuing Penalty (veh)		283						
Storage Bay Dist (ft)	200		100		300		300	
Storage Blk Time (%)	0	39	3	45		5	5	9
Queuing Penalty (veh)	2	45	21	29		2	21	8

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	WB	NB	NB	NB	NB	SB
Directions Served	L	T	T	L	T	T	R	L	T	T	R	L
Maximum Queue (ft)	279	353	331	190	359	301	143	176	213	216	170	174
Average Queue (ft)	115	194	162	85	187	124	7	111	174	176	58	158
95th Queue (ft)	230	325	299	186	311	244	76	191	217	222	168	205
Link Distance (ft)		486	486		423	423			177	177		
Upstream Blk Time (%)		0	0		0	0		1	13	14	0	
Queuing Penalty (veh)		1	1		0	0		0	61	68	0	
Storage Bay Dist (ft)	180			90			160	100			70	100
Storage Blk Time (%)	1	14		7	33	3	0	13	35	43	0	33
Queuing Penalty (veh)	4	26		18	28	8	0	46	55	33	1	93

Intersection: 4: Country Club Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	TR
Maximum Queue (ft)	428	412
Average Queue (ft)	246	236
95th Queue (ft)	386	360
Link Distance (ft)	2000	2000
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)	34	
Queuing Penalty (veh)	83	

Queuing and Blocking Report
Scenario B

11/05/2022

Intersection: 5: Dodge Blvd & 6th/5th Street

Movement	EB	EB	WB	WB	NB	NB
Directions Served	T	R	L	T	L	R
Maximum Queue (ft)	2080	285	257	297	336	265
Average Queue (ft)	1396	209	87	240	152	119
95th Queue (ft)	2568	404	183	332	270	223
Link Distance (ft)	2072			288	2124	
Upstream Blk Time (%)	4		0	1		
Queuing Penalty (veh)	51		0	14		
Storage Bay Dist (ft)		110	300			220
Storage Blk Time (%)	55	0	0	1	3	2
Queuing Penalty (veh)	125	0	0	2	6	4

Intersection: 6: Alvernon Wy & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	TR	L	T	TR
Maximum Queue (ft)	400	842	285	400	595	285	250	2156	2159	250	764	761
Average Queue (ft)	246	809	166	178	483	130	246	1535	1519	179	494	492
95th Queue (ft)	500	955	370	422	690	334	280	2629	2613	314	967	954
Link Distance (ft)		825			580			2113	2113		1973	1973
Upstream Blk Time (%)		19			6			40	35			
Queuing Penalty (veh)		236			58			0	0			
Storage Bay Dist (ft)	300		110	300		110	150			150		
Storage Blk Time (%)	0	60	1		53	0	94	46		27	49	
Queuing Penalty (veh)	2	199	8		113	1	534	77		141	51	

Intersection: 7: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	32	52	80	412	78	49
Average Queue (ft)	4	3	6	113	27	13
95th Queue (ft)	20	24	42	341	62	38
Link Distance (ft)		56		628	311	327
Upstream Blk Time (%)	0	0		0		
Queuing Penalty (veh)	0	1		0		
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0	0		7		
Queuing Penalty (veh)	0	0		1		

Queuing and Blocking Report
Scenario B

11/05/2022

Intersection: 8: Columbus Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	164	532	200	173	400	175	178	391	315	527
Average Queue (ft)	33	293	36	55	221	24	67	216	60	309
95th Queue (ft)	99	491	136	130	386	103	164	345	190	485
Link Distance (ft)		533			436			2075		1939
Upstream Blk Time (%)		0			2					
Queuing Penalty (veh)		3			12					
Storage Bay Dist (ft)	75		100	75		75	250		350	
Storage Blk Time (%)	1	31		10	24	0	1	5		7
Queuing Penalty (veh)	12	31		81	23	1	5	3		4

Intersection: 9: Swan Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	340	715	254	277	475	200	304	470	474	175	368	537
Average Queue (ft)	93	335	61	84	255	79	121	309	302	38	194	326
95th Queue (ft)	259	608	212	180	451	214	289	446	438	108	367	488
Link Distance (ft)		2213			466			1988	1988			2061
Upstream Blk Time (%)				0	1							
Queuing Penalty (veh)				0	8							
Storage Bay Dist (ft)	300		110	500		100	180			350	230	
Storage Blk Time (%)		49		0	33	0	1	31	4		12	22
Queuing Penalty (veh)		89		0	67	1	5	29	4		63	35

Intersection: 9: Swan Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	522	312
Average Queue (ft)	318	51
95th Queue (ft)	481	200
Link Distance (ft)	2061	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		300
Storage Blk Time (%)	11	
Queuing Penalty (veh)	9	

Queuing and Blocking Report
Scenario B

11/05/2022

Intersection: 10: 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	46	73	162	328	170	124
Average Queue (ft)	13	20	25	84	60	44
95th Queue (ft)	40	62	86	252	150	97
Link Distance (ft)		54		579	279	312
Upstream Blk Time (%)	0	1			2	
Queuing Penalty (veh)	0	7			0	
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)	0	1	0	5		
Queuing Penalty (veh)	1	0	1	2		

Intersection: 11: Rosemont Blvd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	TR	L	TR
Maximum Queue (ft)	134	567	180	123	375	162	250	389	159	369
Average Queue (ft)	45	259	46	35	124	15	93	198	39	186
95th Queue (ft)	108	497	155	89	267	84	205	332	109	321
Link Distance (ft)		607			810			1974		2044
Upstream Blk Time (%)		0								
Queuing Penalty (veh)		1								
Storage Bay Dist (ft)	75		110	100		110	150		150	
Storage Blk Time (%)	2	26		0	9		2	19	0	18
Queuing Penalty (veh)	15	42		2	7		6	18	0	7

Queuing and Blocking Report
Scenario B

11/05/2022

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	R	L	T	T	R	L	T
Maximum Queue (ft)	375	549	210	187	423	210	329	470	474	230	299	466
Average Queue (ft)	101	256	104	87	218	75	125	306	306	89	105	290
95th Queue (ft)	246	453	240	156	367	209	281	444	446	249	257	423
Link Distance (ft)		1664			711			1958	1958			2072
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	300		110	500		110	180			180	150	
Storage Blk Time (%)	0	36	1		29	0	1	27	26	0	0	31
Queuing Penalty (veh)	0	115	6		59	0	9	30	22	0	3	25

Intersection: 12: Craycroft Rd & 6th/5th Street

Movement	SB	SB
Directions Served	T	R
Maximum Queue (ft)	482	300
Average Queue (ft)	290	78
95th Queue (ft)	425	253
Link Distance (ft)	2072	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		150
Storage Blk Time (%)	30	
Queuing Penalty (veh)	27	

Intersection: 13: 6th/5th Street

Movement	EB	WB	WB	NB	SB
Directions Served	TR	L	TR	LTR	LTR
Maximum Queue (ft)	423	37	43	136	67
Average Queue (ft)	86	10	3	54	25
95th Queue (ft)	289	34	20	104	54
Link Distance (ft)	1468		56	429	480
Upstream Blk Time (%)		0	0		
Queuing Penalty (veh)		0	1		
Storage Bay Dist (ft)		100			
Storage Blk Time (%)	5	0	0		
Queuing Penalty (veh)	0	2	0		

Queuing and Blocking Report
Scenario B

11/05/2022

Intersection: 14: Sahuara Ave & 6th/5th Street

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	41	6	50	4	46	61
Average Queue (ft)	11	0	12	0	19	27
95th Queue (ft)	36	4	39	3	45	53
Link Distance (ft)		275		1271	622	706
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)	100		100			
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	EB	EB	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	L	LTR	R	LTR	L	T	T	TR	L	T	T	T
Maximum Queue (ft)	223	291	222	102	308	374	357	339	198	416	422	401
Average Queue (ft)	110	136	86	43	159	224	216	187	43	259	252	220
95th Queue (ft)	183	222	169	86	274	339	328	302	135	382	382	352
Link Distance (ft)		1220		2255		1994	1994	1994		2040	2040	2040
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	145		145		240				130			
Storage Blk Time (%)	3	10	1		3	5				29		1
Queuing Penalty (veh)	17	33	6		15	12				12		2

Intersection: 15: Wilmot Rd & 6th/5th Street

Movement	SB
Directions Served	R
Maximum Queue (ft)	126
Average Queue (ft)	39
95th Queue (ft)	90
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	300
Storage Blk Time (%)	
Queuing Penalty (veh)	

Queuing and Blocking Report Scenario B

11/05/2022

Intersection: 20: Wilson HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	92	311
Average Queue (ft)	59	141
95th Queue (ft)	85	335
Link Distance (ft)	55	298
Upstream Blk Time (%)	25	2
Queuing Penalty (veh)	328	18
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 40: Country Club Rd

Movement	EB	NB	NB
Directions Served	R	T	T
Maximum Queue (ft)	88	200	194
Average Queue (ft)	43	56	57
95th Queue (ft)	73	159	157
Link Distance (ft)	450	2095	2095
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 70: 6th/5th Street & Irving Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	416	82
Average Queue (ft)	125	39
95th Queue (ft)	342	89
Link Distance (ft)	514	56
Upstream Blk Time (%)	0	10
Queuing Penalty (veh)	0	81
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report Scenario B

11/05/2022

Intersection: 72: 6th/5th Street

Movement	EB	SW	SW
Directions Served	L	R	R
Maximum Queue (ft)	12	364	150
Average Queue (ft)	1	50	5
95th Queue (ft)	20	217	85
Link Distance (ft)	100	486	486
Upstream Blk Time (%)	0	0	0
Queuing Penalty (veh)	1	0	0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 100: 6th/5th Street & Arcadia Ave HAWK

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	474	71
Average Queue (ft)	134	31
95th Queue (ft)	397	77
Link Distance (ft)	620	54
Upstream Blk Time (%)	0	9
Queuing Penalty (veh)	0	65
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 130: Chantilly Dr HAWK & 6th/5th Street

Movement	EB	WB
Directions Served	T	T
Maximum Queue (ft)	77	211
Average Queue (ft)	31	48
95th Queue (ft)	79	147
Link Distance (ft)	56	275
Upstream Blk Time (%)	8	
Queuing Penalty (veh)	62	
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report
Scenario B

11/05/2022

Intersection: 200: 6th Side Street & 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	318	228	128	440
Average Queue (ft)	276	29	51	374
95th Queue (ft)	408	140	105	544
Link Distance (ft)	298	356	433	422
Upstream Blk Time (%)	15	0		68
Queuing Penalty (veh)	192	0		0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1000: 6th/5th Street

Movement	EB	EB	WB	NB	SB
Directions Served	T	R	LTR	LTR	LTR
Maximum Queue (ft)	453	455	80	98	68
Average Queue (ft)	388	340	11	29	27
95th Queue (ft)	539	564	136	90	53
Link Distance (ft)	418	418	1205	446	415
Upstream Blk Time (%)	12	8			
Queuing Penalty (veh)	78	55			
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 1100: 6th/5th Street

Movement	EB	NB
Directions Served	TR	LTR
Maximum Queue (ft)	73	71
Average Queue (ft)	3	32
95th Queue (ft)	39	58
Link Distance (ft)	579	226
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report

Scenario B

11/05/2022

Intersection: 1101: 5th Side Street & 6th/5th Street

Movement	EB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	8	75	66
Average Queue (ft)	0	32	26
95th Queue (ft)	8	60	54
Link Distance (ft)	810	341	283
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1200: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	145	95
Average Queue (ft)	63	39
95th Queue (ft)	116	69
Link Distance (ft)	214	281
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1400: 6th/5th Street

Movement	NB	SB
Directions Served	LTR	LTR
Maximum Queue (ft)	26	76
Average Queue (ft)	2	32
95th Queue (ft)	13	60
Link Distance (ft)	307	277
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queuing and Blocking Report Scenario B

11/05/2022

Intersection: 3000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	T	LTR	LTR
Maximum Queue (ft)	104	7	155	57
Average Queue (ft)	6	0	65	19
95th Queue (ft)	73	7	121	45
Link Distance (ft)	1877	100	420	430
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 4000: 6th/5th Street

Movement	EB	B9922	B9922	NB	SB
Directions Served	TR	T		LTR	LTR
Maximum Queue (ft)	1022	416	333	276	166
Average Queue (ft)	305	131	65	92	56
95th Queue (ft)	1098	423	297	247	157
Link Distance (ft)	1235	423	423	554	590
Upstream Blk Time (%)	9	2	1		
Queuing Penalty (veh)	104	12	4		
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 5000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	388	224	352	92
Average Queue (ft)	331	58	311	35
95th Queue (ft)	518	169	400	70
Link Distance (ft)	375	825	318	286
Upstream Blk Time (%)	10		81	
Queuing Penalty (veh)	118		0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report Scenario B

11/05/2022

Intersection: 6000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	9	371	376	91
Average Queue (ft)	0	93	241	32
95th Queue (ft)	10	324	434	94
Link Distance (ft)	580	514	362	366
Upstream Blk Time (%)		1	29	
Queuing Penalty (veh)		4	0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 7000: 6th/5th Street

Movement	EB	WB	NB	SB
Directions Served	TR	TR	LTR	LTR
Maximum Queue (ft)	147	5	125	159
Average Queue (ft)	7	0	51	60
95th Queue (ft)	71	5	97	116
Link Distance (ft)	628	533	267	311
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 8000: 6th/5th Street

Movement	WB	NB	SB
Directions Served	TR	LTR	LTR
Maximum Queue (ft)	240	82	137
Average Queue (ft)	28	31	68
95th Queue (ft)	271	78	184
Link Distance (ft)	2213	326	342
Upstream Blk Time (%)			5
Queuing Penalty (veh)			0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Queuing and Blocking Report

Scenario B

11/05/2022

Intersection: 9000: 6th/5th Street

Movement	EB	WB	SB
Directions Served	TR	TR	LTR
Maximum Queue (ft)	12	134	84
Average Queue (ft)	0	9	38
95th Queue (ft)	12	70	67
Link Distance (ft)	466	620	316
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Network Summary

Network wide Queuing Penalty: 5633



Appendix P
Special Event
Traffic Management Plan



CITY OF TUCSON

Department of Transportation
Permit Center

201 N. Stone Ave., 4th Floor
Tucson, AZ 85701 (520) 791-5100

RIGHT OF WAY PERMIT ACTIVITY NO. **T21RW04186**

Start Date: 09/11/2021

Expiration Date: 12/10/2021

Traffic Time: 24 Hour: N Weekend: N Other: Y 4:00PM TO 11:30PM

Address: 565 N CHERRY AV TUC

Township-Range-Section: 14-14E-07

Work Order #:

Location: CAMPBELL / 6TH E UOFA STADIUM

Status: ISSUED

Type of Work: CE

Description of Work:

ARIZONA FOOTBALL 2021 HOME GAMES

ARIZONA HOME FOOTBALL GAMES, FALL 2021. 5 SATURDAYS, 1
FRIDAY THIS SEASON

SET UP TIME WILL BE 3 HOURS PRIOR TO KICK-OFF

TAKE DOWN HOURS WILL BE APPROX 4.5 HOURS AFTER KICK-OFF

Comments

APPLICANT ARIZONA FOOTBALL 2021 HOME G 09/10/2021 Phone: 520-909-5625

SUZY MASON

PO BOX 210096

MCKALE CENTER #229

TUCSON, AZ 85721

License:

BARRICADE NBC NATIONAL BARRICADE 09/10/2021 Phone: 520-293-1063

BOBBY ROBBINS

12455 N 92ND DR

BUILDING C, STE 106

PEORIA AZ 85381

License: C000116088

INSPECTOR TOM ADKINS

09/10/2021 Phone: 520-400-3813

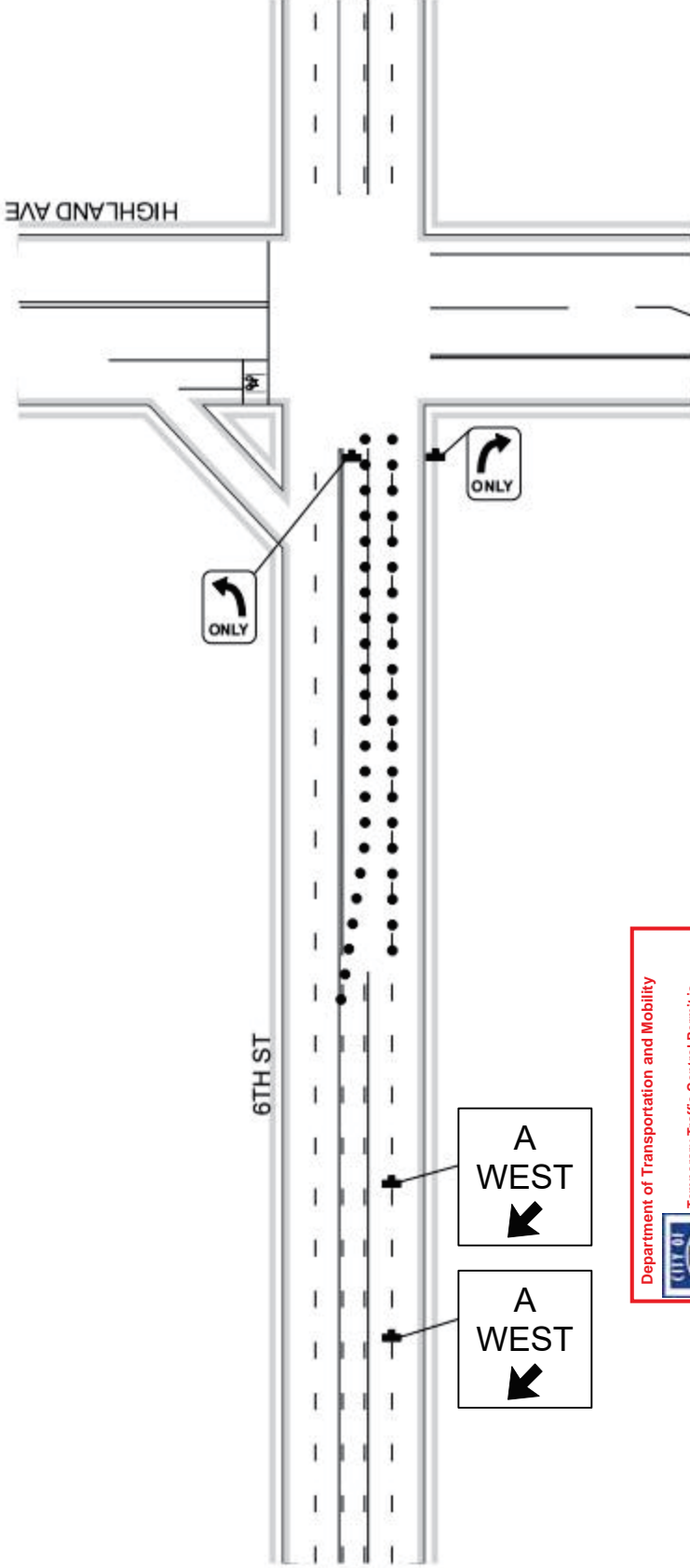
License:

By beginning the work approved by this permit the applicant agrees to all conditions listed below.

1. A copy of this permit shall be available on site for city review at all times.
2. All work shall conform to City of Tucson standards and specifications.
3. Notify in writing at least 5 working days before work begins, any businesses, residences and public transit routes affected by your temporary traffic control.
4. Notify City Communications (791-4144) at least 24 hours prior to closing any street or alley completely.
5. Notify SunTran (623-4301) at least 24 hours before starting work that will impact a transit stop or route.
6. Notify Environmental Services (791-3175) at least 24 hours prior to closing any street or alley completely.
7. Notify ParkTucson (791-5071) at least 24 hours before starting work for any parking restrictions that may occur in the boundaries of South_22nd, North_Speedway, West_Grande and East_Campbell.
8. Excavation work shall start within 30 days of permit issue date. Work shall be completed within 60 days of permit issue date.
9. Any damage to public or private property as a result of contractor's operations shall be restored in kind by contractor at no cost to the city.
10. Pavement patches shall be in accordance to PC/COT Standard Detail 216. Type "B" patches (concrete sub-base) are required for all streets shown on the City's major streets and routes plan and all inverted water carrying streets, see inspector for details.
11. All work conducted without obtaining the proper inspections, or prior approvals, from TDOT construction inspection, shall be subject to removal and replacement at no cost to the city.
12. At the end of each day, clean the roadway of excess dirt and debris.
13. Any attached plans represent a concept only and is subject to field review.
14. Keep a copy of your approved traffic control plan at the work site at all times.
15. The distance shown on the traffic control plan(s) are approximate and subject to adjustments in the field.
16. Category I and Category II work zone traffic control devices shall meet NCHRP Report 350 test and evaluation criteria.
17. Employees that are exposed to public vehicular traffic shall be provided with, and shall wear, warning vests or suitable garments marked with or made of reflectorized or highly visible material rated as Class 2 or above. The warning vests or suitable garments shall meet or exceed ANSI/ISEA 107-2004 Standard for High Visibility Warning Clothing.
18. Mark all unattended traffic and pedestrian hazards, including dumpster and mobile storage units, with type II barricades equipped with type A flashers.
19. Where it is relevant, equip all working and regulatory signs with highly visible flags and clean lights.
20. Place sandbags at the base of your signs and barricades to prevent them from being knocked over by traffic.
21. It is your responsibility to periodically examine traffic control devices and warning signs. Reset, correct or make any needed changes to those devices or signs.
22. "Share the Road with Care" signs are required for roadways that contain striped lanes and/or multi-use paths designated for bike usage.
23. All trenches and excavations must be barricaded and fenced per MUTCD, Part 6 and Additions by the City to the MUTCD, Part 6 standards and guides. Please refer to the City of Tucson Right-Of-Way Permit for additional requirements.
24. As per City Ordinance No. 2657 - Section 25-13, a written permit is required from the City of Tucson Permits and Codes Section (791-5100) prior to cutting, trenching, excavating, improving or removing any object placed in a street, alley, public property or public right-of-way.
25. When sidewalk closures occur, sidewalks will be restricted on one side at a time with notification of sidewalk closure at corresponding intersections.
26. Requires the use of an arrow board for lane closures on collector and arterial streets.
27. ADVANCE WARNING SIGNS SHALL BE USED ON EVERY SETUP, including short term jobs that are less than 30 minutes to perform.
28. Approved for devices placed in City of Tucson right of way.
29. Hours are subject to change at the discretion of TDOT Engineering, based on impacts to an area. Contractor will be notified immediately for any changes that are required.

Special Conditions

U OF A FOOTBALL 2021



Department of Transportation and Mobility



Temporary Traffic Control Permit is subject to the Applicant certifying Temporary Barricading Plan is in compliance with Tucson City Code, Chapter 23, Section 23-90

RL 09/10/2021

PRE-GAME

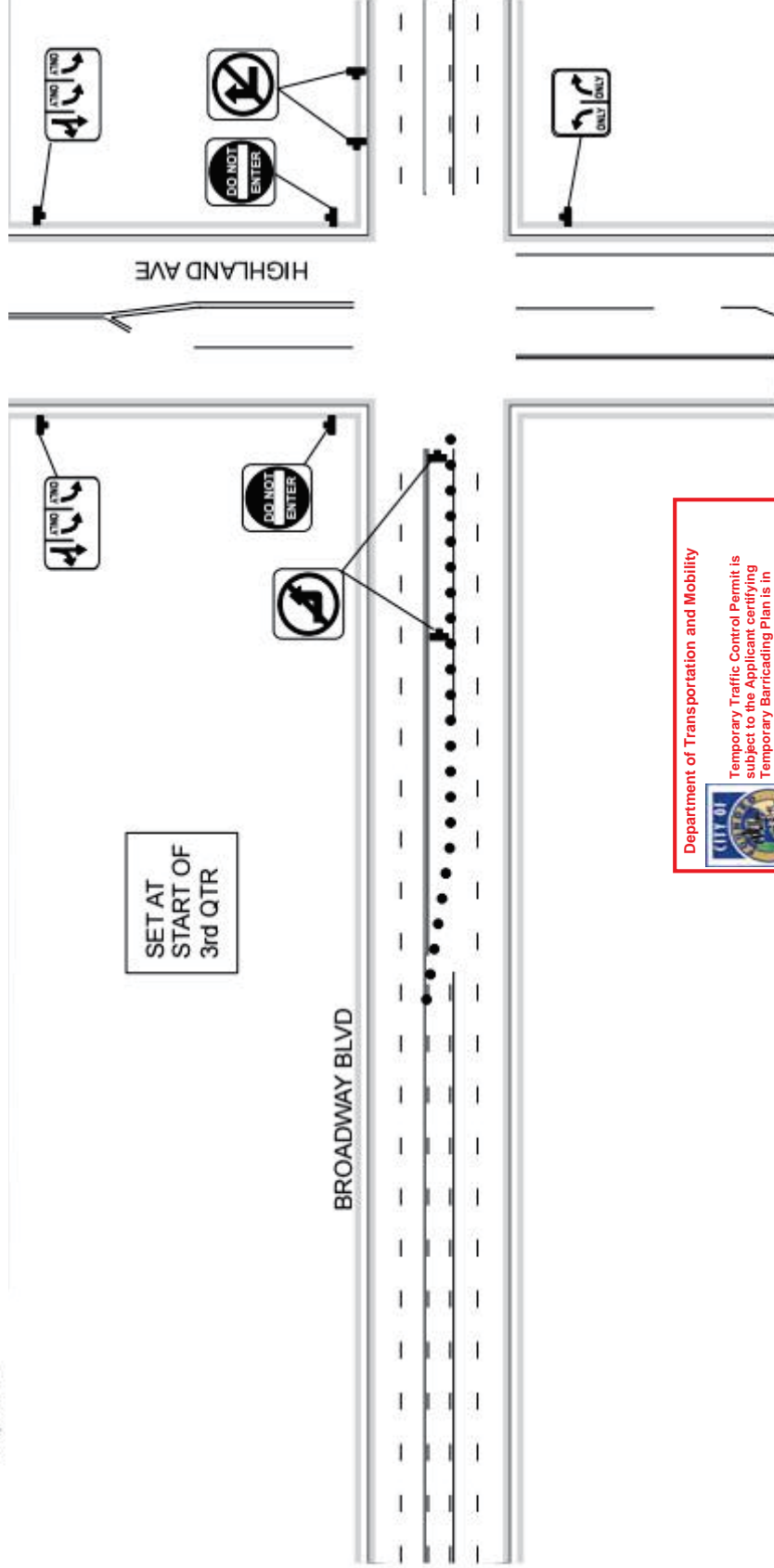
PLAN 2	NOTES	PLAN NOT TO SCALE	LEGEND
JURISDICTION U OF A PROJECT NUMBER WILD CAT FOOTBALL PROJECT CONTRIBUTOR U OF A NATIONAL BARRICADE PERIOD U OF A 2 START DATE 09/08/2021 PREPARED BY ALEX GRANILLO PERMIT NUMBER	THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, AND FLAGS. BUSINESS, AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY	SIGN SIZES OA) 18x24 OG) 48x60 OB) 24x24 OH) 18X18 OC) 24x30 OI) 24x48 OD) 30x30 OJ) 48x30 OE) 36x36 OK) OTHER OF) 48x48	SIGN STAND VERTICAL PANEL
REVIEWED BY _____ DATE _____			



3333 E 36TH ST
TUCSON, AZ 85713
PHONE: (520)-293-1063



U OF A FOOTBALL 2021



Department of Transportation and Mobility
 Temporary Traffic Control Permit is subject to the Applicant certifying Temporary Barricading Plan is in compliance with Tucson City Code, Chapter 25, Section 25-90
RL 09/10/2021

POST-GAME

PLAN 3	PROJECT NAME U OF A	PROJECT NUMBER WILD CAT FOOTBALL	NOTES	PLAN NOT TO SCALE	SIGN SIZES	LEGEND
PROJECT CONTRACTOR U OF A	TRAFFIC CONTROL CONTRACTOR NATIONAL BARRICADE	DATE 09/08/2021	THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, AND FLAGS. BUSINESS, AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY	oA) 18x24 oG) 48x60 oB) 24x24 oH) 18X18 oC) 24x30 oI) 24x48 oD) 30x30 oJ) 48x30 oE) 36x36 oK) OTHER oF) 48x48	— SIGN STAND ● VERTICAL PANEL	REVIEWED BY _____ DATE _____
SCALE U OF A 3	ISSUED BY ALEX GRANILLO	WORK NUMBER				



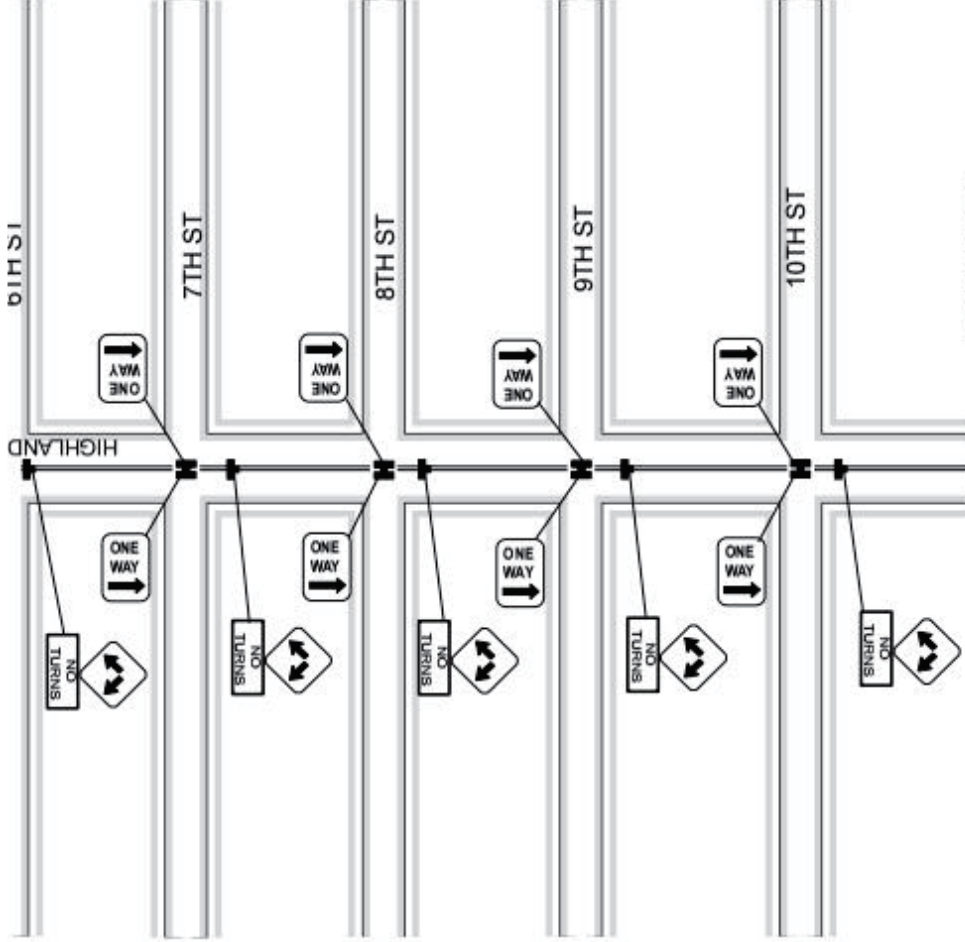
3333 E 36TH ST
TUCSON, AZ 85713
PHONE: (520)-293-1063

U OF A FOOTBALL 2021

T21RW04186

SAME CONCEPT FOR
CHERRY AVE

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RL 09/10/2021

PLAN 4 PROJECT NAME: U OF A PROJECT NUMBER: WILD CAT FOOTBALL PROJECT CONTRACTOR: U OF A PROJECT CONTRACTOR NUMBER: NATIONAL BARRICADE PROJECT NUMBER: U OF A SHEET NUMBER: 4 PREPARED BY: ALEX GRANILLO DATE: 09/08/2021 PROJECT NUMBER: U OF A		POST-GAME PLAN NOT TO SCALE NOTES: THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, AND FLAGS. BUSINESS, AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY		SIGN SIZES: oA) 18x24 oG) 48x60 oB) 24x24 oH) 18X18 oC) 24x30 oI) 24x48 oD) 30x30 oJ) 48x30 oE) 36x36 oK) OTHER oF) 48x48		LEGEND: SIGN STAND VERTICAL PANEL	
			REVIEWED BY _____		DATE _____		



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TUCSON, AZ 85713
PHONE: (520)-293-1063**

U OF A FOOTBALL 2021



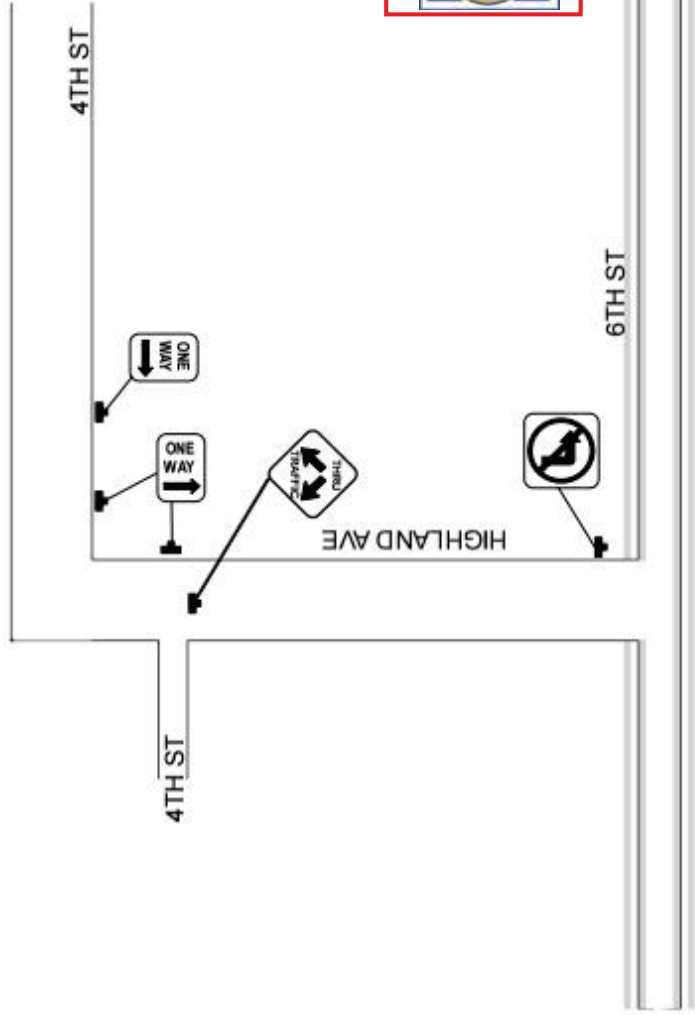
Department of Transportation and Mobility
 Temporary Traffic Control Permit is subject to the Applicant certifying Temporary Barricading Plan is in compliance with Tucson City Code, Chapter 25, Section 25-90
RL 09/10/2021

POST-GAME

PLAN 5	PROJECT NAME U OF A	PROJECT NUMBER U OF A	DATE 09/08/2021
PROJECT NAME WILD CAT FOOTBALL	TRAFFIC CONTROL CONTRACTOR U OF A NATIONAL BARRICADE	SCALE 5	ISSUED BY ALEX GRANILLO
START DATE	END DATE	REVIEWED BY	DATE
NOTES		PLAN NOT TO SCALE	LEGEND
<p>THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, BUSINESS, AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY.</p>		<p>○A) 18x24 ○G) 48x60 ○B) 24x24 ○H) 18X18 ○C) 24x30 ○I) 24x48 ○D) 30x30 ○J) 48x30 ○E) 36x36 ○K) OTHER ○F) 48x48</p>	<p>— SIGN STAND ● VERTICAL PANEL</p>

U OF A FOOTBALL 2021

T21RW04186



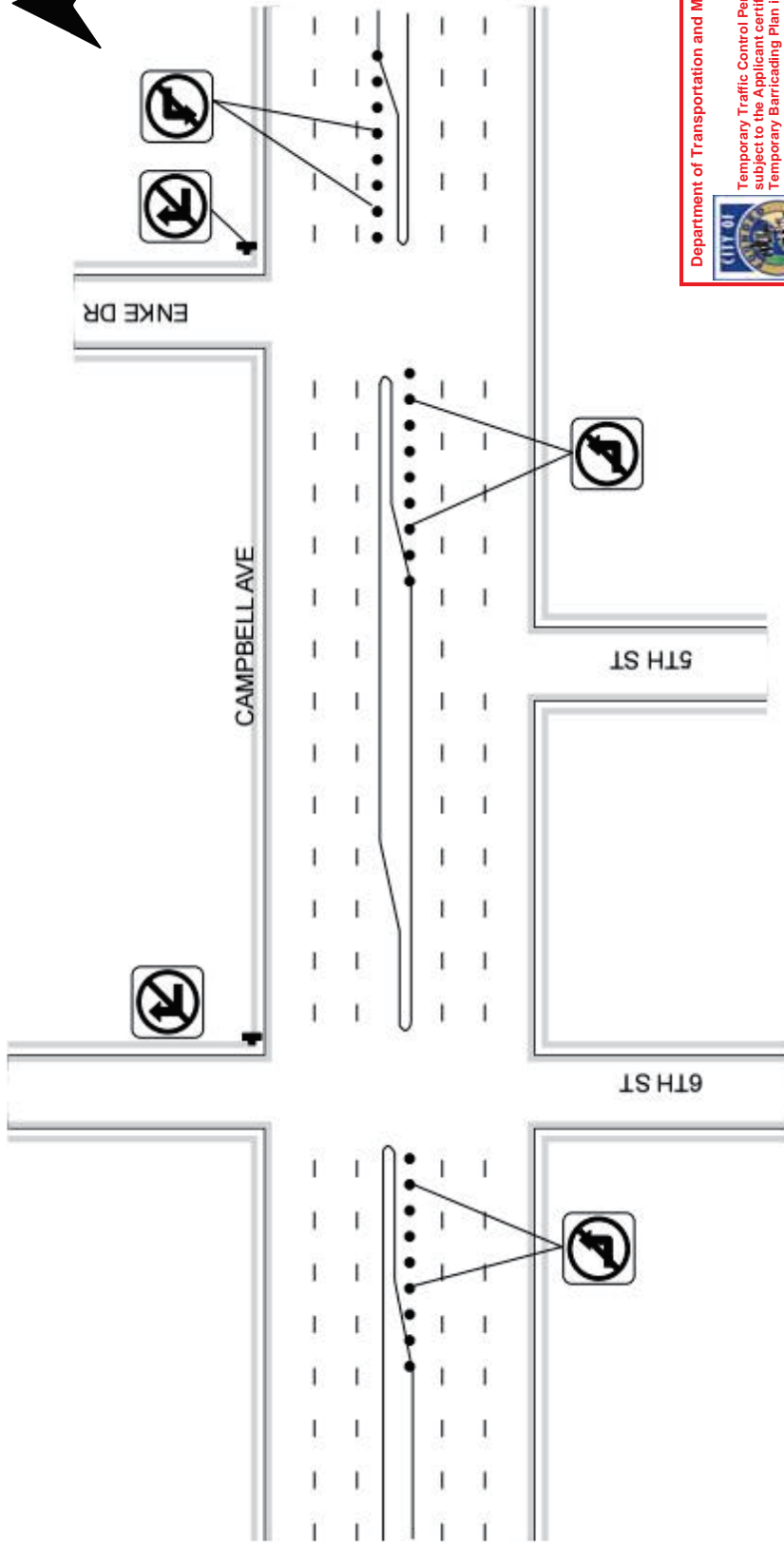
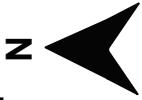
Department of Transportation and Mobility
 Temporary Traffic Control Permit is subject to the Applicant certifying Temporary Barricading Plan is in compliance with Tucson City Code, Chapter 25, Section 25-90
RL 09/10/2021



POST-GAME

PLAN 6 PROJECT NUMBER: U OF A PROJECT NAME: WILD CAT FOOTBALL PERMIT CONTRACTOR: U OF A NATIONAL BARRICADE PERMIT NUMBER: 6 PERMIT DATE: 09/08/2021 SUBMITTED BY: ALEX GRANILLO PERMIT NUMBER:		 3333 E 36TH ST TUCSON, AZ 85713 PHONE: (520)-293-1063		NOTES THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, AND FLAGS. BUSINESS AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY	PLAN NOT TO SCALE	SIGN SIZES (A) 18x24 (G) 48x60 (B) 24x24 (H) 18X18 (C) 24x30 (I) 24x48 (D) 30x30 (J) 48x30 (E) 36x36 (K) OTHER (F) 48x48	LEGEND SIGN STAND VERTICAL PANEL	REVIEWED BY _____ DATE _____
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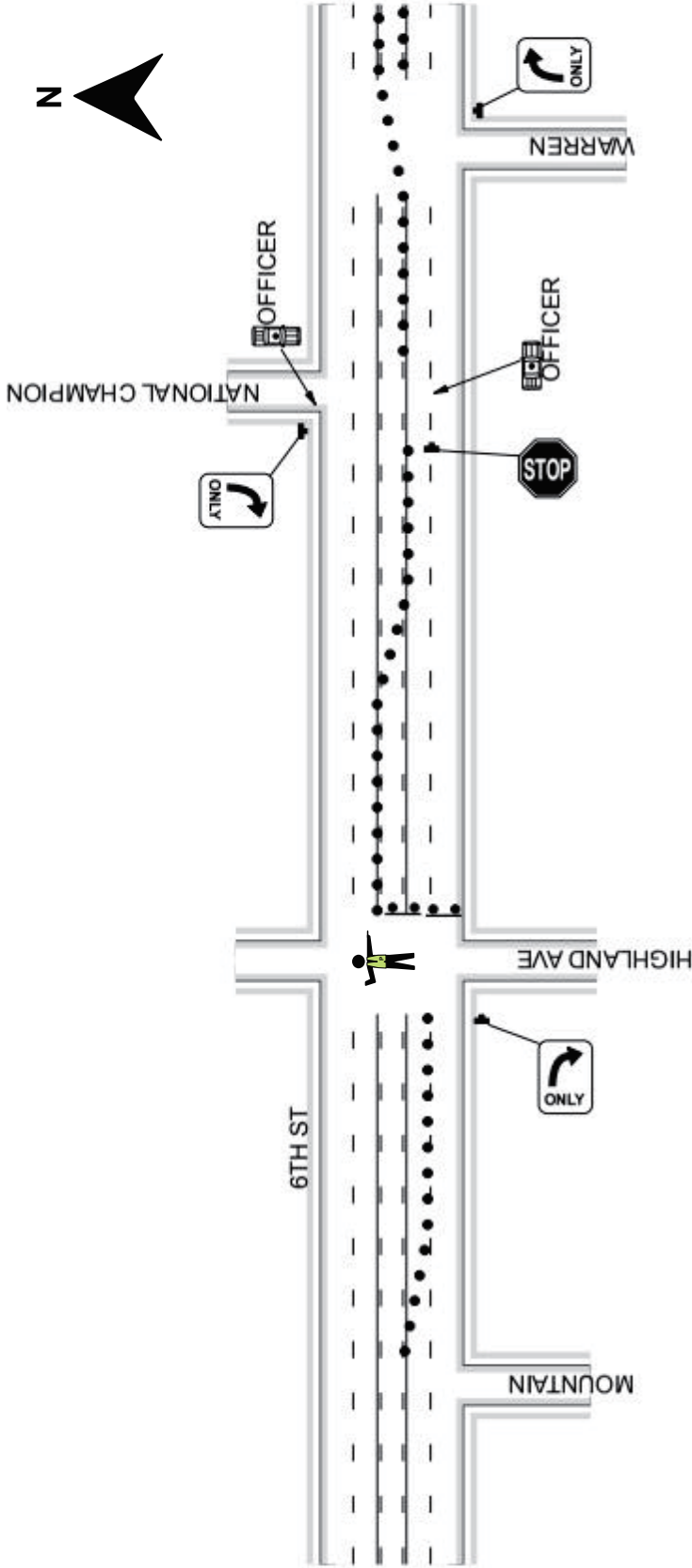
U OF A FOOTBALL 2021



Department of Transportation and Mobility
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RL 09/10/2021

PLAN 7		POST-GAME	
PROJECT NUMBER U OF A PROJECT TITLE WILD CAT FOOTBALL PERM CONTRACTOR U OF A PROJECT NUMBER NATIONAL BARRICADE U OF A SHEET NUMBER 7 PERMIT DATE 09/08/2021 ALEX GRANILLO WORK TITLE PERMIT NUMBER	 3333 E 36TH ST TUCSON, AZ 85713 PHONE: (520)-293-1063	NOTES THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, AND FLAGS. BUSINESS AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY	PLAN NOT TO SCALE
SIGN SIZES oA) 18x24 oG) 48x60 oB) 24x24 oH) 18X18 oC) 24x30 oI) 24x48 oD) 30x30 oJ) 48x30 oE) 36x36 oK) OTHER oF) 48x48		LEGEND SIGN STAND VERTICAL PANEL	
REVIEWED BY _____		DATE _____	

U OF A FOOTBALL 2021



Department of Transportation and Mobility



Temporary Traffic Control Permit is subject to the Applicant certifying Temporary Barricading Plan is in compliance with Tucson City Code, Chapter 25, Section 25-90

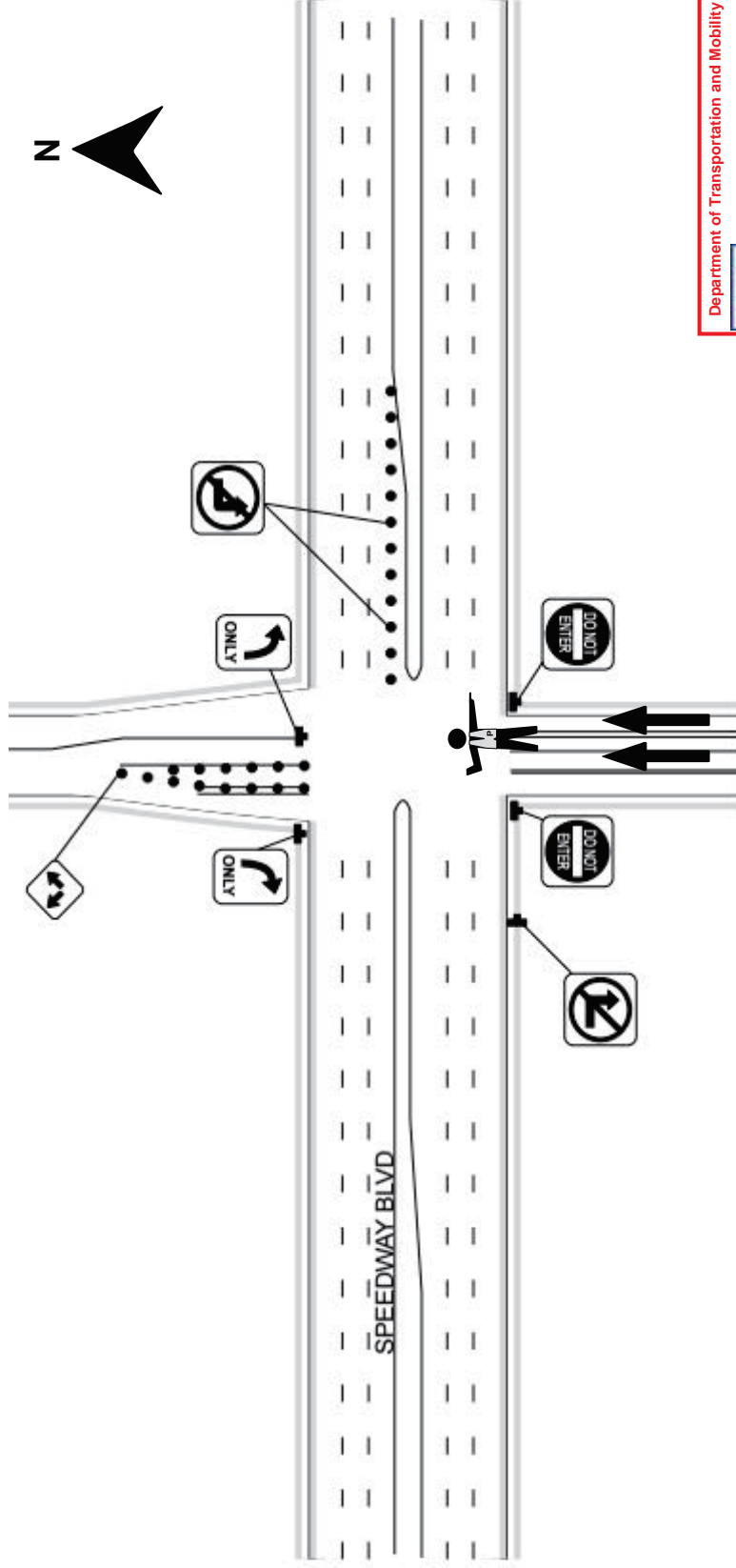
RL 09/10/2021

PLAN 8 JURISDICTION: U OF A PROJECT NAME: WILD CAT FOOTBALL PERMITS CONTRACTOR: U OF A NATIONAL BARRICADE PERMITS NUMBER: 8 START DATE: 09/08/2021 PREPARED BY: ALEX GRANILLO PERMITS NUMBER:		POST-GAME NOTES: PLAN NOT TO SCALE THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, AND FLAGS, AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY		SIGN SIZES OA) 18x24 OG) 48x60 OB) 24x24 OH) 18x18 OC) 24x30 OI) 24x48 OD) 30x30 OJ) 48x30 OE) 36x36 OK) OTHER OF) 48x48	LEGEND SIGN STAND OFFICER VERTICAL PANEL TYPE III BARRICADE
		REVIEWED BY _____ DATE _____			



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U OF A FOOTBALL 2021




Department of Transportation and Mobility



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POST-GAME

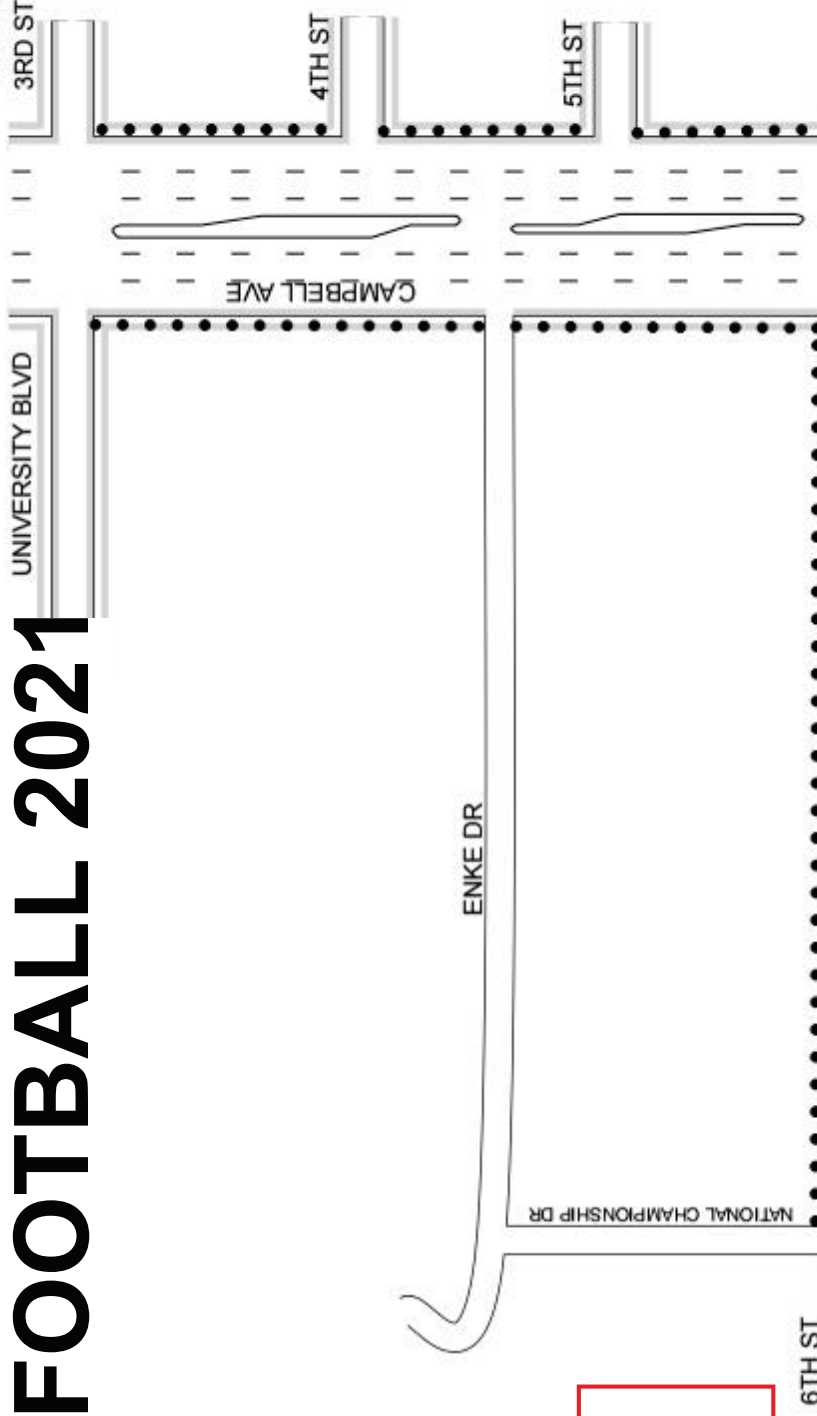
<p>PLAN 9</p> <p>JURISDICTION: U OF A</p> <p>PROJECT NAME: WILD CAT FOOTBALL</p> <p>OWNER/CONTRACTOR: U OF A NATIONAL BARRICADE</p> <p>PHONE: 9</p> <p>ISSUED DATE: 09/08/2021</p> <p>PREPARED BY: ALEX GRANILLO</p> <p>PROJECT NUMBER: _____</p>	 <p>3333 E 36TH ST TUCSON, AZ 85713 PHONE: (520)-293-1063</p>	<p>NOTES PLAN NOT TO SCALE</p> <p>THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, AND FLAGS. BUSINESS, AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY</p>	<p>SIGN SIZES</p> <p>oA) 18x24 oG) 48x60 oB) 24x24 oH) 18X18 oC) 24x30 oI) 24x48 oD) 30x30 oJ) 48x30 oE) 36x36 oK) OTHER oF) 48x48</p>	<p>LEGEND</p> <p>— SIGN STAND — OFFICER — VERTICAL PANEL ● TYPE III BARRICADE</p>
<p>REVIEWED BY _____ DATE _____</p>				

T21RW04186

U OF A FOOTBALL 2021



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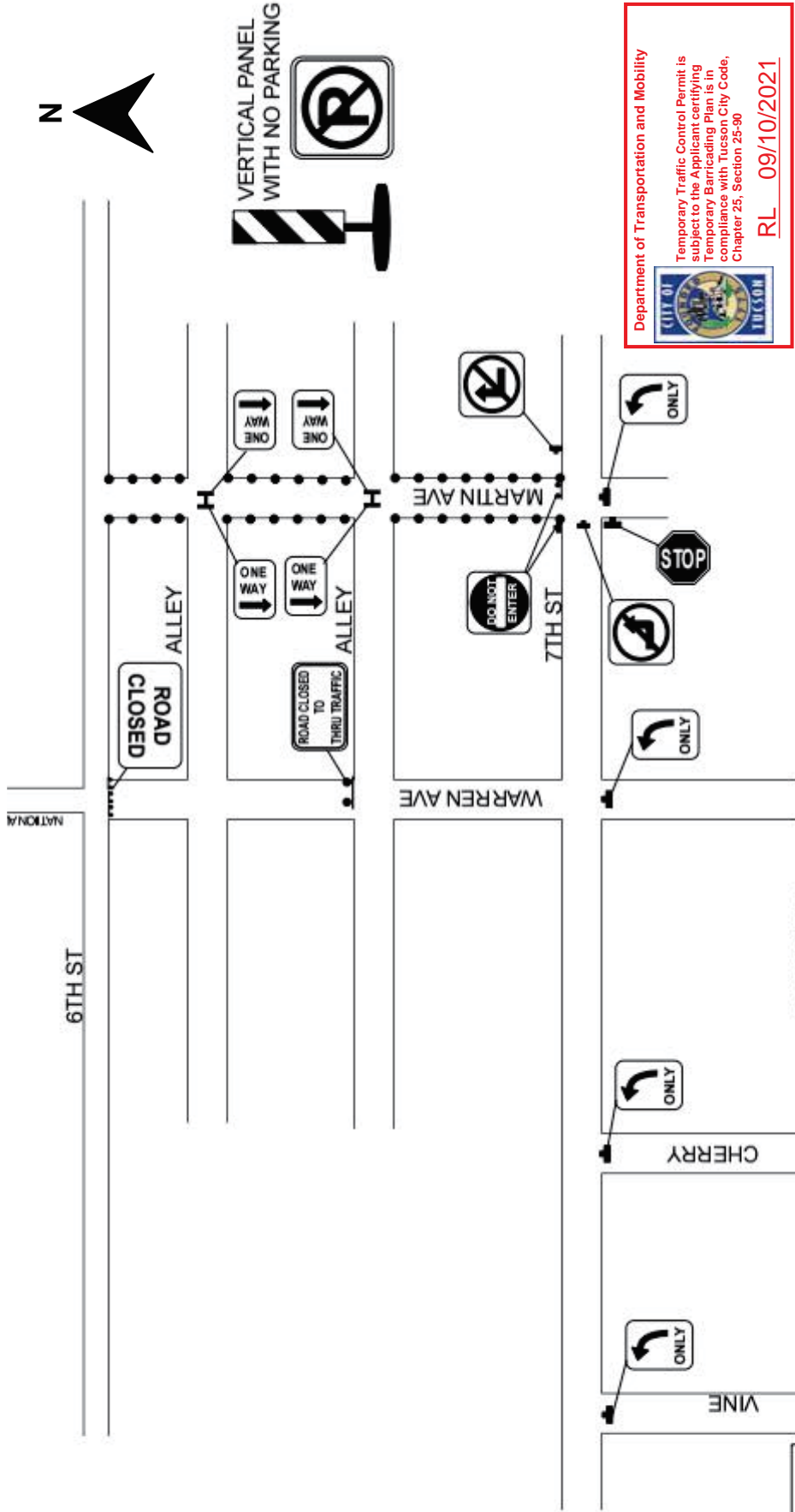
Temporary Traffic Control Permit is subject to the Applicant certifying Temporary Barricading Plan is in compliance with Tucson City Code, Chapter 25, Section 25-90

RL 09/10/2021

PLAN 10		U OF A	
PROJECT NAME WILD CAT FOOTBALL	PROJECT NUMBER	LEGEND	
TRAFFIC CONTROL CONTRACTOR U OF A NATIONAL BARRICADE	TRAFFIC CONTROL CONTRACTOR	SIGN SIZES	LEGEND
DATE 09/08/2021	DATE	○A) 18x24 ○G) 48x60 ○B) 24x24 ○H) 18X18 ○C) 24x30 ○I) 24x48 ○D) 30x30 ○J) 48x30 ○E) 36x36 ○K) OTHER ○F) 48x48	— SIGN STAND — VERTICAL PANEL ● TYPE III BARRICADE
ISSUED BY ALEX GRANILLO	ISSUED BY	NOTES	
START DATE	START DATE	PLAN NOT TO SCALE THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, AND FLAGS. BUSINESS, AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY	
PERMIT NUMBER	PERMIT NUMBER	REVIEWED BY	
		DATE	

PRE-GAME

U OF A FOOTBALL 2021



Department of Transportation and Mobility



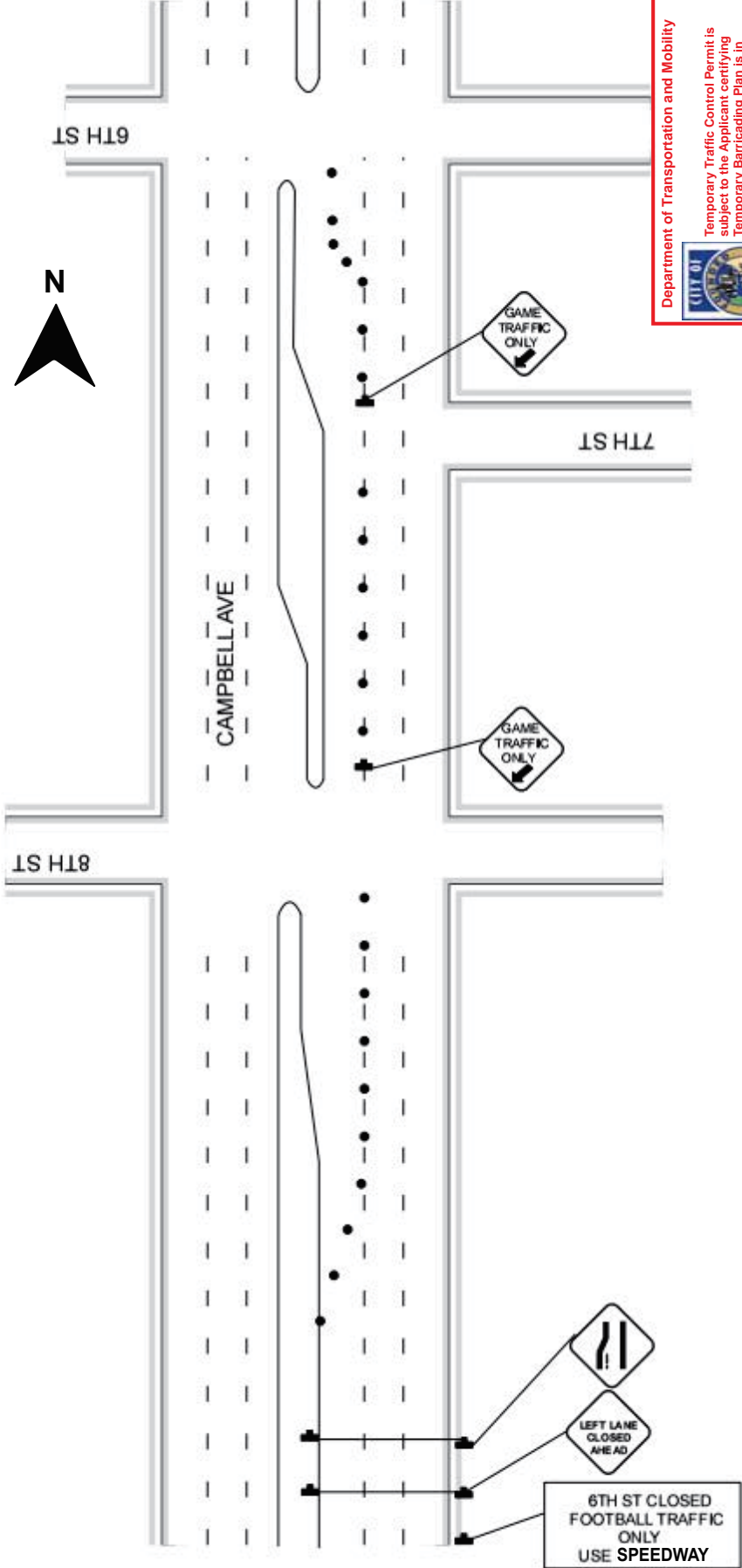
Temporary Traffic Control Permit is subject to the Applicant certifying Temporary Barricading Plan is in compliance with Tucson City Code, Chapter 25, Section 25-90

RL 09/10/2021

PRE-GAME

<p>PLAN 11</p> <p>JURISDICTION: U OF A</p> <p>PROJECT NAME: WILD CAT FOOTBALL</p> <p>PROJECT NUMBER: NATIONAL BARRICADE</p> <p>ISSUED BY: ALEX GRANILLO</p> <p>ISSUE DATE: 09/08/2021</p> <p>PROJECT NUMBER:</p>	<p>NOTES</p> <p>PLAN NOT TO SCALE</p> <p>THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, AND FLAGS. BUSINESS, AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY</p>	<p>SIGN SIZES</p> <p>oA) 18x24 oG) 48x60 oB) 24x24 oH) 18X18 oC) 24x30 oI) 24x48 oD) 30x30 oJ) 48x30 oE) 36x36 oK) OTHER oF) 48x48</p>	<p>LEGEND</p> <p>— SIGN STAND H — VERTICAL PANEL ■ TYPE III BARRICADE</p>
<p>WBC NATIONAL BARRICADE COMPANY</p> <p>3333 E 36TH ST TUCSON, AZ 85713 PHONE: (520)-293-1063</p>		<p>REVIEWED BY _____ DATE _____</p>	

U OF A FOOTBALL 2021



Department of Transportation and Mobility
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PLAN 12	PROJECT NAME U OF A	PROJECT NUMBER MILD CAT FOOTBALL
	TRAFFIC CONTROL CONTRACTOR U OF A	TRAFFIC CONTROL CONTRACTOR NATIONAL BARRICADE
	DATE 12	DATE 09/08/2021
	PREPARED BY ALEX GRANILLO	SCALE AS SHOWN
	START DATE	END DATE
	PROJECT NUMBER	

PRE-GAME
 PLAN NOT TO SCALE

NOTES
 THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, BUSINESS, AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY.

SIGN SIZES	LEGEND
oA) 18x24 oG) 48x60	— SIGN STAND
oB) 24x24 oH) 18X18	— VERTICAL PANEL
oC) 24x30 oI) 24x48	● TYPE III BARRICADE
oD) 30x30 oJ) 48x30	
oE) 36x36 oK) OTHER	
oF) 48x48	

REVIEWED BY	DATE

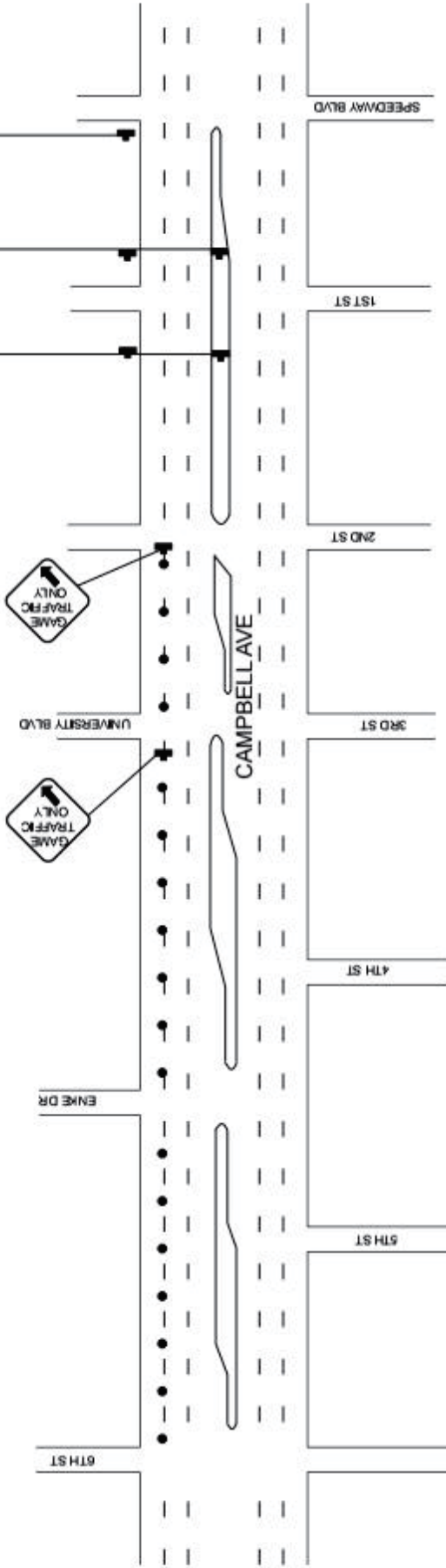


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U OF A FOOTBALL 2021



6TH ST CLOSED
USE BROADWAY
FOOTBALL TRAFFIC
ONLY



Department of Transportation and Mobility



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PRE-GAME

PLAN 13	PROJECT NAME U OF A	PROJECT NUMBER 13
PROJECT OWNER WILD CAT FOOTBALL	CONTRACTOR NATIONAL BARRICADE	DATE 09/08/2021
PREPARED BY ALEX GRANILLO	PHONE 3333 E 36TH ST TUCSON, AZ 85713 PHONE: (520)-293-1063	WORK DATES
PROJECT NUMBER		

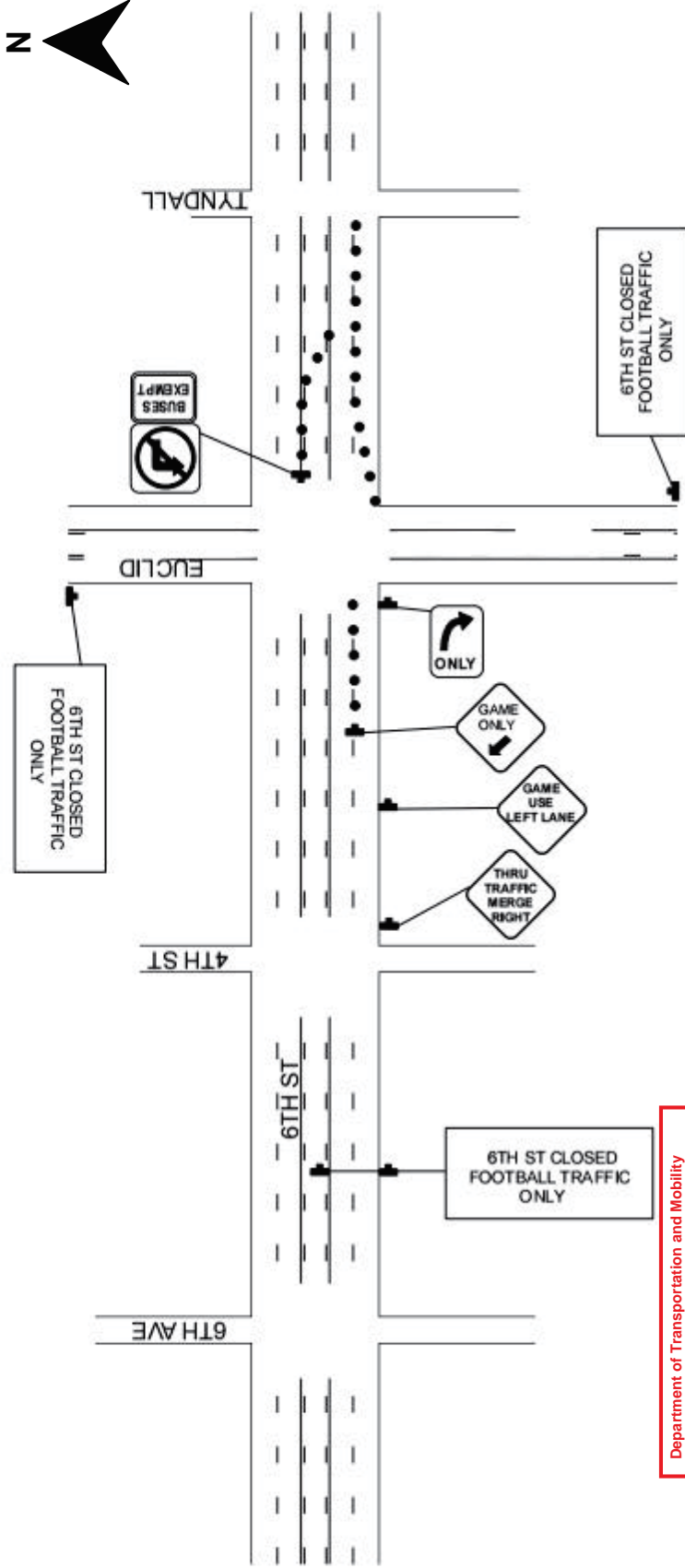


NOTES PLAN NOT TO SCALE

THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, AND FLAGS. BUSINESS, AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY

SIGN SIZES	LEGEND
oA) 18x24 oG) 48x60	— SIGN STAND
oB) 24x24 oH) 18x18	— VERTICAL PANEL
oC) 24x30 oI) 24x48	— TYPE III BARRICADE
oD) 30x30 oJ) 48x30	
oE) 36x36 oK) OTHER	
oF) 48x48	
REVIEWED BY	DATE

U OF A FOOTBALL 2021

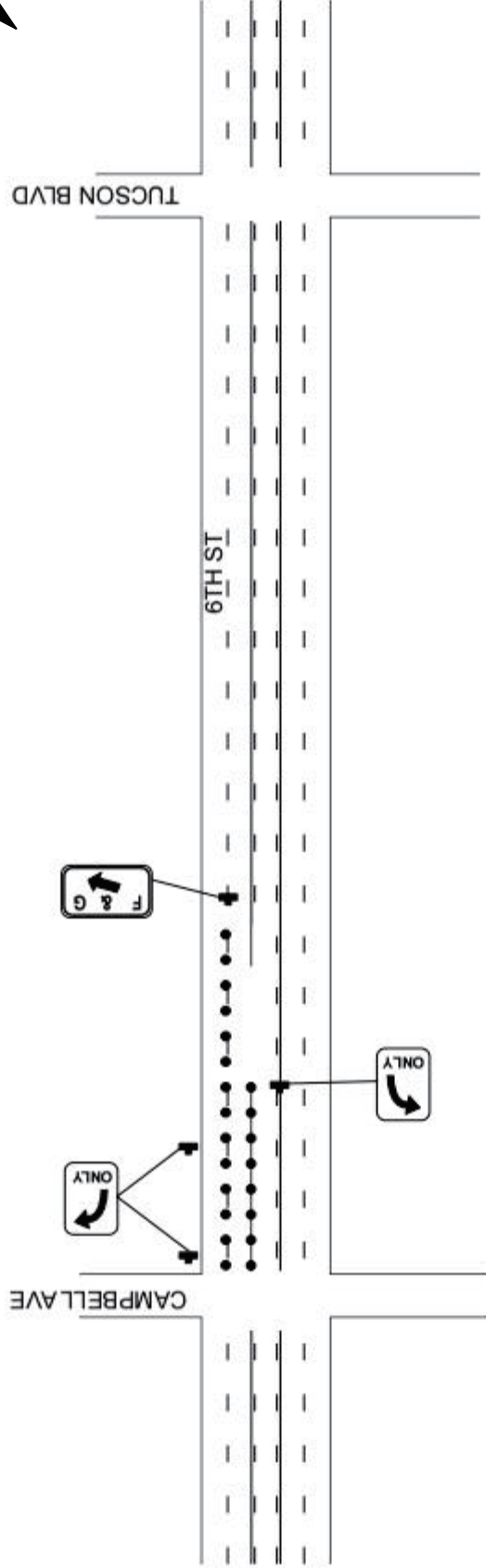


Department of Transportation and Mobility
 Temporary Traffic Control Permit is subject to the Applicant certifying Temporary Barricading Plan is in compliance with Tucson City Code, Chapter 25, Section 25-90
RL 09/10/2021

PRE-GAME

PLAN 14	NOTES	PLAN NOT TO SCALE	LEGEND
PROJECT NUMBER WILD CAT FOOTBALL U OF A NATIONAL BARRICADE SHEET NUMBER 14 PREPARED BY ALEX GRANILLO DATE 09/08/2021	THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, BUSINESS, AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY.	SIGN SIZES oA) 18x24 oG) 48x60 oB) 24x24 oH) 18X18 oC) 24x30 oI) 24x48 oD) 30x30 oJ) 48x30 oE) 36x36 oK) OTHER oF) 48x48	SIGN STAND VERTICAL PANEL TYPE III BARRICADE
PROJECT NAME WILD CAT FOOTBALL U OF A NATIONAL BARRICADE SHEET NUMBER 14 PREPARED BY ALEX GRANILLO DATE 09/08/2021	3333 E 36TH ST TUCSON, AZ 85713 PHONE: (520)-293-1063		REVIEWED BY _____ DATE _____

U OF A FOOTBALL 2021



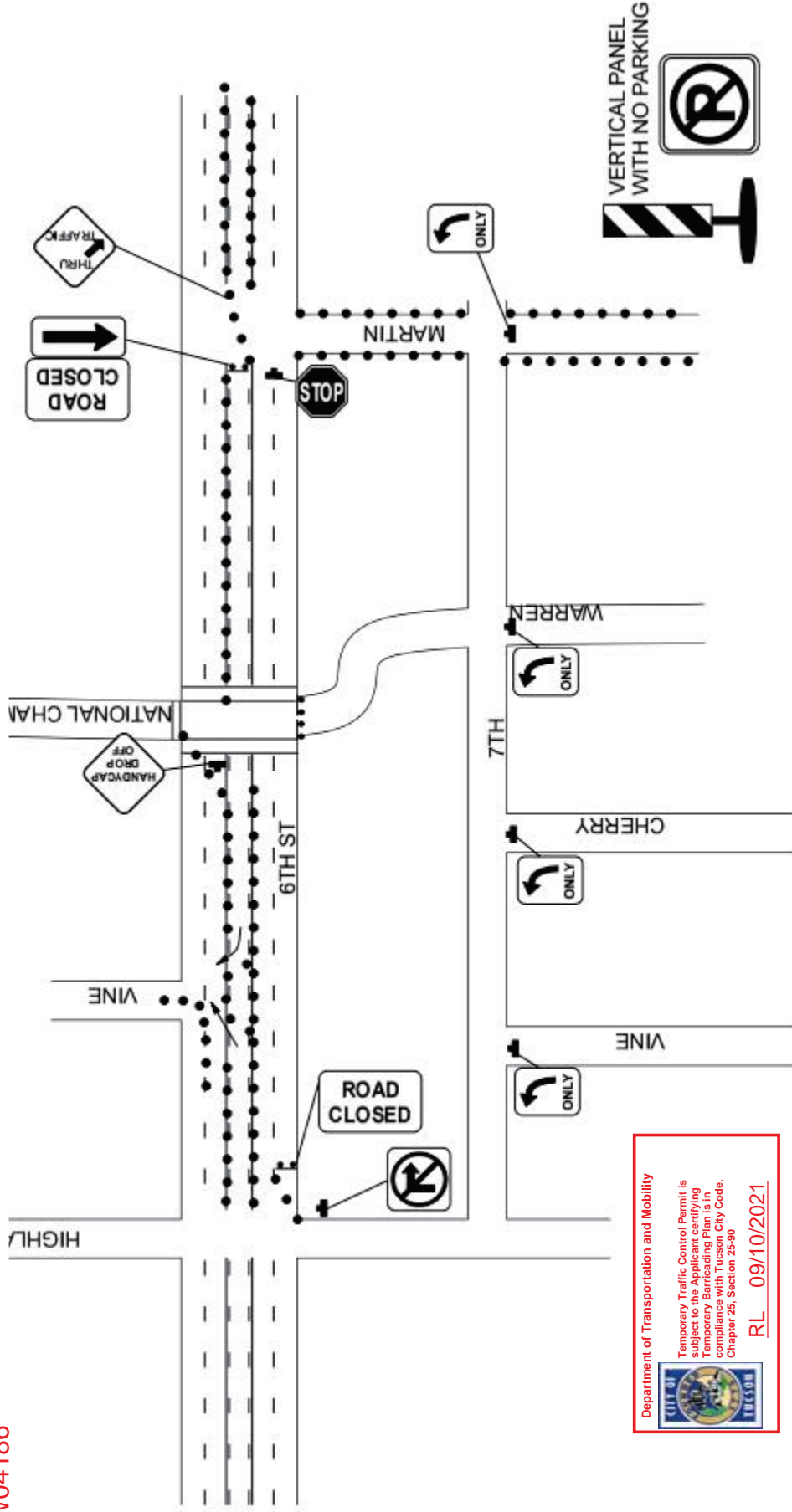
Department of Transportation and Mobility
 Temporary Traffic Control Permit is subject to the Applicant certifying Temporary Barricading Plan is in compliance with Tucson City Code, Chapter 25, Section 25-90
RL 09/10/2021

PRE-GAME

PLAN 15	NOTES	SIGN SIZES	LEGEND
PROJECT NAME WILD CAT FOOTBALL PERMIT CONTRIBUTOR U OF A NATIONAL BARRICADE PERMIT NUMBER 15 PREPARED BY ALEX GRANILLO START DATE 09/08/2021 WORK DATES	PLAN NOT TO SCALE THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, AND FLAGS. BUSINESS, AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY	SIGN SIZES oA) 18x24 oG) 48x60 oB) 24x24 oH) 18x18 oC) 24x30 oI) 24x48 oD) 30x30 oJ) 48x30 oE) 36x36 oK) OTHER oF) 48x48	LEGEND SIGN STAND VERTICAL PANEL TYPE III BARRICADE
PROJECT NUMBER U OF A PROJECT NUMBER NATIONAL BARRICADE PERMIT NUMBER 15 PREPARED BY ALEX GRANILLO START DATE 09/08/2021 WORK DATES	NATIONAL BARRICADE COMPANY 3333 E 36TH ST TUCSON, AZ 85713 PHONE: (520)-293-1063	REVIEWED BY DATE	

U OF A FOOTBALL 2021

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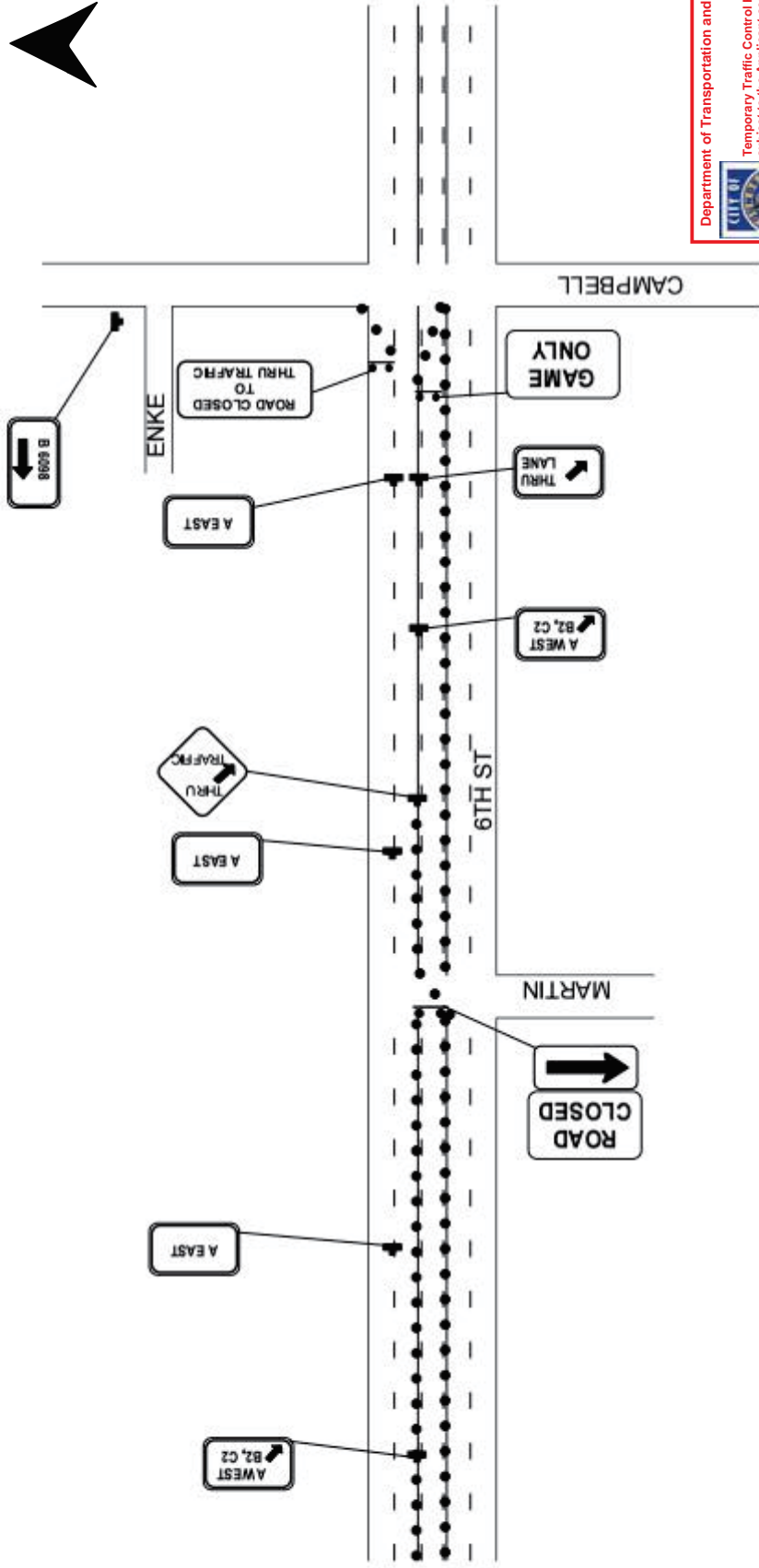
Department of Transportation and Mobility
 Temporary Traffic Control Permit is subject to the Applicant certifying Temporary Traffic Control Plan is in compliance with Tucson City Code, Chapter 25, Section 25-90
RL 09/10/2021

PRE-GAME

PLAN 16	PROJECT NUMBER U OF A	PROJECT NAME WILD CAT FOOTBALL	PROJECT NUMBER NATIONAL BARRICADE
DATE 16	DATE 09/08/2021	PREPARED BY ALEX GRANILLO	DATE 09/10/2021
3333 E 36TH ST TUCSON, AZ 85713 PHONE: (520)-293-1063			

NOTES	PLAN NOT TO SCALE	LEGEND
THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, AND FLAGS. BUSINESS AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY	SIGN SIZES (A) 18x24 (G) 48x60 (B) 24x24 (H) 18X18 (C) 24x30 (I) 24x48 (D) 30x30 (J) 48x30 (E) 36x36 (K) OTHER (F) 48x48	SIGN STAND VERTICAL PANEL TYPE III BARRICADE
REVIEWED BY _____	DATE _____	

U OF A FOOTBALL 2021



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RL 09/10/2021

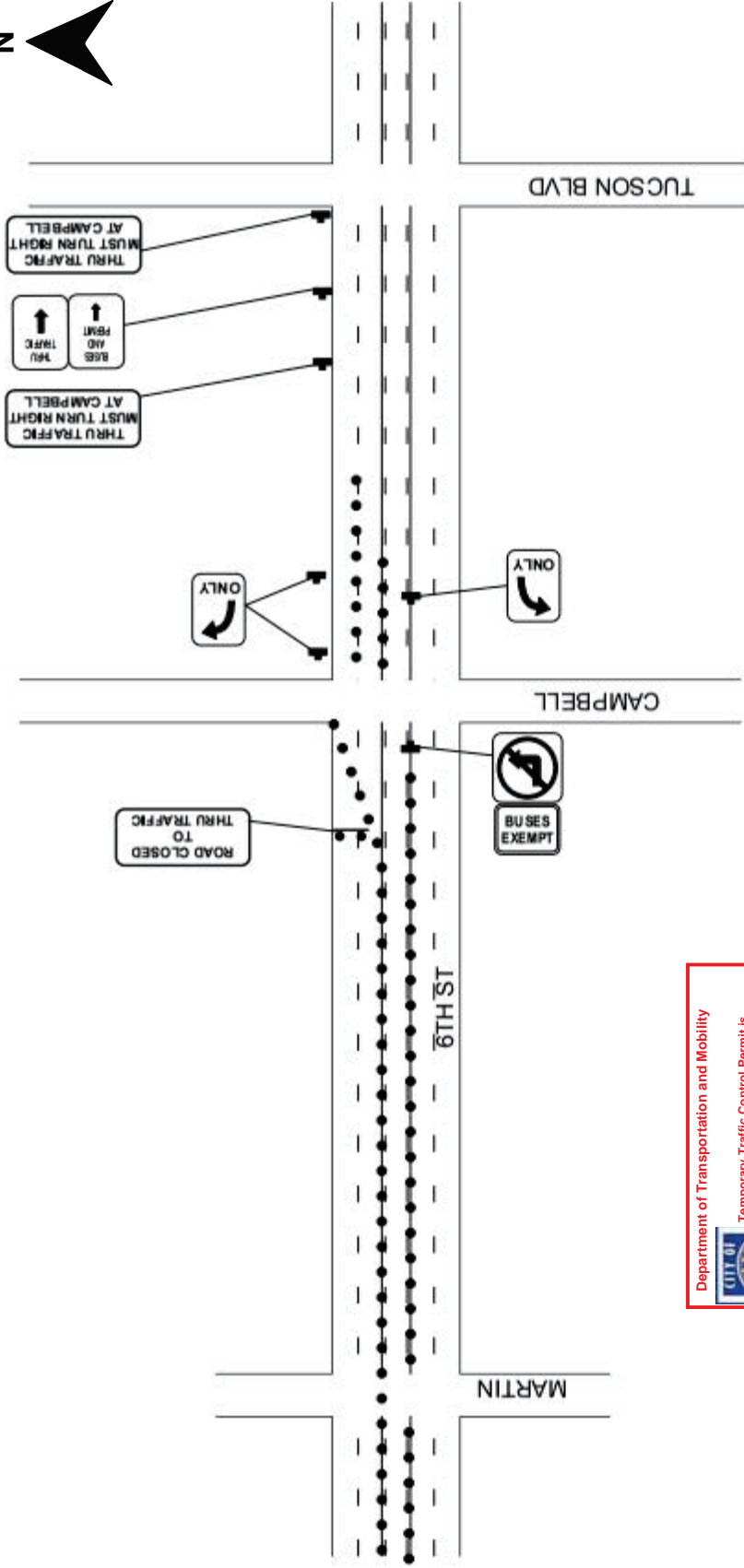
PRE-GAME

PLAN 17	PROJECT NAME WILD CAT FOOTBALL	PROJECT NUMBER U OF A	NOTES	PLAN NOT TO SCALE	SIGN SIZES	LEGEND
CLIENT U OF A	CONTRACTOR NATIONAL BARRICADE	DATE 09/08/2021	THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, AND FLAGS. BUSINESS, AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY	oA) 18x24 oG) 48x60 oB) 24x24 oH) 18X18 oC) 24x30 oI) 24x48 oD) 30x30 oJ) 48x30 oE) 36x36 oK) OTHER oF) 48x48	— SIGN STAND — VERTICAL PANEL — TYPE III BARRICADE	REVIEWED BY _____ DATE _____
PREPARED BY ALEX GRANILLO	PROJECT ADDRESS 3333 E 36TH ST TUCSON, AZ 85713 PHONE: (520)-293-1063	PROJECT NUMBER				



3333 E 36TH ST
TUCSON, AZ 85713
PHONE: (520)-293-1063

U OF A FOOTBALL 2021



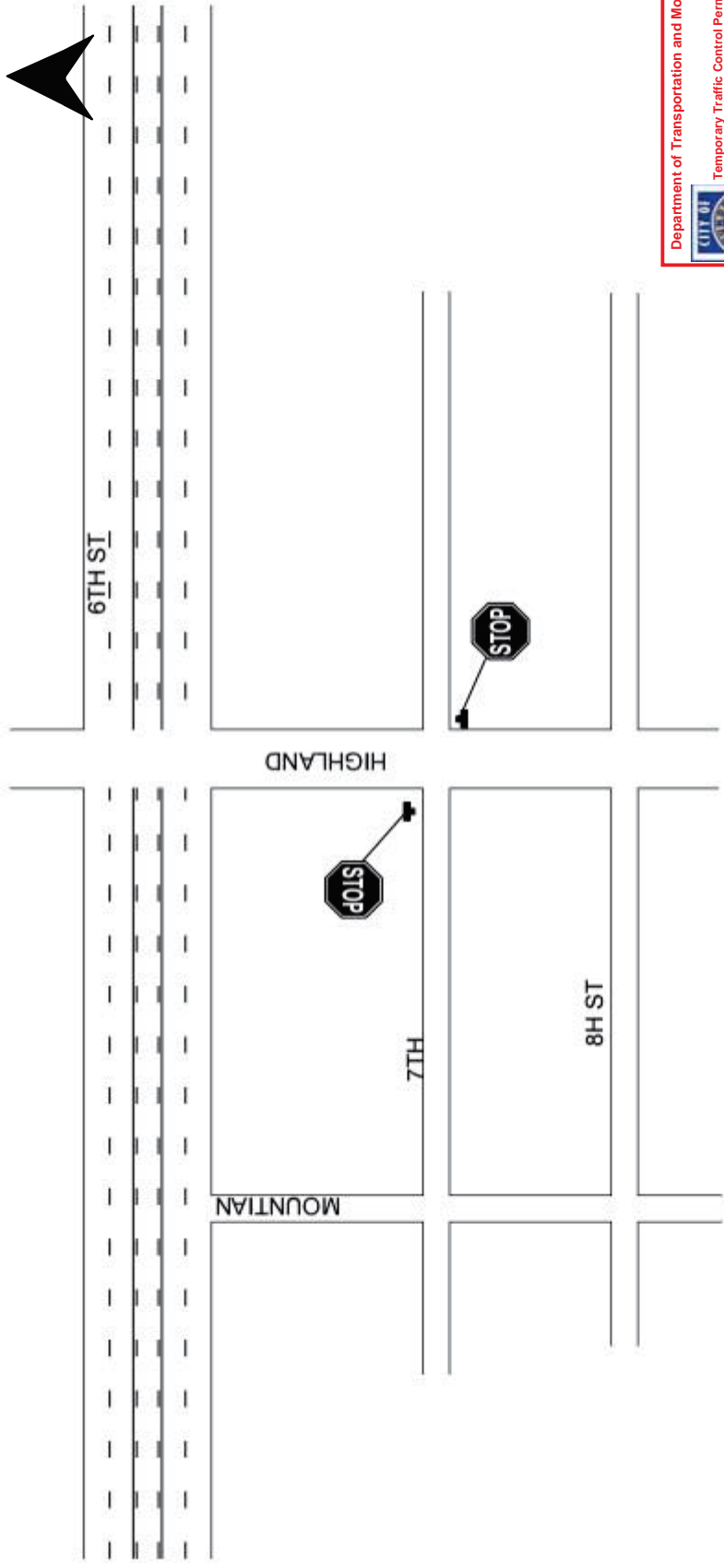
Department of Transportation and Mobility
 Temporary Traffic Control Permit is subject to the Applicant certifying Temporary Barricading Plan is in compliance with Tucson City Code, Chapter 23, Section 23-90
RL 09/10/2021

PLAN 18	PRE-GAME	NOTES	PLAN NOT TO SCALE	LEGEND
PROJECT NAME: U OF A PROJECT NUMBER: WILD CAT FOOTBALL OWNER: U OF A CONTRACTOR: NATIONAL BARRICADE PROJECT NUMBER: U OF A SHEET NUMBER: 18 PREPARED BY: ALEX GRANILLO START DATE: 09/08/2021 SIGN NUMBER:	THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, AND FLAGS. BUSINESS, AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY.	SIGN SIZES: oA) 18x24 oG) 48x60 oB) 24x24 oH) 18x18 oC) 24x30 oI) 24x48 oD) 30x30 oJ) 48x30 oE) 36x36 oK) OTHER oF) 48x48	SIGN STAND VERTICAL PANEL TYPE III BARRICADE	REVIEWED BY: _____ DATE: _____



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PRE-GAME

PLAN 19	PROJECT NUMBER	U OF A	PROJECT NUMBER	U OF A
	PROJECT NAME	WILD CAT FOOTBALL	PROJECT NUMBER	U OF A
	PRIME CONTRACTOR	U OF A	TRAFFIC CONTROL CONTRACTOR	NATIONAL BARRICADE
	ISSUE DATE	19	ISSUE DATE	09/08/2021
	PREPARED BY	ALEX GRANILLO	ISSUE DATE	
	PROJECT NUMBER		ISSUE DATE	



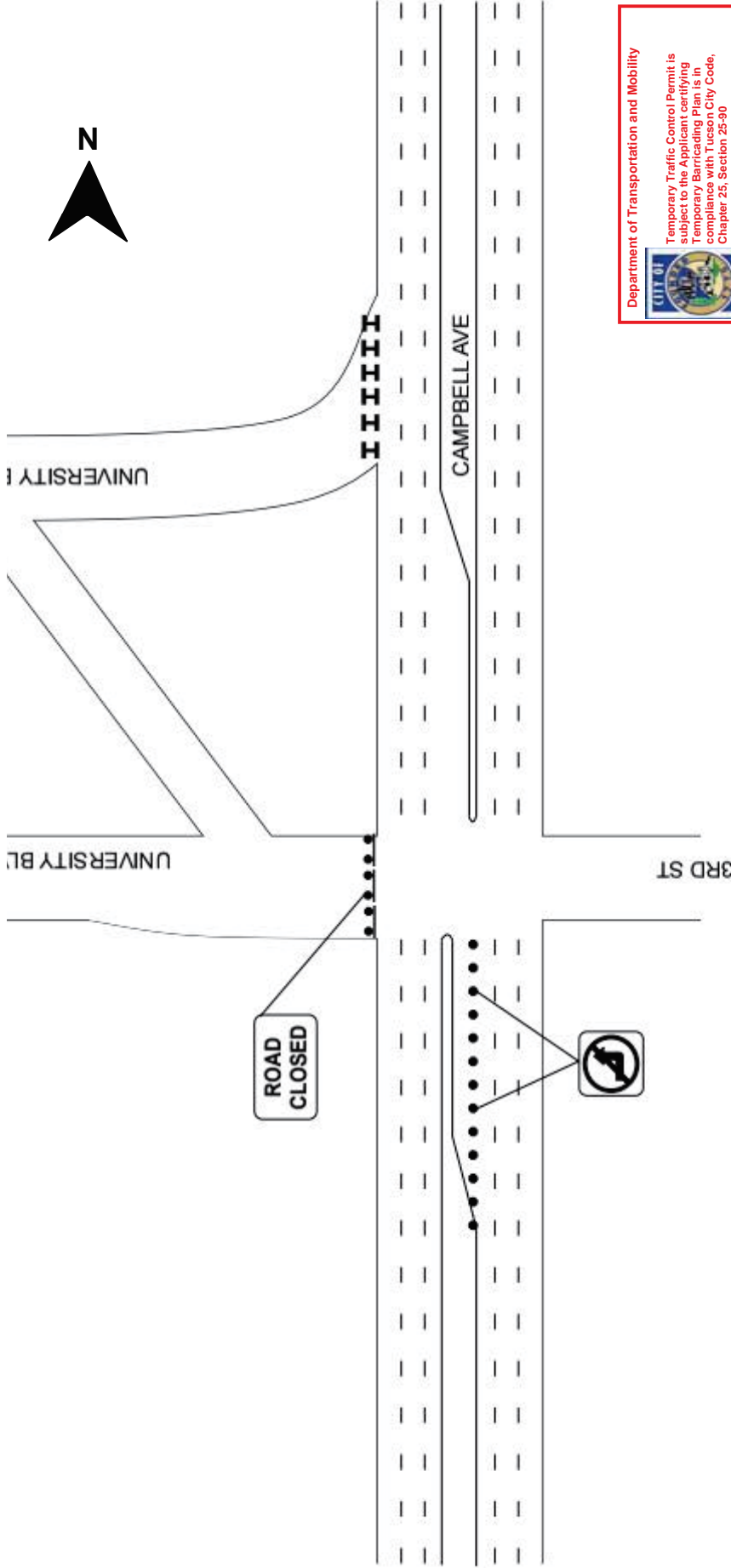
**3333 E 36TH ST
TUCSON, AZ 85713
PHONE: (520)-293-1063**

NOTES	PLAN NOT TO SCALE
THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, AND FLAGS, AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY	

SIGN SIZES	LEGEND
oA) 18x24	— SIGN STAND
oB) 24x24	— VERTICAL PANEL
oC) 24x30	● TYPE III BARRICADE
oD) 30x30	
oE) 36x36	
oF) 48x48	
oG) 48x60	
oH) 18X18	
oI) 24x48	
oJ) 48x30	
oK) OTHER	
oL) 48x30	

REVIEWED BY _____ DATE _____

U OF A FOOTBALL 2021



Department of Transportation and Mobility



Temporary Traffic Control Permit is subject to the Applicant certifying Temporary Barricading Plan is in compliance with Tucson City Code, Chapter 25, Section 25-90

RL 09/10/2021

PLAN 20	PRE-GAME	PLAN NOT TO SCALE	LEGEND
PROJECT NAME U OF A PROJECT NUMBER WILD CAT FOOTBALL PROJECT CONTRACTOR NATIONAL BARRICADE PROJECT NUMBER 20 SUBMIT DATE 09/08/2021 PREPARED BY ALEX GRANILLO PROJECT NUMBER	NOTES THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, AND FLAGS. BUSINESS, AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY	SIGN SIZES oA) 18x24 oG) 48x60 oB) 24x24 oH) 18X18 oC) 24x30 oI) 24x48 oD) 30x30 oJ) 48x30 oE) 36x36 oK) OTHER oF) 48x48	SIGN STAND H TYPE II BARRICADE VERTICAL PANEL TYPE III BARRICADE
3333 E 36TH ST TUCSON, AZ 85713 PHONE: (520)-293-1063		REVIEWED BY _____ DATE _____	

U OF A FOOTBALL 2021



Department of Transportation and Mobility
 Temporary Traffic Control Permit is subject to the Applicant certifying Temporary Barricading Plan is in compliance with Tucson City Code, Chapter 25, Section 25-90
RL 09/10/2021



PLAN 21 JURISDICTION: U OF A PROJECT NAME: WILD CAT FOOTBALL OWNER: U OF A NATIONAL BARRICADE PROJECT NUMBER: 21 SHEET NUMBER: 09/08/2021 DESIGNED BY: ALEX GRANILLO TICKET NUMBER:		 3333 E 36TH ST TUCSON, AZ 85713 PHONE: (520)-293-1063		NOTES THIS IS A VEHICULAR PLAN ONLY. THIS PLAN IS SUBJECT TO CHANGES IN THE FIELD. ALL APPLICABLE EQUIPMENT SHALL HAVE SAND BAGS, AND FLAGS. BUSINESS, AND LOCAL ACCESS SHALL BE MAINTAINED WHEN POSSIBLE. CONFLICTING SIGNS SHALL BE COVERED WHILE TEMPORARY TRAFFIC CONTROL IS IN PLACE. BUMP SIGNS SHALL BE PLACED PRIOR TO ANY STEEL PLATES IN THE ROADWAY	PLAN NOT TO SCALE	LEGEND SIGN STAND TYPE II BARRICADE VERTICAL PANEL TYPE III BARRICADE	SIGN SIZES oA) 18x24 oG) 48x60 oB) 24x24 oH) 18X18 oC) 24x30 oI) 24x48 oD) 30x30 oJ) 48x30 oE) 36x36 oK) OTHER oF) 48x48	REVIEWED BY _____ DATE _____
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DEPARTMENT OF TRANSPORTATION ENGINEERING

201 N. Stone Avenue, 4th Floor, Tucson, AZ 85701
Phone: (520) 791-5100

RECEIPT

ACTIVITY #: T21RW04186

FEES RECEIPT #: TC2104973

Date: 09/10/2021

Time: 04:49 PM

Permit Location:

565 N CHERRY AV TUC

Composition Type: ROW

Activity Description:

ARIZONA FOOTBALL 2021 HOME GAMES

ARIZONA HOME FOOTBALL GAMES, FALL 2021. 5 SATURDAYS, 1
FRIDAY THIS SEASON

SET UP TIME WILL BE 3 HOURS PRIOR TO KICK-OFF

TAKE DOWN HOURS WILL BE APPROX 4.5 HOURS AFTER KICK-OFF

Applicant: ARIZONA FOOTBALL 2021 HOME GAMES

SUZY MASON

PO BOX 210096 85721

520-909-5625

PAID BY:

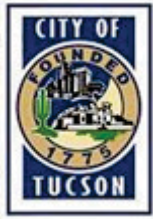
Type	Method	Description	Amount
Payment	Other		275.00

Notation: 2021253015-2 i payment

FEES PAID:

Item	Account Code	Description	Curnt Pmts
8660	015-491-8636-01	BARRICADE - PERMIT FEES	225.00
8661	015-491-8636-02	BARRICADE - PLAN REVIEW	50.00

Issued by: LINZUNZA



DEPARTMENT OF TRANSPORTATION ENGINEERING

201 N. Stone Avenue, 4th Floor, Tucson, AZ 85701
Phone: (520) 791-5100

TOTAL :

275.00



Appendix Q
Special Event Segment
Volume Figures

Site Code: 1
6TH ST E.O CAMPBELL AVE

Start Time	27-Aug-22 Sat	EB	WB	Total
12:00 AM		133	90	223
01:00		107	41	148
02:00		119	32	151
03:00		35	17	52
04:00		23	20	43
05:00		40	45	85
06:00		70	87	157
07:00		154	184	338
08:00		244	264	508
09:00		313	378	691
10:00		397	420	817
11:00		466	434	900
12:00 PM		465	472	937
01:00		486	510	996
02:00		489	435	924
03:00		497	528	1025
04:00		453	499	952
05:00		406	493	899
06:00		394	510	904
07:00		357	418	775
08:00		344	344	688
09:00		306	281	587
10:00		236	237	473
11:00		186	145	331
Total		6720	6884	13604
Percent		49.4%	50.6%	
AM Peak	-	11:00	11:00	-
Vol.	-	466	434	-
PM Peak	-	15:00	15:00	-
Vol.	-	497	528	-
Grand Total		6720	6884	13604
Percent		49.4%	50.6%	
ADT		ADT 13,604	ADT 13,604	AADT 13,604

Start Time	27-Aug-22 Sat	EB	WB	Total
12:00 AM		109	76	185
01:00		98	31	129
02:00		112	33	145
03:00		36	13	49
04:00		20	18	38
05:00		40	46	86
06:00		68	76	144
07:00		129	174	303
08:00		218	281	499
09:00		308	401	709
10:00		372	417	789
11:00		442	440	882
12:00 PM		458	450	908
01:00		496	553	1049
02:00		487	452	939
03:00		476	475	951
04:00		440	469	909
05:00		406	434	840
06:00		370	482	852
07:00		321	367	688
08:00		343	292	635
09:00		277	244	521
10:00		220	186	406
11:00		160	124	284
Total		6406	6534	12940
Percent		49.5%	50.5%	
AM Peak	-	11:00	11:00	-
Vol.	-	442	440	-
PM Peak	-	13:00	13:00	-
Vol.	-	496	553	-
Grand Total		6406	6534	12940
Percent		49.5%	50.5%	
ADT		ADT 13,604	ADT 13,604	AADT 13,604

Start Time	27-Aug-22 Sat	EB	WB	Total
12:00 AM		83	42	125
01:00		60	20	80
02:00		73	20	93
03:00		20	11	31
04:00		19	12	31
05:00		30	42	72
06:00		58	61	119
07:00		137	168	305
08:00		229	273	502
09:00		290	388	678
10:00		352	420	772
11:00		442	461	903
12:00 PM		461	509	970
01:00		463	568	1031
02:00		434	505	939
03:00		445	487	932
04:00		423	471	894
05:00		357	434	791
06:00		304	478	782
07:00		290	317	607
08:00		282	256	538
09:00		215	186	401
10:00		161	155	316
11:00		111	81	192
Total		5739	6365	12104
Percent		47.4%	52.6%	
AM Peak	-	11:00	11:00	-
Vol.	-	442	461	-
PM Peak	-	13:00	13:00	-
Vol.	-	463	568	-
Grand Total		5739	6365	12104
Percent		47.4%	52.6%	
ADT		ADT 12,104	ADT 12,104	AADT 12,104

Start Time	27-Aug-22	EB	WB	Total
12:00 AM	Sat	69	30	99
01:00		33	14	47
02:00		42	18	60
03:00		20	10	30
04:00		12	17	29
05:00		37	33	70
06:00		82	68	150
07:00		150	144	294
08:00		213	177	390
09:00		290	271	561
10:00		293	303	596
11:00		330	320	650
12:00 PM		406	351	757
01:00		365	361	726
02:00		364	305	669
03:00		357	296	653
04:00		356	310	666
05:00		300	270	570
06:00		238	276	514
07:00		191	202	393
08:00		196	158	354
09:00		124	112	236
10:00		122	91	213
11:00		82	61	143
Total		4672	4198	8870
Percent		52.7%	47.3%	
AM Peak	-	11:00	11:00	-
Vol.	-	330	320	-
PM Peak	-	12:00	13:00	-
Vol.	-	406	361	-
Grand Total		4672	4198	8870
Percent		52.7%	47.3%	
ADT		ADT 8,870	ADT 8,870	ADT 8,870

Site Code: 5
5TH ST E.O CRAYCROFT RD

Start Time	27-Aug-22 Sat	EB	WB	Total
12:00 AM		28	20	48
01:00		15	13	28
02:00		20	17	37
03:00		11	7	18
04:00		6	6	12
05:00		18	16	34
06:00		34	42	76
07:00		90	86	176
08:00		105	139	244
09:00		131	183	314
10:00		164	205	369
11:00		175	224	399
12:00 PM		201	268	469
01:00		218	279	497
02:00		193	233	426
03:00		197	214	411
04:00		161	245	406
05:00		145	210	355
06:00		131	188	319
07:00		86	146	232
08:00		83	98	181
09:00		53	81	134
10:00		39	52	91
11:00		20	31	51
Total		2324	3003	5327
Percent		43.6%	56.4%	
AM Peak	-	11:00	11:00	-
Vol.	-	175	224	-
PM Peak	-	13:00	13:00	-
Vol.	-	218	279	-
Grand Total		2324	3003	5327
Percent		43.6%	56.4%	
ADT		ADT 5,327	AADT 5,327	

Start Time	10-Sep-22 Sat	EB	WB	Total
12:00 AM		145	78	223
01:00		74	44	118
02:00		94	37	131
03:00		31	19	50
04:00		18	17	35
05:00		25	37	62
06:00		86	83	169
07:00		128	178	306
08:00		218	262	480
09:00		308	351	659
10:00		414	377	791
11:00		446	450	896
12:00 PM		506	461	967
01:00		489	485	974
02:00		466	471	937
03:00		460	517	977
04:00		429	565	994
05:00		319	616	935
06:00		269	661	930
07:00		218	530	748
08:00		226	254	480
09:00		207	182	389
10:00		351	183	534
11:00		478	148	626
Total		6405	7006	13411
Percent		47.8%	52.2%	
AM Peak	-	11:00	11:00	-
Vol.	-	446	450	-
PM Peak	-	12:00	18:00	-
Vol.	-	506	661	-
Grand Total		6405	7006	13411
Percent		47.8%	52.2%	
ADT		ADT 13,411	ADT 13,411	AADT 13,411

Site Code: 2
6TH ST E.O TUCSON BLVD

Start Time	10-Sep-22 Sat	EB	WB	Total
12:00 AM		125	49	174
01:00		65	39	104
02:00		85	31	116
03:00		30	16	46
04:00		16	16	32
05:00		26	35	61
06:00		77	70	147
07:00		119	147	266
08:00		228	226	454
09:00		328	302	630
10:00		384	304	688
11:00		451	352	803
12:00 PM		434	342	776
01:00		475	376	851
02:00		468	364	832
03:00		431	382	813
04:00		432	402	834
05:00		338	490	828
06:00		246	488	734
07:00		212	329	541
08:00		218	191	409
09:00		192	143	335
10:00		283	120	403
11:00		379	105	484
Total		6042	5319	11361
Percent		53.2%	46.8%	
AM Peak	-	11:00	11:00	-
Vol.	-	451	352	-
PM Peak	-	13:00	17:00	-
Vol.	-	475	490	-
Grand Total		6042	5319	11361
Percent		53.2%	46.8%	
ADT		ADT 11,361	ADT 11,361	AADT 11,361

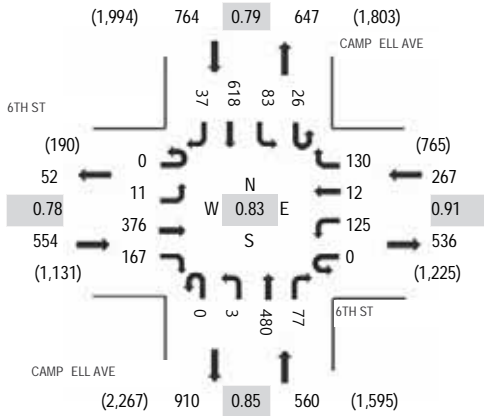
Start Time	10-Sep-22 Sat	EB	WB	Total
12:00 AM		90	43	133
01:00		42	29	71
02:00		55	18	73
03:00		26	13	39
04:00		15	11	26
05:00		28	37	65
06:00		69	77	146
07:00		119	178	297
08:00		229	290	519
09:00		292	345	637
10:00		369	409	778
11:00		415	467	882
12:00 PM		428	465	893
01:00		447	520	967
02:00		442	466	908
03:00		391	488	879
04:00		409	497	906
05:00		298	577	875
06:00		310	589	899
07:00		237	407	644
08:00		181	207	388
09:00		160	154	314
10:00		228	124	352
11:00		270	106	376
Total		5550	6517	12067
Percent		46.0%	54.0%	
AM Peak	-	11:00	11:00	-
Vol.	-	415	467	-
PM Peak	-	13:00	18:00	-
Vol.	-	447	589	-
Grand Total		5550	6517	12067
Percent		46.0%	54.0%	
ADT		ADT 12,067	ADT 12,067	AADT 12,067

Start Time	10-Sep-22 Sat	EB	WB	Total
12:00 AM		44	23	67
01:00		19	22	41
02:00		34	17	51
03:00		19	15	34
04:00		15	16	31
05:00		58	52	110
06:00		79	61	140
07:00		143	149	292
08:00		165	175	340
09:00		236	246	482
10:00		317	281	598
11:00		309	281	590
12:00 PM		304	276	580
01:00		332	384	716
02:00		328	282	610
03:00		340	323	663
04:00		316	304	620
05:00		290	336	626
06:00		210	331	541
07:00		174	225	399
08:00		142	132	274
09:00		126	101	227
10:00		153	77	230
11:00		170	52	222
Total		4323	4161	8484
Percent		51.0%	49.0%	
AM Peak	-	10:00	10:00	-
Vol.	-	317	281	-
PM Peak	-	15:00	13:00	-
Vol.	-	340	384	-
Grand Total		4323	4161	8484
Percent		51.0%	49.0%	
ADT		ADT 8,484	ADT 8,484	AADT 8,484

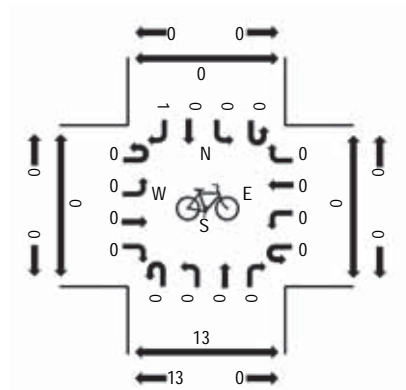
Site Code: 5
5TH ST E.O CRAYCROFT RD

Start Time	10-Sep-22 Sat	EB	WB	Total
12:00 AM		27	8	35
01:00		15	14	29
02:00		16	15	31
03:00		12	7	19
04:00		6	8	14
05:00		19	15	34
06:00		34	42	76
07:00		91	87	178
08:00		107	142	249
09:00		135	187	322
10:00		168	209	377
11:00		179	228	407
12:00 PM		205	273	478
01:00		222	283	505
02:00		197	237	434
03:00		201	218	419
04:00		165	249	414
05:00		149	214	363
06:00		135	192	327
07:00		86	150	236
08:00		84	99	183
09:00		54	77	131
10:00		45	61	106
11:00		20	21	41
Total		2372	3036	5408
Percent		43.9%	56.1%	
AM Peak	-	11:00	11:00	-
Vol.	-	179	228	-
PM Peak	-	13:00	13:00	-
Vol.	-	222	283	-
Grand Total		2372	3036	5408
Percent		43.9%	56.1%	
ADT		ADT 5,408	AADT 5,408	

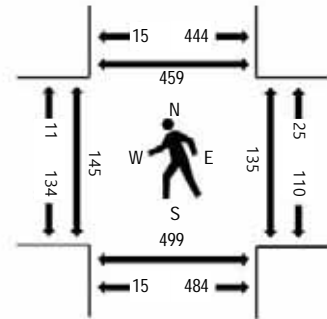
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	6TH ST Eastbound				6TH ST Westbound				CAMP ELL AVE Northbound				CAMP ELL AVE Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
9:00 PM	0	1	42	11	0	25	6	24	1	1	108	23	2	21	111	11	387	1,450	6	11	8	30
9:15 PM	0	4	26	8	0	29	4	25	0	0	91	18	4	15	116	8	348	1,564	1	4	14	8
9:30 PM	0	3	28	13	0	13	9	29	0	0	100	16	2	15	96	9	333	1,714	8	0	50	11
9:45 PM	0	5	30	14	0	28	2	32	0	0	93	24	4	19	120	11	382	1,814	21	9	34	28
10:00 PM	0	7	74	20	0	35	1	32	0	0	129	32	2	22	129	18	501	1,890	13	15	99	80
10:15 PM	0	4	81	43	0	27	7	36	1	0	121	25	3	14	118	18	498	1,812	12	18	67	67
10:30 PM	0	2	59	24	0	26	4	38	0	1	102	13	1	10	140	13	433	1,837	17	19	71	84
10:45 PM	0	7	43	28	0	32	1	33	0	1	111	24	1	15	149	13	458	1,954	21	10	48	23
11:00 PM	0	3	75	24	0	33	4	40	0	0	89	24	8	12	102	9	423	2,145	19	24	97	103
11:15 PM	0	4	83	40	0	29	5	33	0	1	121	12	4	23	157	11	523		43	24	118	103
11:30 PM	0	4	102	41	0	31	1	27	0	2	125	21	8	27	151	10	550		29	59	131	165
11:45 PM	0	0	116	62	0	32	2	30	0	0	145	20	6	21	208	7	649		54	28	153	88
Count Total	0	44	759	328	0	340	46	379	2	6	1,335	252	45	214	1,597	138	5,485		244	221	890	790
Peak Hour	0	11	376	167	0	125	12	130	0	3	480	77	26	83	618	37	2,145		145	135	499	459

Daily volume segments for Saturday gameday and no game

