

**This ERR will be available through 11/20/2023**

**Please direct comments to: Rolanda Mazeika,  
rolanda.mazeika@tucsonaz.gov or 520-668-4453**

**U.S. Department of Housing and Urban  
Development**  
451 Seventh Street, SW  
Washington, DC 20410  
[www.hud.gov](http://www.hud.gov)  
[espanol.hud.gov](http://espanol.hud.gov)

**Project Information**

**Project Name:** HOME-Corazon-del-Pueblo-Res-Infill-Proj-6-Lots

**HEROS Number:** 900000010353111

**Responsible Entity (RE):** TUCSON, PO Box 27210 Tucson AZ, 85726

**RE Preparer:** Rolanda Mazeika

**State / Local Identifier:**

**Certifying Officer:** Anna Rosenberry

**Grant Recipient (if different than Responsible Entity):**

**Point of Contact:**

**Consultant (if applicable):**

**Point of Contact:**

**Project Location:** 5746 E Calle De Hielo Dulce, Tucson, AZ 85756

**Additional Location Information:**

The Corazon del Pueblo Residential Infill Project includes the following properties: 5717, 5725, 5733 E Calle De La Apertura, 5716 E Camino De Las Iguanas, 5746 E Calle De Hielo Dulce, and 5782 E Camino Esperanza Eterna, Tucson, Pima County, Arizona, 85756, Pima County Assessor Parcel Numbers 140-55-0040, 140-55-0030, 140-55-0020, 140-55-0050, 140-55-0150, and 140-55-2950. The Corazon del Pueblo Subdivision is located north of Interstate 10, east of South Craycroft Road, south of East Littletown Road, and west of South Wilmot Road.

**Direct Comments to:** E-mail: [rolanda.mazeika@tucsonaz.gov](mailto:rolanda.mazeika@tucsonaz.gov) or  
Mail: City of Tucson Housing & Community Development  
Department, PO Box 27210, Tucson, Arizona 85726

**Description of the Proposed Project [24 CFR 50.12 & 58.32; 40 CFR 1508.25]:**

The Corazon del Pueblo Residential Infill Project (6 lots) provides for the construction of six, new, single-family, affordable housing units for sale to first-time homebuyers in the Corazon del Pueblo subdivision in the City of Tucson. The housing units will be constructed on vacant, previously undeveloped, residentially zoned lots in an existing urban subdivision. The lots have been previously cleared, graded, and leveled. Construction of the units will contribute to the completion of the build-out of the mixed-income subdivision, adding two- and three-bedroom, two-bath homes to the existing neighborhood. A new hybrid construction technology will be applied for the build-out of the project, combining wall system kits with on-site construction and assembly, which allows for accelerated construction timeframes and durability. Habitat for Humanity Tucson (HFHT), the property owner and project developer, is a state-certified Community Housing Development Organization. Home purchase opportunities are provided to households earning 40-80% of area median income and HFHT's home purchase model integrates home buyer education with designated volunteers who serve as family partners throughout the home building and financing process. Pre-purchase homebuyer counseling will be provided by Family Housing Resources. HFHT receives Energy Star certification on all housing units through a third-party inspection, and building features include efficient HVAC systems, R-19 rated insulation, R-48 rated roofs, dual pane windows, and water efficient fixtures and design elements. The project will comply with fair housing and accessibility requirements. Estimated project funding: \$240,000 in HOME Investment Partnerships Program (HOME) funds, grant number M-22-DC-04-0229 through the City of Tucson Housing & Community Development Department. The Corazon del Pueblo Residential Infill Project includes the following properties: 5717, 5725, 5733 E Calle De La Apertura, 5716 E Camino De Las Iguanas, 5746 E Calle De Hielo Dulce, and 5782 E Camino Esperanza Eterna, Tucson, Pima County, Arizona, 85756, Pima County Assessor Parcel Numbers 140-55-0040, 140-55-0030, 140-55-0020, 140-55-0050, 140-55-0150, and 140-55-2950. The Corazon del Pueblo Subdivision is located north of Interstate 10, east of South Craycroft Road, south of East Littletown Road, and west of South Wilmot Road.

**Statement of Purpose and Need for the Proposal [40 CFR 1508.9(b)]:**

This project supports the City of Tucson Consolidated Plan policies to develop affordable housing including: acquisition and/or rehabilitation of affordable rental, permanent supportive, and HOME-funded transitional housing; construction of affordable rental housing, including permanent supportive housing and transitional housing; monthly rental assistance and security and utility deposits; assistance for first-time home buyers; and construction of or acquisition/rehabilitation/resale of affordable homeownership housing. The project serves the program purpose of increasing the supply of much needed affordable housing in the City of Tucson. The project also supports the goals of Plan Tucson, the 2013 City of Tucson General &

Sustainability Plan, to provide housing, human services, public facilities, and economic development programs to address the housing needs of the most vulnerable populations ([tucsonaz.gov/Departments/Planning-Development-Services/Development-Tools-Resources/Plans#section-1](http://tucsonaz.gov/Departments/Planning-Development-Services/Development-Tools-Resources/Plans#section-1)).

**Existing Conditions and Trends [24 CFR 58.40(a)]:**

This project is located in the Corazon del Pueblo Subdivision, which lies north of Interstate 10, east of South Craycroft Road, south of East Littleton Road, and west of South Wilmot Road. The subdivision is mostly built out including infrastructure, improvements, and amenities. The main subdivision entrances are located off South Wilmot Road from the east and South Van Buren Avenue from the west. South Camino de la Humanidad runs north/south from South Van Buren Avenue to East Elvira Road/East Hackle Lane through the northern half of the subdivision. The subdivision is located less than one mile south of the Davis-Monthan Air Force Base, which has supported avionic operations since 1925. Tucson's regional transit system, Sun Tran, provides service to the nearby street, Craycroft Road, with bus stops within 1/2 mile walking distance of the neighborhood. One bus line travels along Craycroft Road and connects to Kino Gateway/Irvington Plaza and the Roy Laos Transit Center. Amenities within walking distance or directly available via the transit system include: medical providers and pharmacies; supermarkets and grocery; retail shopping; schools; and a park and recreation center. The lots to be developed are vacant, previously undeveloped, residentially zoned lots in an existing urban subdivision. The lots have been previously cleared, graded, and leveled. The adjoining sites to the infill lots consist of residential properties, streets, and detention/retention basins. The area is predominantly residential with some surrounding commercial and industrial uses. Commercial development/improvements are likely to continue along nearby arterial roads. Nearby single family residential construction is likely to continue east and west of the project location.

**Maps, photographs, and other documentation of project location and description:**

[2023-9-8 CDP Aerial Map.pdf](#)

[2023-9-15 CDP 5782 Site Visit Checklist.pdf](#)

[2023-9-15 CDP 5746 Site Visit Checklist.pdf](#)

[2023-9-15 CDP 5717 5725 5733 Site Visit Checklist.pdf](#)

[2023-9-15 CDP 5716 Site Visit Checklist.pdf](#)

[2023-9-8 CDP Aerial Map\(1\).pdf](#)

[2023 HFH CDP Model Home Plans.pdf](#)

**Determination:**

✓	Finding of No Significant Impact [24 CFR 58.40(g)(1); 40 CFR 1508.13] The project will not result in a significant impact on the quality of human environment
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	Finding of Significant Impact
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**Approval Documents:**

[2023-11-1 Signature Page.pdf](#)

**7015.15 certified by Certifying Officer** 10/9/2023  
on:

**7015.16 certified by Authorizing Officer**  
on:

**Funding Information**

Grant / Project Identification Number	HUD Program	Program Name
M-22-DC-04-0229	Community Planning and Development (CPD)	HOME Program

**Estimated Total HUD Funded, Assisted or Insured Amount:** \$240,000.00

**Estimated Total Project Cost [24 CFR 58.2 (a) (5)]:** \$1,611,426.00

**Compliance with 24 CFR §50.4, §58.5 and §58.6 Laws and Authorities**

Compliance Factors: Statutes, Executive Orders, and Regulations listed at 24 CFR §50.4, §58.5, and §58.6	Are formal compliance steps or mitigation required?	Compliance determination (See Appendix A for source determinations)
<b>STATUTES, EXECUTIVE ORDERS, AND REGULATIONS LISTED AT 24 CFR §50.4 &amp; § 58.6</b>		
<b>Airport Hazards</b> Clear Zones and Accident Potential Zones; 24 CFR Part 51 Subpart D	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	The project is within 15,000 feet of a military airport or within 2,500 of a civilian airport. However, it is not within an APZ or RPZ/CZ. The 5746 E Calle de Hielo Dulce site lies within the City of Tucson's Airport Hazard District. The final building elevation at this site is less

		than the 150-foot maximum building height allowance (Sheet 2 of 11, Corazon Del Pueblo Final Plat, 5/24/2005). The project is in compliance with Airport Hazards requirements.
<b>Coastal Barrier Resources Act</b> Coastal Barrier Resources Act, as amended by the Coastal Barrier Improvement Act of 1990 [16 USC 3501]	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	This project is located in a state that does not contain CBRS units. There are no CBRS units in Arizona. Therefore, this project is in compliance with the Coastal Barrier Resources Act.
<b>Flood Insurance</b> Flood Disaster Protection Act of 1973 and National Flood Insurance Reform Act of 1994 [42 USC 4001-4128 and 42 USC 5154a]	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	The structure or insurable property is not located in a FEMA-designated Special Flood Hazard Area. 5717, 5725, 5733 E Calle De La Apertura, 5716 E Camino De Las Iguanas, and 5782 E Camino Esperanza Eterna - FEMA Shaded Zone X (500-year floodplain), 04019C 2315L, 6/16/2011. 5746 E Calle De Hielo Dulce - FEMA Zone X, 04019C 2315L, 6/16/2011. While flood insurance may not be mandatory in this instance, HUD recommends that all insurable structures maintain flood insurance under the National Flood Insurance Program (NFIP). The project is in compliance with flood insurance requirements.
<b>STATUTES, EXECUTIVE ORDERS, AND REGULATIONS LISTED AT 24 CFR §50.4 &amp; § 58.5</b>		
<b>Air Quality</b> Clean Air Act, as amended, particularly section 176(c) & (d); 40 CFR Parts 6, 51, 93	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	The project's county or air quality management district is in non-attainment status for the following: Carbon monoxide (Tucson, maintenance), Particulate Matter, <10 microns (Ajo, maintenance & Rillito, non-attainment). The Arizona Department of Environmental Quality (ADEQ) and PDEQ have reviewed their air quality monitoring data have no concerns about single-family residences, multi-family, or small apartment complex development in existing residential areas changing or adversely impacting air quality control standards. Memo from Richard Grimaldi, PDEQ,

		dated 12/18/2019. This project does not exceed de minimis emissions levels or the screening level established by the state or air quality management district for the pollutant(s) identified above. The project is in compliance with the Clean Air Act.
<b>Coastal Zone Management Act</b> Coastal Zone Management Act, sections 307(c) & (d)	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	This project is located in a state that does not participate in the Coastal Zone Management Program. Arizona has no coastal areas. Therefore, this project is in compliance with the Coastal Zone Management Act.
<b>Contamination and Toxic Substances</b> 24 CFR 50.3(i) & 58.5(i)(2)]	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	On-site or nearby toxic, hazardous, or radioactive substances that could affect the health and safety of project occupants or conflict with the intended use of the property were not found. The project sites are not: 1) listed on an EPA Superfund National Priorities or CERCLA List, or equivalent State list; 2) located within 3,000 feet of a toxic or solid waste landfill site; or (3) have an underground storage tank. Field visit and site evaluation conducted by Rolanda Mazeika, City of Tucson, Environmental Project Coordinator, 9/15/2023. The project is in compliance with contamination and toxic substances requirements.
<b>Endangered Species Act</b> Endangered Species Act of 1973, particularly section 7; 50 CFR Part 402	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	This project will have No Effect on listed species due to the nature of the activities involved in the project. The project consists of infill construction of six residences in a long-occupied and developed urban subdivision. There are no critical habitats in the project area and the lots to be developed have been previously cleared, graded, and leveled. This project is in compliance with the Endangered Species Act.
<b>Explosive and Flammable Hazards</b> Above-Ground Tanks)[24 CFR Part 51 Subpart C	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	There is a current or planned stationary aboveground storage container of concern within 1 mile of the project site.

		One propane ASTs, three ASTs containing diesel/gasoline fuel, and one planned household hazardous waste AST were evaluated. The Separation Distances from the project are acceptable. The project is in compliance with explosive and flammable hazard requirements.
<b>Farmlands Protection</b> Farmland Protection Policy Act of 1981, particularly sections 1504(b) and 1541; 7 CFR Part 658	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	This project does not include any activities that could potentially convert agricultural land to a non-agricultural use. The site does not meet the definition of farmland per 7 CFR Part 658.2(a), "...Farmland" does not include land already in or committed to urban development or water storage..." The site is in a fully-developed, urban environment per the US Census Urban Area map printed 7/5/2022. The project is in compliance with the Farmland Protection Policy Act.
<b>Floodplain Management</b> Executive Order 11988, particularly section 2(a); 24 CFR Part 55	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	Five of the project sites are located in a 500-year floodplain. 5717, 5725, 5733 E Calle De La Apertura, 5716 E Camino De Las Iguanas, and 5782 E Camino Esperanza Eterna - FEMA Shaded Zone X (500-year floodplain), 04019C 2315L, 6/16/2011. 5746 E Calle De Hielo Dulce - FEMA Zone X, 04019C 2315L, 6/16/2011. Based on the project description this project is not a critical action, so an 8-Step process is not required. The project consists of new construction of single-family residences and does not include critical actions. The project is in compliance with Executive Order 11988.
<b>Historic Preservation</b> National Historic Preservation Act of 1966, particularly sections 106 and 110; 36 CFR Part 800	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	A cultural resources survey is in progress in accordance with the Programmatic Agreement dated 1/25/2002 (property outside an established historic district, project includes new construction activities).

		The project is in compliance with Section 106.
<p><b>Noise Abatement and Control</b> Noise Control Act of 1972, as amended by the Quiet Communities Act of 1978; 24 CFR Part 51 Subpart B</p>	<p><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p>	<p>A Noise Assessment was conducted. The noise level was acceptable: 64.0 db. See noise analysis. The 5746 East Calle de Hielo Dulce site is closest to the Davis Monthan Airforce Base and lies outside the associated DNL 65 noise contour. Intervening two-story homes are observed in the direction of the railroad, east arterial, and airbase (noise contributors). Home construction includes wall insulation values of R-19, ceiling values of R-48, and dual pane windows, as well as home layouts with garages facing noise contributors, that may contribute to higher sound transmission class ratings. The project is in compliance with HUD's Noise regulation.</p>
<p><b>Sole Source Aquifers</b> Safe Drinking Water Act of 1974, as amended, particularly section 1424(e); 40 CFR Part 149</p>	<p><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p>	<p>The project is located on a sole source aquifer. The region has an MOU or other working agreement with EPA for HUD projects impacting a sole source aquifer, and the MOU or working agreement excludes the project from further review. The project will utilize existing municipal water and wastewater facilities. The project is in compliance with Sole Source Aquifer requirements.</p>
<p><b>Wetlands Protection</b> Executive Order 11990, particularly sections 2 and 5</p>	<p><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p>	<p>The project will not impact on- or off-site wetlands. The 5746 E Calle De Hielo Dulce site lies closest to the Julian Wash riverine located approximately 368 feet northwest of the site. Very low slope across all neighborhood lots from north to south. Existing sheet flow to street traverses the neighborhood south/southwest to engineered drainage and detention basins throughout the subdivision. No sign of erosion in detention/retention basins. The lot has been previously cleared,</p>

		graded, and leveled, and will be prepared to engineering standards to handle flow appropriately. The project is in compliance with Executive Order 11990.
<b>Wild and Scenic Rivers Act</b> Wild and Scenic Rivers Act of 1968, particularly section 7(b) and (c)	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	This project is not within proximity of a NWSRS river. there are no NWSRS rivers in Southern Arizona. The project is in compliance with the Wild and Scenic Rivers Act.
<b>HUD HOUSING ENVIRONMENTAL STANDARDS</b>		
<b>ENVIRONMENTAL JUSTICE</b>		
<b>Environmental Justice</b> Executive Order 12898	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	No adverse environmental impacts were identified in the project's total environmental review. The project is in compliance with Executive Order 12898.

**Environmental Assessment Factors [24 CFR 58.40; Ref. 40 CFR 1508.8 &1508.27]**

**Impact Codes:** An impact code from the following list has been used to make the determination of impact for each factor.

- (1) Minor beneficial impact
- (2) No impact anticipated
- (3) Minor Adverse Impact – May require mitigation
- (4) Significant or potentially significant impact requiring avoidance or modification which may require an Environmental Impact Statement.

Environment al Assessment Factor	Impac t Code	Impact Evaluation	Mitigatio n
<b>LAND DEVELOPMENT</b>			
Conformance with Plans / Compatible Land Use and Zoning / Scale and Urban Design	2	The project consists of infill construction of six, single-family, affordable housing units for sale to first-time homebuyers in the Corazon del Pueblo subdivision. The newly constructed affordable housing units will conform within the surrounding neighborhood in terms of overall scale, density, size, and massing. The new construction activities will be permitted through the City of Tucson and Pima County. The site is consistent with local and regional planning efforts and has received planning and	

Environmental Assessment Factor	Impact Code	Impact Evaluation	Mitigation
		<p>zoning verification from the City of Tucson. The project site is zoned R-1. The project supports the goals of Plan Tucson, the City of Tucson General &amp; Sustainability Plan to provide housing, human services, public facilities, and economic development programs to address the housing needs of the most vulnerable populations.  <a href="http://www.tucsonaz.gov/Departments/PlanningDevelopment-Services/Development-ToolsResources/Plans#section-1">www.tucsonaz.gov/Departments/PlanningDevelopment-Services/Development-ToolsResources/Plans#section-1</a>. The project also supports the goals of the Tucson-Pima HUD 5 Year Consolidated Plan. Corazon del Pueblo Final Plat approved by Tucson Mayor and Council on 5/24/2005.</p>	
Soil Suitability / Slope/ Erosion / Drainage and Storm Water Runoff	2	<p>The project consists of infill construction of six, single-family, affordable housing units. No evidence of erosion, drainage/storm water runoff during site visit by HCD staff 9/15/2023. Very low slope across all neighborhood lots from north to south. Existing sheet flow to street traverses the neighborhood south/southwest to engineered drainage and detention basins throughout the subdivision. No sign of erosion in detention/retention basins. The lot has been previously cleared, graded, and leveled, and will be prepared to engineering standards to handle flow appropriately.</p>	
Hazards and Nuisances including Site Safety and Site-Generated Noise	2	<p>No evidence of onsite hazards or nuisances including soil contamination; proximity to high pressure pipelines or other volatile and explosive products; high-voltage transmission lines; radio/TV transmission towers; excessive smoke, fumes, odors subsidence, ground water, inadequate surface drainage, flood, etc. There are no visible poisonous plants or animals on the site. No known natural hazards exist. The sites have access to sidewalks, bike lanes, and other safety features within the residential neighborhood. Other than a brief period of construction there should be no increase in noise levels. Field visit and site evaluation conducted by</p>	

Environmental Assessment Factor	Impact Code	Impact Evaluation	Mitigation
		Rolanda Mazeika, City of Tucson, Environmental Project Coordinator, 9/15/2023.	
<b>SOCIOECONOMIC</b>			
Employment and Income Patterns	2	HFHT anticipates sustaining jobs in the construction industries as part of this project. Employment opportunities will be created for construction trades as part of this project. The project consists of infill construction of six, single-family, affordable housing units. The project will follow Section 3 requirements in all employment, construction, and subcontracting activities. The site is within walking distance or a short commute on public transit to retail, restaurant, and service businesses.	
Demographic Character Changes / Displacement	2	The project site is in an area of middle income, in an urban, low density, single-family, residential development, together with schools, parks, and other public services necessary for a satisfactory urban residential environment, the area has a 70.38% minority population, and approximately 83% of housing units being owner-occupied. The project consists of infill construction of six, single-family, affordable housing units.	
Environmental Justice EA Factor	2	The project site is in an area of middle income, in an urban, low density, single-family, residential development, together with schools, parks, and other public services necessary for a satisfactory urban residential environment, the area has a 70.38% minority population, and approximately 83% of housing units being owner-occupied. Elevated EJ Indexes are observed within one mile of the site related to the nearby traffic corridor, commercial/industrial hazardous waste generators, RMP Facilities, UST sites, air quality, State Superfund proximity (Los Reales Landfill WQARF site), and Davis-Monthan Air Force Base Department of Defense (DOD) site. The project consists of infill construction of six, single family, affordable housing units. On-site or nearby toxic, hazardous, or radioactive substances that could affect the health and safety of project occupants or conflict with the	

Environmental Assessment Factor	Impact Code	Impact Evaluation	Mitigation
		<p>intended use of the property were not found. The project supports the goals of City of Tucson Consolidated Plan policies to develop affordable housing including: acquisition and/or rehabilitation of affordable rental, permanent supportive, and HOME-funded transitional housing; construction of affordable rental housing, including permanent supportive housing and transitional housing; monthly rental assistance and security and utility deposits; assistance for first-time home buyers; and construction of or acquisition/rehabilitation/resale of affordable homeownership housing. Development of the Consolidated Plan included a robust consultation and public comment process including agencies/organizations whose operations and clientele would be directly impacted by the Plan's activities. The project will have minimal impact to neighboring residents/occupants while contributing to the build out of the existing mixed-income subdivision.</p>	
<b>COMMUNITY FACILITIES AND SERVICES</b>			
Educational and Cultural Facilities (Access and Capacity)	2	<p>The project consists of infill construction of six, single-family, affordable housing units with minor impact to area schools. Neighborhood residents are served by 5 preschool/daycare, elementary, and middle, and high schools within walking distance and/or a 6-minute driving distance. Educational and cultural enrichment opportunities are available at the nearby community/recreation center, neighborhood and community parks, and a nearby museum. Multimodal access to nearby educational and recreation facilities is available and is not anticipated to be impacted by foreseeable climate changes.</p>	
Commercial Facilities (Access and Proximity)	2	<p>The project consists of infill construction of six, single-family, affordable housing units. The project will not adversely impact or displace commercial facilities. The project site is within 4.5 miles of a major grocery store, markets, retail businesses,</p>	

Environmental Assessment Factor	Impact Code	Impact Evaluation	Mitigation
		medical providers, and discount stores. Multi-modal access to these facilities is available.	
Health Care / Social Services (Access and Capacity)	2	The project site is within 2 miles of a hospital annex with an emergency room. Emergency facilities, clinics, and physician services are accessible via public transit. The additional residents from this infill project will not have an adverse impact on the available services.	
Solid Waste Disposal and Recycling (Feasibility and Capacity)	2	Onsite waste disposal and recycling services are available through the City of Tucson Environmental & General Services Department. The City of Tucson provides extensive recycling options, including construction debris handling and recycling, landfill disposal, green waste recycling, and household hazardous waste disposal services. Construction debris generated during demolition will be routed to a nearby east Tucson Landfill or to a regional landfill depending on the category of waste material.	
Waste Water and Sanitary Sewers (Feasibility and Capacity)	2	The project consists of infill construction of six, single-family, affordable housing units. Pima County Wastewater provides wastewater and sewer service to the project site. Wastewater and sewer service is not anticipated to be impacted by foreseeable climate changes.	
Water Supply (Feasibility and Capacity)	2	The project consists of infill construction of six, single-family, affordable housing units. Tucson Water currently provides water service to the site. The project is located on a sole source aquifer. The region has an MOU or other working agreement with EPA for HUD projects impacting a sole source aquifer.	
Public Safety - Police, Fire and Emergency Medical	2	The project consists of infill construction of six, single-family, affordable housing units. The project site is within 3 miles of two fire stations. The site is within 7.5 miles of the Tucson Police Department Santa Cruz Substation. Response times vary depending on the type of call, but the average time for emergency response is five minutes or less. The project site is within 2 miles of a hospital annex with an emergency room.	

Environmental Assessment Factor	Impact Code	Impact Evaluation	Mitigation
Parks, Open Space and Recreation (Access and Capacity)	2	The project consists of infill construction of six, single-family, affordable housing units. The site is within two miles of the Littletown Community Center at the Thomas Jay Regional Park. The recreation center provides senior and youth program and activities. There are also pocket parks and walking trails within neighborhood. The nearby Julian Wash Greenway/Chuck Huckleberry Loop, "The Loop", with bike lanes and pedestrian paths, loops around the City of Tucson and connects to other bicycle routes.	
Transportation and Accessibility (Access and Capacity)	2	The main subdivision entrances are located off South Wilmot Road from the east and South Van Buren Avenue from the west. South Camino de la Humanidad runs north/south from South Van Buren Avenue to East Elvira Road/East Hackle Lane through the northern half of the subdivision. Traffic signs provide traffic control throughout the subdivision. The subdivision is bounded by the Julian Wash Greenway/Chuck Huckleberry Loop, "The Loop", with bicycle and pedestrian paths to the north and by the Interstate 10 highway to the south. The project sites are located on small residential streets with sidewalks and are within 1/2 mile of Craycroft Road. Tucson's regional transit system, Sun Tran, provides service to Craycroft Road. One bus line travels east/west from Craycroft Road to connecting services via the Roy Laos Transit Center. Small off-street parking spaces are also available throughout the neighborhood.	
<b>NATURAL FEATURES</b>			
Unique Natural Features /Water Resources	2	The project consists of infill construction of six, single-family, affordable housing units. The 6650 S Avenida del Arrabal site lies closest to the Julian Wash riverine located approximately 368 feet northwest of the site. There are engineered drainage and detention basins throughout the subdivision to manage storm water. There are no unique natural or water features on the individual infill lots.	

Environmental Assessment Factor	Impact Code	Impact Evaluation	Mitigation
Vegetation / Wildlife (Introduction, Modification, Removal, Disruption, etc.)	2	The project consists of infill construction of six, single-family, affordable housing units. The subdivision is mostly built out with residential landscaping and landscaped pocket parks and improvements/common areas. The lots have been previously cleared, graded, and leveled. There are no visible signs of wildlife on the infill lots.	
Other Factors 1			
Other Factors 2			
<b>CLIMATE AND ENERGY</b>			
Climate Change	2	The project consists of infill construction of six, single-family, affordable housing units. Climate predictions forecast increased average daily temperatures and decreased annual precipitation for the area. HFHT receives Energy Star certification on all housing units through a third-party inspection, and building features include efficient HVAC systems, R-19 rated insulation, R-48 rated roofs, dual pane windows, and water efficient fixtures and design elements.	
Energy Efficiency	1	HFHT receives Energy Star certification on all housing units through a third-party inspection. HVAC systems are designed to meet Energy Star 3 specifications and are located in a semi conditioned attic; floors are epoxy coated, walls coated with low Volatile Organic Compound paints; insulation is rated at the R-19 level, with roofs at the R-48 level; fluorescent and/or LED light bulbs are installed; windows are .30/.30 dual-pane; roofing tiles contain recycled concrete; washing machines are piped to irrigate yard plants; and rain water is diverted to landscape plants and basins (most plants are native to the Sonoran desert, and all are drought-tolerant). HFHT installs hybrid water heaters in all homes, along with faucet fixtures gauged at 1.5 gpm, and 1.28 gpf for toilets. HFHT offers low-income home buyers an option to participate in the Tucson Electric Power solar program set-aside for a 10-year period. Energy use	

Environmental Assessment Factor	Impact Code	Impact Evaluation	Mitigation
		for construction activities, such as construction equipment, are temporary in nature.	

**Supporting documentation**

- [2005-5-24 CDP Final Plat Sheet 1.pdf](#)
- [2023-7-19 CDP Littletown Rec Center Jay Regional Park.pdf](#)
- [2023-3-21 PC Av Total Precipitation.pdf](#)
- [2023-3-21 PC Av Daily Max Temp.pdf](#)
- [2023-8-4 CDP FFIEC.pdf](#)
- [2023-7-26 CDP Transit Map.pdf](#)
- [SunTran Route 26.pdf](#)
- [2023-9-15 CDP Zoning Map.pdf](#)
- [2023-8-4 CDP Google Maps Schools.pdf](#)
- [2023-8-4 CDP Google Maps Police.pdf](#)
- [2023-8-4 CDP Google Maps Medical Center.pdf](#)
- [2023-8-4 CDP Google Maps Hospital.pdf](#)
- [2023-8-4 CDP Google Maps Grocery.pdf](#)
- [2023-8-4 CDP Google Maps Fire.pdf](#)
- [2023-8-4 CDP Google Maps Discount Store.pdf](#)
- [2023-8-4 CDP Google Maps Daycare.pdf](#)
- [2023-8-4 CDP Google Maps Community Center.pdf](#)

**Additional Studies Performed:**

**Field Inspection [Optional]:** Date and completed

by:

Rolanda Mazeika

9/15/2023 12:00:00 AM

- [2023-9-15 CDP 5782 Site Visit Checklist.pdf](#)
- [2023-9-15 CDP 5746 Site Visit Checklist.pdf](#)
- [2023-9-15 CDP 5717 5725 5733 Site Visit Checklist.pdf](#)
- [2023-9-15 CDP 5716 Site Visit Checklist.pdf](#)
- [2023-9-8 CDP Aerial Map\(1\).pdf](#)
- [2023 HFH CDP Model Home Plans.pdf](#)

**List of Sources, Agencies and Persons Consulted [40 CFR 1508.9(b)]:**

City of Tucson, Office of the Mayor-Mayor Regina Romero; City of Tucson, Housing and Community Development Department-Ann Chanecka, Mary Leon, Ben Carpenter; City of Tucson Planning and Development Services; City of Tucson, Department of Transportation and Mobility; Tucson Fire Department; Tucson Police Department; City of Tucson, Tucson Water Department; City of Tucson, Environmental & General Services Department; Pima County Wastewater Management; Pima County Department of Environmental Quality; City of City of Tucson-Pima County Consortium Plan-[www.tucsonaz.gov/files/sharedassets/public/living-and-working/housingcommunity-development/documents/cot\\_-\\_pima\\_county\\_consortium\\_20202024\\_hud\\_consolidated\\_plan\\_and\\_ffy2020\\_annual\\_action\\_plans.pdf](http://www.tucsonaz.gov/files/sharedassets/public/living-and-working/housingcommunity-development/documents/cot_-_pima_county_consortium_20202024_hud_consolidated_plan_and_ffy2020_annual_action_plans.pdf)

**List of Permits Obtained:**

The reviews that go into each project include but are not limited to: Planning, Zoning, Engineering, Landscape, Fire, ADA, Plumbing, Solid Waste, Right of Way Improvements, Structural, Mechanical and Plumbing, Electrical, Fire, Zoning Engineering, Wastewater, Water, Transportation. The City of Tucson Planning and Development Services Department reviews, approves and issues all permits for: zoning, site plans, grading, storm water, demolition, parking, landscape and construction trades; issues all permits; performs inspections and issues certificate of occupancy. The Department enforces building and zoning codes.

**Public Outreach [24 CFR 58.43]:**

Publication of a combined Notice of Intent to Request Release of Funds/Finding of No Significant Impacts (RROF/FONSI) in the Arizona Daily Star on 11/3/2023. Interested parties contacted by e-mail. The Environmental Review Record (ERR) that documents the environmental determinations for this project is available at [www.tucsonaz.gov/Departments/Housing-and-Community-Development/Documents/Environmental-Review](http://www.tucsonaz.gov/Departments/Housing-and-Community-Development/Documents/Environmental-Review) and in-person at 310 North Commerce Park Loop, Tucson, Arizona 85745. The ERR will also be available and distributed on the HUD HEROS system.

[2023-9-15 CDP contact list.pdf](#)

**Cumulative Impact Analysis [24 CFR 58.32]:**

The proposed infill construction of six, single-family, affordable housing units for sale to first-time homebuyers in the Corazon del Pueblo subdivision is located in south/east Tucson, in an area of middle income, in an urban, low density, single-family, residential development, together with schools, parks, and other public

services necessary for a satisfactory urban residential environment. The project will be a minor contributor to the projected increase in population, traffic, and energy usage already anticipated in the area. The project will contribute to the build out of an existing residential neighborhood that will use existing roadways, sewer systems, municipal water, and other utilities. No significant cumulative impacts on the environment are anticipated from the Proposed Action in conjunction with other activities.

**Alternatives [24 CFR 58.40(e); 40 CFR 1508.9]**

No alternatives other than the No Action Alternative were considered.

**No Action Alternative [24 CFR 58.40(e)]**

Infill construction of six, single-family, affordable housing units for sale to first-time homebuyers in the Corazon del Pueblo subdivision would not occur. Goals of the City of Tucson-Pima County Consortium (Consolidated/Annual Action) Plan would not be fulfilled. Completion of the build out of the existing neighborhood and affordable housing construction would not occur.

**Summary of Findings and Conclusions:**

The proposed Infill construction of six, single-family, affordable housing units for sale to first-time homebuyers in the Corazon del Pueblo subdivision will not adversely affect the environment or the neighborhood. The activity is compatible with existing urban, low density, single-family, residential development, together with schools, parks, and other public services necessary for a satisfactory urban residential environment in the area. There will be little to no impact on existing resources or services in the area. The project locations are owned by the redeveloper and the site plans are approved. The project is a positive step in meeting City of Tucson goals for encouraging appropriate growth and development in the area. The project is a positive step in meeting City of Tucson goals for addressing the housing shortage and housing affordability needs in the community.

**Mitigation Measures and Conditions [CFR 1505.2(c)]:**

Summarized below are all mitigation measures adopted by the Responsible Entity to reduce, avoid or eliminate adverse environmental impacts and to avoid non-compliance or non-conformance with the above-listed authorities and factors. These measures/conditions must be incorporated into project contracts, development agreements and other relevant documents. The staff responsible for implementing and monitoring mitigation measures should be clearly identified in the mitigation plan.

<b>Law, Authority, or Factor</b>	<b>Mitigation Measure or Condition</b>	<b>Comments on Completed Measures</b>	<b>Mitigation Plan</b>	<b>Complete</b>
Historic Preservation	A cultural resources survey is required by the Historic Preservation Programmatic Agreement dated 1/25/2002	N/A	COT-HCDD will conduct a cultural resources survey in accordance with the Programmatic Agreement dated 1/25/2002.	

**Project Mitigation Plan**

COT-HCDD will conduct a cultural resources survey in accordance with the Programmatic Agreement dated 1/25/2002.

**Supporting documentation on completed measures**

## APPENDIX A: Related Federal Laws and Authorities

### Airport Hazards

General policy	Legislation	Regulation
It is HUD's policy to apply standards to prevent incompatible development around civil airports and military airfields.		24 CFR Part 51 Subpart D

1. To ensure compatible land use development, you must determine your site's proximity to civil and military airports. Is your project within 15,000 feet of a military airport or 2,500 feet of a civilian airport?

No

✓ Yes

2. Is your project located within a Runway Projection Zone/Clear Zone (RPZ/CZ) or Accident Potential Zone (APZ) ?

Yes, project is in an APZ

Yes, project is an RPZ/CZ

✓ No, project is not within an APZ or RPZ/CZ

Based on the response, the review is in compliance with this section. Document and upload the map showing that the site is not within either zone below.

### Screen Summary Compliance Determination

### Supporting documentation

[2023-9-8 Camino Esperanza Eterna Airport Map.pdf](#)  
[2023-9-8 Calle de Hielo Dulce Airport Map.pdf](#)

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Pueblo-Res-Infill-Proj-6-  
Lots

Tucson, AZ

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[2023-9-8 Calle de la Apertura Airport Map.pdf](#)

[2023-9-8 Camino de las Iguanas Airport Map.pdf](#)

**Are formal compliance steps or mitigation required?**

### Coastal Barrier Resources

General requirements	Legislation	Regulation
HUD financial assistance may not be used for most activities in units of the Coastal Barrier Resources System (CBRS). See 16 USC 3504 for limitations on federal expenditures affecting the CBRS.	Coastal Barrier Resources Act (CBRA) of 1982, as amended by the Coastal Barrier Improvement Act of 1990 (16 USC 3501)	

**This project is located in a state that does not contain CBRA units. Therefore, this project is in compliance with the Coastal Barrier Resources Act.**

#### Compliance Determination

This project is located in a state that does not contain CBRS units. There are no CBRS units in Arizona. Therefore, this project is in compliance with the Coastal Barrier Resources Act.

#### Supporting documentation

[Coastal Barriers Template 2022-9-14.pdf](#)

**Are formal compliance steps or mitigation required?**

Yes

✓ No

## Flood Insurance

General requirements	Legislation	Regulation
Certain types of federal financial assistance may not be used in floodplains unless the community participates in National Flood Insurance Program and flood insurance is both obtained and maintained.	Flood Disaster Protection Act of 1973 as amended (42 USC 4001-4128)	24 CFR 50.4(b)(1) and 24 CFR 58.6(a) and (b); 24 CFR 55.1(b).

**1. Does this project involve financial assistance for construction, rehabilitation, or acquisition of a mobile home, building, or insurable personal property?**

No. This project does not require flood insurance or is excepted from flood insurance.

Yes

**2. Upload a FEMA/FIRM map showing the site here:**

[2023-9-8 Camino Esperanza Eterna Flood Map.pdf](#)

[2023-9-8 Calle de Hielo Dulce Flood Map.pdf](#)

[2023-9-8 Calle de la Apertura Flood Map.pdf](#)

[2023-9-8 Camino de las Iguanas Flood Map.pdf](#)

The Federal Emergency Management Agency (FEMA) designates floodplains. The [FEMA Map Service Center](#) provides this information in the form of FEMA Flood Insurance Rate Maps (FIRMs). For projects in areas not mapped by FEMA, use the best available information to determine floodplain information. Include documentation, including a discussion of why this is the best available information for the site. Provide FEMA/FIRM floodplain zone designation, panel number, and date within your documentation.

**Is the structure, part of the structure, or insurable property located in a FEMA-designated Special Flood Hazard Area?**

✓ No

Based on the response, the review is in compliance with this section.

Yes

## Screen Summary

**Compliance Determination**

The structure or insurable property is not located in a FEMA-designated Special Flood Hazard Area. 5717, 5725, 5733 E Calle De La Apertura, 5716 E Camino De Las Iguanas, and 5782 E Camino Esperanza Eterna - FEMA Shaded Zone X (500-year floodplain), 04019C 2315L, 6/16/2011. 5746 E Calle De Hielo Dulce - FEMA Zone X, 04019C 2315L, 6/16/2011. While flood insurance may not be mandatory in this instance, HUD recommends that all insurable structures maintain flood insurance under the National Flood Insurance Program (NFIP). The project is in compliance with flood insurance requirements.

**Supporting documentation**

**Are formal compliance steps or mitigation required?**

Yes

✓ No

## Air Quality

General requirements	Legislation	Regulation
The Clean Air Act is administered by the U.S. Environmental Protection Agency (EPA), which sets national standards on ambient pollutants. In addition, the Clean Air Act is administered by States, which must develop State Implementation Plans (SIPs) to regulate their state air quality. Projects funded by HUD must demonstrate that they conform to the appropriate SIP.	Clean Air Act (42 USC 7401 et seq.) as amended particularly Section 176(c) and (d) (42 USC 7506(c) and (d))	40 CFR Parts 6, 51 and 93

**1. Does your project include new construction or conversion of land use facilitating the development of public, commercial, or industrial facilities OR five or more dwelling units?**

Yes

No

### Air Quality Attainment Status of Project's County or Air Quality Management District

**2. Is your project's air quality management district or county in non-attainment or maintenance status for any criteria pollutants?**

No, project's county or air quality management district is in attainment status for all criteria pollutants.

Yes, project's management district or county is in non-attainment or maintenance status for the following criteria pollutants (check all that apply):

**3. What are the *de minimis* emissions levels (40 CFR 93.153) or screening levels for the non-attainment or maintenance level pollutants indicated above**

Carbon monoxide

ppm (parts per million)

Particulate Matter, <10 microns

µg/m<sup>3</sup> (micrograms per cubic meter of air)

**Provide your source used to determine levels here:**

Plma County Department of Environmental Quality (PDEQ)

4. **Determine the estimated emissions levels of your project. Will your project exceed any of the de minimis or threshold emissions levels of non-attainment and maintenance level pollutants or exceed the screening levels established by the state or air quality management district?**

- ✓ No, the project will not exceed *de minimis* or threshold emissions levels or screening levels.

**Enter the estimate emission levels:**

Carbon monoxide	ppm (parts per million)
Particulate Matter, <10 microns	µg/m <sup>3</sup> (micrograms per cubic meter of air)

Based on the response, the review is in compliance with this section.

Yes, the project exceeds *de minimis* emissions levels or screening levels.

**Screen Summary**

**Compliance Determination**

The project's county or air quality management district is in non-attainment status for the following: Carbon monoxide (Tucson, maintenance), Particulate Matter, <10 microns (Ajo, maintenance & Rillito, non-attainment). The Arizona Department of Environmental Quality (ADEQ) and PDEQ have reviewed their air quality monitoring data have no concerns about single-family residences, multi-family, or small apartment complex development in existing residential areas changing or adversely impacting air quality control standards. Memo from Richard Grimaldi, PDEQ, dated 12/18/2019. This project does not exceed de minimis emissions levels or the screening level established by the state or air quality management district for the pollutant(s) identified above. The project is in compliance with the Clean Air Act.

**Supporting documentation**

**Are formal compliance steps or mitigation required?**

Yes

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Pueblo-Res-Infill-Proj-6-  
Lots

Tucson, AZ

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✓ No

## Coastal Zone Management Act

General requirements	Legislation	Regulation
Federal assistance to applicant agencies for activities affecting any coastal use or resource is granted only when such activities are consistent with federally approved State Coastal Zone Management Act Plans.	Coastal Zone Management Act (16 USC 1451-1464), particularly section 307(c) and (d) (16 USC 1456(c) and (d))	15 CFR Part 930

**This project is located in a state that does not participate in the Coastal Zone Management Program. Therefore, this project is in compliance with the Coastal Zone Management Act.**

### Screen Summary

#### **Compliance Determination**

This project is located in a state that does not participate in the Coastal Zone Management Program. Arizona has no coastal areas. Therefore, this project is in compliance with the Coastal Zone Management Act.

#### **Supporting documentation**

[Coastal zones template 11-1-19.pdf](#)

**Are formal compliance steps or mitigation required?**

## Contamination and Toxic Substances

General requirements	Legislation	Regulations
It is HUD policy that all properties that are being proposed for use in HUD programs be free of hazardous materials, contamination, toxic chemicals and gases, and radioactive substances, where a hazard could affect the health and safety of the occupants or conflict with the intended utilization of the property.		24 CFR 58.5(i)(2) 24 CFR 50.3(i)

**1. Evaluate the site for contamination. Were any on-site or nearby toxic, hazardous, or radioactive substances found that could affect the health and safety of project occupants or conflict with the intended use of the property?**

No

Yes

Check here if an ASTM Phase I Environmental Site Assessment (ESA) report was utilized. [Note: HUD regulations does not require an ASTM Phase I ESA report for single family homes]

### Screen Summary

#### Compliance Determination

#### Supporting documentation

[2023-7-26 CDP NEPAAssist.pdf](#)

[2023-9-15 CDP Site Visit Checklist Supplemental.pdf](#)

[2023-9-15 CDP 5782 Site Visit Checklist\(1\).pdf](#)

[2023-9-15 CDP 5746 Site Visit Checklist\(1\).pdf](#)

[2023-9-15 CDP 5717 5725 5733 Site Visit Checklist\(1\).pdf](#)

[2023-9-15 CDP 5716 Site Visit Checklist\(1\).pdf](#)

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Lots

Tucson, AZ

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**Are formal compliance steps or mitigation required?**

Yes

✓ No

## Endangered Species

General requirements	ESA Legislation	Regulations
Section 7 of the Endangered Species Act (ESA) mandates that federal agencies ensure that actions that they authorize, fund, or carry out shall not jeopardize the continued existence of federally listed plants and animals or result in the adverse modification or destruction of designated critical habitat. Where their actions may affect resources protected by the ESA, agencies must consult with the Fish and Wildlife Service and/or the National Marine Fisheries Service (“FWS” and “NMFS” or “the Services”).	The Endangered Species Act of 1973 (16 U.S.C. 1531 <i>et seq.</i> ); particularly section 7 (16 USC 1536).	50 CFR Part 402

### 1. Does the project involve any activities that have the potential to affect species or habitats?

- ✓ No, the project will have No Effect due to the nature of the activities involved in the project.

This selection is only appropriate if none of the activities involved in the project have potential to affect species or habitats. Examples of actions without potential to affect listed species may include: purchasing existing buildings, completing interior renovations to existing buildings, and replacing exterior paint or siding on existing buildings.

Based on the response, the review is in compliance with this section.

No, the project will have No Effect based on a letter of understanding, memorandum of agreement, programmatic agreement, or checklist provided by local HUD office

Yes, the activities involved in the project have the potential to affect species and/or habitats.

### Screen Summary

#### Compliance Determination

This project will have No Effect on listed species due to the nature of the activities involved in the project. The project consists of infill construction of six residences in a long-occupied and developed urban subdivision. There are no critical habitats in the

project area and the lots to be developed have been previously cleared, graded, and leveled. This project is in compliance with the Endangered Species Act.

**Supporting documentation**

[2023-9-14 FWS TE Species List.pdf](#)

[Critical Habitat Map NEPA Assist 7-15-19.pdf](#)

**Are formal compliance steps or mitigation required?**

Yes

✓ No

## Explosive and Flammable Hazards

General requirements	Legislation	Regulation
HUD-assisted projects must meet Acceptable Separation Distance (ASD) requirements to protect them from explosive and flammable hazards.	N/A	24 CFR Part 51 Subpart C

1. Is the proposed HUD-assisted project itself the development of a hazardous facility (a facility that mainly stores, handles or processes flammable or combustible chemicals such as bulk fuel storage facilities and refineries)?

No

Yes

2. Does this project include any of the following activities: development, construction, rehabilitation that will increase residential densities, or conversion?

No

Yes

3. Within 1 mile of the project site, are there any current or planned stationary aboveground storage containers that are covered by 24 CFR 51C? Containers that are NOT covered under the regulation include:

- Containers 100 gallons or less in capacity, containing common liquid industrial fuels OR

- Containers of liquified petroleum gas (LPG) or propane with a water volume capacity of 1,000 gallons or less that meet the requirements of the 2017 or later version of National Fire Protection Association (NFPA) Code 58.

If all containers within the search area fit the above criteria, answer "No." For any other type of aboveground storage container within the search area that holds one of the flammable or explosive materials listed in Appendix I of 24 CFR part 51 subpart C, answer "Yes."

No

Yes

4. Based on the analysis, is the proposed HUD-assisted project located at or beyond the required separation distance from all covered tanks?

Yes

Based on the response, the review is in compliance with this section.

No

#### **Screen Summary**

##### **Compliance Determination**

There is a current or planned stationary aboveground storage container of concern within 1 mile of the project site. One propane ASTs, three ASTs containing diesel/gasoline fuel, and one planned household hazardous waste AST were evaluated. The Separation Distances from the project are acceptable. The project is in compliance with explosive and flammable hazard requirements.

##### **Supporting documentation**

[2023-9-14 CDP AST Summary.pdf](#)

**Are formal compliance steps or mitigation required?**

Yes

No

## Farmlands Protection

General requirements	Legislation	Regulation
The Farmland Protection Policy Act (FPPA) discourages federal activities that would convert farmland to nonagricultural purposes.	Farmland Protection Policy Act of 1981 (7 U.S.C. 4201 et seq.)	<a href="#">7 CFR Part 658</a>

**1. Does your project include any activities, including new construction, acquisition of undeveloped land or conversion, that could convert agricultural land to a non-agricultural use?**

Yes

No

If your project includes new construction, acquisition of undeveloped land or conversion, explain how you determined that agricultural land would not be converted:

The sites are in a fully-developed, urban environment per the US Census Urban Area map printed 7/5/2022.

Based on the response, the review is in compliance with this section. Document and upload all documents used to make your determination below.

### Screen Summary

#### **Compliance Determination**

This project does not include any activities that could potentially convert agricultural land to a non-agricultural use. The site does not meet the definition of farmland per 7 CFR Part 658.2(a), "...Farmland" does not include land already in or committed to urban development or water storage..." The site is in a fully-developed, urban environment per the US Census Urban Area map printed 7/5/2022. The project is in compliance with the Farmland Protection Policy Act.

#### **Supporting documentation**

[US Census Urban Area Map 2022-7-5.pdf](#)

#### **Are formal compliance steps or mitigation required?**

Yes

No



## Floodplain Management

General Requirements	Legislation	Regulation
Executive Order 11988, Floodplain Management, requires federal activities to avoid impacts to floodplains and to avoid direct and indirect support of floodplain development to the extent practicable.	Executive Order 11988	24 CFR 55

**1. Do any of the following exemptions apply? Select the applicable citation? [only one selection possible]**

55.12(c)(3)

55.12(c)(4)

55.12(c)(5)

55.12(c)(6)

55.12(c)(7)

55.12(c)(8)

55.12(c)(9)

55.12(c)(10)

55.12(c)(11)

None of the above

**2. Upload a FEMA/FIRM map showing the site here:**

The Federal Emergency Management Agency (FEMA) designates floodplains. The FEMA Map Service Center provides this information in the form of FEMA Flood Insurance Rate Maps (FIRMs). For projects in areas not mapped by FEMA, use **the best available information** to determine floodplain information. Include documentation, including a discussion of why this is the best available information for the site.

**Does your project occur in a floodplain?**

No

Yes

Select the applicable floodplain using the FEMA map or the best available information:

Floodway

Coastal High Hazard Area (V Zone)

100-year floodplain (A Zone)

✓ 500-year floodplain (B Zone or shaded X Zone)

**500-year Floodplain**

**Is this a critical action?**

✓ No

Based on the response, the review is in compliance with this section.

Yes

**Screen Summary**

**Compliance Determination**

Five of the project sites are located in a 500-year floodplain. 5717, 5725, 5733 E Calle De La Apertura, 5716 E Camino De Las Iguanas, and 5782 E Camino Esperanza Eterna - FEMA Shaded Zone X (500-year floodplain), 04019C 2315L, 6/16/2011. 5746 E Calle De Hielo Dulce - FEMA Zone X, 04019C 2315L, 6/16/2011. Based on the project description this project is not a critical action, so an 8-Step process is not required. The project consists of new construction of single-family residences and does not include critical actions. The project is in compliance with Executive Order 11988.

**Supporting documentation**

**Are formal compliance steps or mitigation required?**

## Historic Preservation

General requirements	Legislation	Regulation
Regulations under Section 106 of the National Historic Preservation Act (NHPA) require a consultative process to identify historic properties, assess project impacts on them, and avoid, minimize, or mitigate adverse effects	Section 106 of the National Historic Preservation Act (16 U.S.C. 470f)	36 CFR 800 "Protection of Historic Properties" <a href="https://www.govinfo.gov/content/pkg/CFR-2012-title36-vol3/pdf/CFR-2012-title36-vol3-part800.pdf">https://www.govinfo.gov/content/pkg/CFR-2012-title36-vol3/pdf/CFR-2012-title36-vol3-part800.pdf</a>

### **Threshold**

#### **Is Section 106 review required for your project?**

No, because the project consists solely of activities listed as exempt in a Programmatic Agreement (PA ). (See the PA Database to find applicable PAs.)

No, because the project consists solely of activities included in a No Potential to Cause Effects memo or other determination [36 CFR 800.3(a)(1)].

- ✓ Yes, because the project includes activities with potential to cause effects (direct or indirect).

### **Step 1 – Initiate Consultation**

#### **Select all consulting parties below (check all that apply):**

Indian Tribes, including Tribal Historic Preservation Officers (THPOs) or Native Hawaiian Organizations (NHOs)

- ✓ Other Consulting Parties

✓ City of Tucson, Historic Preservation Office

In progress

**Describe the process of selecting consulting parties and initiating consultation here:**

A cultural resources survey is in progress in accordance with the Programmatic Agreement dated 1/25/2002 (property outside an established historic district, project includes new construction activities)

Document and upload all correspondence, notices and notes (including comments and objections received below).

**Was the Section 106 Lender Delegation Memo used for Section 106 consultation?**

Yes

No

**Step 2 – Identify and Evaluate Historic Properties**

1. **Define the Area of Potential Effect (APE), either by entering the address(es) or uploading a map depicting the APE below:**

5717, 5725, 5733 E Calle De La Apertura, 5716 E Camino De Las Iguanas, 5746 E Calle De Hielo Dulce, 5782 E Camino Esperanza Eterna, Tucson, Pima County, AZ 85756.

**In the chart below, list historic properties identified and evaluated in the APE. Every historic property that may be affected by the project should be included in the chart.**

Upload the documentation (survey forms, Register nominations, concurrence(s) and/or objection(s), notes, and photos) that justify your National Register Status determination below.

Address / Location / District	National Register Status	SHPO Concurrence	Sensitive Information
----------------------------------	-----------------------------	------------------	--------------------------

**Additional Notes:**

**2. Was a survey of historic buildings and/or archeological sites done as part of the project?**

Yes

Document and upload surveys and report(s) below.  
For Archeological surveys, refer to HP Fact Sheet #6, Guidance on Archeological Investigations in HUD Projects.

Additional Notes:

A cultural resources survey is in progress in accordance with the Programmatic Agreement dated 1/25/2002 (property outside an established historic district, project includes new construction activities)

No

***Step 3 –Assess Effects of the Project on Historic Properties***

Only properties that are listed on or eligible for the National Register of Historic Places receive further consideration under Section 106. Assess the effect(s) of the project by applying the Criteria of Adverse Effect. (36 CFR 800.5)] Consider direct and indirect effects as applicable as per guidance on direct and indirect effects.

**Choose one of the findings below - No Historic Properties Affected, No Adverse Effect, or Adverse Effect; and seek concurrence from consulting parties.**

No Historic Properties Affected

Based on the response, the review is in compliance with this section. Document and upload concurrence(s) or objection(s) below.

**Document reason for finding:**

No historic properties present.

Historic properties present, but project will have no effect upon them.

No Adverse Effect

Adverse Effect

**Screen Summary**

**Compliance Determination**

A cultural resources survey is in progress in accordance with the Programmatic Agreement dated 1/25/2002 (property outside an established historic district, project includes new construction activities). The project is in compliance with Section 106.

**Supporting documentation**

[2023-9-8 CDP HPO Map.pdf](#)

[HPO Programmatic agreement.pdf](#)

**Are formal compliance steps or mitigation required?**

Yes

No

## Noise Abatement and Control

General requirements	Legislation	Regulation
HUD's noise regulations protect residential properties from excessive noise exposure. HUD encourages mitigation as appropriate.	Noise Control Act of 1972  General Services Administration Federal Management Circular 75-2: "Compatible Land Uses at Federal Airfields"	Title 24 CFR 51 Subpart B

**1. What activities does your project involve? Check all that apply:**

- New construction for residential use

NOTE: HUD assistance to new construction projects is generally prohibited if they are located in an Unacceptable zone, and HUD discourages assistance for new construction projects in Normally Unacceptable zones. See 24 CFR 51.101(a)(3) for further details.

Rehabilitation of an existing residential property

A research demonstration project which does not result in new construction or reconstruction

An interstate land sales registration

Any timely emergency assistance under disaster assistance provision or appropriations which are provided to save lives, protect property, protect public health and safety, remove debris and wreckage, or assistance that has the effect of restoring facilities substantially as they existed prior to the disaster  
None of the above

**4. Complete the Preliminary Screening to identify potential noise generators in the vicinity (1000' from a major road, 3000' from a railroad, or 15 miles from an airport).**

**Indicate the findings of the Preliminary Screening below:**

There are no noise generators found within the threshold distances above.

- ✓ Noise generators were found within the threshold distances.

5. **Complete the Preliminary Screening to identify potential noise generators in the**

- ✓ Acceptable: (65 decibels or less; the ceiling may be shifted to 70 decibels in circumstances described in §24 CFR 51.105(a))

Indicate noise level here: 64

Based on the response, the review is in compliance with this section. Document and upload noise analysis, including noise level and data used to complete the analysis below.

Normally Unacceptable: (Above 65 decibels but not exceeding 75 decibels; the floor may be shifted to 70 decibels in circumstances described in §24 CFR 51.105(a))

Unacceptable: (Above 75 decibels)

HUD strongly encourages conversion of noise-exposed sites to land uses compatible with high noise levels.

Check here to affirm that you have considered converting this property to a non-residential use compatible with high noise levels.

Indicate noise level here: 64

Document and upload noise analysis, including noise level and data used to complete the analysis below.

**Screen Summary**

**Compliance Determination**

A Noise Assessment was conducted. The noise level was acceptable: 64.0 db. See noise analysis. The 5746 East Calle de Hielo Dulce site is closest to the Davis Monthan

Airforce Base and lies outside the associated DNL 65 noise contour. Intervening two-story homes are observed in the direction of the railroad, east arterial, and airbase (noise contributors). Home construction includes wall insulation values of R-19, ceiling values of R-48, and dual pane windows, as well as home layouts with garages facing noise contributors, that may contribute to higher sound transmission class ratings. The project is in compliance with HUD's Noise regulation.

**Supporting documentation**

[2023-9-14 CDP Noise Calculations.pdf](#)

**Are formal compliance steps or mitigation required?**

Yes

✓ No

## Sole Source Aquifers

General requirements	Legislation	Regulation
<b>The Safe Drinking Water Act of 1974 protects drinking water systems which are the sole or principal drinking water source for an area and which, if contaminated, would create a significant hazard to public health.</b>	Safe Drinking Water Act of 1974 (42 U.S.C. 201, 300f et seq., and 21 U.S.C. 349)	40 CFR Part 149

1. Does the project consist solely of acquisition, leasing, or rehabilitation of an existing building(s)?

Yes

✓ No

2. Is the project located on a sole source aquifer (SSA)?

A sole source aquifer is defined as an aquifer that supplies at least 50 percent of the drinking water consumed in the area overlying the aquifer. This includes streamflow source areas, which are upstream areas of losing streams that flow into the recharge area.

No

✓ Yes

### Screen Summary

#### Compliance Determination

The project is located on a sole source aquifer. The region has an MOU or other working agreement with EPA for HUD projects impacting a sole source aquifer, and the MOU or working agreement excludes the project from further review. The project will utilize existing municipal water and wastewater facilities. The project is in compliance with Sole Source Aquifer requirements.

**Supporting documentation**

[Sole Source Aquifer Template 5-11-18.pdf](#)

**Are formal compliance steps or mitigation required?**

Yes

✓ No

## Wetlands Protection

General requirements	Legislation	Regulation
Executive Order 11990 discourages direct or indirect support of new construction impacting wetlands wherever there is a practicable alternative. The Fish and Wildlife Service's National Wetlands Inventory can be used as a primary screening tool, but observed or known wetlands not indicated on NWI maps must also be processed. Off-site impacts that result in draining, impounding, or destroying wetlands must also be processed.	Executive Order 11990	24 CFR 55.20 can be used for general guidance regarding the 8 Step Process.

**1. Does this project involve new construction as defined in Executive Order 11990, expansion of a building's footprint, or ground disturbance? The term "new construction" shall include draining, dredging, channelizing, filling, diking, impounding, and related activities and any structures or facilities begun or authorized after the effective date of the Order**

No

Yes

**2. Will the new construction or other ground disturbance impact an on- or off-site wetland? The term "wetlands" means those areas that are inundated by surface or ground water with a frequency sufficient to support, and under normal circumstances does or would support, a prevalence of vegetative or aquatic life that requires saturated or seasonally saturated soil conditions for growth and reproduction. Wetlands generally include swamps, marshes, bogs, and similar areas such as sloughs, potholes, wet meadows, river overflows, mud flats, and natural ponds.**

**"Wetlands under E.O. 11990 include isolated and non-jurisdictional wetlands."**

No, a wetland will not be impacted in terms of E.O. 11990's definition of new construction.

Based on the response, the review is in compliance with this section. Document and upload a map or any other relevant documentation below which explains your determination

Yes, there is a wetland that be impacted in terms of E.O. 11990's definition of new construction.

**Screen Summary**

**Compliance Determination**

The project will not impact on- or off-site wetlands. The 5746 E Calle De Hielo Dulce site lies closest to the Julian Wash riverine located approximately 368 feet northwest of the site. Very low slope across all neighborhood lots from north to south. Existing sheet flow to street traverses the neighborhood south/southwest to engineered drainage and detention basins throughout the subdivision. No sign of erosion in detention/retention basins. The lot has been previously cleared, graded, and leveled, and will be prepared to engineering standards to handle flow appropriately. The project is in compliance with Executive Order 11990.

**Supporting documentation**

[2023-9-15 CDP Wetlands Map.pdf](#)

**Are formal compliance steps or mitigation required?**

Yes

✓ No

## Wild and Scenic Rivers Act

General requirements	Legislation	Regulation
The Wild and Scenic Rivers Act provides federal protection for certain free-flowing, wild, scenic and recreational rivers designated as components or potential components of the National Wild and Scenic Rivers System (NWSRS) from the effects of construction or development.	The Wild and Scenic Rivers Act (16 U.S.C. 1271-1287), particularly section 7(b) and (c) (16 U.S.C. 1278(b) and (c))	36 CFR Part 297

### 1. Is your project within proximity of a NWSRS river?

✓ No

Yes, the project is in proximity of a Designated Wild and Scenic River or Study Wild and Scenic River.

Yes, the project is in proximity of a Nationwide Rivers Inventory (NRI) River.

### Screen Summary

#### **Compliance Determination**

This project is not within proximity of a NWSRS river. there are no NWSRS rivers in Southern Arizona. The project is in compliance with the Wild and Scenic Rivers Act.

#### **Supporting documentation**

[NWSRS Wild-Scenic Rivers 2022-7-7.pdf](#)

#### **Are formal compliance steps or mitigation required?**

Yes

✓ No

## Environmental Justice

General requirements	Legislation	Regulation
Determine if the project creates adverse environmental impacts upon a low-income or minority community. If it does, engage the community in meaningful participation about mitigating the impacts or move the project.	Executive Order 12898	

**HUD strongly encourages starting the Environmental Justice analysis only after all other laws and authorities, including Environmental Assessment factors if necessary, have been completed.**

**1. Were any adverse environmental impacts identified in any other compliance review portion of this project's total environmental review?**

Yes

No

Based on the response, the review is in compliance with this section.

### **Screen Summary**

#### **Compliance Determination**

No adverse environmental impacts were identified in the project's total environmental review. The project is in compliance with Executive Order 12898.

#### **Supporting documentation**

[2023-9-14 EJScreen Community Report.pdf](#)

**Are formal compliance steps or mitigation required?**

Yes

✓ No

# Corazon del Pueblo, Tucson, AZ 85756



**Notes**

Aerial Map  
 6650 S Avenida del Arrabal  
 6760 S Camino del Anheló  
 6766 S Camino del Anheló  
 6772 S Camino del Anheló  
 6778 S Camino del Anheló  
 6788 S Via de la Verdad  
 6794 S Via de la Verdad

**Legend**

Parcels

1: 2,257



364.4                      0                      182.21                      364.4 Feet

WGS\_1984\_Web\_Mercator\_Auxiliary\_Sphere  
 © City of Tucson

This map is a user generated static output from an Internet mapping site and is for reference only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable.

**THIS MAP IS NOT TO BE USED FOR NAVIGATION**

INDEX OF DRAWINGS	
NUMBER	NAME
A0	COVER SHEET / ROOF PLANS
A1	FLOOR / CEILING PLANS
A1.1	DIMENSIONED FLOOR PLANS
A2	EXTERIOR ELEVATIONS 'A'
A2.1	EXTERIOR ELEVATIONS 'B'
A2.2	EXTERIOR ELEVATIONS 'C'
A3	BUILDING SECTIONS
A4	FRAMING PLAN & DIMENSIONED SLAB PLAN
A5	BUILDING DETAILS
E-1	ELECTRICAL / LIGHTING PLAN & CALCS.
M-1	MECHANICAL PLAN & DETAILS
P-1	PLUMBING PLAN & DETAILS

**Inclusive Home Design Notes:**

Provide at least one exterior accessible route and entrance. The route may originate from the carport, driveway, public street or sidewalk. The slope of this route shall not exceed one-foot vertical rise per twenty feet horizontal distance (1:20), unless a ramp per IRC is constructed.

NOTE: Indicate the "Exterior Accessible Route" and "Accessible Entrance" on Single Family Residence site plan. (NOT A REQUIREMENT on model plans)

Accessible Entrance: Provide minimum one no-step level entrance that is a minimum 32" wide and meets the door hardware requirements per this ordinance. The access floor shall have a minimum 36" wide hallway and accessible doors

to all spaces and elements to be minimum 28" door. NOTE: Exit doors are required to be 36" minimum and the garage shall not be used as an exit. All doors, hallway and opening to be clearly dimensioned and indicated on floor plan.

Change in floor levels and thresholds at the accessible entrance and along accessible routes may be 1/2" high maximum.

This must be beveled from 1/4" to 1/2" maximum with a slope not steeper than 1 inch rise in 2 inch run (1:2).

Door Hardware: Handles, pull latches, locks and other operable parts on accessible doors shall have a shape that is easy

to grasp with one hand and that does not require tight grasping, pinching or twisting of the wrist to operate. Lever hardware satisfies the requirements of this provision.

Bathroom Wall Reinforcement: In all bathrooms on the accessible route, reinforcement shall be installed to allow the future installation of grab bars on walls adjacent to the tub and toilet. In addition, reinforcement shall be installed in shower compartments for future installation of grab bars.

This reinforcement shall be installed flush with the studs and at the following locations:  
Toilet: 33"-36" above the floor on all adjacent walls.  
Horizontal length of reinforcement shall be sufficient to allow a 42" grab bar and 24" rear grab bar.

NOTE: Nothing in the ordinance requires that toilet be placed by a sidewall.  
Tub: Horizontal length reinforcement shall be sufficient to allow for:

Back Wall: Two backing reinforcements, one backing reinforcement horizontal position 33" minimum and 36" maximum

above the floor, and one backing reinforcement 9" above the rim of the bathtub. Each backing reinforcement shall be 24" long minimum and shall be 24" maximum from the head end wall and 12" maximum from the foot end wall.

Foot End Wall: One backing reinforcement 24" long minimum on the foot end wall at the front edge of the bathtub.

Head End wall: One backing reinforcement 12" long minimum on the head end wall at the front edge of the bathtub.

Shower compartments shall have backing on a minimum of two walls not to include control valve wall mounted at 33"-36" above shower floor.

All wall reinforcement shall be capable of resisting shear and bending forces of a minimum of 250 pounds. Reinforcement

is not required at the location of vanities, linen closets, and pre-molded shower/tub surrounds, or in a room containing only a sink and a toilet, provided that the room does not contain the only sink or toilet on the accessible floor of the home.

Electrical:  
All light controls shall be placed no higher than 48" on center, above the floor

Where practical, all electrical receptacles shall be placed no lower than 15" on center, above the floor.

All Thermostats shall be placed no higher than 54" on center, above the floor.

The exceptions to these provisions are as follows:  
Electrical receptacles serving a dedicated use.  
Appliance mounted controls or switches.

A single outlet where all of the following conditions are met:  
The outlet is above a length of countertop that is uninterrupted by a sink or appliance and  
At least one receptacle is provided for that length of countertop, and  
All other receptacles provided for that length of countertop set no higher than 48".

Floor electrical receptacles.  
Plumbing fixture controls.  
HVAC diffusers  
Ceiling fan mounted controls.

**ROOF PLAN KEYNOTES:**

- MISSION TILE ROOFING OVER TYVEK ROOFING MEMBRANE OVER 5/8" PLYWOOD SHEATHING / OSB.
- 2x6 PAINTED WOOD FASCIA.
- 300 SF CLEAR SOLAR READY ZONE PER T103.3.

**PROJECT NARRATIVE**

THIS PROJECT IS A NEW 2,204 SF 3 BEDROOM 2 BATH RESIDENCE WITH 478 SF ATTACHED GARAGE AND 100 SF PORCHES.

**AREA CALCULATIONS (3-BEDROOM HOUSE):**

LEVEL 1 LIVING:	677.33 SQ. FT.
LEVEL 2 LIVING:	685.06 SQ. FT.
1-CAR GARAGE:	288.28 SQ. FT.
REAR PORCH / STORAGE:	66.42 SQ. FT.
FRONT PORCH:	47.20 SQ. FT.

TOTAL UNDER ROOF: 2,782 SQ. FT.

**DEFERRED SUBMITTALS:**

- ROOF TRUSS DESIGN / CALCS
- FLOOR TRUSS DESIGN / CALCS

**DRAFTSMAN**

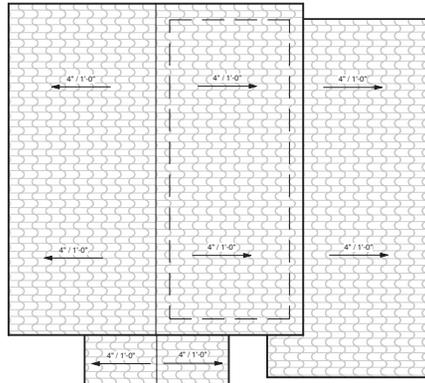
VANCE GOODMAN  
P.O. BOX 384  
VERNON, AZ 85940  
520-245-1214

**STRUCTURAL ENGINEER**

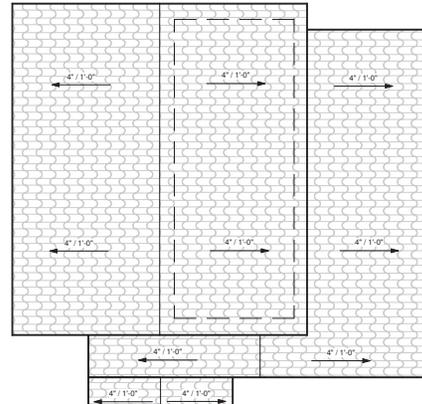
XXXX XXXXX  
ADDRESS  
TUCSON, AZ 857XX  
520-XXX-XXXX

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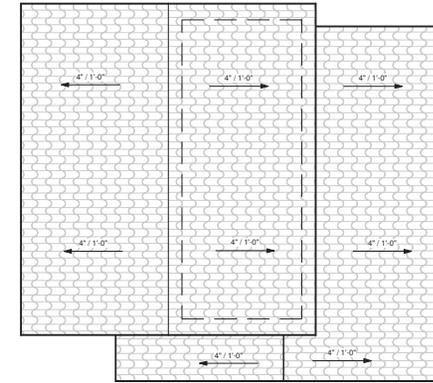
- PER 2018 INTERNATIONAL RESIDENTIAL CODE
- PER 2017 NATIONAL ELECTRIC CODE
- PER 2018 INTERNATIONAL ENERGY CONSERVATION CODE
- PER 2018 INTERNATIONAL MECHANICAL CODE
- PER 2018 UNIFORM PLUMBING CODE
- PER 2018 INTERNATIONAL FIRE CODE
- CITY OF TUCSON AMENDMENTS



**1 ROOF PLAN 'A'**  
3/16" = 1'-0"



**2 ROOF PLAN 'B'**  
3/16" = 1'-0"



**3 ROOF PLAN 'C'**  
3/16" = 1'-0"

NEW MODEL HOME FOR:  
**HABITAT FOR HUMANITY**  
TUCSON ARIZONA

**CONSTRUCTION DOCUMENTS**

REVISION	DATE	DESCRIPTION	BY
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PROJECT NO.

DATE: 1/20/23

SCALE: 3/16" = 1'-0" DRAWN BY: VHG

DRAWING TITLE: COVER SHEET / ROOF PLANS

DRAWING NUMBER: A0

CONSTRUCTION DOCUMENTS

REVISION	DATE	DESCRIPTION	BY
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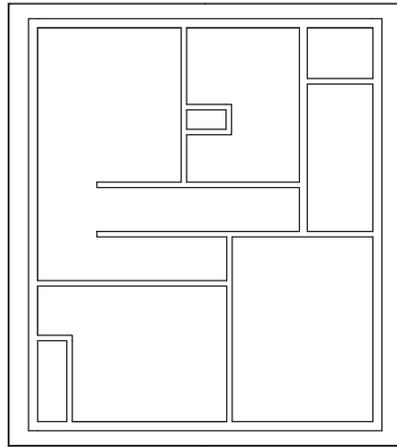
PROJECT NO.

DATE 1/20/23

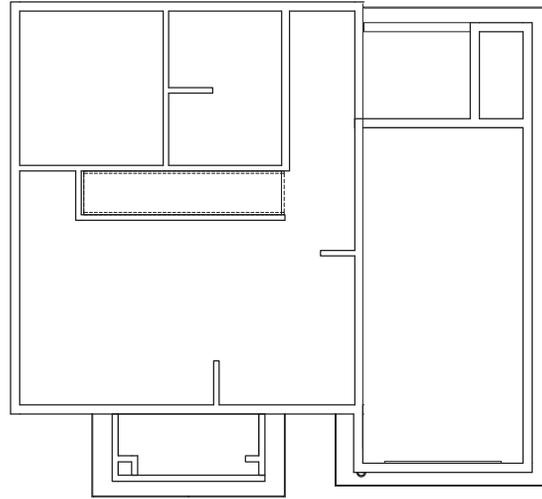
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DRAWING TITLE FLOOR / CEILING PLANS

DRAWING NUMBER A1



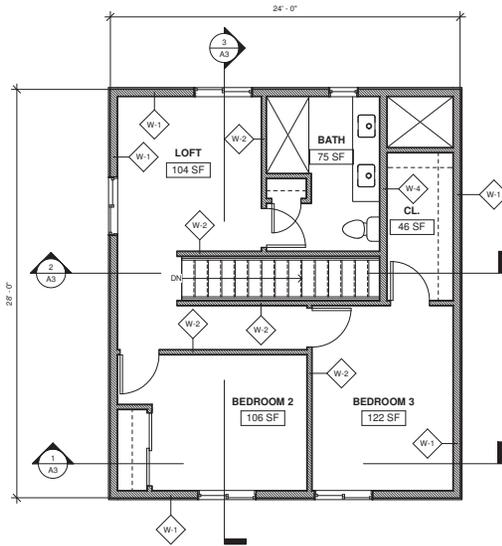
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A1 1/4" = 1'-0"



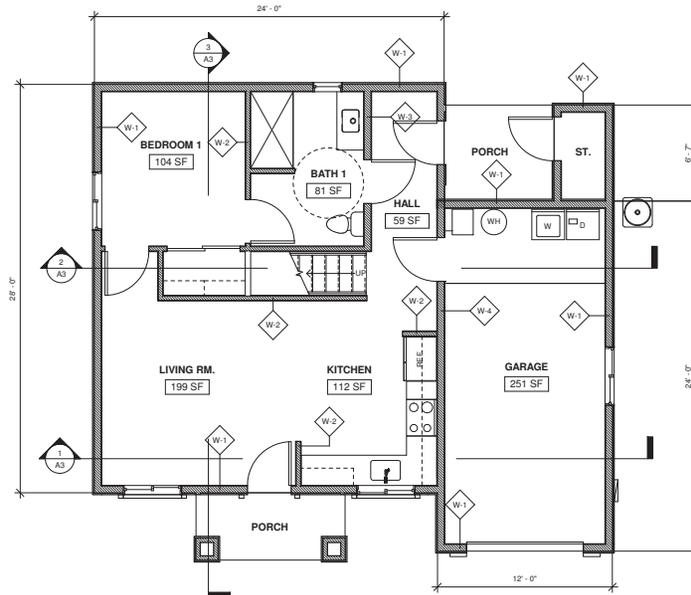
3 LEVEL 1 CEILING PLAN  
A1 1/4" = 1'-0"

WALL SCHEDULE: W-

1. 2"x6" WOOD STUDS @ 24" O.C. w/ EXTERIOR WALL W/ R-21 BATT INSULATION WITH WESTERN 1 KOTE STUCCO SYS. ON MTL. LATH OVER 1" EPS FOAM BD. OVER TYVEK BLDG. WRAP OVER 1/2" PLYWOOD PANELS. PROVIDE 1/2" GYP. BD. AT INTERIOR.
2. 2"x4" WOOD STUD @ 24" O.C. PARTITION WITH 1/2" GYP. BD. TEXTURED AND PAINTED UP TO TRUSSES. USE WATER RESISTANT GREEN BOARD IN WET LOCATIONS.
3. 2"x6" WOOD STUD @ 24" O.C. PARTITION WITH 1/2" GYP. BD. TEXTURED AND PAINTED UP TO ROOF TRUSSES. USE WATER RESISTANT GREEN BOARD IN WET LOCATIONS. R-13 BATT INSULATION AS OPTIONAL SOUND PROOFING.
4. 2"x6" WOOD STUD @ 24" O.C. LOAD BEARING PARTITION WITH 5/8" FIRE RATED TYPE 'X' GYP. BD. (INDICATED WITH DASHED LINE) TEXTURED AND PAINTED UP TO CEILING ON GARAGE SIDE AND 1/2" GYP. BD. ON HOUSE SIDE.



2 FLOOR PLAN LEVEL 2  
A1 1/4" = 1'-0"



1 FLOOR PLAN LEVEL 1  
A1 1/4" = 1'-0"

REVISION	DATE	DESCRIPTION	BY
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PROJECT NO.

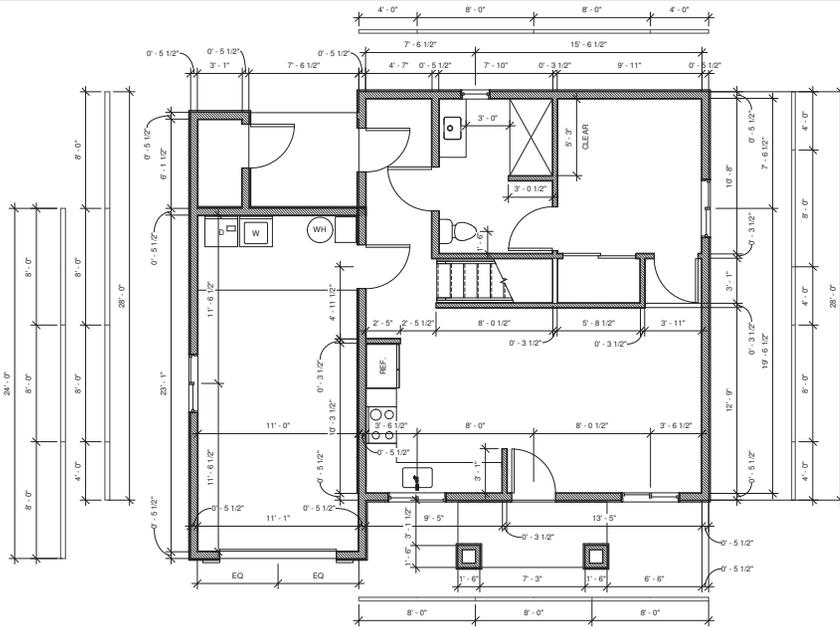
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1/20/23

SCALE  
1/4" = 1'-0"

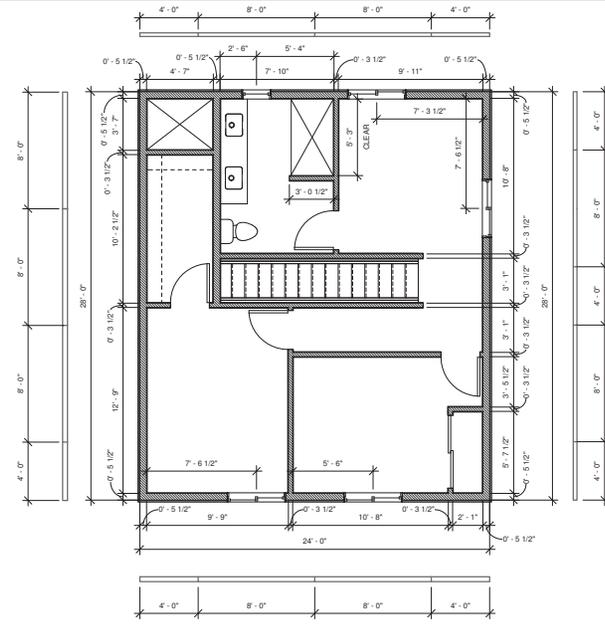
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VHG

DRAWING TITLE  
DIMENSIONED FLOOR PLANS

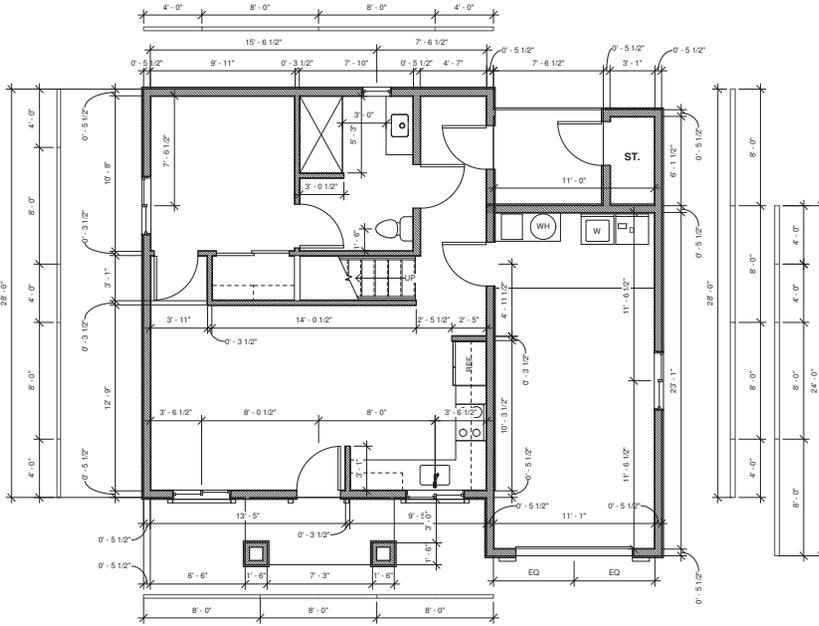
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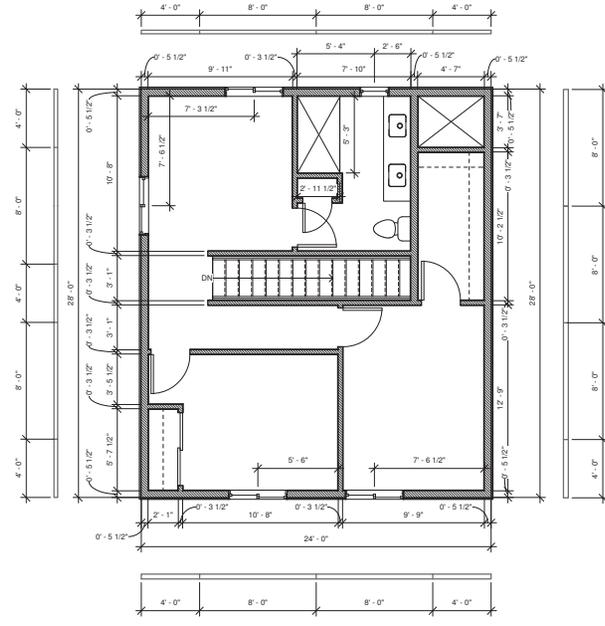
3 DIMENSION FLOOR PLAN LEVEL 1 MIRRORED  
1/4" = 1'-0"



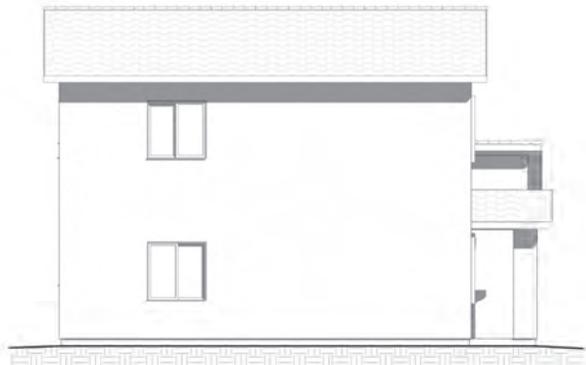
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1/4" = 1'-0"



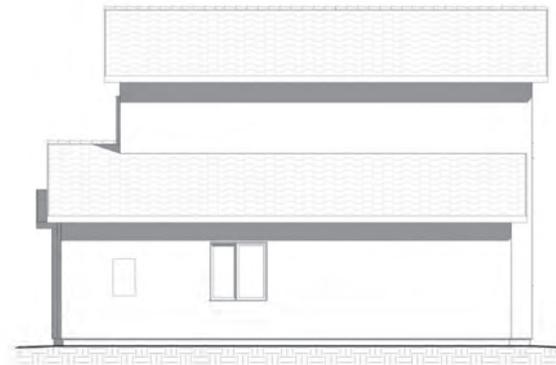
1 DIMENSION FLOOR PLAN LEVEL 1  
1/4" = 1'-0"



2 DIMENSION FLOOR PLAN LEVEL 2  
1/4" = 1'-0"



**1**  
SIDE ELEVATION 'A'  
1/4" = 1'-0"



**4**  
GARAGE SIDE ELEVATION 'A'  
1/4" = 1'-0"



**2**  
REAR ELEVATION 'A'  
1/4" = 1'-0"



**3**  
FRONT ELEVATION 'A'  
1/4" = 1'-0"

**ELEVATION KEYNOTES** ○

1. 2"x6" WOOD STUDS @ 24" O.C. w/ EXTERIOR WALL W/ R-21 BATT INSULATION WITH WESTERN 1 KOTE STUCCO SYS. ON MTL. LATH OVER 1" EPS FOAM BD. OVER TYVEK. BLDG. WRAP OVER 1/2" PLYWOOD PANELS. PROVIDE 1/2" GYP. BD. AT INTERIOR. SEE WALL SCHEDULE.
2. 2x WOOD INTERIOR NON-BEARING PARTITION WITH 1/2" GYP. BD. EACH SIDE. SEE WALL SCHEDULE.
3. 2x6 WOOD STUDS @ 16" O.C. INTERIOR BEARING WALL AND FOOTING. SEE WALL SCHEDULE.
4. DOOR PER FLOOR PLAN.
5. MISSION TILE ROOFING OVER TYVEK ROOFING MEMBRANE OVER 5/8" PLYWOOD SHEATHING / OSB.
6. 2x6 WOOD FASCIA, PTD. SEE FRAMING PLAN.
8. R-38 ROOF BATT INSULATION BETWEEN TRUSSES.
9. WOOD BASE CABINETS WITH QUARTZ COUNTERTOP CHOSEN BY OWNER AND INSTALLED BY G.C.
10. NEW PREFAB WOOD TRUSSES @ 24" O.C. PER TRUSS MANUFACTURER. PROVIDE BOXED OPENING FOR DUCTWORK AS SHOWN. SEE MECHANICAL PLAN FOR DUCT PATHS.
11. 4" THICK CONCRETE PAD OVER 4" A.B.C. W/ THICKENED FOOTINGS PER DETAILS.
12. DOUBLE PANE VINYL WINDOW, SIZE PER FLOOR PLAN.
14. FULLY SHIELDED WALL SCONCE CHOSEN BY OWNER. SEE ELECTRICAL.
16. ELECTRICAL MAIN PANEL PER ELECTRICAL SHEET.
17. 2" RIGID FOAM POPOUT W/ STUCCO FINISH.

NEW MODEL HOME FOR:  
HABITAT FOR HUMANITY  
TUCSON ARIZONA

**CONSTRUCTION DOCUMENTS**

REVISION	DATE	DESCRIPTION	BY
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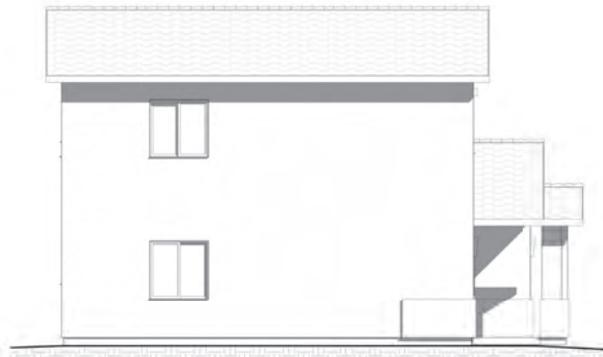
PROJECT NO.

DATE: 1/20/23

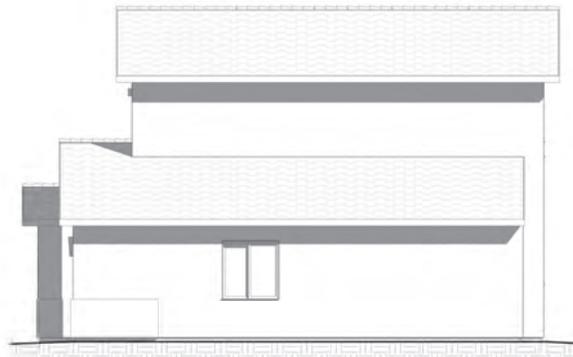
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DRAWING TITLE: EXTERIOR ELEVATIONS 'A'

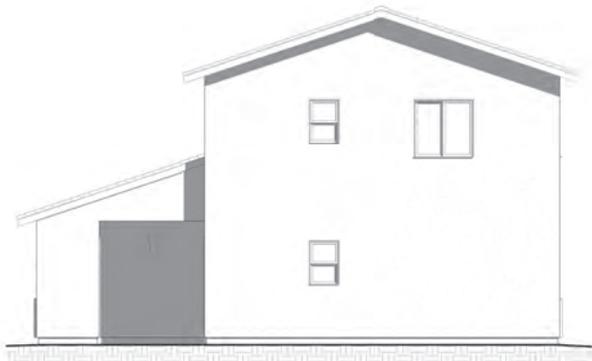
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3 SIDE ELEVATION 'B'  
A2.1 1/4" = 1'-0"



1 GARAGE SIDE ELEVATION 'B'  
A2.1 1/4" = 1'-0"



2 REAR ELEVATION 'B'  
A2.1 1/4" = 1'-0"



4 FRONT ELEVATION 'B'  
A2.1 1/4" = 1'-0"

**ELEVATION KEYNOTES** ☐

1. 2"x6" WOOD STUDS @ 24" O.C. w/ EXTERIOR WALL W/ R-21 BATT INSULATION WITH WESTERN 1 KOTE STUCCO SYS. ON MTL. LATH OVER 1" EPS FOAM BD. OVER TYVEK BLDG. WRAP OVER 1/2" PLYWOOD PANELS. PROVIDE 1/2" GYP. BD. AT INTERIOR. SEE WALL SCHEDULE.
2. 2x WOOD INTERIOR NON-BEARING PARTITION WITH 1/2" GYP. BD. EACH SIDE. SEE WALL SCHEDULE.
3. 2x6 WOOD STUDS @ 16" O.C. INTERIOR BEARING WALL AND FOOTING. SEE WALL SCHEDULE.
4. DOOR PER FLOOR PLAN.
5. MISSION TILE ROOFING OVER TYVEK ROOFING MEMBRANE OVER 5/8" PLYWOOD SHEATHING / OSB.
6. 2x6 WOOD FASCIA. PTD. SEE FRAMING PLAN.
8. R-38 ROOF BATT INSULATION BETWEEN TRUSSES.
9. WOOD BASE CABINETS WITH QUARTZ COUNTERTOP CHOSEN BY OWNER AND INSTALLED BY G.C.
10. NEW PREFAB WOOD TRUSSES @ 24" O.C. PER TRUSS MANUFACTURER. PROVIDE BOXED OPENING FOR DUCTWORK AS SHOWN. SEE MECHANICAL PLAN FOR DUCT PATHS.
11. 4" THICK CONCRETE PAD OVER 4" A.B.C. W/ THICKENED FOOTINGS PER DETAILS.
12. DOUBLE PANE VINYL WINDOW, SIZE PER FLOOR PLAN.
14. FULLY SHIELDED WALL SCONCE CHOSEN BY OWNER. SEE ELECTRICAL.
16. ELECTRICAL MAIN PANEL PER ELECTRICAL SHEET.
17. 2" RIGID FOAM POPOUT W/ STUCCO FINISH.

NEW MODEL HOME FOR:  
HABITAT FOR HUMANITY  
TUCSON ARIZONA

**CONSTRUCTION DOCUMENTS**

REVISION	DATE	DESCRIPTION	BY
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PROJECT NO.

DATE 1/20/23

SCALE As indicated DRAWN BY VHG

DRAWING TITLE EXTERIOR ELEVATIONS 'B'

DRAWING NUMBER A2.1

CONSTRUCTION DOCUMENTS

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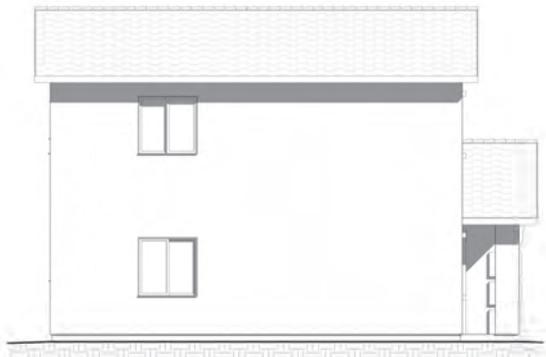
PROJECT NO.

DATE **1/20/23**

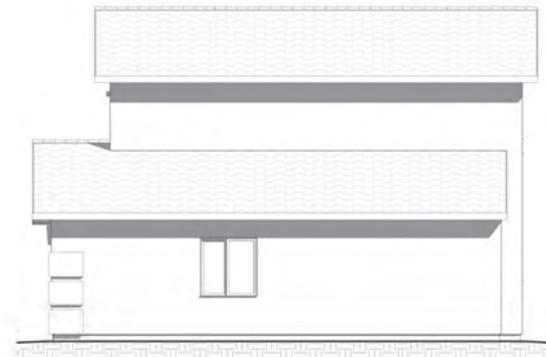
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DRAWING TITLE **EXTERIOR ELEVATIONS 'C'**

DRAWING NUMBER **A2.2**



**4 SIDE ELEVATION 'C'**  
1/4" = 1'-0"



**3 GARAGE SIDE ELEVATION 'C'**  
1/4" = 1'-0"



**2 REAR ELEVATION 'C'**  
1/4" = 1'-0"



**1 FRONT ELEVATION 'C'**  
1/4" = 1'-0"

ELEVATION KEYNOTES ☐

- 2"x6" WOOD STUDS @ 24" O.C. w/ EXTERIOR WALL W/ R-21 BATT INSULATION WITH WESTERN 1 KOTE STUCCO SYS. ON MTL. LATH OVER 1" EPS FOAM BD. OVER TYVEK BLDG. WRAP OVER 1/2" PLYWOOD PANELS. PROVIDE 1/2" GYP. BD. AT INTERIOR. SEE WALL SCHEDULE.
- 2x WOOD INTERIOR NON-BEARING PARTITION WITH 1/2" GYP. BD. EACH SIDE. SEE WALL SCHEDULE.
- 2x6 WOOD STUDS @ 16" O.C. INTERIOR BEARING WALL AND FOOTING. SEE WALL SCHEDULE.
- DOOR PER FLOOR PLAN.
- MISSION TILE ROOFING OVER TYVEK ROOFING MEMBRANE OVER 5/8" PLYWOOD SHEATHING / OSB.
- 2x6 WOOD FASCIA, PTD. SEE FRAMING PLAN.
- R-38 ROOF BATT INSULATION BETWEEN TRUSSES.
- WOOD BASE CABINETS WITH QUARTZ COUNTERTOP CHOSEN BY OWNER AND INSTALLED BY G.C.
- NEW PREFAB WOOD TRUSSES @ 24" O.C. PER TRUSS MANUFACTURER. PROVIDE BOXED OPENING FOR DUCTWORK AS SHOWN. SEE MECHANICAL PLAN FOR DUCT PATHS.
- 4" THICK CONCRETE PAD OVER 4" A.B.C. W/ THICKENED FOOTINGS PER DETAILS.
- DOUBLE PANE VINYL WINDOW, SIZE PER FLOOR PLAN.
- FULLY SHIELDED WALL SCONCE CHOSEN BY OWNER. SEE ELECTRICAL.
- ELECTRICAL MAIN PANEL PER ELECTRICAL SHEET.
- 2" RIGID FOAM POPOUT W/ STUCCO FINISH.

NEW MODEL HOME FOR:  
HABITAT FOR HUMANITY  
TUCSON ARIZONA

CONSTRUCTION DOCUMENTS

REVISION	DATE	DESCRIPTION	BY
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PROJECT NO.

DATE 1/20/23

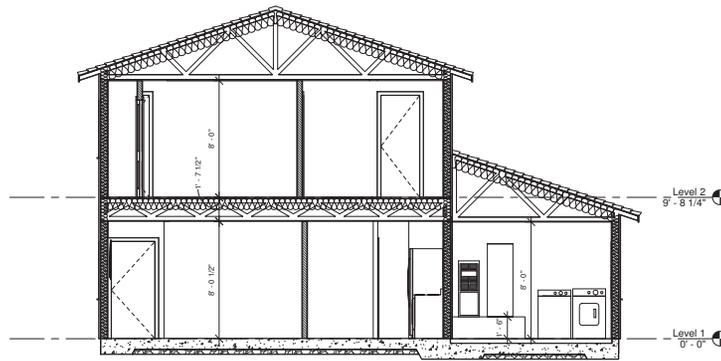
SCALE 1/4" = 1'-0" DRAWN BY VHG

DRAWING TITLE BUILDING SECTIONS

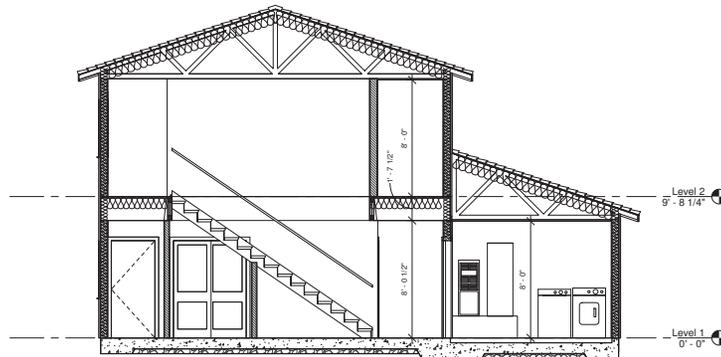
DRAWING NUMBER A3

**SECTION KEYNOTES** ○

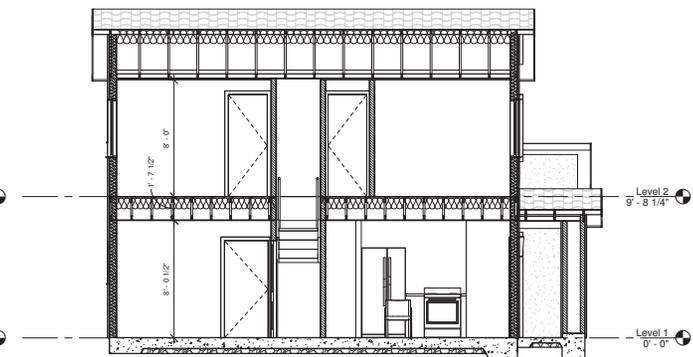
1. 2"x6" WOOD STUDS @ 24" O.C. w/ EXTERIOR WALL W/ R-21 BATT INSULATION WITH WESTERN 1 KOTE STUCCO SYS. ON MTL. LATH OVER 1" EPS FOAM BD. OVER TYVEK BLDG. WRAP OVER 1/2" PLYWOOD PANELS. PROVIDE 1/2" GYP. BD. AT INTERIOR. SEE WALL SCHEDULE.
2. 2x WOOD INTERIOR NON-BEARING PARTITION WITH 1/2" GYP. BD. EACH SIDE.
3. 2x6 WOOD STUDS @ 16" O.C. INTERIOR BEARING WALL AND FOOTING WITH 1/2" GYP. BD. EACH SIDE.
4. MISSION TILE ROOFING OVER TYVEK ROOFING MEMBRANE OVER 5/8" PLYWOOD SHEATHING / OSB.
5. 2x6 WOOD FASCIA, PTD. SEE FRAMING PLAN.
6. R-38 ROOF BATT INSULATION BETWEEN TRUSSES.
7. R-30 BATT INSULATION BETWEEN TRUSSES.
8. NEW PREFAB ROOF WOOD TRUSSES @ 24" O.C. PER TRUSS MANUFACTURER.
9. NEW PREFAB FLOOR WOOD TRUSSES @ 16" O.C. PER TRUSS MANUFACTURER.
10. 4" THICK CONCRETE PAD OVER 4" A.B.C. W/ THICKENED FOOTINGS PER DETAILS.
11. DOUBLE PANE VINYL WINDOW, SIZE PER FLOOR PLAN.
12. SAG RESISTANT 1/2" GYP. BD. TEXTURED AND PAINTED.
13. PREFABRICATED WOOD STAIR W/ 7" MAX RISE AND MIN. 11" TREADS. CONNECTIONS PER DETAILS AND FRAMING PLAN. PROVIDE 1 1/4" HANDRAIL ATTACHED TO WALL @ 36" ABOVE TREADS.



**1 BLDG. SECTION 1**  
A3 1/4" = 1'-0"



**2 BLDG. SECTION 2**  
A3 1/4" = 1'-0"



**3 BLDG. SECTION 3**  
A3 1/4" = 1'-0"

REVISION	DATE	DESCRIPTION	BY
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PROJECT NO.

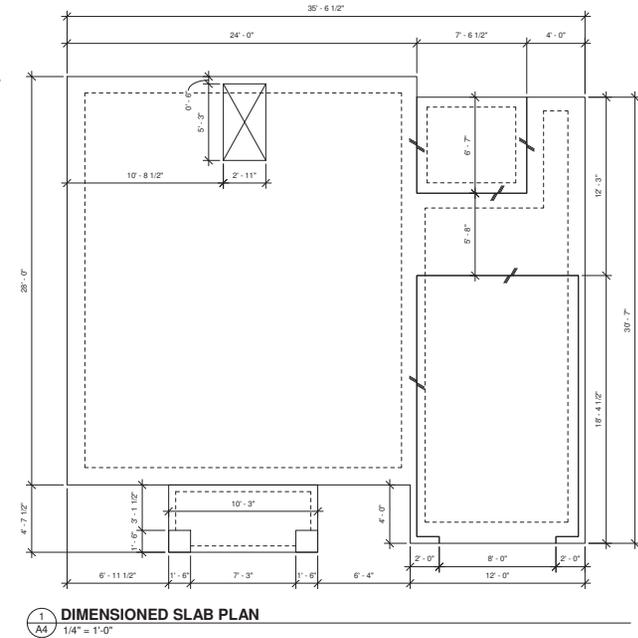
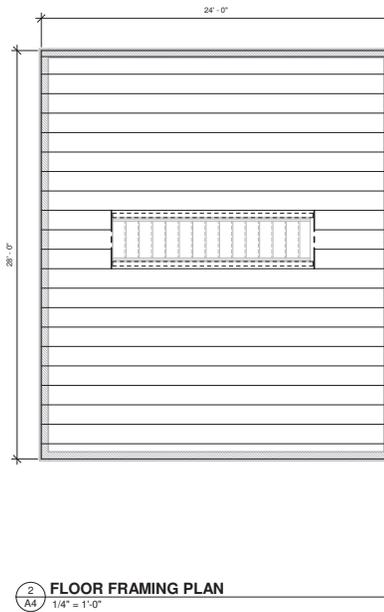
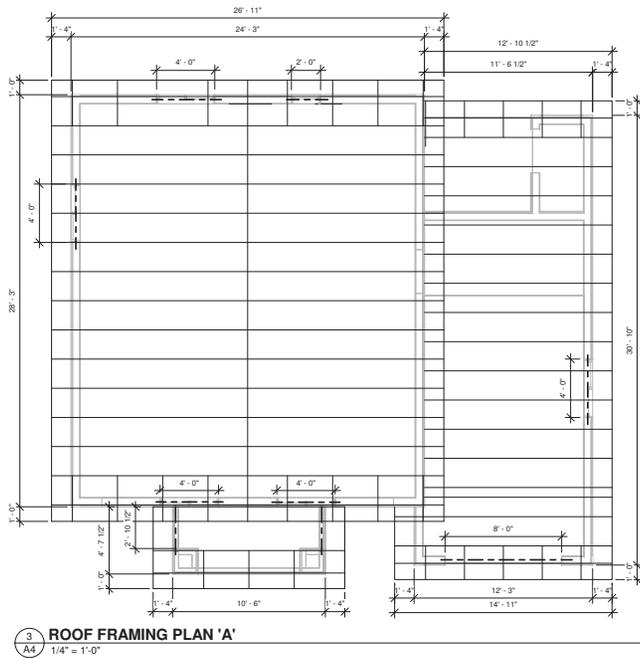
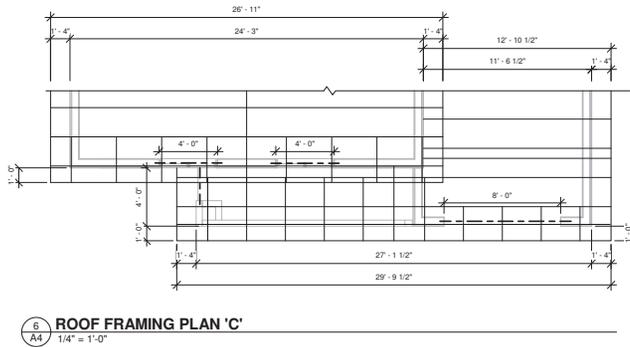
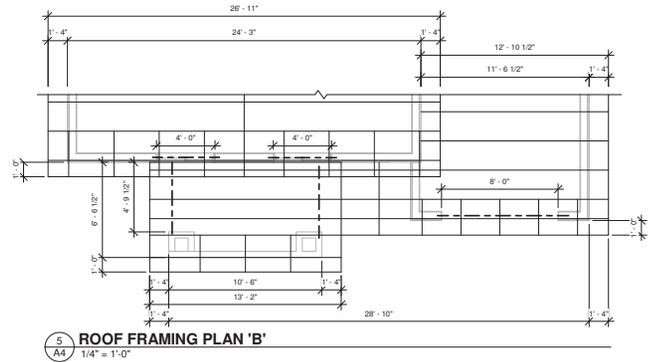
DATE  
1/20/23

SCALE  
1/4" = 1'-0"

DRAWN BY  
VHG

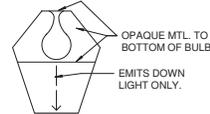
DRAWING TITLE  
FRAMING PLAN &  
DIMENSIONED SLAB PLAN

DRAWING NUMBER  
A4

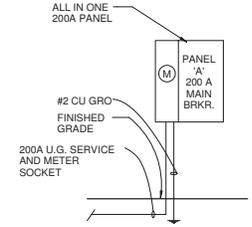


PANEL 'B'						
BUS 1P 3 WIRE, 200 A MB, 120 / 240V						
RECESSED INDOOR						
#	DESCRIPTION	BRKR	WIRE	BRKR	DESCRIPTION	#
1	C/U #1	40	10	10	A/H-1	2
3		2	8	2		4
5	RANGE 4-WIRE	50	8	10	DRYER 4-WIRE	6
7		2	8	2		8
9	RECEPTS / LTS.	20	12	20	KITCHEN GFCI	10
11	RECEPTS / LTS.	20	12	20	KITCHEN GFCI	12
13	RECEPTS / LTS.	20	12	20	RECEPTS / LTS.	14
15	RECEPTS / LTS.	20	12	20	RECEPTS / LTS.	16
17	RECEPTS / LTS.	20	12	20	RECEPTS / LTS.	18
19	RECEPTS / LTS.	20	12	20	RECEPTS / LTS.	20
21	SMOKE DETECT.	20	12	20	BATH GFCI	22
23	GARAGE GFCI	20	12	20	BATH GFCI	24
25	WATER HEATER	30	10	12	DW / DISPOSAL	26
27		2	10	2	REFRIGERATOR	28
29	OUTDOOR GFCI	20	12	20	WASHER	30
31						32
33						34
35						36
37						38
39	FUTURE SOLAR ELECTRIC			40		40
41				2	EV READY	42

REDO



**8 FULL CUT OFF FIXTURE**  
E-1 1/4" = 1'-0"



**7 ONE LINE DIAGRAM**  
E-1 1/4" = 1'-0"

ELECTRICAL SYMBOL KEY	
	CEILING FAN WITH LIGHT KIT. OWNER TO PROVIDE AND INSTALL AT LATER DATE.
	INTERIOR L.E.D. / EXTERIOR L.E.D. WALL MOUNTED FULL CUT-OFF FIXTURE. CHOSEN BY OWNER, CONTRACTOR TO INSTALL. (E FOR EXTERIOR)
	L.E.D. RECESSED DOWNLIGHT AND TRIM BY G.C. 5" DIA. WITH WHITE TRIM. ALL SUITABLE FOR INSTALLATION WITHIN INSULATED CEILING.
	DUPLEX RECEPTACLE, 18" A.F.F., U.N.O.
	220v RECEPTACLE, 18" A.F.F., U.N.O.
	DUPLEX RECEPTACLE. GFCI PROTECTED.
	4' (2 LAMP) SURFACE MOUNTED GARAGE STRIP L.E.D. LIGHT FIXTURE. CHOSEN BY OWNER, CONTRACTOR TO INSTALL.
	PROVIDE ELECTRIC VEHICLE SUPPLY EQUIPMENT 40 AMP, 240 VOLT DEDICATED BRANCH CIRCUIT WITH A NEMA 14-50R RECEPTACLE AND LABEL THE CIRCUIT "EV READY" AND LOCATE THE RECEPTACLE AT A PARKING AREA PER N1104.2.1.
	SINGLE-POLE SWITCH, 48" A.F.F., U.N.O.
	SINGLE-POLE SWITCH, 48" A.F.F., U.N.O.
	120V SMOKE / CARBON MONOXIDE COMBO DETECTOR W/ BATTERY BACKUP WIRE TO HOUSE LIGHTING CIRCUIT.
	COAXIAL CABLE T.V. RECEPTACLE.
	DATA / TELEPHONE RECEPTACLE.
	DOORBELL BUTTON & CHIME.
	EQUIPMENT FUSED OR NON-FUSED DISCONNECT SIZE AND LOCATION PER EQUIPMENT MANUFACTURER INSTRUCTIONS.
	EXHAUST FAN. SEE MECHANICAL FOR DETAILS.
	RANGE HOOD, CHOSEN BY OWNER. SEE MECHANICAL FOR DETAILS.
	EXHAUST FAN. SEE MECHANICAL FOR DETAILS.

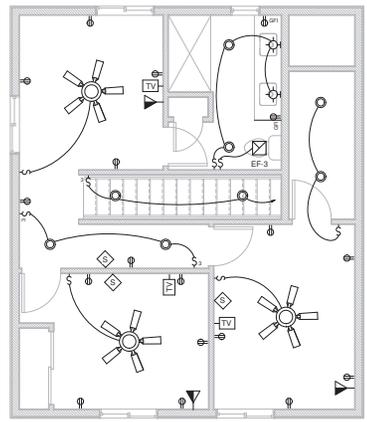
**ELECTRIC LOAD CALC.:**

1.651 S. F. @ 3 VA/S.F.	4953 VA
2 APPLIANCE CIR. AT 1500 VA	3000 VA
1 LAUNDRY CIRCUIT	1500 VA
DRYER	5500 VA
RANGE	8000 VA
DW	750 VA
WATER HEATER	4600 VA
MICROWAVE	1000 VA
TOTAL:	29,303 VA
FIRST 10 KW AT 100% REMAINDER AT 40%	10.0 KW 7.6 KW
17.8 KW/240 V=	74.2 AMPS
EQUIPMENT LOADS @ 100% A/C	42 AMPS
FUTURE SOLAR LOAD	16 AMPS
ELECTRIC VEHICLE LOAD	32 AMPS
<b>TOTAL LOAD:</b>	<b>164.2 AMPS &lt; 200 AMPS</b>

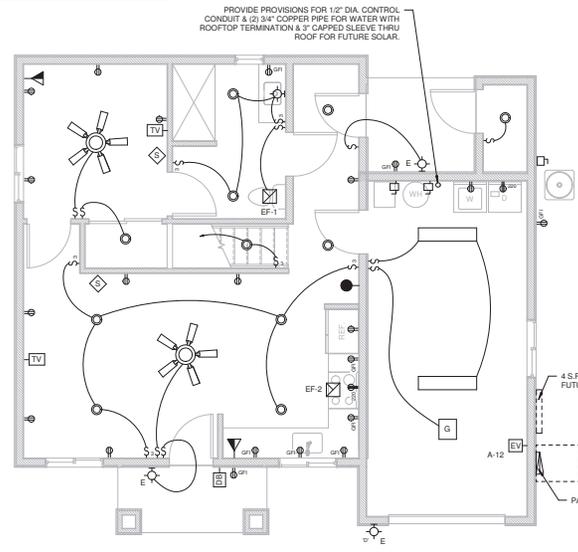
**ELECTRICAL NOTES**

- THE WORK COVERED ON THESE DRAWINGS SHALL INCLUDE THE FURNISHING OF ALL LABOR, MATERIALS, TRANSPORTATION, TOOLS, APPLIANCES, FEES, AND PERMITS REQUIRED FOR THE INSTALLATION OF A COMPLETE AND OPERATING ELECTRICAL SYSTEM. DRAWINGS ARE DIAGRAMMATIC IN NATURE AND ARE NOT INTENDED TO SHOW EVERY DETAIL. THE CONTRACTOR SHALL PROVIDE BOXES, ACCESS PANELS, ETC. AS REQUIRED BY CODE AND INDUSTRY PRACTICE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING THE WORK WITH OTHER TRADES.
- ALL MATERIALS SHALL BE NEW AND BEAR THE U.L. SEAL. MATERIALS SHALL CONFORM TO REQUIREMENTS OF THE N.E.C., WHERE APPLICABLE.
- ALL ELECTRICAL CONDUCTORS IN MASONRY WALLS AND BELOW SLAB SHALL BE INSTALLED IN P.V.C. OR METAL CONDUIT.
- ALL CONDUCTORS SHALL BE STRANDED SOFT-DRAWN ANNEALED COPPER WITH THHN/THWN OR THW INSULATION. MINIMUM WIRE SIZE SHALL BE #12 UNLESS OTHERWISE NOTED.
- THE COMPLETE ELECTRICAL SYSTEM SHALL BE GROUNDED IN ACCORDANCE WITH N.E.C. ART. 250.
- THE ELECTRICAL CONTRACTOR SHALL GUARANTEE AGAINST DEFECTS IN MATERIALS, EQUIPMENT, OR WORKMANSHIP FOR A PERIOD OF ONE (1) YEAR UPON OWNER'S FINAL ACCEPTANCE. CONTRACTOR SHALL REPAIR OR REPLACE ANY DEFECTS TO THE SATISFACTION OF THE OWNER AT NO ADDITIONAL COST.
- ALL ELECTRICAL PRODUCTS SHALL BE INSTALLED AS PER MANUFACTURERS INSTRUCTIONS.
- THE ELECTRICIAN SHALL FURNISH LAMP BULBS FOR ALL ELECTRICIAN SUPPLIED LIGHT FIXTURES.
- ALL DEVICES ARE TO BE SET LOOSELY IN PLACE PRIOR TO WIRING. THE OWNER IS TO APPROVE ALL LOCATIONS BEFORE BOXES ARE SECURE FOR WIRING.
- NO SUBSTITUTIONS OR FIELD CHANGES WITHOUT PRIOR APPROVAL OF THE OWNER THROUGH THE ARCHITECT.
- THE ELECTRICIAN SHALL FURNISH RAIN TIGHT LANDSCAPE COVER BOXES FOR ALL LANDSCAPE GFCI OUTLETS. LANDSCAPE TRANSFORMERS AND LOW VOLTAGE LIGHTS BY OWNER.
- THE ELECTRICIAN SHALL FURNISH WEATHER PROOF HOUSINGS FOR ALL OTHER EXTERIOR GFCI OUTLETS.
- INSTALL ENERGY STAR QUALIFIED LIGHT FIXTURES THROUGHOUT.
- PROTECT ALL INTERIOR OUTLETS IN KITCHENS, FAMILY ROOMS, DINING ROOMS, LIVING ROOMS, PARLORS, LIBRARIES, DEN'S, BEDROOMS, SUNROOMS, RECREATION ROOMS, CLOSETS, HALLWAYS, LAUNDRY AREAS AND SIM. ROOMS OR AREAS (15A & 20A) PROTECTED BY ARC-FAULT CIRCUIT INTERRUPTERS PER E3902.16.
- TAMPER RESISTANT RECEPTACLES REQUIRED FOR 125 VOLT, 15A AND 20A PER R4002.14.
- PROVIDE LOCKING BREAKER FOR WATER HEATER DISCONNECT PER E401.5.
- PROVIDE CARBON MONOXIDE ALARMS IN THE VICINITY OF ALL BEDROOMS IN DWELLINGS WITH FUEL-FIRED APPLIANCES AND/OR ATTACHED GARAGES PER R315.5.2.2.
- NOT LESS THAN ONE RECEPTACLE OUTLET, IN ADDITION TO ANY PROVIDED FOR SPECIFIC EQUIPMENT, SHALL BE INSTALLED IN EACH SEPARATE UNFINISHED PORTION OF A BASEMENT; IN EACH VEHICLE BAY NOT MORE THAN 5.5 FEET ABOVE THE FLOOR IN DETACHED GARAGES THAT ARE PROVIDED WITH ELECTRIC POWER AND IN ACCESSORY BUILDINGS, THAT ARE PROVIDED WITH ELECTRIC POWER PER E901.3.

**INTERNATIONAL ENERGY CONSERVATION CODE NOTE:**  
1. PLACE ENERGY CERTIFICATE NEAR THE INDOOR FURNACE OR IN UTILITY ROOM PER N1101.14.



**2 ELECTRICAL PLAN LEVEL 2**  
E-1 1/4" = 1'-0"



**1 ELECTRICAL PLAN LEVEL 1**  
E-1 1/4" = 1'-0"

HABITAT FOR HUMANITY - TUCSON

NEW MODEL HOME FOR:  
HABITAT FOR HUMANITY  
TUCSON ARIZONA

**CONSTRUCTION DOCUMENTS**

REVISION	DATE	DESCRIPTION	BY
△			
△			
△			
△			

PROJECT NO. \_\_\_\_\_

DATE: 1/20/23

SCALE: As indicated DRAWN BY: VHG

DRAWING TITLE: ELECTRICAL / LIGHTING PLAN & CALCS.

DRAWING NUMBER: E-1

REVISION	DATE	DESCRIPTION	BY
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PROJECT NO.

DATE: 1/20/23

SCALE: As indicated DRAWN BY: VHG

DRAWING TITLE: PLUMBING PLAN & DETAILS

DRAWING NUMBER: P-1

PLUMBING FIXTURE SCHEDULE	
P-1 THRU P-6	FIXTURES SHALL BE AS SELECTED BY OWNERS. ALL FIXTURES SHALL BE OF THE LOW FLOW WATER CONSERVATION DESIGN. THE MIXING VALVE FOR SHOWER FIXTURES SHALL BE OF THE PRESSURE BALANCING TYPE. PLUMBING CONTRACTOR SHALL COORDINATE, ROUGH-IN, INSTALL AND MAKE ALL FINAL CONNECTIONS.
WH	RHEEM MODEL #XE40M0ST45U0 40 GALLON ELECTRIC WATER HEATER OR EQUAL. P&T LINE & VALVE PER PLAN. TURN DOWN AND TERMINATE 6" ABOVE GRADE MAX. INSTALL PER MANUFACTURER'S INSTRUCTIONS.

PLUMBING SYMBOL LEGEND		
SYMBOL	ABBREVIATION	ITEM
---	CW	COLD WATER
----	HW	HOT WATER
---	W	STANDARD WASTE
---	V	VENT
---	SOV	SHUT OFF VALVE
---	2-WAY	DIRECTIONAL GROUND CLEANOUT
⊥	HB	HOSE BIBB WITH VACUUM BREAKER

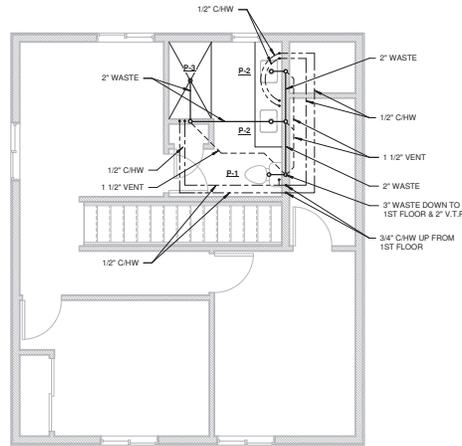
PLUMBING FIXTURE CONNECTION SCHEDULE											
PER 2018 IRC TABLES P2903.6 & P3004.1											
MARK	FIXTURE TYPE	H.W.	C.W.	WASTE	VENT	QTY.	WASTE		WATER		
							UNITS	TOT.	UNITS	TOT.	
P-1	TOILET	-	1/2"	3"	2"	2	3	6	2.2	4.4	
P-2	LAVATORY	1/2"	1/2"	2"	1 1/2"	3	1	3	0.7	2.1	
P-3	SHOWER	1/2"	1/2"	2"	1 1/2"	2	2	4	1.4	2.8	
P-4	CLOTHES WASHER	3/4"	3/4"	2"	1 1/2"	1	3	3	1.4	1.4	
P-5	DISH WASHER	1/2"	1/2"	2"	1 1/2"	1	2	2	1.4	1.4	
P-6	KITCHEN SINK	1/2"	1/2"	2"	1 1/2"	1	2	2	1.4	1.4	
WASTE TOTAL FIXTURE UNITS:							20				
WATER TOTAL FIXTURE UNITS:								22.5			
*HOSE BIBB SHALL HAVE AN INTEGRAL BACK FLOW PREVENTER.											
** ASSUMED ONLY ONE HOSE BIBB IS IN USE AT A TIME.											

TOTAL DEVELOPED LENGTH - 90'  
ASSUMED WATER PRESSURE - 80+ PSI  
NEW WATER METER SIZE - 3/4"

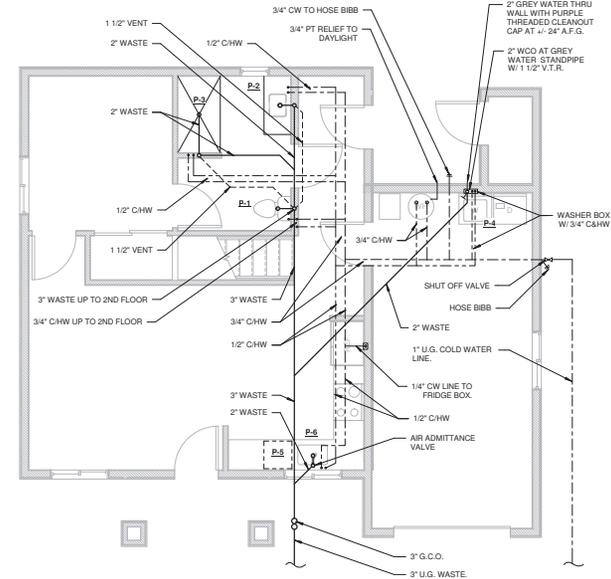
TOTAL WATER UNITS G.P.M. 22.5  
20.5

### PLUMBING NOTES

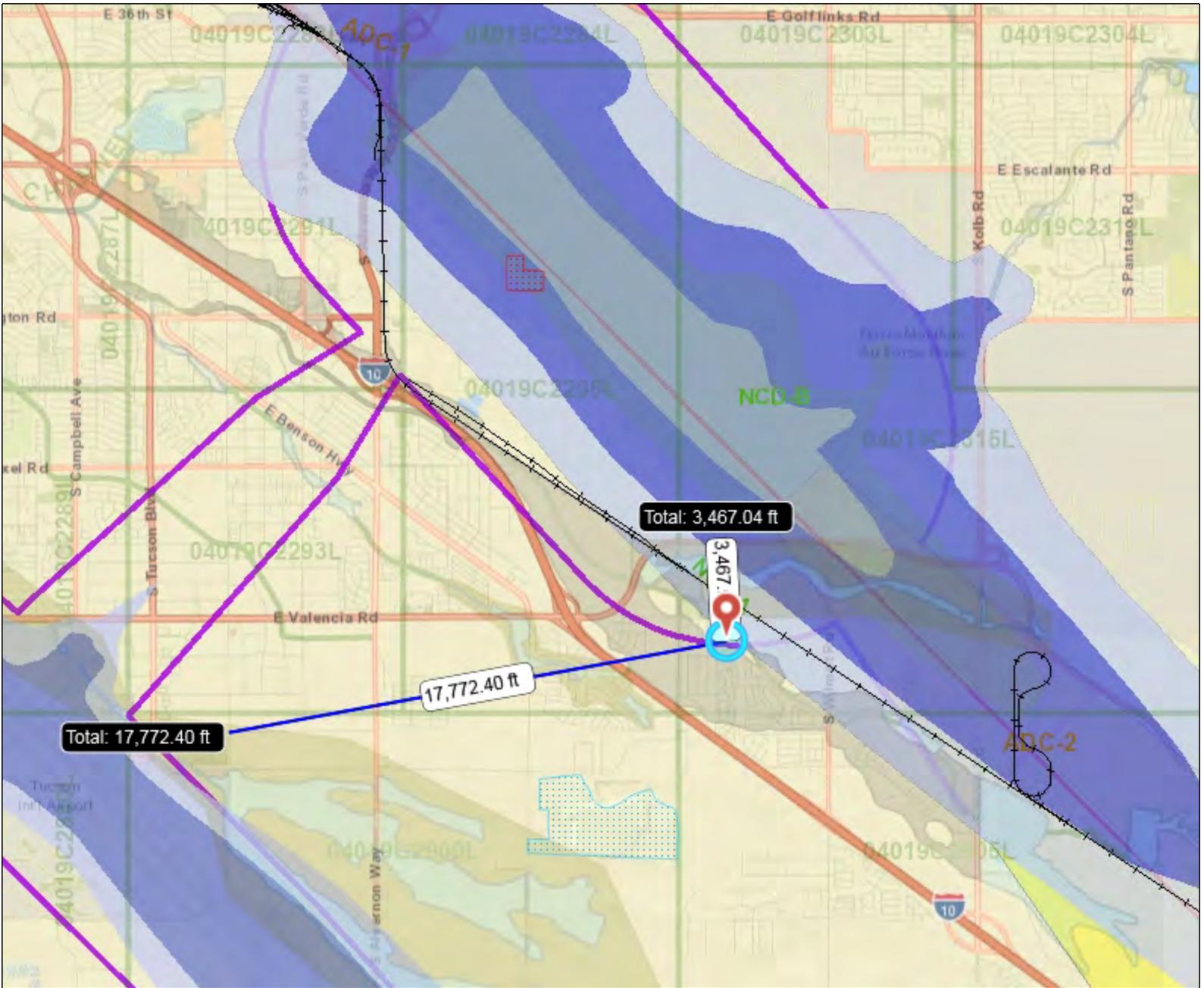
- INSTALLATION SHALL COMPLY WITH THE 2018 IRC / IPC WITH CITY OF TUCSON AMENDMENTS.
- THIS CONTRACTOR SHALL FIELD VERIFY ALL EXISTING CONDITIONS INCLUDING SEWER INVERT & ALL PIPE ROUTING AND SHALL NOTIFY OWNER OF ANY CONFLICTS WHICH MAY AFFECT THE SCOPE OF WORK.
- ALL FIXTURES TO HAVE ACCESSIBLE STOPS WHETHER SPECIFICALLY MENTIONED IN FIXTURE SPECIFICATIONS OR NOT.
- A DIELECTRIC UNION SHALL BE USED TO JOIN ANY DISSIMILAR METAL PIPING OR FITTINGS.
- CONTRACTOR SHALL FURNISH ANY MISCELLANEOUS ITEMS NORMALLY USED, OR GENERALLY INFERRED TO RENDER A COMPLETE WORKING AND COUNTY APPROVED INSTALLATION.
- CONTRACTOR SHALL GUARANTEE ALL PARTS AND LABOR FOR ONE (1) YEAR FROM DATE OF FINAL ACCEPTANCE OF PROJECT.
- DOMESTIC WATER PIPING SHALL BE TYPE "L" COPPER OR EQUAL. ALL JOINTS SHALL BE MADE WITH A LEAD-FREE SOLDER. PEX IS AN EQUIVALENT SUBSTITUTE.
- WASTE AND VENT PIPING SHALL BE ABS PLASTIC PIPE. VENT PIPING IN RETURN AIR PLENUM SHALL BE NO-HUB CAST IRON PIPE. INTERIOR WASTE PIPING SHALL SLOPE AT MINIMUM 2%.
- PLUMBING VENTS SHALL BE A MINIMUM OF 10 FEET AWAY FROM ALL AIR INTAKES PER 2018 IRC / IPC.
- HOT WATER PIPING SHALL HAVE MINIMUM OF R-3 INSULATION AT ALL OF THE FOLLOWING CONDITIONS: PIPING FROM THE WATER HEATER TO KITCHEN OUTLETS, PIPING LOCATED OUTSIDE THE CONDITIONED SPACE, OR PIPING WITH RUN LENGTHS GREATER THAN 20 NOMINAL FEET FOR 1/2" PIPING OR 10 NOMINAL FEET FOR 3/4" PIPING PER TABLE N1103.4.2.



2 PLUMBING PLAN LEVEL 2  
1/4" = 1'-0"



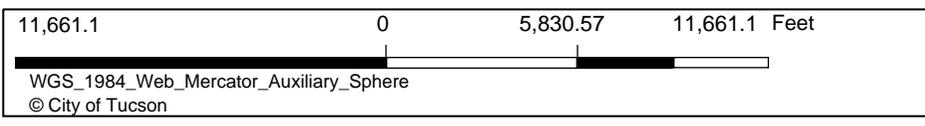
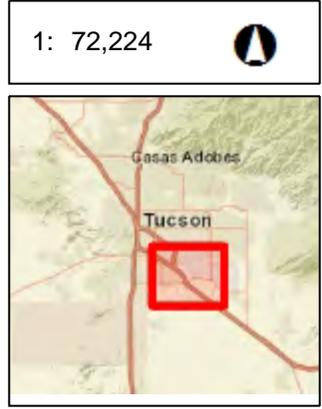
1 PLUMBING PLAN LEVEL 1  
1/4" = 1'-0"



**Notes**

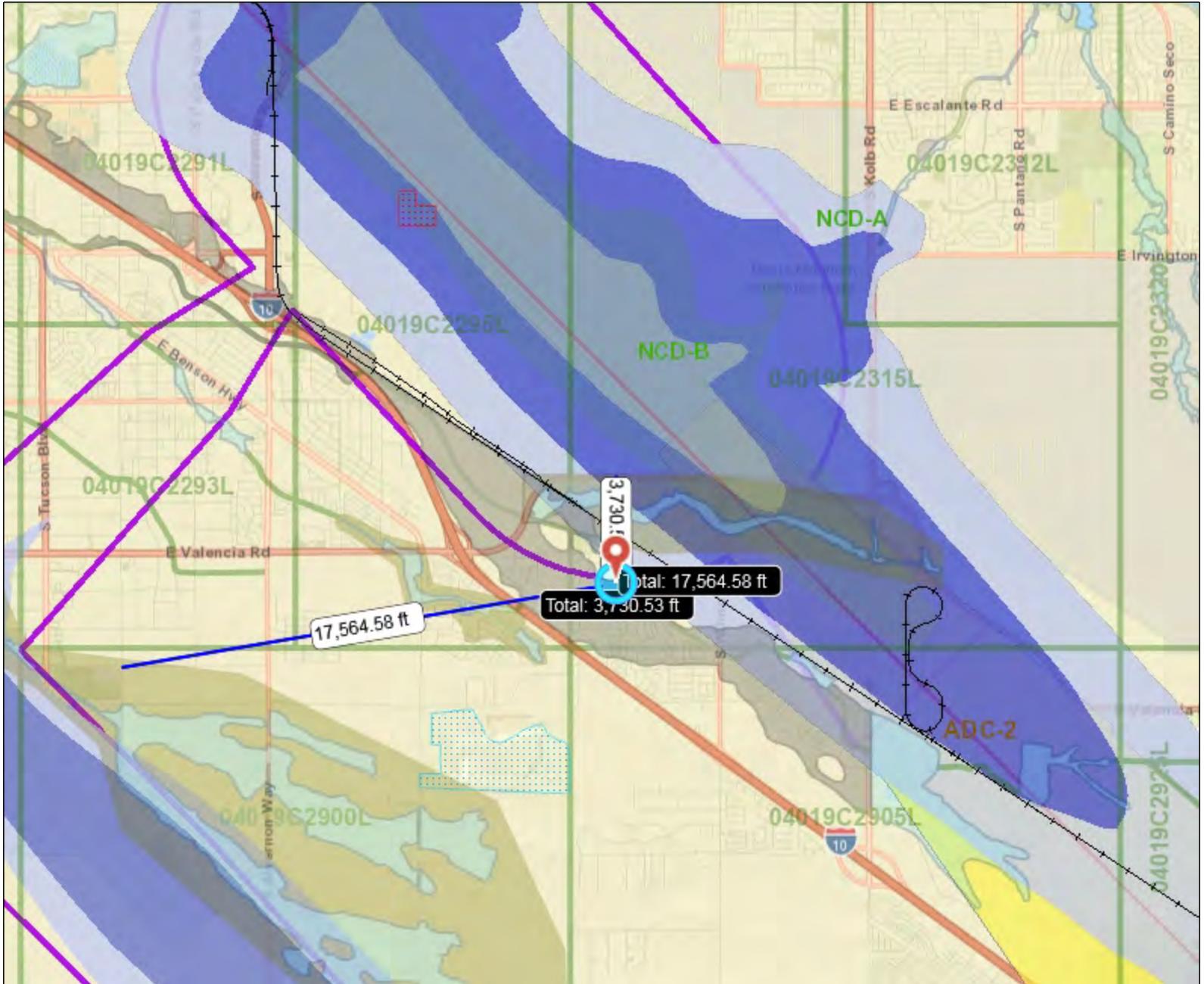
Airport Map

Legend	
Land Fills - City/County	LOMR
Land Fills - State/Federal	Excluded Structures/Parcels
Leaking Underground Storage Tanks - COT Owned	Other
A Zones	Floodway
ZONE A	1% Chance Flood Contained in Culvert/Channel
ZONE AE	FIRM
ZONE AH	Historic Properties
ZONE AO	Listed
X Zones	Listed and Local
Ineligible	Contributor
Non-Contributor	Eligible
No Data	City of Tucson Historic Zoning
Demolished Contributor	City of Tucson National Register Districts
Demolished	Eligible National
Demolished	Archaeological Sensitivity Zones
Vacant	Railroads
Outside of HD	AEZ Noise
City of Tucson Historic Zoning	65-70 LDN
City of Tucson National Register Districts	70-75 LDN
Current National	70-PLUS LDN
Eligible National	75-80 LDN
Archaeological Sensitivity Zones	80-PLUS LDN
Parcels	Avigation Easement & Disclosure Areas
DM AFB Approach-Departure Corridors	Airport Hazard Districts
ADC-1	Historic Landmark Zones
ADC-2	
ADC-3	
DM AFB Noise Control Districts	
NCD-A	
NCD-B	



This map is a user generated static output from an Internet mapping site and is for reference only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable.

**THIS MAP IS NOT TO BE USED FOR NAVIGATION**



Notes

Airport Map

Legend

- |   |   |  |   |  |
|---|---|--|---|--|
| <ul style="list-style-type: none"> <li> Land Fills - City/County</li> <li> Land Fills - State/Federal</li> <li> Leaking Underground Storage Tanks - COT Owned</li> <li><b>A Zones</b></li> <li> ZONE A</li> <li> ZONE AE</li> <li> ZONE AH</li> <li> ZONE AO</li> <li><b>X Zones</b></li> </ul> | <ul style="list-style-type: none"> <li> LOMR</li> <li> Excluded Structures/Parcels</li> <li> Other</li> <li> Floodway</li> <li> 1% Chance Flood Contained in Culvert/Channel</li> <li> FIRM</li> <li><b>Historic Properties</b></li> <li> Listed</li> <li> Listed and Local</li> <li> Contributor</li> <li> Eligible</li> </ul> | <ul style="list-style-type: none"> <li> Ineligible</li> <li> Non-Contributor</li> <li> No Data</li> <li> Demolished Contributor</li> <li> Demolished</li> <li> Demolished</li> <li> Vacant</li> <li> Outside of HD</li> <li> City of Tucson Historic Zoning</li> <li> City of Tucson National Register Districts</li> <li> Current National</li> </ul> | <ul style="list-style-type: none"> <li> Eligible National</li> <li> Archaeological Sensitivity Zones</li> <li> Railroads</li> <li><b>AEZ Noise</b></li> <li> 65-70 LDN</li> <li> 70-75 LDN</li> <li> 70-PLUS LDN</li> <li> 75-80 LDN</li> <li> 80-PLUS LDN</li> <li> Avigation Easement &amp; Disclosure Areas</li> </ul> | <ul style="list-style-type: none"> <li><b>DM AFB Approach-Departure Corridors</b></li> <li> ADC-1</li> <li> ADC-2</li> <li> ADC-3</li> <li><b>DM AFB Noise Control Districts</b></li> <li> NCD-A</li> <li> NCD-B</li> <li> Airport Hazard Districts</li> <li> Historic Landmark Zones</li> </ul> |
|---|---|--|---|--|

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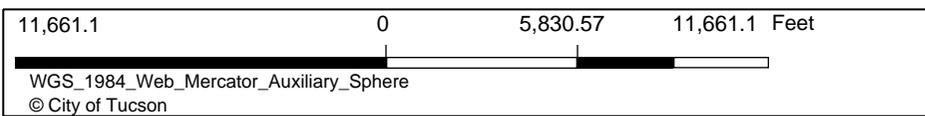
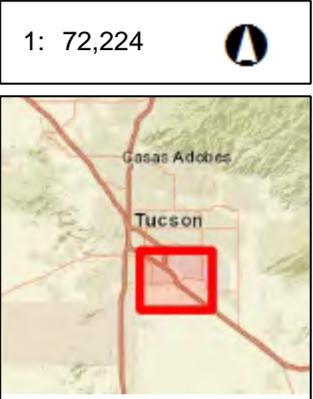
11,661.1 0 5,830.57 11,661.1 Feet



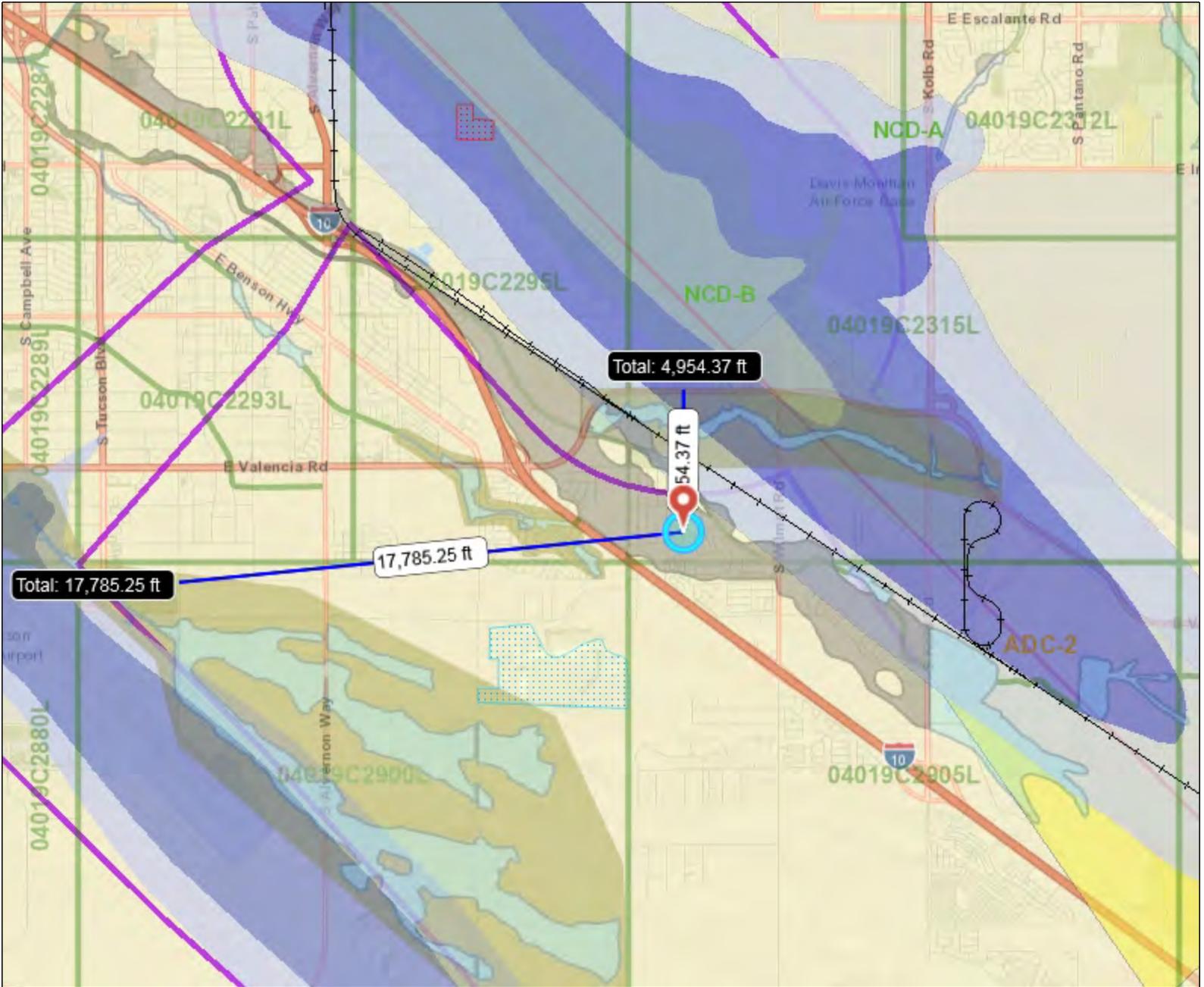
**Notes**

Airport Map

Legend	
Land Fills - City/County	LOMR
Land Fills - State/Federal	Excluded Structures/Parcels
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X Zone 2	Eligible
X Zone 3	Ineligible
X Zone 4	Non-Contributor
X Zone 5	No Data
X Zone 6	Demolished Contributor
X Zone 7	Demolished
X Zone 8	Demolished
X Zone 9	Vacant
X Zone 10	Outside of HD
X Zone 11	City of Tucson Historic Zoning
X Zone 12	City of Tucson National Register Districts
X Zone 13	Current National
Eligible National	Archaeological Sensitivity Zones
Parcels	Railroads
AEZ Noise	65-70 LDN
65-70 LDN	70-75 LDN
70-75 LDN	70-PLUS LDN
70-PLUS LDN	75-80 LDN
75-80 LDN	80-PLUS LDN
80-PLUS LDN	Avigation Easement & Disclosure Areas
Avigation Easement & Disclosure Areas	DM AFB Approach-Departure Corridors
DM AFB Approach-Departure Corridors	ADC-1
ADC-1	ADC-2
ADC-2	ADC-3
ADC-3	DM AFB Noise Control Districts
DM AFB Noise Control Districts	NCD-A
NCD-A	NCD-B
NCD-B	Airport Hazard Districts
Airport Hazard Districts	Historic Landmark Zones
Historic Landmark Zones	



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THIS MAP IS NOT TO BE USED FOR NAVIGATION

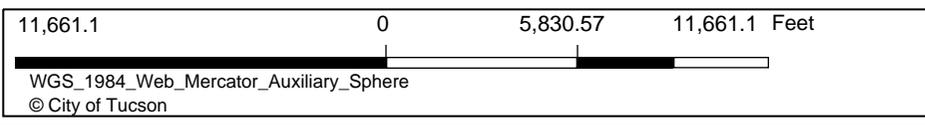


**Notes**

Airport Map

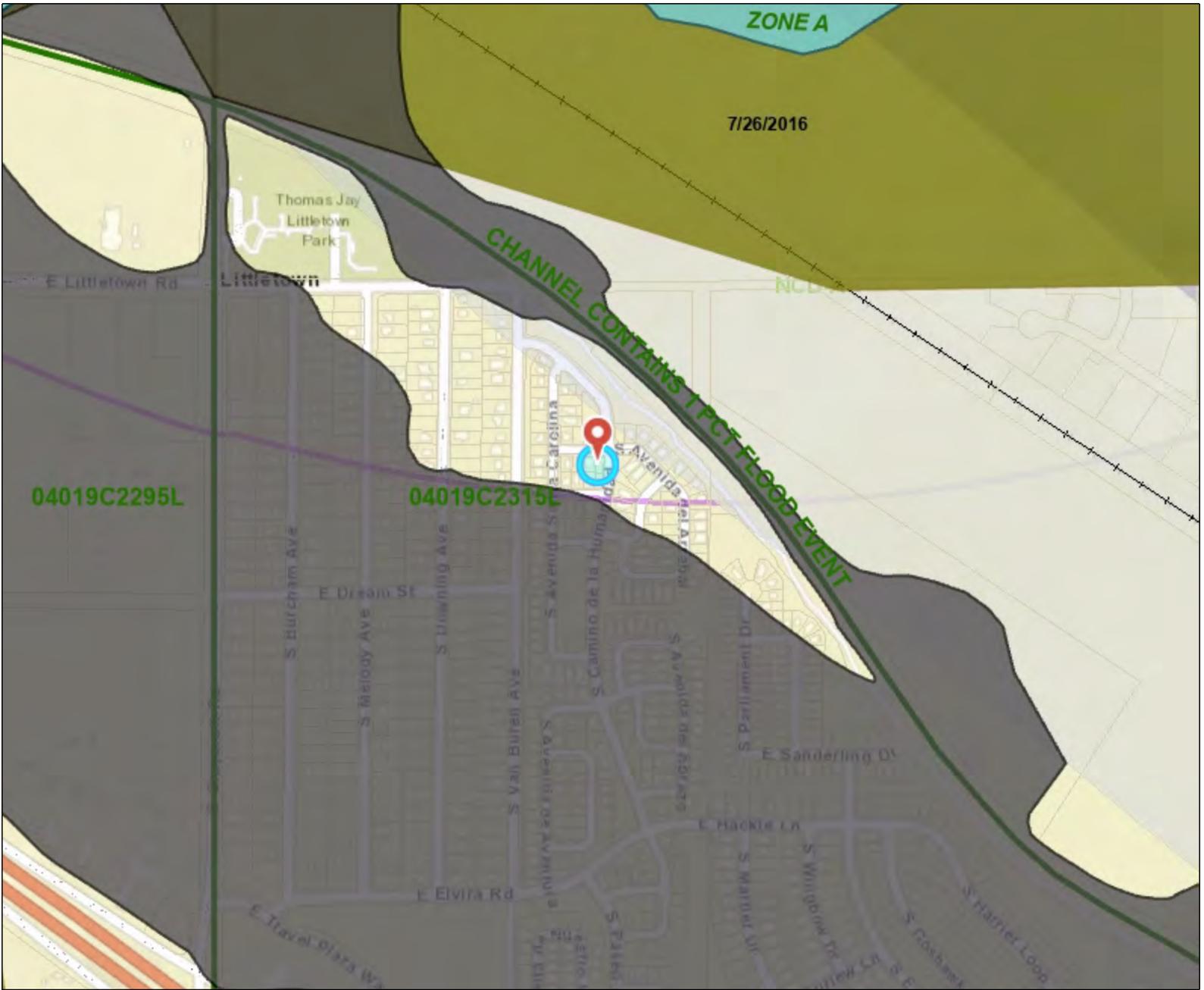
Legend	
Land Fills - City/County	LOMR
Land Fills - State/Federal	Excluded Structures/Parcels
Leaking Underground Storage Tanks - COT Owned	Other
A Zones	Floodway
ZONE A	1% Chance Flood Contained in Culvert/Channel
ZONE AE	FIRM
ZONE AH	Historic Properties
ZONE AO	Listed
X Zones	Listed and Local
Ineligible	Contributor
Non-Contributor	Eligible
No Data	City of Tucson Historic Zoning
Demolished Contributor	City of Tucson National Register Districts
Demolished	Eligible National
Demolished	Archaeological Sensitivity Zones
Vacant	Parcels
Outside of HD	Railroads
City of Tucson Historic Zoning	AEZ Noise
City of Tucson National Register Districts	65-70 LDN
Current National	70-75 LDN
Eligible National	70-PLUS LDN
Archaeological Sensitivity Zones	75-80 LDN
Parcels	80-PLUS LDN
Railroads	DM AFB Approach-Departur Corridors
AEZ Noise	ADC-1
65-70 LDN	ADC-2
70-75 LDN	ADC-3
70-PLUS LDN	DM AFB Noise Control Districts
75-80 LDN	NCD-A
80-PLUS LDN	NCD-B
DM AFB Approach-Departur Corridors	Airport Hazard Districts
ADC-1	Historic Landmark Zones
ADC-2	
ADC-3	
DM AFB Noise Control Districts	
NCD-A	
NCD-B	
Airport Hazard Districts	
Historic Landmark Zones	

Scale: 1: 72,224



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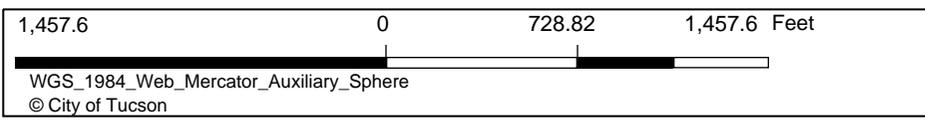


**Notes**

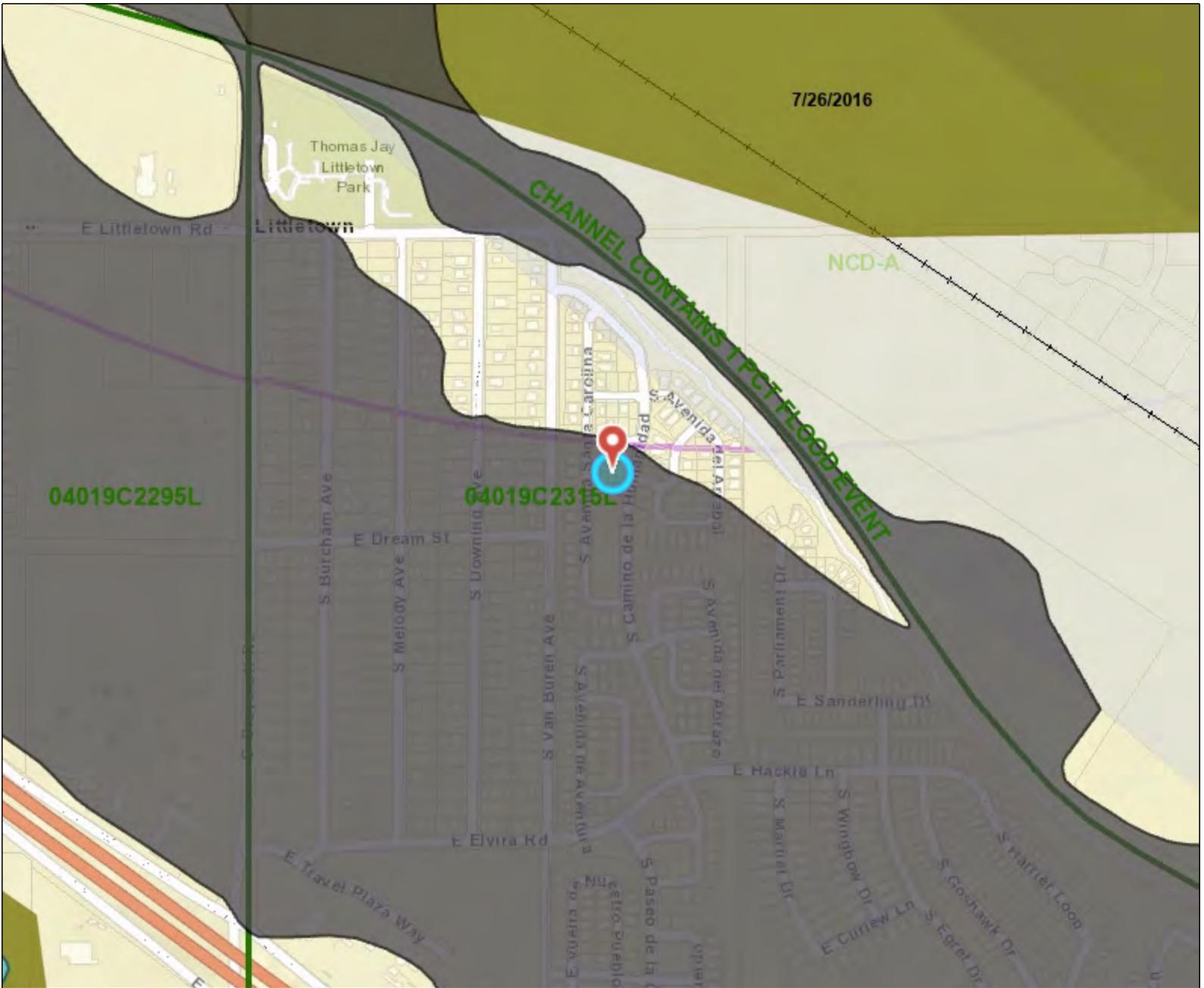
Flood Map

Legend	
Land Fills - City/County	LOMR
Land Fills - State/Federal	Excluded Structures/Parcels
Leaking Underground Storage Tanks - COT Owned	Other
<b>A Zones</b>	Floodway
ZONE A	1% Chance Flood Contained in Culvert/Channel
ZONE AE	FIRM
ZONE AH	Historic Properties
ZONE AO	Listed
<b>X Zones</b>	Listed and Local
X Zone	Contributor
Ineligible	City of Tucson Historic Zoning
Non-Contributor	City of Tucson National Register Districts
No Data	Current National
Demolished Contributor	Eligible National
Demolished	Archaeological Sensitivity Zones
Demolished	Parcels
Vacant	Railroads
Outside of HD	<b>AEZ Noise</b>
City of Tucson Historic Zoning	65-70 LDN
City of Tucson National Register Districts	70-75 LDN
Current National	70-PLUS LDN
	75-80 LDN
	80-PLUS LDN
	Avigation Easement & Disclosure Areas
	<b>DM AFB Approach-Departure Corridors</b>
	ADC-1
	ADC-2
	ADC-3
	<b>DM AFB Noise Control Districts</b>
	NCD-A
	NCD-B
	Airport Hazard Districts
	Historic Landmark Zones

1: 9,028



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THIS MAP IS NOT TO BE USED FOR NAVIGATION



Notes

Flood Map

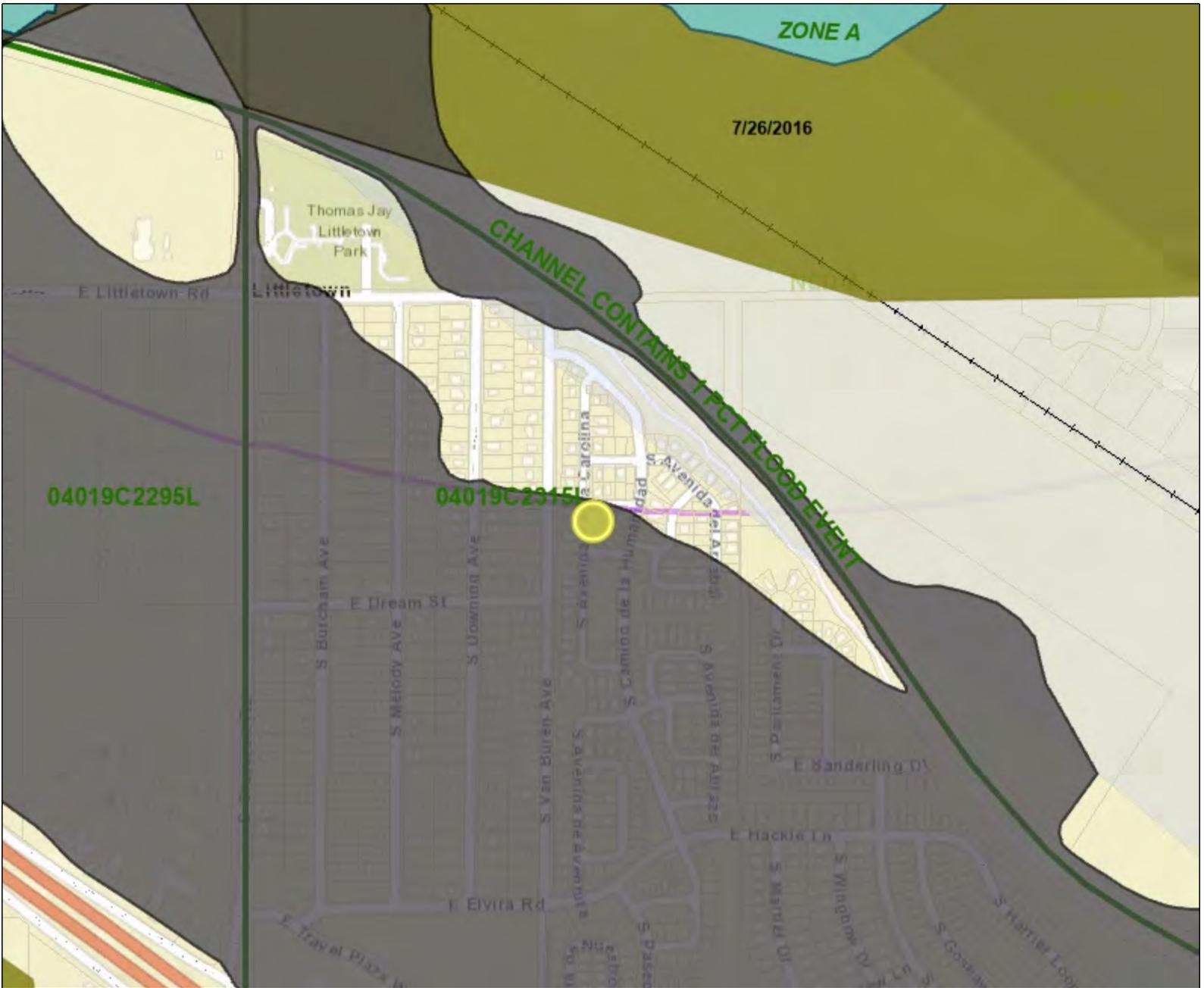
Legend

- |   |   |  |   |  |
|---|---|--|---|--|
| <ul style="list-style-type: none"> <li> Land Fills - City/County</li> <li> Land Fills - State/Federal</li> <li> Leaking Underground Storage Tanks - COT Owned</li> <li><b>A Zones</b></li> <li> ZONE A</li> <li> ZONE AE</li> <li> ZONE AH</li> <li> ZONE AO</li> <li><b>X Zones</b></li> </ul> | <ul style="list-style-type: none"> <li> LOMR</li> <li> Excluded Structures/Parcels</li> <li> Other</li> <li> Floodway</li> <li> 1% Chance Flood Contained in Culvert/Channel</li> <li> FIRM</li> <li><b>Historic Properties</b></li> <li> Listed</li> <li> Listed and Local</li> <li> Contributor</li> <li> Eligible</li> </ul> | <ul style="list-style-type: none"> <li> Ineligible</li> <li> Non-Contributor</li> <li> No Data</li> <li> Demolished Contributor</li> <li> Demolished</li> <li> Demolished</li> <li> Vacant</li> <li> Outside of HD</li> <li> City of Tucson Historic Zoning</li> <li> City of Tucson National Register Districts</li> <li> Current National</li> </ul> | <ul style="list-style-type: none"> <li> Eligible National</li> <li> Archaeological Sensitivity Zones</li> <li> Railroads</li> <li><b>AEZ Noise</b></li> <li> 65-70 LDN</li> <li> 70-75 LDN</li> <li> 70-PLUS LDN</li> <li> 75-80 LDN</li> <li> 80-PLUS LDN</li> <li> Avigation Easement &amp; Disclosure Areas</li> </ul> | <ul style="list-style-type: none"> <li><b>DM AFB Approach-Departure Corridors</b></li> <li> ADC-1</li> <li> ADC-2</li> <li> ADC-3</li> <li><b>DM AFB Noise Control Districts</b></li> <li> NCD-A</li> <li> NCD-B</li> <li> Airport Hazard Districts</li> <li> Historic Landmark Zones</li> </ul> |
|---|---|--|---|--|

1: 9,028



1,457.6 0 728.82 1,457.6 Feet



Notes

Flood Map

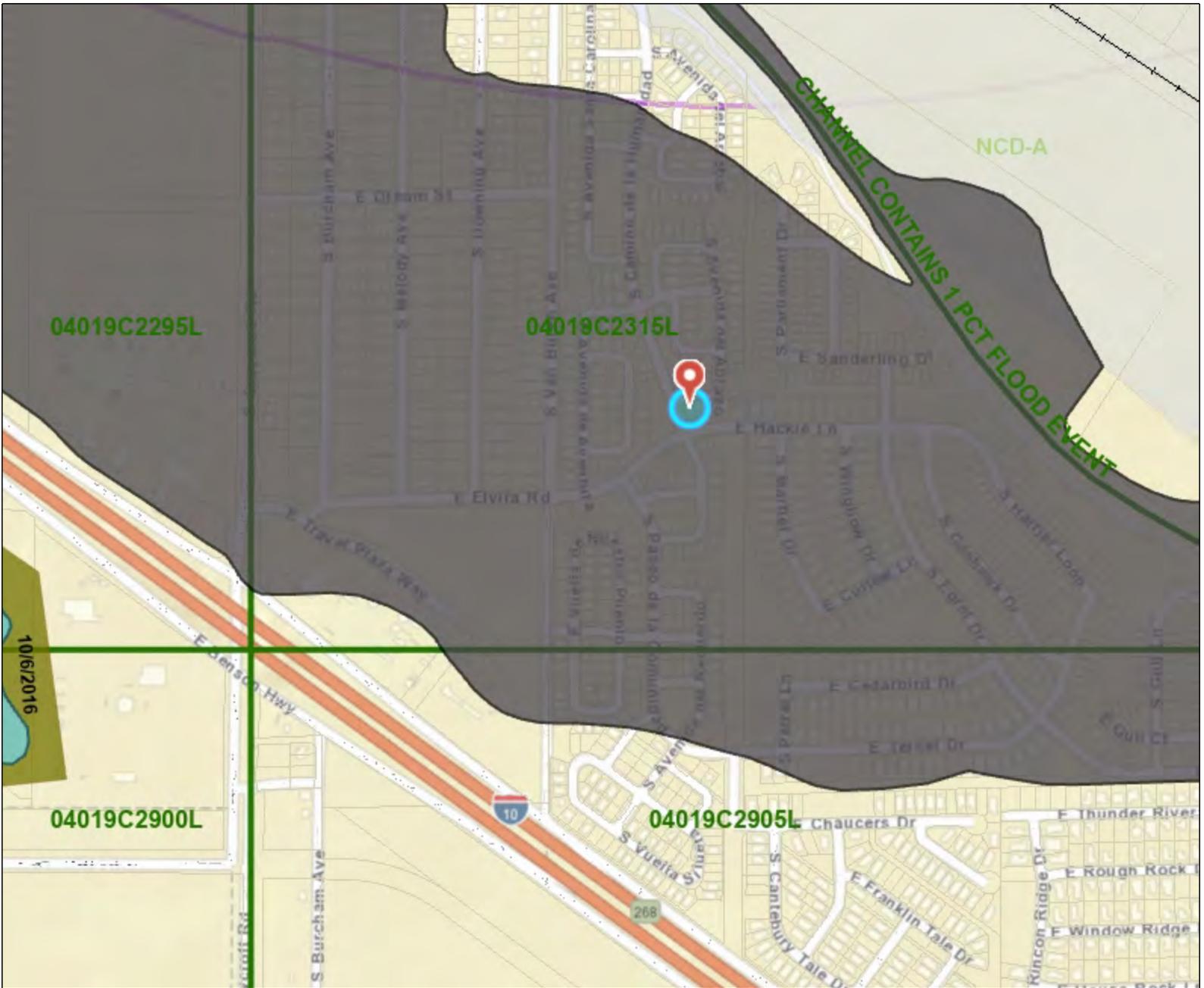
Legend

- |   |   |  |   |  |
|---|---|--|---|--|
| <ul style="list-style-type: none"> <li> Land Fills - City/County</li> <li> Land Fills - State/Federal</li> <li> Leaking Underground Storage Tanks - COT Owned</li> <li><b>A Zones</b></li> <li> ZONE A</li> <li> ZONE AE</li> <li> ZONE AH</li> <li> ZONE AO</li> <li><b>X Zones</b></li> </ul> | <ul style="list-style-type: none"> <li> LOMR</li> <li> Excluded Structures/Parcels</li> <li> Other</li> <li> Floodway</li> <li> 1% Chance Flood Contained in Culvert/Channel</li> <li> FIRM</li> <li><b>Historic Properties</b></li> <li> Listed</li> <li> Listed and Local</li> <li> Contributor</li> <li> Eligible</li> </ul> | <ul style="list-style-type: none"> <li> Ineligible</li> <li> Non-Contributor</li> <li> No Data</li> <li> Demolished Contributor</li> <li> Demolished</li> <li> Demolished</li> <li> Vacant</li> <li> Outside of HD</li> <li> City of Tucson Historic Zoning</li> <li> City of Tucson National Register Districts</li> <li> Current National</li> </ul> | <ul style="list-style-type: none"> <li> Eligible National</li> <li> Archaeological Sensitivity Zones</li> <li> Railroads</li> <li><b>AEZ Noise</b></li> <li> 65-70 LDN</li> <li> 70-75 LDN</li> <li> 70-PLUS LDN</li> <li> 75-80 LDN</li> <li> 80-PLUS LDN</li> <li> Avigation Easement &amp; Disclosure Areas</li> </ul> | <ul style="list-style-type: none"> <li><b>DM AFB Approach-Departure Corridors</b></li> <li> ADC-1</li> <li> ADC-2</li> <li> ADC-3</li> <li><b>DM AFB Noise Control Districts</b></li> <li> NCD-A</li> <li> NCD-B</li> <li> Airport Hazard Districts</li> <li> Historic Landmark Zones</li> </ul> |
|---|---|--|---|--|

1: 9,028



1,457.6 0 728.82 1,457.6 Feet



Notes

Flood Map

Legend

- |   |   |  |   |   |
|---|---|--|---|---|
| <ul style="list-style-type: none"> <li> Land Fills - City/County</li> <li> Land Fills - State/Federal</li> <li> Leaking Underground Storage Tanks - COT Owned</li> <li><b>A Zones</b> <ul style="list-style-type: none"> <li> ZONE A</li> <li> ZONE AE</li> <li> ZONE AH</li> <li> ZONE AO</li> </ul> </li> <li><b>X Zones</b></li> </ul> | <ul style="list-style-type: none"> <li> LOMR</li> <li> Excluded Structures/Parcels</li> <li> Other</li> <li> Floodway</li> <li> 1% Chance Flood Contained in Culvert/Channel</li> <li> FIRM</li> <li><b>Historic Properties</b> <ul style="list-style-type: none"> <li> Listed</li> <li> Listed and Local</li> <li> Contributor</li> <li> Eligible</li> </ul> </li> </ul> | <ul style="list-style-type: none"> <li> Ineligible</li> <li> Non-Contributor</li> <li> No Data</li> <li> Demolished Contributor</li> <li> Demolished</li> <li> Demolished</li> <li> Vacant</li> <li> Outside of HD</li> <li> City of Tucson Historic Zoning</li> <li> City of Tucson National Register Districts</li> <li> Current National</li> </ul> | <ul style="list-style-type: none"> <li> Eligible National</li> <li> Archaeological Sensitivity Zones</li> <li> Parcels</li> <li> Railroads</li> <li><b>AEZ Noise</b> <ul style="list-style-type: none"> <li> 65-70 LDN</li> <li> 70-75 LDN</li> <li> 70-PLUS LDN</li> <li> 75-80 LDN</li> <li> 80-PLUS LDN</li> </ul> </li> <li> Avigation Easement &amp; Disclosure Areas</li> </ul> | <ul style="list-style-type: none"> <li><b>DM AFB Approach-Departur Corridors</b> <ul style="list-style-type: none"> <li> ADC-1</li> <li> ADC-2</li> <li> ADC-3</li> </ul> </li> <li><b>DM AFB Noise Control Districts</b> <ul style="list-style-type: none"> <li> NCD-A</li> <li> NCD-B</li> </ul> </li> <li> Airport Hazard Districts</li> <li> Historic Landmark Zones</li> </ul> |
|---|---|--|---|---|

1: 9,028

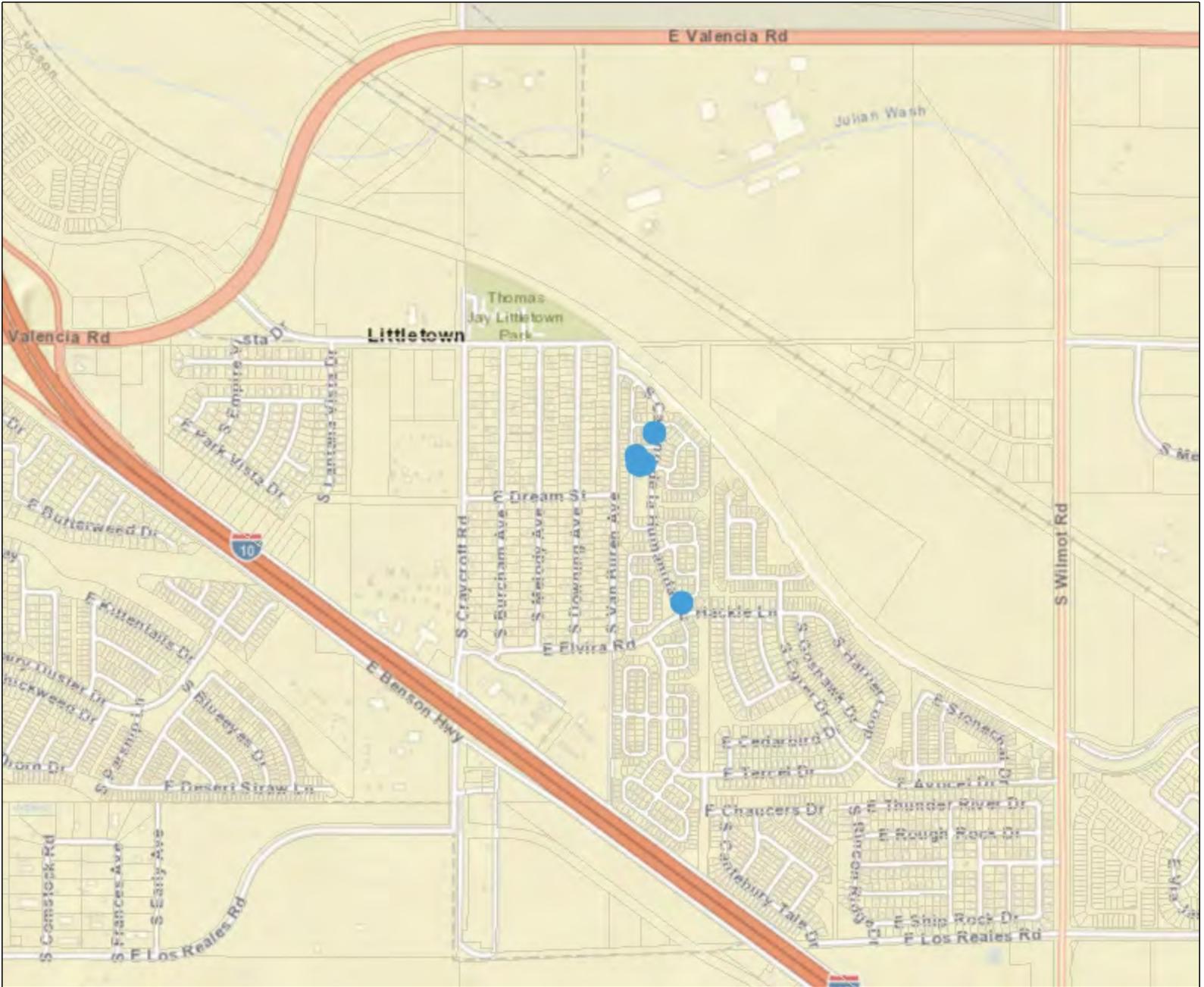


1,457.6 0 728.82 1,457.6 Feet

WGS\_1984\_Web\_Mercator\_Auxiliary\_Sphere  
© City of Tucson

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**Notes**

HPO Map  
 5717, 5725, 5733 E Calle De La Apertura  
 5716 E Camino De Las Iguanas  
 5746 E Calle De Hielo Dulce  
 5782 E Camino Esperanza Eterna

**Legend**

- |                                  |  |
|----------------------------------|--|
| ▲ City Individual Designations   | ■ Demolished                                 |
| ▲ County Individual Designations | ■ Demolished                                 |
| Historic Properties              | ■ Vacant                                     |
| ■ Listed                         | ■ Outside of HD                              |
| ■ Listed and Local Contributor   | ■ City of Tucson Historic Zoning             |
| ■ Eligible                       | ■ City of Tucson National Register Districts |
| ■ Ineligible                     | ■ Current National                           |
| ■ Non-Contributor                | ■ Eligible National                          |
| ■ No Data                        | ■ Archaeological Sensitivity Zones           |
| ■ Demolished Contributor         | ■ Parcels                                    |

1: 18,056



2,915.3      0      1,457.64      2,915.3 Feet

# FIELD VISIT CHECKLIST & SITE EVALUATION

**Date of Visit:** 9/15/2023

**Time:** 1:30 pm

**Grantee/Applicant:** Habitat for Humanity

**Project Name:** Corazon Del Pueblo

**NSP Project # (if funded):**

**Project Location/Address:** 5716 East Camino de las Iguanas, Tucson, AZ 85756

**Site Area (Acres):** approximately 0.08 acres

**Project Area Bounded By:**

North – East Camino de las Iguanas, followed by residence

East - residence

South – vacant residential lot

West – South Avenida Santa Carolina, followed by residence

**Field Visit Conducted By:** Rolanda Mazeika, Environmental Project Coordinator

**Attachments:**

Photographs  Yes  No

Maps  Yes  No

Aerials  Yes  No

**EXISTING ENVIRONMENTAL CONDITIONS ON & AROUND SITE:**

**Land Use/Zoning** - Check all that apply:

Residential

Recreational

Forest

Retail

Commercial

Pasture

Never Developed

Heavy industrial

Other:

Light Industrial

Currently Farmed

**General Description of Existing Conditions:** Explain any present on or applicable to site: air quality/odors, water quality, noise, flooding, drought, auto/pedestrian traffic, vegetation, spills, waste, drainage, livestock, construction occurring, structures, etc. Attach additional pages as necessary.

The lot is currently vacant and is located in the mostly completed Corazon del Pueblo residential subdivision. The site is located at the intersection of two residential streets and adjacent to a vacant lot and an existing residence. The subdivision streets are paved, with an unpaved access road traversing north to south on the eastern edge between adjacent subdivisions. The subdivision has paved sidewalks, detention/retention basins, small off street parking spaces, common areas, playgrounds, and other improvements supporting the neighborhood. Topographic maps indicate the elevation slopes slightly west to northwest. Surface flow traverses the neighborhood streets south/southwest to detention basins throughout the subdivision. No signs of erosion in detention/retention basins.

**Describe Site Access – Ingress & Egress:** Ease of accessing and exiting site, traffic control at site, safety, and line of site, etc.

The main subdivision entrances are located off South Wilmot Road from the east and South Van Buren Avenue from the west. South Camino de la Humanidad runs north/south from South Van Buren Avenue to East Elvira Road/East Hackle Lane through the northern half of the subdivision. Traffic signs provide traffic control throughout the subdivision. The subdivision is bounded the Julian Wash Greenway/Chuck Huckleberry Loop, "The Loop", with bike lanes followed by the Julian Wash (a riverine), followed by vacant industrially zoned property to the north and by the Interstate 10 highway to the south. Line of sight to the property is unobscured.

**Interviews Conducted** (include name and title of interviewees): Charlie Buchanan, Chief Executive Officer, and Jessica Raizk Carney, Chief Financial Officer, Habitat for Humanity Tucson

## FIELD VISIT CHECKLIST & SITE EVALUATION

**Existing infrastructure on or near site. Check all that apply and comment as appropriate.**

- |   |   |   |
|---|---|---|
| <input checked="" type="checkbox"/> Unpaved Roads<br>Condition: good<br><input checked="" type="checkbox"/> Paved Roads<br>Condition: good<br><input checked="" type="checkbox"/> Intersections<br>Condition: good<br><input checked="" type="checkbox"/> Curb/Guttering<br>Condition: good<br><input checked="" type="checkbox"/> Culverts<br>Condition: good<br><input checked="" type="checkbox"/> Drop Inlets<br>Condition: good<br><input type="checkbox"/> Ditches<br>Condition:<br><input type="checkbox"/> Other: | <input checked="" type="checkbox"/> Sidewalks<br>Condition: good<br><input type="checkbox"/> Crosswalks<br>Condition:<br><input checked="" type="checkbox"/> Parking Facilities<br>Condition: good<br><input checked="" type="checkbox"/> Fencing/Security<br>Condition: good<br><input type="checkbox"/> Bridges<br>Condition:<br><input checked="" type="checkbox"/> Railroad Facilities/Tracks/Spurs<br>Condition:<br><input type="checkbox"/> Water Treatment Facility<br>Condition:<br><input type="checkbox"/> Other: | <input type="checkbox"/> Sewer Treatment Facilities<br>Condition:<br><input type="checkbox"/> Septic Tanks<br>Condition:<br><input checked="" type="checkbox"/> Electricity<br><input checked="" type="checkbox"/> Water Lines<br><input checked="" type="checkbox"/> Gas Lines<br><input type="checkbox"/> Centralized Sewer System<br><input checked="" type="checkbox"/> Airport/Airport Clear Zones<br><input checked="" type="checkbox"/> Traffic Signs<br><input type="checkbox"/> Traffic Lights<br><input checked="" type="checkbox"/> Fire Hydrants<br><input type="checkbox"/> Bike/Pedestrian Lanes/Paths<br><input type="checkbox"/> 911 Emergency Systems<br><input type="checkbox"/> Other: |
|---|---|---|

**Check all that exist on or near the project site. Further evaluation and documentation may be warranted if there is potential for environmental concerns.**

- |   |   |  |
|---|---|--|
| <input type="checkbox"/> Aboveground Storage Tanks<br><input checked="" type="checkbox"/> Airport<br><input type="checkbox"/> Ambulance Facility<br><input type="checkbox"/> Animal Processing Plant<br><input type="checkbox"/> Auto Repair Shop<br><input type="checkbox"/> Biodiesel Facility<br><input type="checkbox"/> Bus Station<br><input type="checkbox"/> Car Dealership<br><input type="checkbox"/> Cemeteries<br><input type="checkbox"/> Chemical Manufacturer<br><input type="checkbox"/> Churches<br><input type="checkbox"/> Community Center<br><input type="checkbox"/> Conservation Areas<br><input type="checkbox"/> Daycare Center<br><input type="checkbox"/> Doctor's Office/Clinic<br><input type="checkbox"/> Dry Cleaner<br><input type="checkbox"/> Ethanol Plant<br><input type="checkbox"/> Fire Station<br><input type="checkbox"/> Funeral Home<br><input type="checkbox"/> Gas Station<br><input type="checkbox"/> Grocery Store | <input type="checkbox"/> Group Home<br><input type="checkbox"/> Heavily Traveled Roads<br><input type="checkbox"/> Heavy/Light Manufacturing Facilities<br><input checked="" type="checkbox"/> Highways – Interstate/State<br><input type="checkbox"/> Historical Sites/Buildings<br><input type="checkbox"/> Hospitals<br><input type="checkbox"/> Lakes/Ponds<br><input checked="" type="checkbox"/> Landfills<br><input type="checkbox"/> Library<br><input checked="" type="checkbox"/> Low Income Housing<br><input checked="" type="checkbox"/> Museum<br><input checked="" type="checkbox"/> Neighborhoods<br><input type="checkbox"/> Nursing Homes<br><input type="checkbox"/> Office Buildings<br><input type="checkbox"/> Open Spaces<br><input type="checkbox"/> Paint Facilities<br><input checked="" type="checkbox"/> Parks/Playgrounds<br><input type="checkbox"/> Police Station<br><input type="checkbox"/> Power Station<br><input type="checkbox"/> Printing Facilities | <input type="checkbox"/> Prison/Jail/Detention Center<br><input type="checkbox"/> Quarry<br><input type="checkbox"/> Recycling Facilities<br><input type="checkbox"/> Restaurants<br><input type="checkbox"/> Retaining Walls<br><input type="checkbox"/> Rivers<br><input checked="" type="checkbox"/> Schools<br><input type="checkbox"/> Senior Center<br><input type="checkbox"/> Senior Housing<br><input type="checkbox"/> Sheltered Workshop<br><input type="checkbox"/> Shopping Centers<br><input type="checkbox"/> Storage/Use of Explosives/Flammables<br><input checked="" type="checkbox"/> Streams/Creeks<br><input type="checkbox"/> Train Depot<br><input type="checkbox"/> Underground Storage Tanks<br><input type="checkbox"/> Veterinary Clinic/Hospital<br><input type="checkbox"/> Youth Center<br><input type="checkbox"/> Other:<br><input type="checkbox"/> Other:<br><input type="checkbox"/> Other: |
|---|---|--|

**FIELD VISIT CHECKLIST & SITE EVALUATION - *Supplemental***  
**CONTAMINATION & TOXIC MATERIALS**

**None** identified on or adjacent to project site

Based upon visual inspections of the project site and adjacent properties, indicate evidence of the following. An explanation should accompany any checked items.

- Distressed Vegetation** – A possible indication of soil contamination
- Vent or Fill Pipes** – A possible indication of current or previous existence of underground storage tanks
- Storage/Oil Tanks or Questionable Containers** – Possible indication of the use of heating fuels, chemicals, and petroleum products
- Pits, Ponds, or Lagoons** – These have the potential to hold liquids or sludge-containing hazardous substances or petroleum products. The potential is increased if there also exists water discoloration, distressed vegetation, and/or wastewater discharge.
- Stained Soil or Pavement** (other than water stains) – A possible indication soil is contaminated as well as a sign of current or previous leakage of piping and liquid storage containers
- Pungent, Foul or Noxious Odors** – A possible indication of leaks of hazardous substances or petroleum products or contaminants
- Dumped Material or Soil, Mounds of Dirt, Rubble Fill, etc.** – There is potential for transfer of contamination as the origins may be unknown. Look for other signs of contamination as described above.

Provide any supplementary information, descriptions, explanations, and/or comments below. Attach additional pages as appropriate.

- A network of detention/retention basins are located throughout the subdivision for stormwater management

Signature & Title of Preparer

Date Form Completed: 9/15/2023



Rolanda Mazeika, Environmental Project Coordinator  
City of Tucson, Housing & Community Development Department

5716 East Camino de las Iguanas - North elevation (facing south)



East adjacent residence (facing southeast)



South Avenida Santa Carolina, followed by residence  
(facing west)



# FIELD VISIT CHECKLIST & SITE EVALUATION

Date of Visit: 9/15/2023

Time: 1:30 pm

Grantee/Applicant: Habitat for Humanity

Project Name: Corazon Del Pueblo

NSP Project # (if funded):

Project Location/Address: 5717, 5725, and 5733 East Calle de la Apertura, Tucson, AZ 85756

Site Area (Acres): approximately 0.08, 0.07, and 0.07 acres, respectively

**Project Area Bounded By:**

North – vacant residential lot (northwest) and residences

East – residence

South – East Calle de la Apertura, followed by park/detention/retention basin

West – South Avenida Santa Carolina, followed by residence

Field Visit Conducted By: Rolanda Mazeika, Environmental Project Coordinator

**Attachments:**

Photographs  Yes  No

Maps  Yes  No

Aerials  Yes  No

**EXISTING ENVIRONMENTAL CONDITIONS ON & AROUND SITE:**

**Land Use/Zoning** - Check all that apply:

Residential

Recreational

Forest

Retail

Commercial

Pasture

Never Developed

Heavy industrial

Other:

Light Industrial

Currently Farmed

**General Description of Existing Conditions:** Explain any present on or applicable to site: air quality/odors, water quality, noise, flooding, drought, auto/pedestrian traffic, vegetation, spills, waste, drainage, livestock, construction occurring, structures, etc. Attach additional pages as necessary.

The lots are currently vacant and are located in the mostly completed Corazon del Pueblo residential subdivision. The lots at 5725 and 5733 East Calle de la Apertura contain construction debris consisting of soil, concrete, and rock. The lot at 5725 also contains a concrete form constructed of concrete, plywood, lumber, and plastic sheeting. The site is located at the intersection of two residential streets and adjacent to a vacant lot and an existing residence. The subdivision streets are paved, with an unpaved access road traversing north to south on the eastern edge between adjacent subdivisions. The subdivision has paved sidewalks, detention/retention basins, small off street parking spaces, common areas, playgrounds, and other improvements supporting the neighborhood. Topographic maps indicate the elevation slopes slightly west to northwest. Surface flow traverses the neighborhood streets south/southwest to detention basins throughout the subdivision. No signs of erosion in detention/retention basins.

**Describe Site Access – Ingress & Egress:** Ease of accessing and exiting site, traffic control at site, safety, and line of site, etc.

The main subdivision entrances are located off South Wilmot Road from the east and South Van Buren Avenue from the west. South Camino de la Humanidad runs north/south from South Van Buren Avenue to East Elvira Road/East Hackle Lane through the northern half of the subdivision. Traffic signs provide traffic control throughout the subdivision. The subdivision is bounded the Julian Wash Greenway/Chuck Huckleberry Loop, "The Loop", with bike lanes followed by the Julian Wash (a riverine), followed by vacant industrially zoned property to the north and by the Interstate 10 highway to the south. Line of sight to the property is unobscured.

**Interviews Conducted** (include name and title of interviewees): Charlie Buchanan, Chief Executive Officer, and Jessica Raizk Carney, Chief Financial Officer, Habitat for Humanity Tucson

## FIELD VISIT CHECKLIST & SITE EVALUATION

**Existing infrastructure on or near site. Check all that apply and comment as appropriate.**

- |   |   |   |
|---|---|---|
| <input checked="" type="checkbox"/> Unpaved Roads<br>Condition: good<br><input checked="" type="checkbox"/> Paved Roads<br>Condition: good<br><input checked="" type="checkbox"/> Intersections<br>Condition: good<br><input checked="" type="checkbox"/> Curb/Guttering<br>Condition: good<br><input checked="" type="checkbox"/> Culverts<br>Condition: good<br><input checked="" type="checkbox"/> Drop Inlets<br>Condition: good<br><input type="checkbox"/> Ditches<br>Condition:<br><input type="checkbox"/> Other: | <input checked="" type="checkbox"/> Sidewalks<br>Condition: good<br><input type="checkbox"/> Crosswalks<br>Condition:<br><input checked="" type="checkbox"/> Parking Facilities<br>Condition: good<br><input checked="" type="checkbox"/> Fencing/Security<br>Condition: good<br><input type="checkbox"/> Bridges<br>Condition:<br><input checked="" type="checkbox"/> Railroad Facilities/Tracks/Spurs<br>Condition:<br><input type="checkbox"/> Water Treatment Facility<br>Condition:<br><input type="checkbox"/> Other: | <input type="checkbox"/> Sewer Treatment Facilities<br>Condition:<br><input type="checkbox"/> Septic Tanks<br>Condition:<br><input checked="" type="checkbox"/> Electricity<br><input checked="" type="checkbox"/> Water Lines<br><input checked="" type="checkbox"/> Gas Lines<br><input type="checkbox"/> Centralized Sewer System<br><input checked="" type="checkbox"/> Airport/Airport Clear Zones<br><input checked="" type="checkbox"/> Traffic Signs<br><input type="checkbox"/> Traffic Lights<br><input checked="" type="checkbox"/> Fire Hydrants<br><input type="checkbox"/> Bike/Pedestrian Lanes/Paths<br><input type="checkbox"/> 911 Emergency Systems<br><input type="checkbox"/> Other: |
|---|---|---|

**Check all that exist on or near the project site. Further evaluation and documentation may be warranted if there is potential for environmental concerns.**

- |   |   |  |
|---|---|--|
| <input type="checkbox"/> Aboveground Storage Tanks<br><input checked="" type="checkbox"/> Airport<br><input type="checkbox"/> Ambulance Facility<br><input type="checkbox"/> Animal Processing Plant<br><input type="checkbox"/> Auto Repair Shop<br><input type="checkbox"/> Biodiesel Facility<br><input type="checkbox"/> Bus Station<br><input type="checkbox"/> Car Dealership<br><input type="checkbox"/> Cemeteries<br><input type="checkbox"/> Chemical Manufacturer<br><input type="checkbox"/> Churches<br><input type="checkbox"/> Community Center<br><input type="checkbox"/> Conservation Areas<br><input type="checkbox"/> Daycare Center<br><input type="checkbox"/> Doctor's Office/Clinic<br><input type="checkbox"/> Dry Cleaner<br><input type="checkbox"/> Ethanol Plant<br><input type="checkbox"/> Fire Station<br><input type="checkbox"/> Funeral Home<br><input type="checkbox"/> Gas Station<br><input type="checkbox"/> Grocery Store | <input type="checkbox"/> Group Home<br><input type="checkbox"/> Heavily Traveled Roads<br><input type="checkbox"/> Heavy/Light Manufacturing Facilities<br><input checked="" type="checkbox"/> Highways – Interstate/State<br><input type="checkbox"/> Historical Sites/Buildings<br><input type="checkbox"/> Hospitals<br><input type="checkbox"/> Lakes/Ponds<br><input checked="" type="checkbox"/> Landfills<br><input type="checkbox"/> Library<br><input checked="" type="checkbox"/> Low Income Housing<br><input checked="" type="checkbox"/> Museum<br><input checked="" type="checkbox"/> Neighborhoods<br><input type="checkbox"/> Nursing Homes<br><input type="checkbox"/> Office Buildings<br><input type="checkbox"/> Open Spaces<br><input type="checkbox"/> Paint Facilities<br><input checked="" type="checkbox"/> Parks/Playgrounds<br><input type="checkbox"/> Police Station<br><input type="checkbox"/> Power Station<br><input type="checkbox"/> Printing Facilities | <input type="checkbox"/> Prison/Jail/Detention Center<br><input type="checkbox"/> Quarry<br><input type="checkbox"/> Recycling Facilities<br><input type="checkbox"/> Restaurants<br><input type="checkbox"/> Retaining Walls<br><input type="checkbox"/> Rivers<br><input checked="" type="checkbox"/> Schools<br><input type="checkbox"/> Senior Center<br><input type="checkbox"/> Senior Housing<br><input type="checkbox"/> Sheltered Workshop<br><input type="checkbox"/> Shopping Centers<br><input type="checkbox"/> Storage/Use of Explosives/Flammables<br><input checked="" type="checkbox"/> Streams/Creeks<br><input type="checkbox"/> Train Depot<br><input type="checkbox"/> Underground Storage Tanks<br><input type="checkbox"/> Veterinary Clinic/Hospital<br><input type="checkbox"/> Youth Center<br><input type="checkbox"/> Other:<br><input type="checkbox"/> Other:<br><input type="checkbox"/> Other: |
|---|---|--|

**FIELD VISIT CHECKLIST & SITE EVALUATION - *Supplemental***  
**CONTAMINATION & TOXIC MATERIALS**

**None** identified on or adjacent to project site

Based upon visual inspections of the project site and adjacent properties, indicate evidence of the following. An explanation should accompany any checked items.

- Distressed Vegetation** – A possible indication of soil contamination
- Vent or Fill Pipes** – A possible indication of current or previous existence of underground storage tanks
- Storage/Oil Tanks or Questionable Containers** – Possible indication of the use of heating fuels, chemicals, and petroleum products
- Pits, Ponds, or Lagoons** – These have the potential to hold liquids or sludge-containing hazardous substances or petroleum products. The potential is increased if there also exists water discoloration, distressed vegetation, and/or wastewater discharge.
- Stained Soil or Pavement** (other than water stains) – A possible indication soil is contaminated as well as a sign of current or previous leakage of piping and liquid storage containers
- Pungent, Foul or Noxious Odors** – A possible indication of leaks of hazardous substances or petroleum products or contaminants
- Dumped Material or Soil, Mounds of Dirt, Rubble Fill, etc.** – There is potential for transfer of contamination as the origins may be unknown. Look for other signs of contamination as described above.

Provide any supplementary information, descriptions, explanations, and/or comments below. Attach additional pages as appropriate.

- A network of detention/retention basins are located throughout the subdivision for stormwater management
- The lots at 5725 and 5733 East Calle de la Apertura contain construction debris consisting of soil, concrete, and rock

Signature & Title of Preparer

Date Form Completed: 9/15/2023



Rolanda Mazeika, Environmental Project Coordinator  
City of Tucson, Housing & Community Development Department

5733 East Calle de la Apertura - soil/rock pile



5733 East Calle de la Apertura - south elevation (facing north)



5725 East Calle de la Apertura - south elevation  
(facing north)



5717 East Calle de la Apertura - south elevation  
(facing north)



East adjacent residence - south elevation (facing north)



East Calle de la Apertura, followed by community park/detention/retention basin (facing south)



South Avenida Santa Carolina, followed by residence  
- East elevation (facing west)



South Avenida Santa Carolina, followed by residence  
- East elevation (facing west)



5725 East Calle de la Apertura - concrete form (facing south)



# FIELD VISIT CHECKLIST & SITE EVALUATION

**Date of Visit:** 9/15/2023

**Time:** 1:30 pm

**Grantee/Applicant:** Habitat for Humanity

**Project Name:** Corazon Del Pueblo

**NSP Project # (if funded):**

**Project Location/Address:** 5746 East Calle de Hielo Dulce, Tucson, AZ 85756

**Site Area (Acres):** approximately 0.08 acres

**Project Area Bounded By:**

- North – East Calle de Hielo Dulce, followed by common area
- East - South Camino de la Humanidad, followed by common area/detention basin
- South – common area
- West – residence

**Field Visit Conducted By:** Rolanda Mazeika, Environmental Project Coordinator

**Attachments:**

Photographs  Yes  No                      Maps  Yes  No                      Aerials  Yes  No

**EXISTING ENVIRONMENTAL CONDITIONS ON & AROUND SITE:**

**Land Use/Zoning** - Check all that apply:

- |   |   |                                  |
|---|---|----------------------------------|
| <input checked="" type="checkbox"/> Residential | <input type="checkbox"/> Recreational     | <input type="checkbox"/> Forest  |
| <input type="checkbox"/> Retail                 | <input type="checkbox"/> Commercial       | <input type="checkbox"/> Pasture |
| <input type="checkbox"/> Never Developed        | <input type="checkbox"/> Heavy industrial | <input type="checkbox"/> Other:  |
| <input type="checkbox"/> Light Industrial       | <input type="checkbox"/> Currently Farmed |                                  |

**General Description of Existing Conditions:** Explain any present on or applicable to site: air quality/odors, water quality, noise, flooding, drought, auto/pedestrian traffic, vegetation, spills, waste, drainage, livestock, construction occurring, structures, etc. Attach additional pages as necessary.

The lot is currently vacant and is located in the mostly completed Corazon del Pueblo residential subdivision. The site is located at the intersection of two residential streets and to an existing residence and a common area. The subdivision streets are paved, with an unpaved access road traversing north to south on the eastern edge between adjacent subdivisions. The subdivision has paved sidewalks, detention/retention basins, small off street parking spaces, common areas, playgrounds, and other improvements supporting the neighborhood. Topographic maps indicate the elevation slopes slightly west to northwest. Surface flow traverses the neighborhood streets south/southwest to detention basins throughout the subdivision. No signs of erosion in detention/retention basins.

**Describe Site Access – Ingress & Egress:** Ease of accessing and exiting site, traffic control at site, safety, and line of site, etc.

The main subdivision entrances are located off South Wilmot Road from the east and South Van Buren Avenue from the west. South Camino de la Humanidad runs north/south from South Van Buren Avenue to East Elvira Road/East Hackle Lane through the northern half of the subdivision. Traffic signs provide traffic control throughout the subdivision. The subdivision is bounded the Julian Wash Greenway/Chuck Huckleberry Loop, "The Loop", with bike lanes followed by the Julian Wash (a riverine), followed by vacant industrially zoned property to the north and by the Interstate 10 highway to the south. Line of sight to the property is unobscured.

**Interviews Conducted** (include name and title of interviewees): Charlie Buchanan, Chief Executive Officer, and Jessica Raizk Carney, Chief Financial Officer, Habitat for Humanity Tucson

## FIELD VISIT CHECKLIST & SITE EVALUATION

**Existing infrastructure on or near site. Check all that apply and comment as appropriate.**

- |   |   |   |
|---|---|---|
| <input checked="" type="checkbox"/> Unpaved Roads<br>Condition: good<br><input checked="" type="checkbox"/> Paved Roads<br>Condition: good<br><input checked="" type="checkbox"/> Intersections<br>Condition: good<br><input checked="" type="checkbox"/> Curb/Guttering<br>Condition: good<br><input checked="" type="checkbox"/> Culverts<br>Condition: good<br><input checked="" type="checkbox"/> Drop Inlets<br>Condition: good<br><input type="checkbox"/> Ditches<br>Condition:<br><input type="checkbox"/> Other: | <input checked="" type="checkbox"/> Sidewalks<br>Condition: good<br><input type="checkbox"/> Crosswalks<br>Condition:<br><input checked="" type="checkbox"/> Parking Facilities<br>Condition: good<br><input checked="" type="checkbox"/> Fencing/Security<br>Condition: good<br><input type="checkbox"/> Bridges<br>Condition:<br><input checked="" type="checkbox"/> Railroad Facilities/Tracks/Spurs<br>Condition:<br><input type="checkbox"/> Water Treatment Facility<br>Condition:<br><input type="checkbox"/> Other: | <input type="checkbox"/> Sewer Treatment Facilities<br>Condition:<br><input type="checkbox"/> Septic Tanks<br>Condition:<br><input checked="" type="checkbox"/> Electricity<br><input checked="" type="checkbox"/> Water Lines<br><input checked="" type="checkbox"/> Gas Lines<br><input type="checkbox"/> Centralized Sewer System<br><input checked="" type="checkbox"/> Airport/Airport Clear Zones<br><input checked="" type="checkbox"/> Traffic Signs<br><input type="checkbox"/> Traffic Lights<br><input checked="" type="checkbox"/> Fire Hydrants<br><input type="checkbox"/> Bike/Pedestrian Lanes/Paths<br><input type="checkbox"/> 911 Emergency Systems<br><input type="checkbox"/> Other: |
|---|---|---|

**Check all that exist on or near the project site. Further evaluation and documentation may be warranted if there is potential for environmental concerns.**

- |   |   |  |
|---|---|--|
| <input type="checkbox"/> Aboveground Storage Tanks<br><input checked="" type="checkbox"/> Airport<br><input type="checkbox"/> Ambulance Facility<br><input type="checkbox"/> Animal Processing Plant<br><input type="checkbox"/> Auto Repair Shop<br><input type="checkbox"/> Biodiesel Facility<br><input type="checkbox"/> Bus Station<br><input type="checkbox"/> Car Dealership<br><input type="checkbox"/> Cemeteries<br><input type="checkbox"/> Chemical Manufacturer<br><input type="checkbox"/> Churches<br><input type="checkbox"/> Community Center<br><input type="checkbox"/> Conservation Areas<br><input type="checkbox"/> Daycare Center<br><input type="checkbox"/> Doctor's Office/Clinic<br><input type="checkbox"/> Dry Cleaner<br><input type="checkbox"/> Ethanol Plant<br><input type="checkbox"/> Fire Station<br><input type="checkbox"/> Funeral Home<br><input type="checkbox"/> Gas Station<br><input type="checkbox"/> Grocery Store | <input type="checkbox"/> Group Home<br><input type="checkbox"/> Heavily Traveled Roads<br><input type="checkbox"/> Heavy/Light Manufacturing Facilities<br><input checked="" type="checkbox"/> Highways – Interstate/State<br><input type="checkbox"/> Historical Sites/Buildings<br><input type="checkbox"/> Hospitals<br><input type="checkbox"/> Lakes/Ponds<br><input checked="" type="checkbox"/> Landfills<br><input type="checkbox"/> Library<br><input checked="" type="checkbox"/> Low Income Housing<br><input checked="" type="checkbox"/> Museum<br><input checked="" type="checkbox"/> Neighborhoods<br><input type="checkbox"/> Nursing Homes<br><input type="checkbox"/> Office Buildings<br><input type="checkbox"/> Open Spaces<br><input type="checkbox"/> Paint Facilities<br><input checked="" type="checkbox"/> Parks/Playgrounds<br><input type="checkbox"/> Police Station<br><input type="checkbox"/> Power Station<br><input type="checkbox"/> Printing Facilities | <input type="checkbox"/> Prison/Jail/Detention Center<br><input type="checkbox"/> Quarry<br><input type="checkbox"/> Recycling Facilities<br><input type="checkbox"/> Restaurants<br><input type="checkbox"/> Retaining Walls<br><input type="checkbox"/> Rivers<br><input checked="" type="checkbox"/> Schools<br><input type="checkbox"/> Senior Center<br><input type="checkbox"/> Senior Housing<br><input type="checkbox"/> Sheltered Workshop<br><input type="checkbox"/> Shopping Centers<br><input type="checkbox"/> Storage/Use of Explosives/Flammables<br><input checked="" type="checkbox"/> Streams/Creeks<br><input type="checkbox"/> Train Depot<br><input type="checkbox"/> Underground Storage Tanks<br><input type="checkbox"/> Veterinary Clinic/Hospital<br><input type="checkbox"/> Youth Center<br><input type="checkbox"/> Other:<br><input type="checkbox"/> Other:<br><input type="checkbox"/> Other: |
|---|---|--|

**FIELD VISIT CHECKLIST & SITE EVALUATION - *Supplemental***  
**CONTAMINATION & TOXIC MATERIALS**

**None** identified on or adjacent to project site

Based upon visual inspections of the project site and adjacent properties, indicate evidence of the following. An explanation should accompany any checked items.

- Distressed Vegetation** – A possible indication of soil contamination
- Vent or Fill Pipes** – A possible indication of current or previous existence of underground storage tanks
- Storage/Oil Tanks or Questionable Containers** – Possible indication of the use of heating fuels, chemicals, and petroleum products
- Pits, Ponds, or Lagoons** – These have the potential to hold liquids or sludge-containing hazardous substances or petroleum products. The potential is increased if there also exists water discoloration, distressed vegetation, and/or wastewater discharge.
- Stained Soil or Pavement** (other than water stains) – A possible indication soil is contaminated as well as a sign of current or previous leakage of piping and liquid storage containers
- Pungent, Foul or Noxious Odors** – A possible indication of leaks of hazardous substances or petroleum products or contaminants
- Dumped Material or Soil, Mounds of Dirt, Rubble Fill, etc.** – There is potential for transfer of contamination as the origins may be unknown. Look for other signs of contamination as described above.

Provide any supplementary information, descriptions, explanations, and/or comments below. Attach additional pages as appropriate.

- A network of detention/retention basins are located throughout the subdivision for stormwater management

Signature & Title of Preparer

Date Form Completed: 9/15/2023



Rolanda Mazeika, Environmental Project Coordinator  
City of Tucson, Housing & Community Development Department

5746 E Calle de Hielo Dulce - lot north elevation followed common area to the south (facing south)



South Camino de la Humanidad followed by common area/detention basin (facing southeast)



West adjacent residence (facing southwest)



East Calle de Hielo Dulce followed by common area (facing north)



# FIELD VISIT CHECKLIST & SITE EVALUATION

**Date of Visit:** 9/15/2023

**Time:** 1:30 pm

**Grantee/Applicant:** Habitat for Humanity

**Project Name:** Corazon Del Pueblo

**NSP Project # (if funded):**

**Project Location/Address:** 5782 East Camino Esperanza Eterna, Tucson, AZ 85756

**Site Area (Acres):** approximately 0.10 acres

**Project Area Bounded By:**

North – East Camino Esperanza Eterna, followed by residence

East – residence

South – East Hackle Lane, followed by residence

West – South Camino de la Humanidad, followed by park and detention/retention basin

**Field Visit Conducted By:** Rolanda Mazeika, Environmental Project Coordinator

**Attachments:**

Photographs  Yes  No

Maps  Yes  No

Aerials  Yes  No

**EXISTING ENVIRONMENTAL CONDITIONS ON & AROUND SITE:**

**Land Use/Zoning** - Check all that apply:

Residential

Recreational

Forest

Retail

Commercial

Pasture

Never Developed

Heavy industrial

Other:

Light Industrial

Currently Farmed

**General Description of Existing Conditions:** Explain any present on or applicable to site: air quality/odors, water quality, noise, flooding, drought, auto/pedestrian traffic, vegetation, spills, waste, drainage, livestock, construction occurring, structures, etc. Attach additional pages as necessary.

The lot is currently vacant and is located in the mostly completed Corazon del Pueblo residential subdivision. The lot is covered with rock and a small area of oil (approximately 1 ½ x 1 foot) was observed on the north portion of the property along the curb. The site is located at the intersection of two residential streets and adjacent to a common area and an existing residence. The subdivision streets are paved, with an unpaved access road traversing north to south on the eastern edge between adjacent subdivisions. The subdivision has paved sidewalks, detention/retention basins, small off street parking spaces, common areas, playgrounds, and other improvements supporting the neighborhood. Topographic maps indicate the elevation slopes slightly west to northwest. Surface flow traverses the neighborhood streets south/southwest to detention basins throughout the subdivision. No signs of erosion in detention/retention basins.

**Describe Site Access – Ingress & Egress:** Ease of accessing and exiting site, traffic control at site, safety, and line of site, etc.

The main subdivision entrances are located off South Wilmot Road from the east and South Van Buren Avenue from the west. South Camino de la Humanidad runs north/south from South Van Buren Avenue to East Elvira Road/East Hackle Lane through the northern half of the subdivision. Traffic signs provide traffic control throughout the subdivision. The subdivision is bounded the Julian Wash Greenway/Chuck Huckleberry Loop, "The Loop", with bike lanes followed by the Julian Wash (a riverine), followed by vacant industrially zoned property to the north and by the Interstate 10 highway to the south. Line of sight to the property is unobscured.

**Interviews Conducted** (include name and title of interviewees): Charlie Buchanan, Chief Executive Officer, and Jessica Raizk Carney, Chief Financial Officer, Habitat for Humanity Tucson

## FIELD VISIT CHECKLIST & SITE EVALUATION

**Existing infrastructure on or near site. Check all that apply and comment as appropriate.**

- |   |   |   |
|---|---|---|
| <input checked="" type="checkbox"/> Unpaved Roads<br>Condition: good<br><input checked="" type="checkbox"/> Paved Roads<br>Condition: good<br><input checked="" type="checkbox"/> Intersections<br>Condition: good<br><input checked="" type="checkbox"/> Curb/Guttering<br>Condition: good<br><input checked="" type="checkbox"/> Culverts<br>Condition: good<br><input checked="" type="checkbox"/> Drop Inlets<br>Condition: good<br><input type="checkbox"/> Ditches<br>Condition:<br><input type="checkbox"/> Other: | <input checked="" type="checkbox"/> Sidewalks<br>Condition: good<br><input type="checkbox"/> Crosswalks<br>Condition:<br><input checked="" type="checkbox"/> Parking Facilities<br>Condition: good<br><input checked="" type="checkbox"/> Fencing/Security<br>Condition: good<br><input type="checkbox"/> Bridges<br>Condition:<br><input checked="" type="checkbox"/> Railroad Facilities/Tracks/Spurs<br>Condition:<br><input type="checkbox"/> Water Treatment Facility<br>Condition:<br><input type="checkbox"/> Other: | <input type="checkbox"/> Sewer Treatment Facilities<br>Condition:<br><input type="checkbox"/> Septic Tanks<br>Condition:<br><input checked="" type="checkbox"/> Electricity<br><input checked="" type="checkbox"/> Water Lines<br><input checked="" type="checkbox"/> Gas Lines<br><input type="checkbox"/> Centralized Sewer System<br><input checked="" type="checkbox"/> Airport/Airport Clear Zones<br><input checked="" type="checkbox"/> Traffic Signs<br><input type="checkbox"/> Traffic Lights<br><input checked="" type="checkbox"/> Fire Hydrants<br><input type="checkbox"/> Bike/Pedestrian Lanes/Paths<br><input type="checkbox"/> 911 Emergency Systems<br><input type="checkbox"/> Other: |
|---|---|---|

**Check all that exist on or near the project site. Further evaluation and documentation may be warranted if there is potential for environmental concerns.**

- |   |   |  |
|---|---|--|
| <input type="checkbox"/> Aboveground Storage Tanks<br><input checked="" type="checkbox"/> Airport<br><input type="checkbox"/> Ambulance Facility<br><input type="checkbox"/> Animal Processing Plant<br><input type="checkbox"/> Auto Repair Shop<br><input type="checkbox"/> Biodiesel Facility<br><input type="checkbox"/> Bus Station<br><input type="checkbox"/> Car Dealership<br><input type="checkbox"/> Cemeteries<br><input type="checkbox"/> Chemical Manufacturer<br><input type="checkbox"/> Churches<br><input type="checkbox"/> Community Center<br><input type="checkbox"/> Conservation Areas<br><input type="checkbox"/> Daycare Center<br><input type="checkbox"/> Doctor's Office/Clinic<br><input type="checkbox"/> Dry Cleaner<br><input type="checkbox"/> Ethanol Plant<br><input type="checkbox"/> Fire Station<br><input type="checkbox"/> Funeral Home<br><input type="checkbox"/> Gas Station<br><input type="checkbox"/> Grocery Store | <input type="checkbox"/> Group Home<br><input type="checkbox"/> Heavily Traveled Roads<br><input type="checkbox"/> Heavy/Light Manufacturing Facilities<br><input checked="" type="checkbox"/> Highways – Interstate/State<br><input type="checkbox"/> Historical Sites/Buildings<br><input type="checkbox"/> Hospitals<br><input type="checkbox"/> Lakes/Ponds<br><input checked="" type="checkbox"/> Landfills<br><input type="checkbox"/> Library<br><input checked="" type="checkbox"/> Low Income Housing<br><input checked="" type="checkbox"/> Museum<br><input checked="" type="checkbox"/> Neighborhoods<br><input type="checkbox"/> Nursing Homes<br><input type="checkbox"/> Office Buildings<br><input type="checkbox"/> Open Spaces<br><input type="checkbox"/> Paint Facilities<br><input checked="" type="checkbox"/> Parks/Playgrounds<br><input type="checkbox"/> Police Station<br><input type="checkbox"/> Power Station<br><input type="checkbox"/> Printing Facilities | <input type="checkbox"/> Prison/Jail/Detention Center<br><input type="checkbox"/> Quarry<br><input type="checkbox"/> Recycling Facilities<br><input type="checkbox"/> Restaurants<br><input type="checkbox"/> Retaining Walls<br><input type="checkbox"/> Rivers<br><input checked="" type="checkbox"/> Schools<br><input type="checkbox"/> Senior Center<br><input type="checkbox"/> Senior Housing<br><input type="checkbox"/> Sheltered Workshop<br><input type="checkbox"/> Shopping Centers<br><input type="checkbox"/> Storage/Use of Explosives/Flammables<br><input checked="" type="checkbox"/> Streams/Creeks<br><input type="checkbox"/> Train Depot<br><input type="checkbox"/> Underground Storage Tanks<br><input type="checkbox"/> Veterinary Clinic/Hospital<br><input type="checkbox"/> Youth Center<br><input type="checkbox"/> Other:<br><input type="checkbox"/> Other:<br><input type="checkbox"/> Other: |
|---|---|--|

**FIELD VISIT CHECKLIST & SITE EVALUATION - *Supplemental***  
**CONTAMINATION & TOXIC MATERIALS**

**None** identified on or adjacent to project site

Based upon visual inspections of the project site and adjacent properties, indicate evidence of the following. An explanation should accompany any checked items.

- Distressed Vegetation** – A possible indication of soil contamination
- Vent or Fill Pipes** – A possible indication of current or previous existence of underground storage tanks
- Storage/Oil Tanks or Questionable Containers** – Possible indication of the use of heating fuels, chemicals, and petroleum products
- Pits, Ponds, or Lagoons** – These have the potential to hold liquids or sludge-containing hazardous substances or petroleum products. The potential is increased if there also exists water discoloration, distressed vegetation, and/or wastewater discharge.
- Stained Soil or Pavement** (other than water stains) – A possible indication soil is contaminated as well as a sign of current or previous leakage of piping and liquid storage containers
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- Dumped Material or Soil, Mounds of Dirt, Rubble Fill, etc.** – There is potential for transfer of contamination as the origins may be unknown. Look for other signs of contamination as described above.

Provide any supplementary information, descriptions, explanations, and/or comments below. Attach additional pages as appropriate.

- A network of detention/retention basins are located throughout the subdivision for stormwater management
- A small area of oil (approximately 1 ½ x 1 foot) was observed on the north portion of the property along the curb

Signature & Title of Preparer

Date Form Completed: 9/15/2023



Rolanda Mazeika, Environmental Project Coordinator  
City of Tucson, Housing & Community Development Department

5782 East Camino Esperanza Eterna - Oil spot  
(north elevation, facing south)



5782 East Camino Esperanza Eterna - North elevation  
(facing south), followed by East Hackle Lane and  
residence



East adjacent residence (facing southeast)



South Camino de la Humanidad, followed by detention/retention basin and park (facing west)



East Camino Esperanza Eterna, followed by residence



## **FIELD VISIT CHECKLIST & SITE EVALUATION - *Supplemental***

The Corazon del Pueblo Project is located in the Tucson Basin, a northwest trending alluvial valley. Groundwater flow within the regional aquifer is generally to the north and northwest. (Arizona Department of Water Resources, Tucson Water)

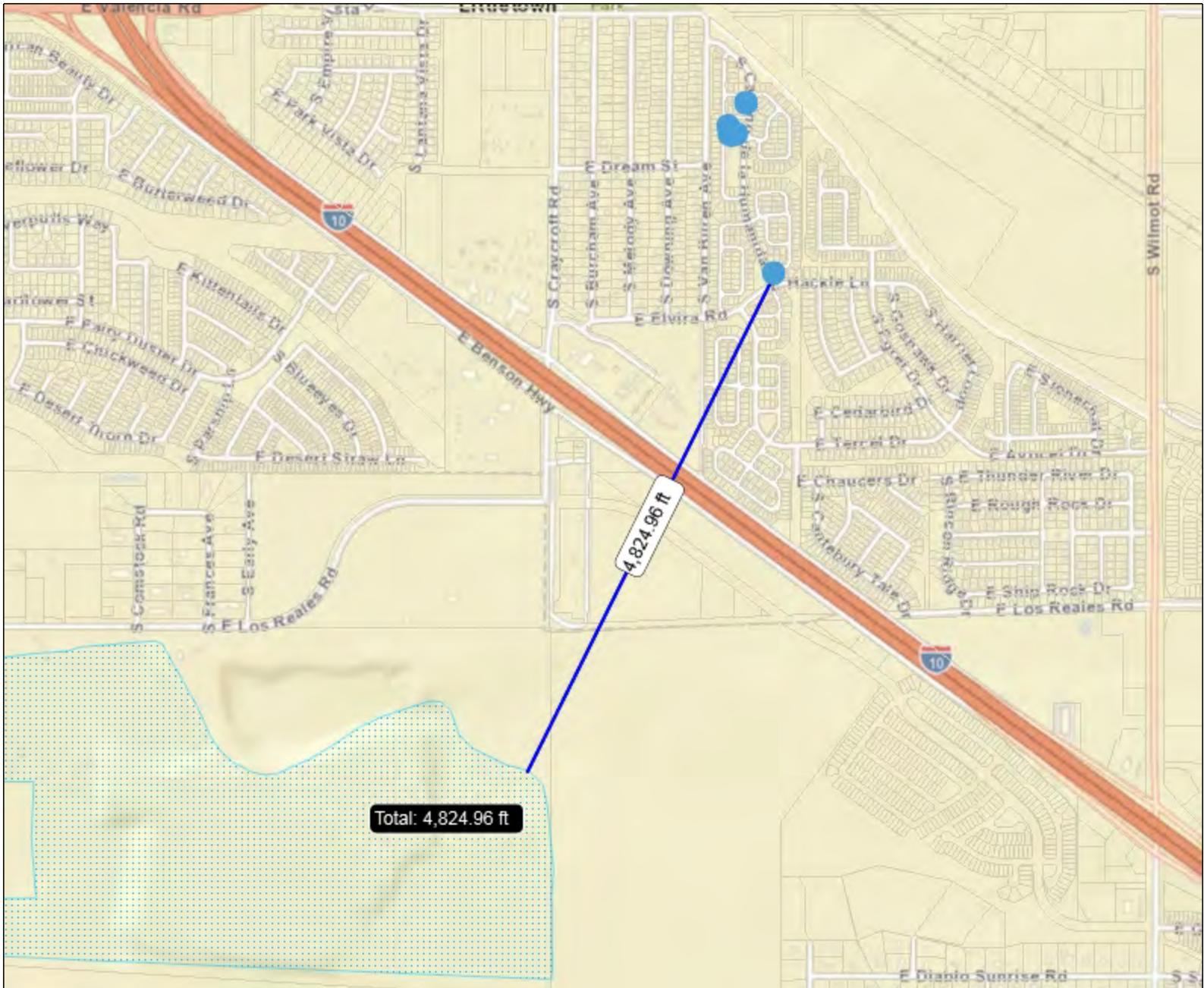
The project is located approximately 0.91 miles northeast of the Los Reales Landfill Water Quality Assurance Revolving Fund (WQARF, or state Superfund) site and the Los Reales Landfill Southwest Disposal Area Voluntary Response (VRP) site and approximately 0.66 miles south of the Davis-Monthan Air Force Base Department of Defense (DOD) site. (Arizona Department of Environmental Quality eMaps)

There are 109 underground storage tank (UST) sites and 23 closed Leaking UST (LUST) sites within one mile of the project location. The closest LUST site is approximately 1,900 feet west of the subdivision. The closest upgradient LUST site is more than 4,900 feet southeast of the project location. (Arizona Department of Environmental Quality eMaps)

The nearest active Resource Conservation and Recovery Act (RCRA) active small quantity generator (SQG) site is approximately 0.29 miles southwest of the project location with no violations reported in the last three years. The nearest active large quantity generator (LQG) is approximately 0.44 miles southwest of the project location with no violations reported in the last three years. The nearest upgradient RCRA site is located approximately 0.31 miles southeast of the project location, is inactive, and had no violations reported in the last three years. One SQG located approximately 0.93 miles northeast of the project location had one violation or compliance issue, with an associated state informal action, reported in one quarter in 2021. No other violations or enforcement actions were reported for the remaining EPA Facilities within one mile of the project location. (NEPAssist)

A site investigation was completed at the former Wilmot N.G. Target Range Formerly Used Defense Site (FUDS) property and Military Munitions Response Program (MMRP) site in 2011. No unacceptable risk to human health from soil exposure was found. (ECHO)

Review of historic aerial maps indicate the property was undeveloped, vacant land in 1941, with a southeast to northwest railroad line to the north, and an east to west highway (Interstate 10, I-10) to the south. In 1958, the property was still vacant and undeveloped with nearby unpaved roads, minimal residential development to the west, the railroad to the north, and the highway to the south. In 1966, the property remains vacant with the Julian Wash channel improvement visible to the north, followed by the rail line. The TTT Truck stop is visible southwest of the residential properties located west of the site. From 1972 to 1996, increased residential development is observed west of the property. In 1996, truck stop facilities to the southwest have been expanded. In 2003, the land east of the site is being prepared for development. From 2004 to 2015, the project area is being prepared and developed with residential units, increased residential development is observed east of the site, and the Julian Wash Greenway/The Loop improvement is being developed. From 2017 to 2019, a new truck stop and truck repair facility are visible southwest of the site. The properties between the Julian Wash and I-10 are mostly developed with residential units. A school, park, and gas stations are also visible in the area. (historicaerials.com, MapTucson)



**Notes**

Landfill Map

**Legend**

-  Land Fills - City/County
-  Land Fills - State/Federal
-  Parcels
-  World Street Map

1: 18,056



2,915.3      0      1,457.64      2,915.3 Feet



WGS\_1984\_Web\_Mercator\_Auxiliary\_Sphere

© City of Tucson

This map is a user generated static output from an Internet mapping site and is for reference only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable.

**THIS MAP IS NOT TO BE USED FOR NAVIGATION**



Home (<https://www.tucsonaz.gov/Home>) / Departments  
(<https://www.tucsonaz.gov/Departments>) / Environmental and General Services  
(<https://www.tucsonaz.gov/Departments/Environmental-and-General-Services>) /  
**Los Reales Landfill**

## Los Reales Landfill

### Address

5300 E. Los Reales Road  
Tucson, AZ 85756  
(South of Los Reales Rd. at Craycroft Rd., Turn south on Craycroft off I-10)

### Contact

General Info Monday-Friday: [520-791-4183 \(tel:5207914183\)](tel:5207914183).

For Holiday hours: [\(520\) 791-3171 \(tel:5207913171\)](tel:5207913171).

### Hours

Monday-Saturday: 6 a.m. to 5 p.m.

Sunday: CLOSED

### HHW Drop-off Site at Los Reales Residential Drop-off:

Thursdays, Fridays, Saturdays; 7 am - 2 pm

The entrance to Los Reales is located at the intersection of Los Reales Rd. and Craycro

The Los Reales Landfill opened in 1967 and is the only active landfill owned and managed by the City of Tucson. Los Reales is a regional landfill and serves the residents and businesses in Pima County. Each day, approximately 2,300 tons of solid waste is brought to the landfill for disposal. Los Reales is open to private, commercial haulers, and residential self-haulers.

## Landfill Information

Landfill Policies (<https://www.tucsonaz.gov/Departments/Environmental-and-General-Services/Los-Reales-Landfill/Landfill-Policies>)

Residential Self Hauler Rates and Information  
(<https://www.tucsonaz.gov/Departments/Environmental-and-General-Services/Los-Reales-Landfill/Residential-Self-Hauler-Rates-and-Information>)

Special Waste Disposal  
(<https://www.tucsonaz.gov/Departments/Environmental-and-General-Services/Los-Reales-Landfill/Special-Waste-Disposal>)



Clean Air, Safe Water, Healthy Land for Everyone



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MY COMMUNITY

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# Los Reales Landfill | Site Overview

Revised on: May 2, 2023 - 11:27am

Registry placement date: April 23, 1999

Score: 32

County: Pima

**Location:** An active municipal solid waste landfill (approximately 283-acre waste footprint) in southeast Tucson that has been in operation since 1967.

If you own a private well, live in or near the site boundaries, and have not had your well tested, please contact City of Tucson Environmental Services at 520-791-3175 | [Learn More >](#)

**Contaminants of concern:** Tetrachloroethene (PCE) and trichloroethene (TCE)

**Action taken:** In 1999, the City of Tucson (COT) implemented a groundwater Pump and Treat System with Re-injection and Reuse (PTR) of the treated water as required by a 1995 Remedial Action Plan (RAP).

**Status:** COT has continued operations of the PTR system onsite.

Since startup of the PTR system in 1999, approximately 782 million gallons of groundwater have been treated and approximately 38.7 pounds of PCE and 14.4 pounds of TCE have been removed.

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COT also monitors two privately owned wells in the area annually, neither detected PCE or TCE above their respective AWQS in 2022.

**Community Advisory Board (CAB):** Not applicable under "Old WQARF" law/rules

ADEQ Project Manager  
520-628-6709  
[Email >](#)

ADEQ Community Involvement Coordinator  
602-771-8007  
[Email >](#)

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# Detailed Facility Report



## Detailed Facility Report

### Facility Summary

CITY OF TUCSON - LOS REALES LANDFILL

5400 E. LOS REALES, TUCSON, AZ 85726

**FRS (Facility Registry Service) ID:** 110015714798

**EPA Region:** 09

**Latitude:** 32.122413

**Longitude:** -110.877756

**Locational Data Source:** FRS

**Industries:** Mining (except Oil and Gas)

**Indian Country:** N

### Enforcement and Compliance Summary

Statute	CAA
Compliance Monitoring Activities (5 years)	--
Date of Last Compliance Monitoring Activity	07/27/2017
<b>Compliance Status</b>	<b>No Violation Identified</b>
Qtrs in Noncompliance (of 12)	0
Qtrs with Significant Violation	0
Informal Enforcement Actions (5 years)	--
Formal Enforcement Actions (5 years)	--
Penalties from Formal Enforcement Actions (5 years)	--
EPA Cases (5 years)	--
Penalties from EPA Cases (5 years)	--

### Regulatory Information

**Clean Air Act (CAA):** Operating Major (AZPMC00004019P3618)

**Clean Water Act (CWA):** No Information

**Resource Conservation and Recovery Act (RCRA):** No Information

**Safe Drinking Water Act (SDWA):** No Information

[Go To Enforcement/Compliance Details](#)  
[Known Data Problems](#)

### Other Regulatory Reports

**Air Emissions Inventory (EIS):** No Information

**Greenhouse Gas Emissions (eGGRT):** No Information

**Toxic Releases (TRI):** No Information

**Compliance and Emissions Data Reporting Interface (CEDRI):** No Information

## Facility/System Characteristics

## Facility/System Characteristics

System	Statute	Identifier	Universe	Status	Areas	Permit Expiration Date	Indian Country	Latitude	Longitude
FRS		<a href="#">110015714798</a>					N	32.122413	-110.877756
ICIS-Air	CAA	AZPMC00004019P3618	Major Emissions	Operating	CAAMACT, CAANSPS, CAASIP, CAATVP		N	32.122413	-110.877756

## Facility Address

System	Statute	Identifier	Facility Name	Facility Address	Facility County
FRS		<a href="#">110015714798</a>	CITY OF TUCSON - LOS REALES LANDFILL	5400 E. LOS REALES, TUCSON, AZ 85726	
ICIS-Air	CAA	AZPMC00004019P3618	CITY OF TUCSON - LOS REALES LANDFILL	5400 E. LOS REALES, TUCSON, AZ 85726	Pima County

## Facility SIC (Standard Industrial Classification) Codes

System	Identifier	SIC Code	SIC Description
ICIS-Air	AZPMC00004019P3618	4953	Refuse Systems

## Facility NAICS (North American Industry Classification System) Codes

System	Identifier	NAICS Code	NAICS Description
ICIS-Air	AZPMC00004019P3618	212234	Copper Ore and Nickel Ore Mining

## Facility Tribe Information

Reservation Name	Tribe Name	EPA Tribal ID	Distance to Tribe (miles)
Pascua Pueblo Yaqui Reservation	Pascua Yaqui Tribe of Arizona	100000193	10.37
Pascua Pueblo Yaqui Reservation	Pascua Yaqui Tribe of Arizona	100000193	10.85
Tohono O'odham Nation Reservation	Tohono O'odham Nation of Arizona	100000302	4.86

## Enforcement and Compliance

### Compliance Monitoring History Last 5 Years

Statute	Source ID	System	Activity Type	Compliance Monitoring Type	Lead Agency	Date	Finding (if applicable)
---------	-----------	--------	---------------	----------------------------	-------------	------	-------------------------

No data records returned

*Entries in italics are not counted as EPA official inspections.*

### Compliance Summary Data

Statute	Source ID	Current SNC (Significant Noncompliance)/HPV (High Priority Violation)	Current As Of	Qtrs with NC (Noncompliance) (of 12)	Data Last Refreshed
CAA	AZPMC00004019P3618	No	07/15/2023	0	07/14/2023

### Three-Year Compliance History by Quarter

Statute	Program/Pollutant/Violation Type	QTR 1	QTR 2	QTR 3	QTR 4	QTR 5	QTR 6	QTR 7	QTR 8	QTR 9	QTR 10	QTR 11	QTR 12+
CAA	(Source ID: AZPMC00004019P3618)	10/01-12/31/20	01/01-03/31/21	04/01-06/30/21	07/01-09/30/21	10/01-12/31/21	01/01-03/31/22	04/01-06/30/22	07/01-09/30/22	10/01-12/31/22	01/01-03/31/23	04/01-06/30/23	07/01-09/30/23
	Facility-Level Status	No Violation Identified											
	HPV History												
	Violation Type	Agency	Programs	Pollutants									

### Informal Enforcement Actions Last 5 Years

Statute	System	Source ID	Type of Action	Lead Agency	Date
---------	--------	-----------	----------------	-------------	------

No data records returned

*Entries in italics are not counted as "informal enforcement actions" in EPA policies pertaining to enforcement response tools.*

## Formal Enforcement Actions Last 5 Years

Statute	System	Law/Section	Source ID	Type of Action	Case No.	Lead Agency	Case Name	Issued/Filed Date	Settlements/Actions	Settlement/Action Date	Federal Penalty Assessed	State/Local Penalty Assessed	Penalty Amount Collected	SEP Value	Comp Action Cost
---------	--------	-------------	-----------	----------------	----------	-------------	-----------	-------------------	---------------------	------------------------	--------------------------	------------------------------	--------------------------	-----------	------------------

No data records returned

## Environmental Conditions

### Watersheds

12-Digit WBD (Watershed Boundary Dataset) HUC (RAD (Reach Address Database))	WBD (Watershed Boundary Dataset) Subwatershed Name (RAD (Reach Address Database))	State Water Body Name (ICIS (Integrated Compliance Information System))	Beach Closures Within Last Year	Beach Closures Within Last Two Years	Pollutants Potentially Related to Impairment	Watershed with ESA (Endangered Species Act)-listed Aquatic Species?
--	---	---	---------------------------------	--------------------------------------	--	---

No data records returned

### Assessed Waters From Latest State Submission (ATTAINS)

State	Report Cycle	Assessment Unit ID	Assessment Unit Name	Water Condition	Cause Groups Impaired	Drinking Water Use	Aquatic Life	Fish Consumption Use	Recreation Use	Other Use
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No data records returned

### Air Quality Nonattainment Areas

Pollutant	Within Nonattainment Status Area?	Nonattainment Status Applicable Standard(s)	Within Maintenance Status Area?	Maintenance Status Applicable Standard(s)
Ozone	No	--	No	--
Lead	No	--	No	--
Particulate Matter	No	--	No	--
Carbon Monoxide	No	--	Yes	Carbon Monoxide (1971)
Sulfur Dioxide	No	--	No	--

## Pollutants

### Toxics Release Inventory History of Reported Chemicals Released in Pounds per Year at Site

TRI Facility ID	Year	Total Air Emissions	Surface Water Discharges	Off-Site Transfers to POTWs (Publicly Owned Treatment Works)	Underground Injections	Releases to Land	Total On-Site Releases	Total Off-Site Transfers
-----------------	------	---------------------	--------------------------	--	------------------------	------------------	------------------------	--------------------------

No data records returned

### Toxics Release Inventory Total Releases and Transfers in Pounds by Chemical and Year

Chemical Name
---------------

No data records returned

## Community

### Environmental Justice

This section shows indexes from EJScreen, EPA's screening tool for environmental justice (EJ) concerns. EPA uses these indexes to identify geographic areas that may warrant further consideration or analysis for potential EJ concerns. Use of these indexes does not designate an area as an "EJ community" or "EJ facility." EJScreen provides screening level indicators, not a determination of the existence or absence of EJ concerns. For more information, see the [EJScreen home page](#).

#### EJScreen Indexes Shown

Compare to  US  State

Index Type  Environmental Justice  Supplemental

#### Related Reports

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## DAVIS-MONTHAN AIR FORCE BASE

The appearance of U.S. Department of Defense (DoD) visual information does not imply or constitute DoD endorsement

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# Davis-Monthan Air Force Base

Revised on: April 8, 2021 - 2:55pm

### Location

Eastern Tucson, Ariz. The northern boundary gradually descends to the south from Golf Links Road to Irvington Road. The eastern boundary is Harrison Road and the western boundary is Alvernon Way. The southern-most boundary is Valencia Road on the east side of the site.

### Contaminants of Concern

Contaminants of concern for sites where petroleum was released include petroleum hydrocarbons and BTEX in soils. The capped landfill contaminants of concern includes methane gas, volatile organic compounds (VOCs) and metals in soil. Per and polyfluorinated alkyl substances (PFAS) at concentrations above the EPA Health Advisory level of 70 nanograms per liter were found in groundwater.

### Public Health Concerns

PFAS at concentrations above the EPA Health Advisory level of 70 nanograms per liter are present in groundwater. Investigations are on going to assess the nature and extent

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### 📞 CONTACTS

DMAFB Project Manager  
702-652-3035  
[Email >](#)

ADEQ Project Manager  
P: 520-209-4265  
F: 520-628-6745  
[Email >](#)

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of PFAS in the environment. All other COC exposure pathways have been eliminated through remediation or restricted access/use.

## Site Hydrogeology

Davis-Monthan Air Force Base (DMAFB) is located within the Tucson basin, a northwest trending alluvial valley. The Santa Cruz River flows northwesterly and provides the main drainage for the Tucson basin and is located approximately 4.6 miles west of the base. DMAFB is located in the Sonoran Desert which has an arid climate with a low precipitation rate of 12 inches per year and a high evaporation rate of 65 inches per year.

Groundwater occurs within the unconsolidated alluvial deposits consisting of interfingering sand, gravel, silt, and clay. The saturated thickness of these sediments is extremely variable, being thin (less than 200 feet) toward the mountains and thickening (greater than 5,000 feet) toward the center of the basin. These deposits were distributed laterally over time by a constantly changing stream course.

In the vicinity of DMAFB, the Pantano Formation, Tinaja Beds, and Fort Lowell Formation are the primary water bearing units. Groundwater is encountered at approximately 350 feet below ground surface near DMAFB, and groundwater generally flows in a northwest direction.

## Action Taken

Petroleum contaminated sites ST-35, ST-36, and ST-52 were treated with soil-vapor extraction (SVE). An additional petroleum contaminated site ST-55 was proposed for risk-based closure. The low-level radioactive material waste storage area RW-16 was excavated and remediated. Geophysical surveys and limited excavations were conducted at the three Munitions Response Sites (MRS) TM553, TM553a, and TM554a. The closed base landfill LF-01 had a methane control system and landfill cap installed in 1999. A focused Remedial Investigation (RI) of PFAS is in-progress.

## Status

The following is the status on the sites that action is currently being conducted:

- Final Record of Decisions (RODs) for sites ST-35 and ST-52 were completed in December 2018 so remediation for these two sites are complete.
- Site ST-36, the United States Air Force (USAF) published a Final Proposed Plan in April 2020 which proposed a preferred alternative of "No Further Action." The ROD is being finalized and expect to be completed early 2021.
- Excavation and disposal was complete at site RW-16 in 2018. In July 2020 a Draft Final ROD was prepared with a selected remedy for RW016 of NFA, based on completion of a Multi-Agency Radiation Survey and Site Investigation Manual (MARSSIM) Site Characterization Investigation and Interim Removal Action. The finalized ROD is expected to be available early 2021.
- In April 2019, the USAF published a Draft ROD for MRS sites TM553, TM553a, and TM554a with Land Use Controls (LUCs) as the selected remedies. The finalized ROD is expected to be available early 2021.
- Site LF-01, the USAF continues operation and maintenance of the landfill cap, the methane collection system, and annual inspection reporting which is typically available for review by the middle of the following year.
- In September 2015, the USAF released the Final Preliminary Assessment Report (PA) for Perfluorinated Compounds. The purpose and objective of the PA report was to identify locations at DMAFB where PFCs may have been released to the environment and to conduct an initial assessment of possible migration pathways and receptors of potential contamination. The findings from the PA report recommended identified locations to proceed to a Site Inspection (SI).
- In 2019, USAF released the Final SI report for PFAS releases at DM. The SI report identified areas of interest (AOI) that were recommended to proceed to a Remedial

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Investigation (RI).

- In August 2020, the USAF published a Relative Risk Site Evaluation (RRSE) for PFAS releases at DM. The RRSE identified the Storm water Outfall Canal along the north side of the base with an overall site category of High. Five other AFFF release areas had overall site categories of medium or low.
- In 2020, USAF is in the process of preparing a focused RI to continue to investigate PFAS possible migration pathways and receptors of potential contamination.

### Community Involvement Activities

DMAFB is currently investigating the nature and extent of PFAS contamination on the base. Tucson Water supply wells just north of the base property have been impacted with PFAS at concentrations above the EPA Health Advisory level of 70 nanograms per liter.

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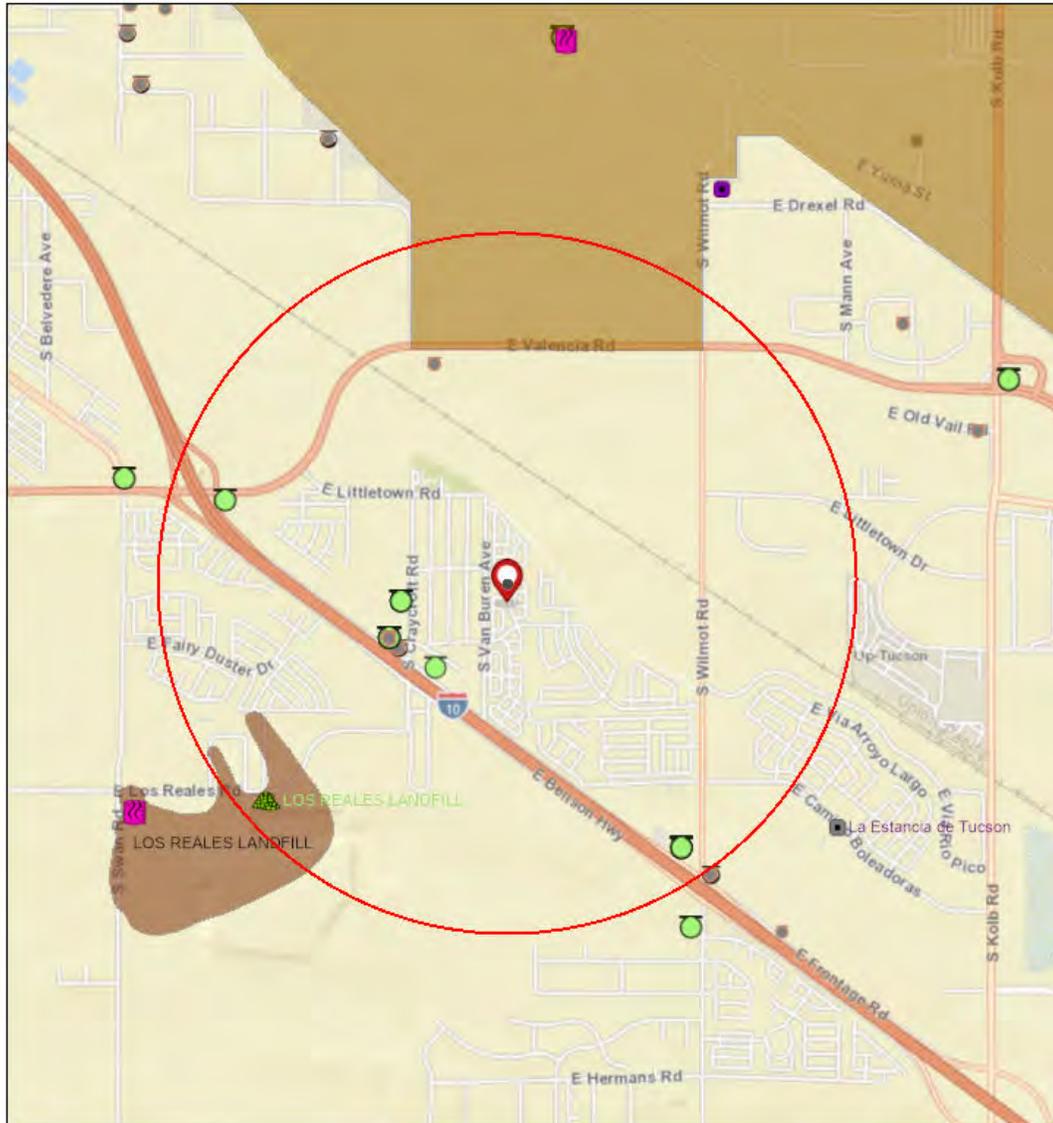
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**Area of Interest (AOI) Information**

Area : 2,893.42 acres

Jul 26 2023 11:39:59 Mountain Standard Time



## Summary

Name	Count	Area(acres)	Length(mi)
Brownfield Grant Sites	0	0	N/A
Brownfield Grant Sites - FY21	0	N/A	N/A
Brownfield Grant Sites - FY16-20	0	N/A	N/A
DEUR	0	N/A	N/A
Hazardous Waste - LQG	0	N/A	N/A
Hazardous Waste - TSD	0	N/A	N/A
Landfills - Municipal	1	N/A	N/A
Landfills - Non-Municipal	0	N/A	N/A
Superfund	2	285.33	N/A
UST - Tanks	109	N/A	N/A
UST - Releases	23	N/A	N/A
Voluntary Remediation	1	N/A	N/A

## Landfills - Municipal

#	NAME	ADDRESS	CITY	ZIP	COUNTY
1	LOS REALES LANDFILL	5300 E LOS REALES RD	TUCSON	85756	PIMA

#	IDNO	STATUS	Count
1	3024	ACTIVE	1

## Superfund

#	NAME	CITY	COUNTY	TYPE	PLACE_ID
1	LOS REALES LANDFILL	TUCSON	PIMA	WQARF	3024
2	DAVIS MONTHAN AIR FORCE BASE	TUCSON	PIMA	DOD	3171

#	LastUpdate	PubNumb	DateListed	url	Area(acres)
1	12/1/2022, 1:00 AM	M 23-46	4/23/1999, 12:00 AM	<a href="https://azdeg.gov/los-reales-landfill">https://azdeg.gov/los-reales-landfill</a>	61.68
2	1/1/2007, 1:00 AM	M 23-59	No Data	<a href="https://azdeg.gov/DOD/DavisMonthan">https://azdeg.gov/DOD/DavisMonthan</a>	223.65

## UST - Tanks

#	FACILITY	LUST_NUM	PLACE_ID	PLACE_TYPE	PLACE_NAME
1	0-001331	4139	4118	GAS FILLING STATION - COMMERCIAL	CIRCLE K #843
2	0-001331	4139	4118	GAS FILLING STATION - COMMERCIAL	CIRCLE K #843
3	0-001331	4139	4118	GAS FILLING STATION - COMMERCIAL	CIRCLE K #843
4	0-001331	4139	4118	GAS FILLING STATION - COMMERCIAL	CIRCLE K #843
5	0-001331	4139	4118	GAS FILLING STATION - COMMERCIAL	CIRCLE K #843
6	0-001331	<i>No Data</i>	4118	GAS FILLING STATION - COMMERCIAL	CIRCLE K #843
7	0-001331	<i>No Data</i>	4118	GAS FILLING STATION - COMMERCIAL	CIRCLE K #843
8	0-001331	<i>No Data</i>	4118	GAS FILLING STATION - COMMERCIAL	CIRCLE K #843
9	0-001331	<i>No Data</i>	4118	GAS FILLING STATION - COMMERCIAL	CIRCLE K #843
10	0-001331	<i>No Data</i>	4118	GAS FILLING STATION - COMMERCIAL	CIRCLE K #843
11	0-002022	2213	3533	GAS FILLING STATION - COMMERCIAL	EXXON #7-6390
12	0-002022	2213	3533	GAS FILLING STATION - COMMERCIAL	EXXON #7-6390
13	0-002022	2213	3533	GAS FILLING STATION - COMMERCIAL	EXXON #7-6390
14	0-002022	2213	3533	GAS FILLING STATION - COMMERCIAL	EXXON #7-6390
15	0-002590	2402	33075	AUTO/VEHICLE REPAIR FACILITY	ARIZONA ARMY NATIONAL GUARD - ORGANIZATIONAL MAINTENANCE
16	0-002590	2402	33075	AUTO/VEHICLE REPAIR FACILITY	ARIZONA ARMY NATIONAL GUARD - ORGANIZATIONAL MAINTENANCE
17	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
18	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
19	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
20	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
21	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
22	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
23	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
24	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
25	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL

26	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
27	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
28	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
29	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
30	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
31	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
32	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
33	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
34	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
35	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
36	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
37	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
38	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
39	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
40	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
41	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
42	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
43	0-005199	183	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
44	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
45	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
46	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
47	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
48	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
49	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
50	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
51	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
52	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL

53	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
54	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
55	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
56	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
57	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
58	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
59	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
60	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
61	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
62	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
63	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
64	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
65	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
66	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
67	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
68	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
69	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
70	0-005199	<i>No Data</i>	3684	TRUCKING COMPANY	TUCSON TRUCK TERMINAL
71	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
72	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
73	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
74	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
75	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
76	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
77	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON

78	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
79	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
80	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
81	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
82	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
83	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
84	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
85	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
86	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
87	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
88	0-007522	312	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
89	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
90	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
91	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
92	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
93	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
94	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
95	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
96	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
97	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON

98	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
99	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
100	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
101	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
102	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
103	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
104	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
105	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
106	0-007522	<i>No Data</i>	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY	HATTAR CHEVRON
107	0-009895	<i>No Data</i>	32467	GAS FILLING STATION - COMMERCIAL	SPEEDWAY STORE #2948
108	0-009895	<i>No Data</i>	32467	GAS FILLING STATION - COMMERCIAL	SPEEDWAY STORE #2948
109	0-010478	<i>No Data</i>	145184	GAS FILLING STATION - COMMERCIAL	PILOT TRAVEL CENTER #593

#	ADDRESS	CITY	COUNTY	ZIP_CODE	CADASTRAL
1	6750 S CRAYCROFT RD	TUCSON	Pima	85756	No Data
2	6750 S CRAYCROFT RD	TUCSON	Pima	85756	No Data
3	6750 S CRAYCROFT RD	TUCSON	Pima	85756	No Data
4	6750 S CRAYCROFT RD	TUCSON	Pima	85756	No Data
5	6750 S CRAYCROFT RD	TUCSON	Pima	85756	No Data
6	6750 S CRAYCROFT RD	TUCSON	Pima	85756	No Data
7	6750 S CRAYCROFT RD	TUCSON	Pima	85756	No Data
8	6750 S CRAYCROFT RD	TUCSON	Pima	85756	No Data
9	6750 S CRAYCROFT RD	TUCSON	Pima	85756	No Data
10	6750 S CRAYCROFT RD	TUCSON	Pima	85756	No Data
11	5471 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 dad
12	5471 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 dad
13	5471 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 dad
14	5471 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 dad
15	5500 E VALENCIA RD	TUCSON	Pima	85756	No Data
16	5500 E VALENCIA RD	TUCSON	Pima	85756	No Data
17	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
18	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
19	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
20	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
21	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
22	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
23	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
24	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
25	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
26	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
27	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
28	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
29	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
30	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
31	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
32	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
33	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
34	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
35	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa
36	5451 E BENSON HWY	TUCSON	Pima	85756	D 15 14 14 aaa



80	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
81	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
82	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
83	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
84	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
85	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
86	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
87	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
88	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
89	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
90	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
91	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
92	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
93	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
94	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
95	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
96	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
97	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
98	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
99	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
100	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
101	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
102	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
103	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
104	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
105	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
106	6261 E BENSON HWY	TUCSON	Pima	85756	D 15 14 24 aad
107	5000 E VALENCIA RD	TUCSON	Pima	85756	<i>No Data</i>
108	5000 E VALENCIA RD	TUCSON	Pima	85756	<i>No Data</i>
109	5570 E TRAVEL PLAZA WAY	TUCSON	Pima	85756	D 15 14 13

#	LATITUDE	LONGITUDE	TANK_NUM	STATUS	INSTALL_DT
1	32.128837	-110.875645	1	Perm Closed	1/1/1972, 1:00 AM
2	32.128837	-110.875645	2	Perm Closed	1/1/1972, 1:00 AM
3	32.128837	-110.875645	3	Temp Closed	6/30/1995, 12:00 AM
4	32.128837	-110.875645	4	Open	6/30/1995, 12:00 AM
5	32.128837	-110.875645	5	Open	6/30/1995, 12:00 AM
6	32.128837	-110.875645	1	Perm Closed	1/1/1972, 1:00 AM
7	32.128837	-110.875645	2	Perm Closed	1/1/1972, 1:00 AM
8	32.128837	-110.875645	3	Temp Closed	6/30/1995, 12:00 AM
9	32.128837	-110.875645	4	Open	6/30/1995, 12:00 AM
10	32.128837	-110.875645	5	Open	6/30/1995, 12:00 AM
11	32.126529	-110.875757	1	Perm Closed	1/1/1966, 1:00 AM
12	32.126529	-110.875757	2	Perm Closed	1/1/1966, 1:00 AM
13	32.126529	-110.875757	3	Perm Closed	1/1/1966, 1:00 AM
14	32.126529	-110.875757	4	Perm Closed	1/1/1976, 1:00 AM
15	32.140676	-110.873645	1	Perm Closed	12/6/1988, 1:00 AM
16	32.140676	-110.873645	2	Perm Closed	12/6/1988, 1:00 AM
17	32.127025	-110.876312	1	Perm Closed	1/1/1986, 1:00 AM
18	32.127025	-110.876312	2	Perm Closed	1/1/1966, 1:00 AM
19	32.127025	-110.876312	3	Perm Closed	1/1/1966, 1:00 AM
20	32.127025	-110.876312	4	Perm Closed	1/1/1966, 1:00 AM
21	32.127025	-110.876312	5	Perm Closed	1/1/1966, 1:00 AM
22	32.127025	-110.876312	6	Perm Closed	1/1/1966, 1:00 AM
23	32.127025	-110.876312	7	Perm Closed	1/1/1966, 1:00 AM
24	32.127025	-110.876312	8	Perm Closed	1/1/1966, 1:00 AM
25	32.127025	-110.876312	9	Perm Closed	1/1/1985, 1:00 AM
26	32.127025	-110.876312	10	Perm Closed	1/1/1985, 1:00 AM
27	32.127025	-110.876312	11	Perm Closed	1/1/1985, 1:00 AM
28	32.127025	-110.876312	12	Perm Closed	1/1/1985, 1:00 AM
29	32.127025	-110.876312	13	Perm Closed	1/1/1985, 1:00 AM
30	32.127025	-110.876312	14	Open	7/1/1986, 12:00 AM
31	32.127025	-110.876312	15	Open	7/1/1986, 12:00 AM
32	32.127025	-110.876312	16	Open	7/1/1986, 12:00 AM
33	32.127025	-110.876312	17	Temp Closed	7/1/1986, 12:00 AM
34	32.127025	-110.876312	18	Perm Closed	1/1/1973, 1:00 AM
35	32.127025	-110.876312	19	Perm Closed	1/1/1973, 1:00 AM
36	32.127025	-110.876312	20	Perm Closed	1/1/1973, 1:00 AM
37	32.127025	-110.876312	21	Perm Closed	1/1/1978, 1:00 AM
38	32.127025	-110.876312	22	Perm Closed	1/1/1978, 1:00 AM
39	32.127025	-110.876312	23	Perm Closed	1/1/1978, 1:00 AM
40	32.127025	-110.876312	24	Perm Closed	1/1/1978, 1:00 AM
41	32.127025	-110.876312	25	Perm Closed	1/1/1978, 1:00 AM
42	32.127025	-110.876312	26	Perm Closed	1/1/1978, 1:00 AM

43	32.127025	-110.876312	27	Perm Closed	1/1/1978, 1:00 AM
44	32.127025	-110.876312	1	Perm Closed	1/1/1986, 1:00 AM
45	32.127025	-110.876312	2	Perm Closed	1/1/1966, 1:00 AM
46	32.127025	-110.876312	3	Perm Closed	1/1/1966, 1:00 AM
47	32.127025	-110.876312	4	Perm Closed	1/1/1966, 1:00 AM
48	32.127025	-110.876312	5	Perm Closed	1/1/1966, 1:00 AM
49	32.127025	-110.876312	6	Perm Closed	1/1/1966, 1:00 AM
50	32.127025	-110.876312	7	Perm Closed	1/1/1966, 1:00 AM
51	32.127025	-110.876312	8	Perm Closed	1/1/1966, 1:00 AM
52	32.127025	-110.876312	9	Perm Closed	1/1/1985, 1:00 AM
53	32.127025	-110.876312	10	Perm Closed	1/1/1985, 1:00 AM
54	32.127025	-110.876312	11	Perm Closed	1/1/1985, 1:00 AM
55	32.127025	-110.876312	12	Perm Closed	1/1/1985, 1:00 AM
56	32.127025	-110.876312	13	Perm Closed	1/1/1985, 1:00 AM
57	32.127025	-110.876312	14	Open	7/1/1986, 12:00 AM
58	32.127025	-110.876312	15	Open	7/1/1986, 12:00 AM
59	32.127025	-110.876312	16	Open	7/1/1986, 12:00 AM
60	32.127025	-110.876312	17	Temp Closed	7/1/1986, 12:00 AM
61	32.127025	-110.876312	18	Perm Closed	1/1/1973, 1:00 AM
62	32.127025	-110.876312	19	Perm Closed	1/1/1973, 1:00 AM
63	32.127025	-110.876312	20	Perm Closed	1/1/1973, 1:00 AM
64	32.127025	-110.876312	21	Perm Closed	1/1/1978, 1:00 AM
65	32.127025	-110.876312	22	Perm Closed	1/1/1978, 1:00 AM
66	32.127025	-110.876312	23	Perm Closed	1/1/1978, 1:00 AM
67	32.127025	-110.876312	24	Perm Closed	1/1/1978, 1:00 AM
68	32.127025	-110.876312	25	Perm Closed	1/1/1978, 1:00 AM
69	32.127025	-110.876312	26	Perm Closed	1/1/1978, 1:00 AM
70	32.127025	-110.876312	27	Perm Closed	1/1/1978, 1:00 AM
71	32.116586	-110.859214	1	Perm Closed	1/1/1963, 1:00 AM
72	32.116586	-110.859214	2	Perm Closed	1/1/1963, 1:00 AM
73	32.116586	-110.859214	3	Perm Closed	1/1/1984, 1:00 AM
74	32.116586	-110.859214	4	Perm Closed	1/1/1984, 1:00 AM
75	32.116586	-110.859214	5	Perm Closed	1/1/1963, 1:00 AM
76	32.116586	-110.859214	6	Perm Closed	1/1/1963, 1:00 AM
77	32.116586	-110.859214	7	Perm Closed	1/1/1963, 1:00 AM
78	32.116586	-110.859214	8	Perm Closed	1/1/1963, 1:00 AM
79	32.116586	-110.859214	9	Perm Closed	1/1/1963, 1:00 AM
80	32.116586	-110.859214	10	Perm Closed	1/1/1963, 1:00 AM
81	32.116586	-110.859214	11	Perm Closed	1/1/1963, 1:00 AM
82	32.116586	-110.859214	12	Perm Closed	1/1/1963, 1:00 AM
83	32.116586	-110.859214	13	Perm Closed	1/1/1963, 1:00 AM
84	32.116586	-110.859214	14	Perm Closed	1/1/1963, 1:00 AM
85	32.116586	-110.859214	15	Perm Closed	1/1/1963, 1:00 AM

86	32.116586	-110.859214	16	Open	12/22/1999, 1:00 AM
87	32.116586	-110.859214	17	Open	12/22/1999, 1:00 AM
88	32.116586	-110.859214	18	Open	12/22/1999, 1:00 AM
89	32.116586	-110.859214	1	Perm Closed	1/1/1963, 1:00 AM
90	32.116586	-110.859214	2	Perm Closed	1/1/1963, 1:00 AM
91	32.116586	-110.859214	3	Perm Closed	1/1/1984, 1:00 AM
92	32.116586	-110.859214	4	Perm Closed	1/1/1984, 1:00 AM
93	32.116586	-110.859214	5	Perm Closed	1/1/1963, 1:00 AM
94	32.116586	-110.859214	6	Perm Closed	1/1/1963, 1:00 AM
95	32.116586	-110.859214	7	Perm Closed	1/1/1963, 1:00 AM
96	32.116586	-110.859214	8	Perm Closed	1/1/1963, 1:00 AM
97	32.116586	-110.859214	9	Perm Closed	1/1/1963, 1:00 AM
98	32.116586	-110.859214	10	Perm Closed	1/1/1963, 1:00 AM
99	32.116586	-110.859214	11	Perm Closed	1/1/1963, 1:00 AM
100	32.116586	-110.859214	12	Perm Closed	1/1/1963, 1:00 AM
101	32.116586	-110.859214	13	Perm Closed	1/1/1963, 1:00 AM
102	32.116586	-110.859214	14	Perm Closed	1/1/1963, 1:00 AM
103	32.116586	-110.859214	15	Perm Closed	1/1/1963, 1:00 AM
104	32.116586	-110.859214	16	Open	12/22/1999, 1:00 AM
105	32.116586	-110.859214	17	Open	12/22/1999, 1:00 AM
106	32.116586	-110.859214	18	Open	12/22/1999, 1:00 AM
107	32.133883	-110.885930	1	Open	8/16/2001, 12:00 AM
108	32.133883	-110.885930	2	Open	8/16/2001, 12:00 AM
109	32.125556	-110.873611	1	Open	8/12/2013, 12:00 AM

#	CONST_DESC	MATERIAL	GENERATED	Count
1	No Data	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
2	No Data	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
3	Single Walled	Fiberglass Reinforced Plastic	7/25/2023, 12:00 AM	1
4	Single Walled	Fiberglass Reinforced Plastic	7/25/2023, 12:00 AM	1
5	Single Walled	Fiberglass Reinforced Plastic	7/25/2023, 12:00 AM	1
6	No Data	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
7	No Data	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
8	Single Walled	Fiberglass Reinforced Plastic	7/25/2023, 12:00 AM	1
9	Single Walled	Fiberglass Reinforced Plastic	7/25/2023, 12:00 AM	1
10	Single Walled	Fiberglass Reinforced Plastic	7/25/2023, 12:00 AM	1
11	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
12	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
13	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
14	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
15	Double Walled	Fiberglass Reinforced Plastic	7/25/2023, 12:00 AM	1
16	Double Walled	Fiberglass Reinforced Plastic	7/25/2023, 12:00 AM	1
17	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
18	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
19	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
20	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
21	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
22	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
23	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
24	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
25	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
26	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
27	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
28	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
29	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
30	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
31	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
32	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
33	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
34	No Data	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
35	No Data	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
36	No Data	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
37	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
38	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
39	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
40	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
41	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
42	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1

43	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
44	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
45	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
46	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
47	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
48	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
49	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
50	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
51	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
52	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
53	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
54	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
55	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
56	Double Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
57	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
58	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
59	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
60	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
61	No Data	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
62	No Data	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
63	No Data	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
64	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
65	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
66	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
67	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
68	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
69	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
70	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
71	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
72	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
73	Double Walled	Composite (Steel/Fiberglass or Steel/Polyethylene)	7/25/2023, 12:00 AM	1
74	Double Walled	Composite (Steel/Fiberglass or Steel/Polyethylene)	7/25/2023, 12:00 AM	1
75	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
76	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
77	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
78	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
79	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
80	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
81	Double Walled	Composite (Steel/Fiberglass or Steel/Polyethylene)	7/25/2023, 12:00 AM	1
82	Double Walled	Composite (Steel/Fiberglass or Steel/Polyethylene)	7/25/2023, 12:00 AM	1
83	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1

84	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
85	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
86	Double Walled	Fiberglass Reinforced Plastic	7/25/2023, 12:00 AM	1
87	Double Walled	Fiberglass Reinforced Plastic	7/25/2023, 12:00 AM	1
88	Double Walled	Fiberglass Reinforced Plastic	7/25/2023, 12:00 AM	1
89	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
90	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
91	Double Walled	Composite (Steel/Fiberglass or Steel/Polyethylene)	7/25/2023, 12:00 AM	1
92	Double Walled	Composite (Steel/Fiberglass or Steel/Polyethylene)	7/25/2023, 12:00 AM	1
93	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
94	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
95	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
96	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
97	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
98	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
99	Double Walled	Composite (Steel/Fiberglass or Steel/Polyethylene)	7/25/2023, 12:00 AM	1
100	Double Walled	Composite (Steel/Fiberglass or Steel/Polyethylene)	7/25/2023, 12:00 AM	1
101	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
102	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
103	Single Walled	Asphalt coated or Bare Steel	7/25/2023, 12:00 AM	1
104	Double Walled	Fiberglass Reinforced Plastic	7/25/2023, 12:00 AM	1
105	Double Walled	Fiberglass Reinforced Plastic	7/25/2023, 12:00 AM	1
106	Double Walled	Fiberglass Reinforced Plastic	7/25/2023, 12:00 AM	1
107	Double Walled	Fiberglass Reinforced Plastic	7/25/2023, 12:00 AM	1
108	Double Walled	Fiberglass Reinforced Plastic	7/25/2023, 12:00 AM	1
109	Double Walled	Fiberglass Reinforced Plastic	7/25/2023, 12:00 AM	1

## UST - Releases

#	LUST_NUM	STATUS	DISCOVERED	PLACE_ID	PLACE_TYPE
1	4139	Closed	6/19/1995, 12:00 AM	4118	GAS FILLING STATION - COMMERCIAL
2	4139	Closed	6/19/1995, 12:00 AM	4118	GAS FILLING STATION - COMMERCIAL
3	4139	Closed	6/19/1995, 12:00 AM	4118	GAS FILLING STATION - COMMERCIAL
4	4139	Closed	6/19/1995, 12:00 AM	4118	GAS FILLING STATION - COMMERCIAL
5	2213	Closed	3/6/1992, 1:00 AM	3533	GAS FILLING STATION - COMMERCIAL
6	183	Closed	6/2/1986, 12:00 AM	3684	TRUCKING COMPANY
7	183	Closed	7/22/1988, 12:00 AM	3684	TRUCKING COMPANY
8	183	Closed	7/22/1988, 12:00 AM	3684	TRUCKING COMPANY
9	183	Closed	6/30/2017, 12:00 AM	3684	TRUCKING COMPANY
10	183	Closed	6/30/2017, 12:00 AM	3684	TRUCKING COMPANY
11	312	Closed	5/28/1987, 12:00 AM	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY
12	312	Closed	4/16/1996, 12:00 AM	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY
13	312	Closed	10/27/1998, 1:00 AM	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY
14	312	Closed	10/27/1998, 1:00 AM	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY
15	312	Closed	10/28/1998, 1:00 AM	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY
16	312	Closed	10/28/1998, 1:00 AM	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY
17	312	Closed	11/12/1998, 1:00 AM	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY
18	312	Closed	11/12/1998, 1:00 AM	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY
19	312	Closed	11/12/1998, 1:00 AM	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY
20	312	Closed	11/12/1998, 1:00 AM	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY
21	312	Closed	11/15/1998, 1:00 AM	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY
22	312	Closed	11/12/1998, 1:00 AM	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY
23	312	Closed	10/6/1998, 12:00 AM	350	AIRCRAFT REPAIR/MAINTENANCE FACILITY

#	LATITUDE	LONGITUDE	GENERATED	FACILITY	CASE_NUM
1	32.128837	-110.875645	7/26/2023, 12:00 AM	0-001331	4139.01
2	32.128837	-110.875645	7/26/2023, 12:00 AM	0-001331	4139.02
3	32.128837	-110.875645	7/26/2023, 12:00 AM	0-001331	4139.03
4	32.128837	-110.875645	7/26/2023, 12:00 AM	0-001331	4139.04
5	32.126529	-110.875757	7/26/2023, 12:00 AM	0-002022	2213.01
6	32.127025	-110.876312	7/26/2023, 12:00 AM	0-005199	0183.01
7	32.127025	-110.876312	7/26/2023, 12:00 AM	0-005199	0183.02
8	32.127025	-110.876312	7/26/2023, 12:00 AM	0-005199	0183.03
9	32.127025	-110.876312	7/26/2023, 12:00 AM	0-005199	0183.05
10	32.127025	-110.876312	7/26/2023, 12:00 AM	0-005199	0183.06
11	32.116586	-110.859214	7/26/2023, 12:00 AM	0-007522	0312.01
12	32.116586	-110.859214	7/26/2023, 12:00 AM	0-007522	0312.02
13	32.116586	-110.859214	7/26/2023, 12:00 AM	0-007522	0312.03
14	32.116586	-110.859214	7/26/2023, 12:00 AM	0-007522	0312.04
15	32.116586	-110.859214	7/26/2023, 12:00 AM	0-007522	0312.05
16	32.116586	-110.859214	7/26/2023, 12:00 AM	0-007522	0312.06
17	32.116586	-110.859214	7/26/2023, 12:00 AM	0-007522	0312.07
18	32.116586	-110.859214	7/26/2023, 12:00 AM	0-007522	0312.08
19	32.116586	-110.859214	7/26/2023, 12:00 AM	0-007522	0312.09
20	32.116586	-110.859214	7/26/2023, 12:00 AM	0-007522	0312.10
21	32.116586	-110.859214	7/26/2023, 12:00 AM	0-007522	0312.11
22	32.116586	-110.859214	7/26/2023, 12:00 AM	0-007522	0312.12
23	32.116586	-110.859214	7/26/2023, 12:00 AM	0-007522	0312.13

#	PLACE_NAME	ADDRESS	CITY	COUNTY	ZIP_CODE
1	CIRCLE K #843	6750 S CRAYCROFT RD	TUCSON	Pima	85756
2	CIRCLE K #843	6750 S CRAYCROFT RD	TUCSON	Pima	85756
3	CIRCLE K #843	6750 S CRAYCROFT RD	TUCSON	Pima	85756
4	CIRCLE K #843	6750 S CRAYCROFT RD	TUCSON	Pima	85756
5	EXXON #7-6390	5471 E BENSON HWY	TUCSON	Pima	85756
6	TUCSON TRUCK TERMINAL	5451 E BENSON HWY	TUCSON	Pima	85756
7	TUCSON TRUCK TERMINAL	5451 E BENSON HWY	TUCSON	Pima	85756
8	TUCSON TRUCK TERMINAL	5451 E BENSON HWY	TUCSON	Pima	85756
9	TUCSON TRUCK TERMINAL	5451 E BENSON HWY	TUCSON	Pima	85756
10	TUCSON TRUCK TERMINAL	5451 E BENSON HWY	TUCSON	Pima	85756
11	HATTAR CHEVRON	6261 E BENSON HWY	TUCSON	Pima	85756
12	HATTAR CHEVRON	6261 E BENSON HWY	TUCSON	Pima	85756

13	HATTAR CHEVRON	6261 E BENSON HWY	TUCSON	Pima	85756
14	HATTAR CHEVRON	6261 E BENSON HWY	TUCSON	Pima	85756
15	HATTAR CHEVRON	6261 E BENSON HWY	TUCSON	Pima	85756
16	HATTAR CHEVRON	6261 E BENSON HWY	TUCSON	Pima	85756
17	HATTAR CHEVRON	6261 E BENSON HWY	TUCSON	Pima	85756
18	HATTAR CHEVRON	6261 E BENSON HWY	TUCSON	Pima	85756
19	HATTAR CHEVRON	6261 E BENSON HWY	TUCSON	Pima	85756
20	HATTAR CHEVRON	6261 E BENSON HWY	TUCSON	Pima	85756
21	HATTAR CHEVRON	6261 E BENSON HWY	TUCSON	Pima	85756
22	HATTAR CHEVRON	6261 E BENSON HWY	TUCSON	Pima	85756
23	HATTAR CHEVRON	6261 E BENSON HWY	TUCSON	Pima	85756

#	CADASTRAL	PRIORITY	LOCATION	REPORTED	CONFIRMED	CLOSURE	Count
1	No Data	Closed soil-only CoC levels meet RBCA Tier 1 standards	10K gasoline UST	6/30/1995, 12:00 AM	6/19/1995, 12:00 AM	2/27/1997, 1:00 AM	1
2	No Data	Closed soil-only CoC levels meet RBCA Tier 1 standards	10K UL gasoline UST	6/30/1995, 12:00 AM	6/19/1995, 12:00 AM	2/27/1997, 1:00 AM	1
3	No Data	Closed soil-only CoC levels meet RBCA Tier 1 standards	Gasoline product piping	6/30/1995, 12:00 AM	6/19/1995, 12:00 AM	2/27/1997, 1:00 AM	1
4	No Data	Closed soil-only CoC levels meet RBCA Tier 1 standards	UL gasoline fuel dispenser	6/30/1995, 12:00 AM	6/19/1995, 12:00 AM	2/27/1997, 1:00 AM	1
5	D 15 14 14 dad	Closed soil-only CoC levels meet RBCA Tier 1 standards	Gasoline product piping @ South pump island	3/6/1992, 1:00 AM	3/6/1992, 1:00 AM	5/17/2000, 12:00 AM	1
6	D 15 14 14 aaa	Closed groundwater and soil CoC levels meet RBCA Tier 1 standards	Diesel fuel/gasoline/bulk/used oil USTs West of the Terminal Building	6/2/1986, 12:00 AM	6/2/1986, 12:00 AM	8/14/2008, 12:00 AM	1
7	D 15 14 14 aaa	Closed soil-only CoC levels meet RBCA Tier 1 standards	12K diesel fuel USTs West of Mr. T's building	7/22/1988, 12:00 AM	7/22/1988, 12:00 AM	11/15/2006, 1:00 AM	1
8	D 15 14 14 aaa	Closed soil-only CoC levels meet RBCA Tier 1 standards	12K Gasoline USTs West of AZ Truckomat building	7/22/1988, 12:00 AM	7/22/1988, 12:00 AM	11/15/2006, 1:00 AM	1
9	D 15 14 14 aaa	Closed soil-only CoC levels meet RBCA Tier 1 standards	Truck Side: tank pit, piping and dispensers	8/24/2017, 12:00 AM	6/30/2017, 12:00 AM	3/19/2019, 12:00 AM	1
10	D 15 14 14 aaa	Closed soil-only CoC levels meet RBCA Tier 1 standards	Auto Side Sample Locations: TTT-D5/D6 and TTT-D9/D10	8/24/2017, 12:00 AM	6/30/2017, 12:00 AM	3/19/2019, 12:00 AM	1
11	D 15 14 24 aad	Closed soil/GW levels meet RBCA Tier 2	Gasoline UST	5/28/1987, 12:00 AM	5/28/1987, 12:00 AM	11/26/2007, 1:00 AM	1
12	D 15 14 24 aad	Closed soil-only CoC levels meet RBCA Tier 1 standards	New oil product piping/lines 1-8	5/6/1996, 12:00 AM	4/16/1996, 12:00 AM	8/23/2000, 12:00 AM	1
13	D 15 14 24 aad	Closed soil-only CoC levels meet RBCA Tier 1 standards	Gasoline/diesel fuel pump islands #2 & #3	2/16/1999, 1:00 AM	10/27/1998, 1:00 AM	8/23/2000, 12:00 AM	1
14	D 15 14 24 aad	Closed soil-only CoC levels meet RBCA Tier 1 standards	Gasoline/diesel fuel pump island #5	2/16/1999, 1:00 AM	10/27/1998, 1:00 AM	8/23/2000, 12:00 AM	1
15	D 15 14 24 aad	Closed soil-only CoC levels meet RBCA Tier 1 standards	Gasoline/diesel fuel product piping/18	2/16/1999, 1:00 AM	10/28/1998, 1:00 AM	8/23/2000, 12:00 AM	1

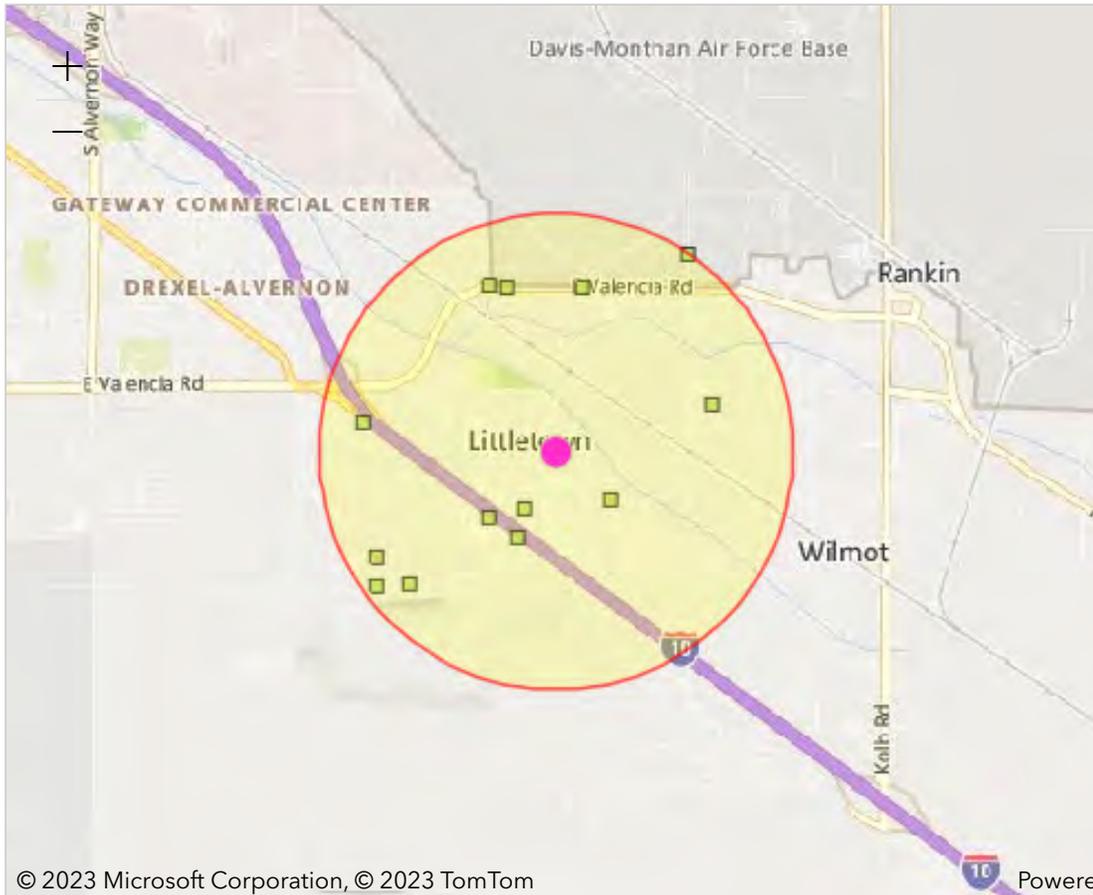
16	D 15 14 24 aad	Closed soil-only CoC levels meet RBCA Tier 1 standards	Gasoline/diesel fuel product piping/19	2/16/1999, 1:00 AM	10/28/1998, 1:00 AM	8/23/2000, 12:00 AM	1
17	D 15 14 24 aad	Closed soil/GW levels meet RBCA Tier 2	Gasoline/diesel fuel M pump #1 & #2/W pump #18	2/16/1999, 1:00 AM	11/12/1998, 1:00 AM	11/26/2007, 1:00 AM	1
18	D 15 14 24 aad	Closed soil/GW levels meet RBCA Tier 2	Gasoline/diesel fuel M pump #3/W pump #17	2/16/1999, 1:00 AM	11/12/1998, 1:00 AM	11/26/2007, 1:00 AM	1
19	D 15 14 24 aad	Closed soil/GW levels meet RBCA Tier 2	Gasoline/diesel fuel M pump #4 & #5/W pump #15 & #16	2/16/1999, 1:00 AM	11/12/1998, 1:00 AM	11/26/2007, 1:00 AM	1
20	D 15 14 24 aad	Closed soil-only CoC levels meet RBCA Tier 1 standards	Gasoline/diesel fuel M pump #6/W pump #14	2/16/1999, 1:00 AM	11/12/1998, 1:00 AM	8/23/2000, 12:00 AM	1
21	D 15 14 24 aad	Closed soil-only CoC levels meet RBCA Tier 1 standards	Gasoline/diesel fuel M pump #7 & #8/W pump #12	2/16/1999, 1:00 AM	11/15/1998, 1:00 AM	8/23/2000, 12:00 AM	1
22	D 15 14 24 aad	Closed soil-only CoC levels meet RBCA Tier 1 standards	Gasoline/diesel fuel M pump #9	2/16/1999, 1:00 AM	11/12/1998, 1:00 AM	8/23/2000, 12:00 AM	1
23	D 15 14 24 aad	Closed soil/GW levels meet RBCA Tier 2	12K diesel fuel UST #2	2/16/1999, 1:00 AM	10/6/1998, 12:00 AM	11/26/2007, 1:00 AM	1

## Voluntary Remediation

#	SITENAME	SITECODE	SITETYPE	ADDRESS	CITY
1	Los Reales/Southwest Disposal Area	505220-00	Landfill	5306 E Los Reales Rd	Tucson

#	ZIP	ACCEPTED	STATUS	CLOSED	VOLUNTEER
1	85706	7/25/2003, 12:00 AM	Active	<i>No Data</i>	City of Tucson Solid Waste Management

#	PLACE_ID	PLACE_TYPE	Count
1	3024	Closed Municipal Solid Waste Landfill	1



**Report question: *Within 1.2 of a Hazardous waste site? yes***

Modify question by entering a new buffer distance and unit for the selected study area:

Name	Distance
<input type="checkbox"/> GLASTEC INC (COMBINED TECHNOLOGIES) (TUCSON,AZ) ( <a href="https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZS000048645">https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZS000048645</a> ) <b>REGISTRY_ID:</b> 110070571966 <b>LATITUDE:</b> 32.12187 <b>LONGITUDE:</b> -110.88478 <b>PGM_SYS_ACRNM:</b> RCRAINFO <b>PGM_SYS_ID:</b> AZS000048645 <b>LOCATION_ADDRESS:</b> 7140 S FRANCES <b>CITY_NAME:</b> TUCSON <b>COUNTY_NAME:</b> PIMA <b>STATE_CODE:</b> AZ <b>EPA_REGION:</b> Region 9 <b>POSTAL_CODE:</b> 85706 <b>FIPS_CODE:</b> 04019 <b>HUC_CODE:</b>	1.05 miles

Name	Distance
<input type="checkbox"/> ARIZONA ARMY NATIONAL GUARD - ORGANIZATIONAL MAINTENANCE (TUCSON,AZ) (https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZ0150090058) <b>REGISTRY_ID:</b> 110002566054 <b>LATITUDE:</b> 32.14151 <b>LONGITUDE:</b> -110.8736 <b>PGM_SYS_ACRNM:</b> RCRAINFO <b>PGM_SYS_ID:</b> AZ0150090058 <b>LOCATION_ADDRESS:</b> 5500 E VALENCIA RD <b>CITY_NAME:</b> TUCSON <b>COUNTY_NAME:</b> PIMA <b>STATE_CODE:</b> AZ <b>EPA_REGION:</b> Region 9 <b>POSTAL_CODE:</b> 85756 <b>FIPS_CODE:</b> 04019 <b>HUC_CODE:</b>	0.86 miles
<input type="checkbox"/> PIMA COUNTY DOT FCD (TUCSON,AZ) (https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZE990414001) <b>REGISTRY_ID:</b> 110013369693 <b>LATITUDE:</b> 32.14164 <b>LONGITUDE:</b> -110.87503 <b>PGM_SYS_ACRNM:</b> RCRAINFO <b>PGM_SYS_ID:</b> AZE990414001 <b>LOCATION_ADDRESS:</b> VALENCIA RD AND CRAYCROFT RD <b>CITY_NAME:</b> TUCSON <b>COUNTY_NAME:</b> PIMA <b>STATE_CODE:</b> AZ <b>EPA_REGION:</b> Region 9 <b>POSTAL_CODE:</b> 85706 <b>FIPS_CODE:</b> 04019 <b>HUC_CODE:</b>	0.89 miles
<input type="checkbox"/> TOSCO NORTHWEST 02481 (TUCSON,AZ) (https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZD983470949) <b>REGISTRY_ID:</b> 110008256733 <b>LATITUDE:</b> 32.13177 <b>LONGITUDE:</b> -110.88601 <b>PGM_SYS_ACRNM:</b> RCRAINFO <b>PGM_SYS_ID:</b> AZD983470949 <b>LOCATION_ADDRESS:</b> 4900 W INA RD AT HWY I-10 <b>CITY_NAME:</b> TUCSON <b>COUNTY_NAME:</b> PIMA <b>STATE_CODE:</b> AZ <b>EPA_REGION:</b> Region 9 <b>POSTAL_CODE:</b> 85743-9746 <b>FIPS_CODE:</b> 04019 <b>HUC_CODE:</b>	0.99 miles
<input type="checkbox"/> DEALERS AUTO EXCHANGE OF TUCSON (TUCSON,AZ) (https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZD982436461) <b>REGISTRY_ID:</b> 110002580252 <b>LATITUDE:</b> 32.12475 <b>LONGITUDE:</b> -110.87521 <b>PGM_SYS_ACRNM:</b> RCRAINFO <b>PGM_SYS_ID:</b> AZD982436461 <b>LOCATION_ADDRESS:</b> 7090 S CRAYCROFT RD <b>CITY_NAME:</b> TUCSON <b>COUNTY_NAME:</b> PIMA <b>STATE_CODE:</b> AZ <b>EPA_REGION:</b> Region 9 <b>POSTAL_CODE:</b> 85756 <b>FIPS_CODE:</b> 04019 <b>HUC_CODE:</b>	0.48 miles

Name	Distance
<input type="checkbox"/> VELOCITY TRUCK CENTERS - TUCSON (TUCSON,AZ) ( <a href="https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZR000521534">https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZR000521534</a> ) <b>REGISTRY_ID:</b> 110070558702 <b>LATITUDE:</b> 32.125342 <b>LONGITUDE:</b> -110.872052 <b>PGM_SYS_ACRNM:</b> RCRAINFO <b>PGM_SYS_ID:</b> AZR000521534 <b>LOCATION_ADDRESS:</b> 5650 E TRAVEL PLAZA WAY <b>CITY_NAME:</b> TUCSON <b>COUNTY_NAME:</b> PIMA <b>STATE_CODE:</b> AZ <b>EPA_REGION:</b> Region 9 <b>POSTAL_CODE:</b> 85756 <b>FIPS_CODE:</b> 04019 <b>HUC_CODE:</b>	0.33 miles
<input type="checkbox"/> APC EQUIPMENT (TUCSON,AZ) ( <a href="https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZR000513036">https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZR000513036</a> ) <b>REGISTRY_ID:</b> 110039265586 <b>LATITUDE:</b> 32.1198 <b>LONGITUDE:</b> -110.88479 <b>PGM_SYS_ACRNM:</b> RCRAINFO <b>PGM_SYS_ID:</b> AZR000513036 <b>LOCATION_ADDRESS:</b> 7291 S FRANCES AVE <b>CITY_NAME:</b> TUCSON <b>COUNTY_NAME:</b> PIMA <b>STATE_CODE:</b> AZ <b>EPA_REGION:</b> Region 9 <b>POSTAL_CODE:</b> 85756-9704 <b>FIPS_CODE:</b> 04019 <b>HUC_CODE:</b>	1.13 miles
<input type="checkbox"/> SHANNON ROAD/EL CAMINO DEL CERRO SITE (TUCSON,AZ) ( <a href="https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZR000039628">https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZR000039628</a> ) <b>REGISTRY_ID:</b> 110012190627 <b>LATITUDE:</b> 32.123269 <b>LONGITUDE:</b> -110.87259 <b>PGM_SYS_ACRNM:</b> RCRAINFO <b>PGM_SYS_ID:</b> AZR000039628 <b>LOCATION_ADDRESS:</b> TAKE I-10 TO SUNSET RD EXIT; CONTINUE SOUTH ON <b>CITY_NAME:</b> TUCSON <b>COUNTY_NAME:</b> PIMA <b>STATE_CODE:</b> AZ <b>EPA_REGION:</b> Region 9 <b>POSTAL_CODE:</b> 85743 <b>FIPS_CODE:</b> 04019 <b>HUC_CODE:</b>	0.47 miles
<input type="checkbox"/> PIMA AIR & SPACE MUSEUM (TUCSON,AZ) ( <a href="https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZR000508572">https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZR000508572</a> ) <b>REGISTRY_ID:</b> 110032633773 <b>LATITUDE:</b> 32.1415 <b>LONGITUDE:</b> -110.86701 <b>PGM_SYS_ACRNM:</b> RCRAINFO <b>PGM_SYS_ID:</b> AZR000508572 <b>LOCATION_ADDRESS:</b> 6000 E VALENCIA RD <b>CITY_NAME:</b> TUCSON <b>COUNTY_NAME:</b> PIMA <b>STATE_CODE:</b> AZ <b>EPA_REGION:</b> Region 9 <b>POSTAL_CODE:</b> 85756 <b>FIPS_CODE:</b> 04019 <b>HUC_CODE:</b>	0.83 miles

Name	Distance
<input type="checkbox"/> WESTERN INTL AVIATION INC (TUCSON,AZ) ( <a href="https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZD982435182">https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZD982435182</a> ) <b>REGISTRY_ID:</b> 110002580074 <b>LATITUDE:</b> 32.14401 <b>LONGITUDE:</b> -110.85795 <b>PGM_SYS_ACRNM:</b> RCRAINFO <b>PGM_SYS_ID:</b> AZD982435182 <b>LOCATION_ADDRESS:</b> 5951 S WILMOT RD <b>CITY_NAME:</b> TUCSON <b>COUNTY_NAME:</b> PIMA <b>STATE_CODE:</b> AZ <b>EPA_REGION:</b> Region 9 <b>POSTAL_CODE:</b> 85706 <b>FIPS_CODE:</b> 04019 <b>HUC_CODE:</b>	1.20 miles
<input type="checkbox"/> PIMA AIR & SPACE MUSEUM (TUCSON,AZ) ( <a href="https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZE070730001">https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZE070730001</a> ) <b>REGISTRY_ID:</b> 110032633773 <b>LATITUDE:</b> 32.1415 <b>LONGITUDE:</b> -110.86701 <b>PGM_SYS_ACRNM:</b> RCRAINFO <b>PGM_SYS_ID:</b> AZE070730001 <b>LOCATION_ADDRESS:</b> 6000 E VALENCIA RD <b>CITY_NAME:</b> TUCSON <b>COUNTY_NAME:</b> PIMA <b>STATE_CODE:</b> AZ <b>EPA_REGION:</b> Region 9 <b>POSTAL_CODE:</b> 85756-9403 <b>FIPS_CODE:</b> 04019 <b>HUC_CODE:</b>	0.83 miles
<input type="checkbox"/> GHOST LIGHTING (TUCSON,AZ) ( <a href="https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZR000509224">https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZR000509224</a> ) <b>REGISTRY_ID:</b> 110070206048 <b>LATITUDE:</b> 32.12605 <b>LONGITUDE:</b> -110.86469 <b>PGM_SYS_ACRNM:</b> RCRAINFO <b>PGM_SYS_ID:</b> AZR000509224 <b>LOCATION_ADDRESS:</b> 6949 S GOSHAWK DR <b>CITY_NAME:</b> TUCSON <b>COUNTY_NAME:</b> PIMA <b>STATE_CODE:</b> AZ <b>EPA_REGION:</b> Region 9 <b>POSTAL_CODE:</b> 85756-8608 <b>FIPS_CODE:</b> 04019 <b>HUC_CODE:</b>	0.37 miles
<input type="checkbox"/> LOS REALES LANDFILL (TUCSON,AZ) ( <a href="https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZR000032573">https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZR000032573</a> ) <b>REGISTRY_ID:</b> 110002606109 <b>LATITUDE:</b> 32.1199 <b>LONGITUDE:</b> -110.88195 <b>PGM_SYS_ACRNM:</b> RCRAINFO <b>PGM_SYS_ID:</b> AZR000032573 <b>LOCATION_ADDRESS:</b> 5300 E LOS REALES RD <b>CITY_NAME:</b> TUCSON <b>COUNTY_NAME:</b> PIMA <b>STATE_CODE:</b> AZ <b>EPA_REGION:</b> Region 9 <b>POSTAL_CODE:</b> 85706 <b>FIPS_CODE:</b> 04019 <b>HUC_CODE:</b>	0.99 miles

Name	Distance
<input type="checkbox"/> AVENT INC (TUCSON,AZ) ( <a href="https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZR000047118">https://enviro.epa.gov/envirofacts/rcrainfo/facility?handlerId=AZR000047118</a> ) <b>REGISTRY_ID:</b> 110017206398 <b>LATITUDE:</b> 32.13305 <b>LONGITUDE:</b> -110.85586 <b>PGM_SYS_ACRNM:</b> RCRAINFO <b>PGM_SYS_ID:</b> AZR000047118 <b>LOCATION_ADDRESS:</b> 6620 S MEMORIAL PL <b>CITY_NAME:</b> TUCSON <b>COUNTY_NAME:</b> PIMA <b>STATE_CODE:</b> AZ <b>EPA_REGION:</b> Region 9 <b>POSTAL_CODE:</b> 85756 <b>FIPS_CODE:</b> 04019 <b>HUC_CODE:</b>	0.83 miles

# Detailed Facility Report



## Detailed Facility Report

### Facility Summary

AVENT INC

6620 S MEMORIAL PL, TUCSON, AZ 85756

FRS (Facility Registry Service) ID: 110017206398

EPA Region: 09

Latitude: 32.13305

Longitude: -110.85586

Locational Data Source: FRS

Industries: Miscellaneous Manufacturing

Indian Country: N

### Enforcement and Compliance Summary

Statute	RCRA
Compliance Monitoring Activities (5 years)	1
Date of Last Compliance Monitoring Activity	04/21/2021
Compliance Status	No Violation Identified
Qtrs in Noncompliance (of 12)	1
Qtrs with Significant Violation	0
Informal Enforcement Actions (5 years)	--
Formal Enforcement Actions (5 years)	--
Penalties from Formal Enforcement Actions (5 years)	--
EPA Cases (5 years)	--
Penalties from EPA Cases (5 years)	--

### Regulatory Information

Clean Air Act (CAA): No Information

Clean Water Act (CWA): No Information

Resource Conservation and Recovery Act (RCRA): Active  
SQG, (AZR000047118)

Safe Drinking Water Act (SDWA): No Information

[Go To Enforcement/Compliance Details](#)  
[Known Data Problems](#)

### Other Regulatory Reports

Air Emissions Inventory (EIS): No Information

Greenhouse Gas Emissions (eGGRT): No Information

Toxic Releases (TRI): No Information

Compliance and Emissions Data Reporting Interface (CEDRI):  
No Information



Statute	Program/Pollutant/Violation Type	QTR 1	QTR 2	QTR 3	QTR 4	QTR 5	QTR 6	QTR 7	QTR 8	QTR 9	QTR 10	QTR 11	QTR 12+
RCRA	262.A: Generators - General AZ			04/21/2021-06/25/2021									
RCRA	262.A: Generators - General AZ			04/21/2021-06/25/2021									
RCRA	262.A: Generators - General AZ			04/21/2021-06/25/2021									

**Informal Enforcement Actions** Last 5 Years ▼

Statute	System	Source ID	Type of Action	Lead Agency	Date
<i>RCRA</i>	<i>RCRAInfo</i>	<i>AZR000047118</i>	<i>NOC/NOTICE OF OPPORTUNITY TO CORRECT</i>	<i>State</i>	<i>06/02/2021</i>

*Entries in italics are not counted as "informal enforcement actions" in EPA policies pertaining to enforcement response tools.*

**Formal Enforcement Actions** Last 5 Years ▼

Statute	System	Law/Section	Source ID	Type of Action	Case No.	Lead Agency	Case Name	Issued/Filed Date	Settlements/Actions	Settlement/Action Date	Federal Penalty Assessed	State/Local Penalty Assessed	Penalty Amount Collected	SEP Value	Comp Action Cost
No data records returned															

## Environmental Conditions

### Watersheds

12-Digit WBD (Watershed Boundary Dataset) HUC (RAD (Reach Address Database))	WBD (Watershed Boundary Dataset) Subwatershed Name (RAD (Reach Address Database))	State Water Body Name (ICIS (Integrated Compliance Information System))	Beach Closures Within Last Year	Beach Closures Within Last Two Years	Pollutants Potentially Related to Impairment	Watershed with ESA (Endangered Species Act)-listed Aquatic Species?
No data records returned						

### Assessed Waters From Latest State Submission (ATTAINS)

State	Report Cycle	Assessment Unit ID	Assessment Unit Name	Water Condition	Cause Groups Impaired	Drinking Water Use	Aquatic Life	Fish Consumption Use	Recreation Use	Other Use
No data records returned										

### Air Quality Nonattainment Areas

Pollutant	Within Nonattainment Status Area?	Nonattainment Status Applicable Standard(s)	Within Maintenance Status Area?	Maintenance Status Applicable Standard(s)
Ozone	No	--	No	--
Lead	No	--	No	--
Particulate Matter	No	--	No	--
Carbon Monoxide	No	--	Yes	Carbon Monoxide (1971)
Sulfur Dioxide	No	--	No	--

## Pollutants

### Toxics Release Inventory History of Reported Chemicals Released in Pounds per Year at Site

TRI Facility ID	Year	Total Air Emissions	Surface Water Discharges	Off-Site Transfers to POTWs (Publicly Owned Treatment Works)	Underground Injections	Releases to Land	Total On-Site Releases	Total Off-Site Transfers
No data records returned								

### Toxics Release Inventory Total Releases and Transfers in Pounds by Chemical and Year

Chemical Name
---------------

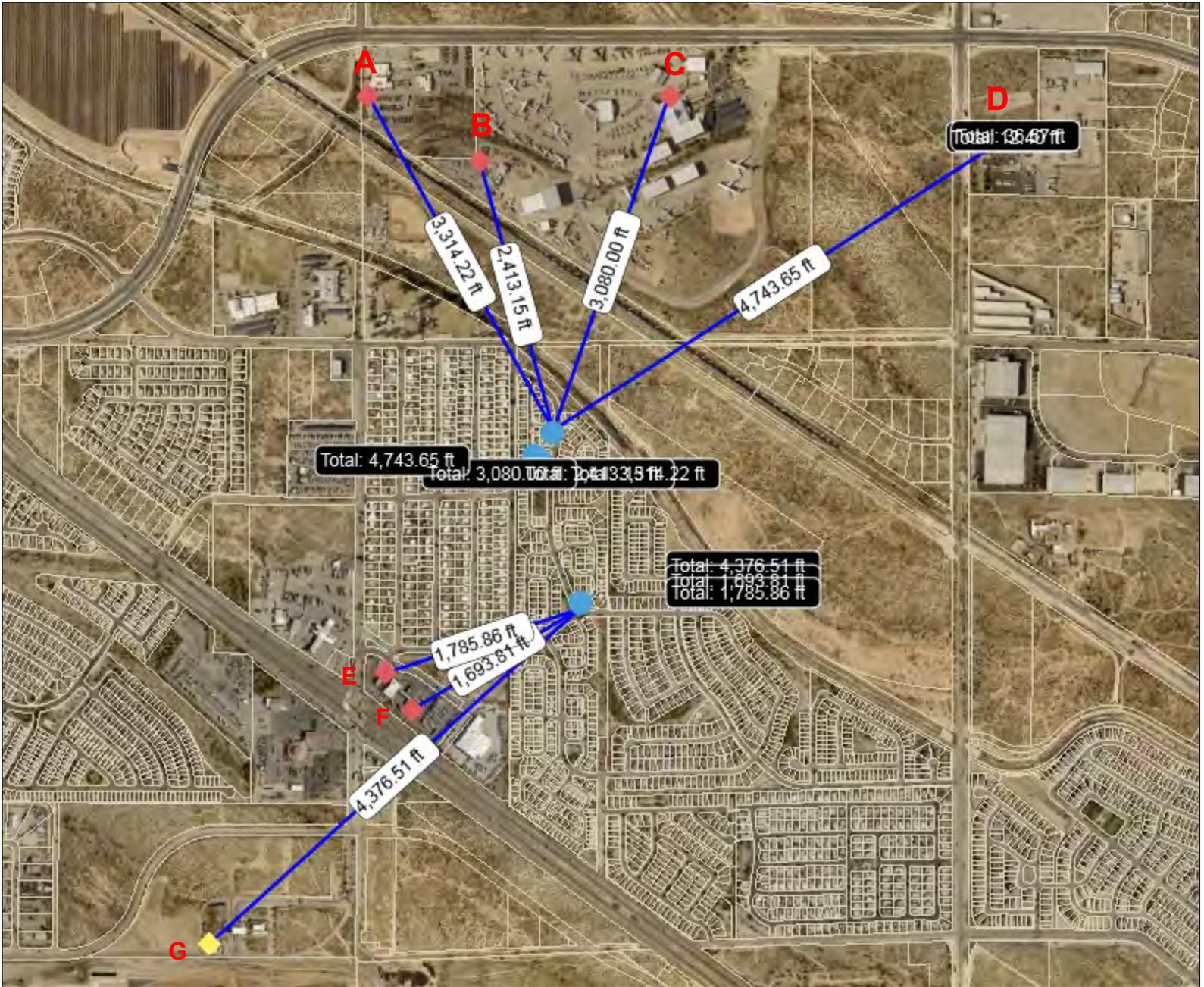


## Corazon del Pueblo – Aboveground Storage Tank Summary

AST	Location	Container	ASDBOP (ft)	ASDPPU (ft)	ASDBPU (ft)	Measured Distance (ft)
A	5500 E Valencia Rd	1,920 gallons (water volume capacity), propane (two tanks onsite, assessed closest to project site*)	271.74	362.93	67.98	3,314.22
B	6000 E Valencia Rd	1,500 gallons, diesel/gasoline (split tank), liquid, undiked	n/a	327.46	60.65	2,413.15
C	6000 E Valencia Rd	500 gallons (water volume capacity), propane†	n/a	n/a	n/a	3,080.00
D	6216 S Wilmot Rd	12,000 gallons, diesel no. 2, liquid, undiked	n/a	778.73	158.60	4,743.65
E	5570 E Travel Plaza Way	800 gallons (water volume capacity), propane†	n/a	n/a	n/a	1,785.86
F	5570 E Travel Plaza Way	9,000 gallons, diesel, liquid, undiked (three, 9,000 gallon diesel ASTs onsite, selected closest to property; one 9,000 gallon diesel exhaust fluid (DEF) also stored within containment area)	n/a	690.78	138.85	1,693.81
G	5300 E Los Reales Rd	Planned aboveground storage containers, Household Hazardous Waste, diked (containment area) approximately 14 x 52 ft	n/a	135.48 (ASDPNPD)	22.94 (ASDBNPD)	4,376.51

\*Note – Approximate container volumes based on aerial maps and site surveys.

†AST E contains less than 1,000 gallons (water volume capacity) of propane and meet the requirements of the 2017 version of National Fire Protection Association (NFPA) Code 58



Notes

AST Map

Legend

□ Parcels

1: 18,056



2,915.3 0 1,457.64 2,915.3 Feet

WGS\_1984\_Web\_Mercator\_Auxiliary\_Sphere  
© City of Tucson

This map is a user generated static output from an Internet mapping site and is for reference only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable.

THIS MAP IS NOT TO BE USED FOR NAVIGATION



U.S. Fish and Wildlife Service, National Standards and Support Team,  
wetlands\_team@fws.gov

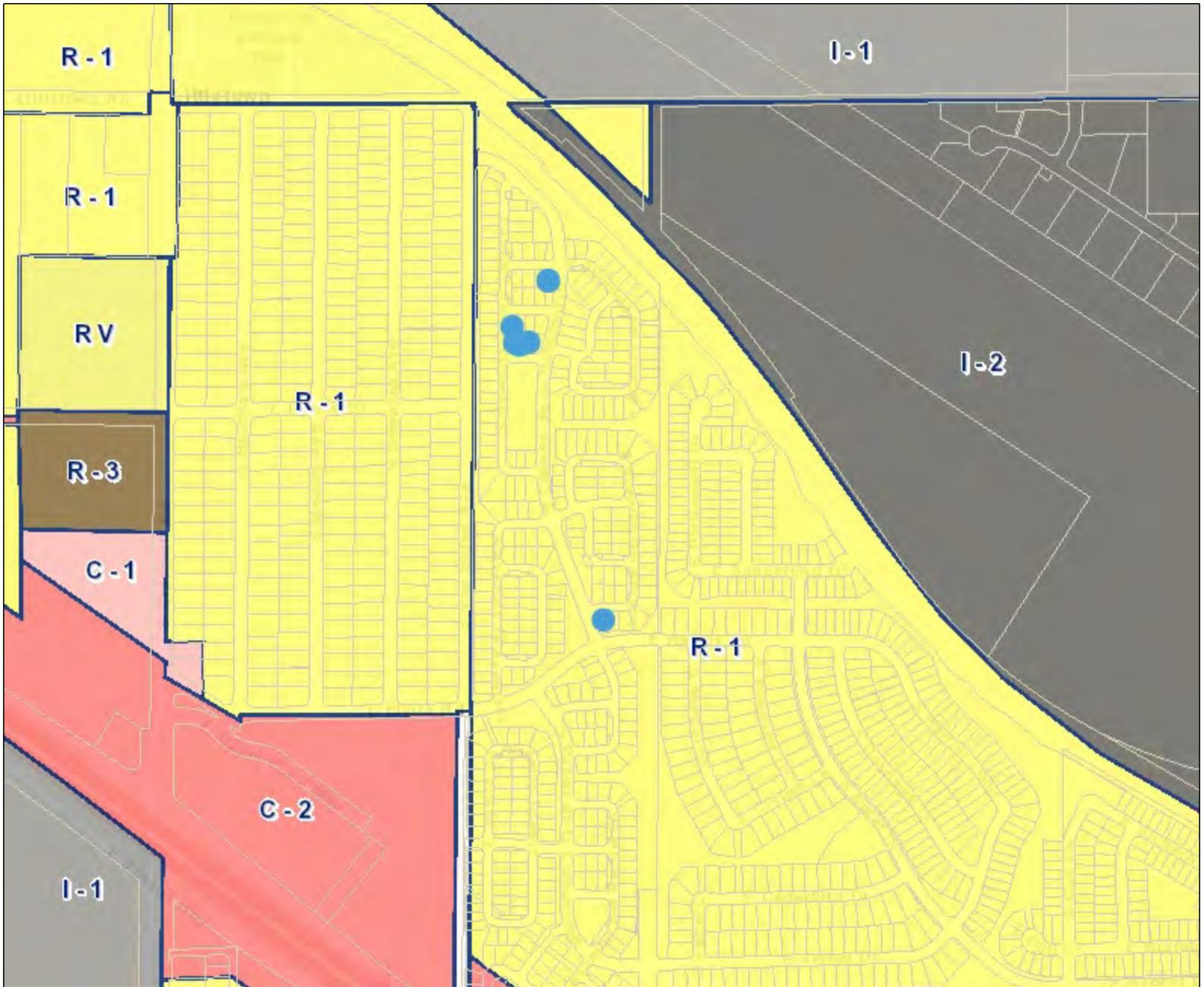
September 15, 2023

**Wetlands**

- |   |                                |   |                                   |   |          |
|---|--------------------------------|---|-----------------------------------|---|----------|
|  | Estuarine and Marine Deepwater |  | Freshwater Emergent Wetland       |  | Lake     |
|  | Estuarine and Marine Wetland   |  | Freshwater Forested/Shrub Wetland |  | Other    |
|  | Freshwater Pond                |  |                                   |  | Riverine |

This map is for general reference only. The US Fish and Wildlife Service is not responsible for the accuracy or currentness of the base data shown on this map. All wetlands related data should be used in accordance with the layer metadata found on the Wetlands Mapper web site.

Project locations - 5717, 5725, 5733 E Calle De La Apertura, 5716 E Camino De Las Iguanas, 5746 E Calle De Hielo Dulce, 5782 E Camino Esperanza Eterna



Notes

Zoning Map

Legend

□ Parcels	■ NR-2	■ HC-2	■ HO-3	■ I-1
■ Tucson Zoning	■ R-3	■ C-3	■ P	■ I-2
■ <all other values>	■ HR-3	■ HC-3	■ HP	■ OCR-1
■ PADs	■ NR-3	■ HLC-3	■ RX-1	■ OCR-2
■ R-1	■ UR-3	■ UC-3	■ HLRX-1	■ HOCR-2
■ HLR-1	■ C-1	■ HNC	■ HRX-1	■ MU
■ HR-1	■ HC-1	■ O-1	■ RX-2	■ RH
■ NR-1	■ HLC-1	■ HO-1	■ HRX-2	■ SH
■ R-2	■ UC-1	■ O-2	■ SR	■ RV
■ HLR-2	■ UHC-1	■ HO-2	■ HSR	■ MH-1
■ HR-2	■ C-2	■ O-3	■ P-I	■ MH-2

1: 9,028



1,457.6 0 728.82 1,457.6 Feet

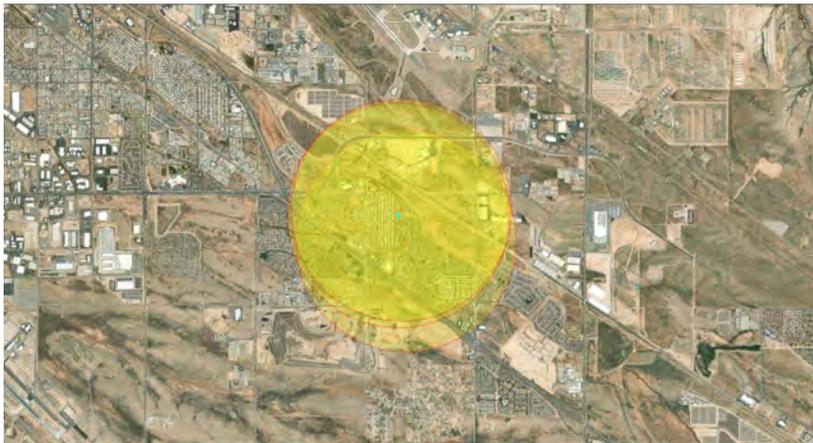


# EJScreen Community Report

This report provides environmental and socioeconomic information for user-defined areas, and combines that data into environmental justice and supplemental indexes.

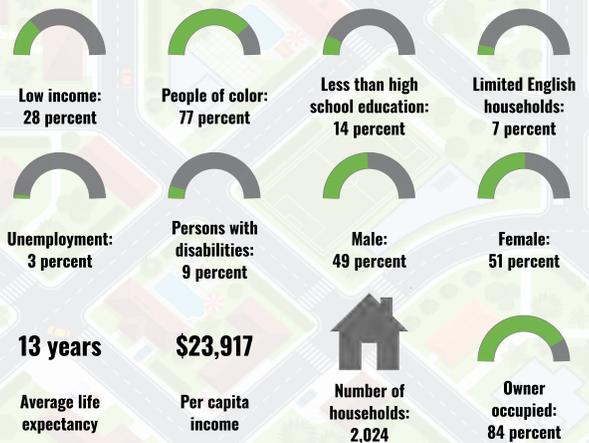
## Tucson, AZ

1 mile Ring around the Area  
Population: 6,415  
Area in square miles: 3.25

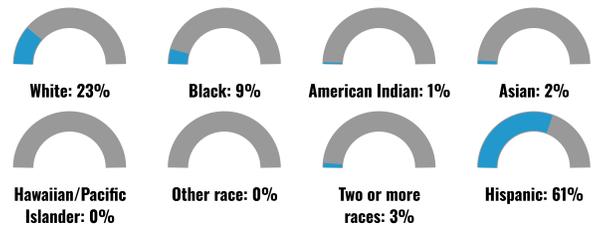


September 14, 2023  
Project 3  
Project 2  
Project 1  
1:36,112  
0 0.5 1 1.5 mi  
EPA/NERL/Geoarc/Map

### COMMUNITY INFORMATION



### BREAKDOWN BY RACE



### BREAKDOWN BY AGE



### LIMITED ENGLISH SPEAKING BREAKDOWN



### LANGUAGES SPOKEN AT HOME

LANGUAGE	PERCENT
English	62%
Spanish	32%
French, Haitian, or Cajun	1%
Other Indo-European	1%
Other and Unspecified	2%
Total Non-English	38%

Notes: Numbers may not sum to totals due to rounding. Hispanic population can be of any race. Source: U.S. Census Bureau, American Community Survey (ACS) 2017-2021. Life expectancy data comes from the Centers for Disease Control.

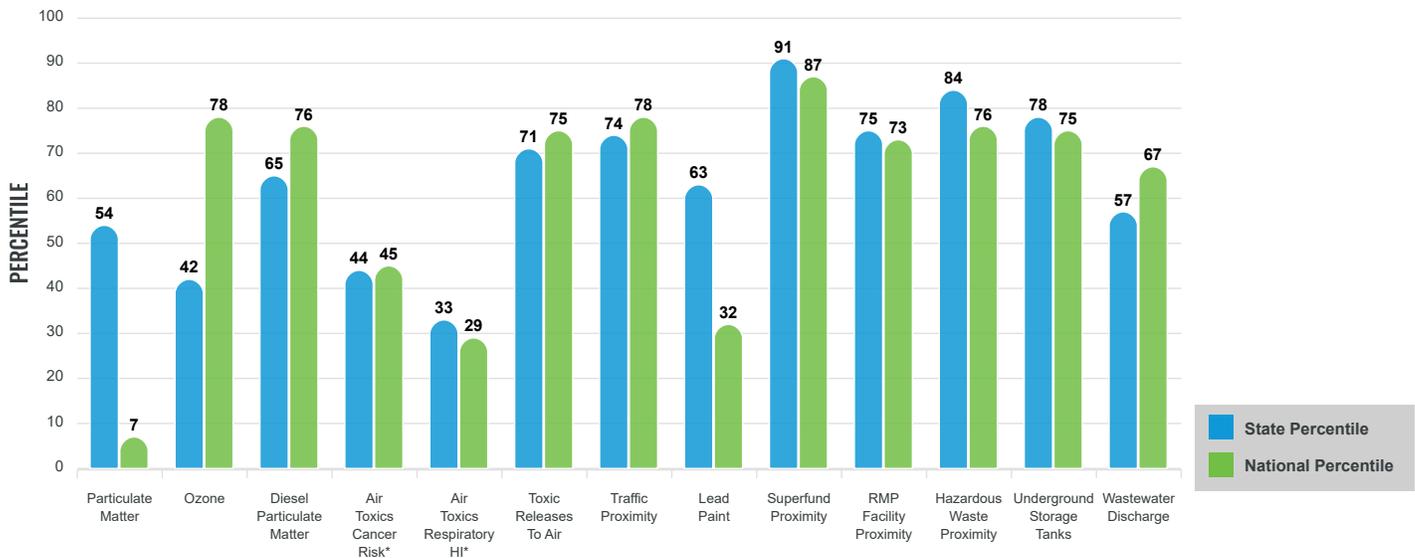
# Environmental Justice & Supplemental Indexes

The environmental justice and supplemental indexes are a combination of environmental and socioeconomic information. There are thirteen EJ indexes and supplemental indexes in EJScreen reflecting the 13 environmental indicators. The indexes for a selected area are compared to those for all other locations in the state or nation. For more information and calculation details on the EJ and supplemental indexes, please visit the [EJScreen website](#).

## EJ INDEXES

The EJ indexes help users screen for potential EJ concerns. To do this, the EJ index combines data on low income and people of color populations with a single environmental indicator.

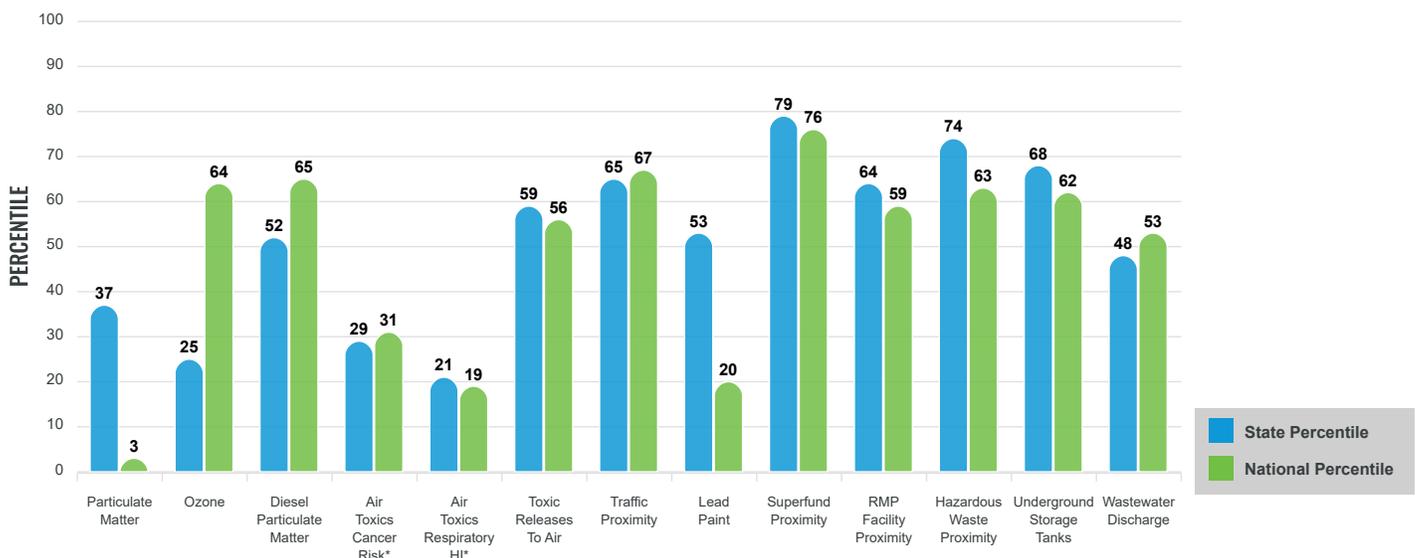
### EJ INDEXES FOR THE SELECTED LOCATION



## SUPPLEMENTAL INDEXES

The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on percent low-income, percent linguistically isolated, percent less than high school education, percent unemployed, and low life expectancy with a single environmental indicator.

### SUPPLEMENTAL INDEXES FOR THE SELECTED LOCATION



These percentiles provide perspective on how the selected block group or buffer area compares to the entire state or nation.

Report for 1 mile Ring around the Area

# EJScreen Environmental and Socioeconomic Indicators Data

SELECTED VARIABLES	VALUE	STATE AVERAGE	PERCENTILE IN STATE	USA AVERAGE	PERCENTILE IN USA
<b>POLLUTION AND SOURCES</b>					
Particulate Matter (µg/m <sup>3</sup> )	4.81	5.87	28	8.08	2
Ozone (ppb)	62.1	66.1	21	61.6	57
Diesel Particulate Matter (µg/m <sup>3</sup> )	0.246	0.278	40	0.261	57
Air Toxics Cancer Risk* (lifetime risk per million)	20	25	13	25	5
Air Toxics Respiratory HI*	0.2	0.31	10	0.31	4
Toxic Releases to Air	570	2,800	51	4,600	49
Traffic Proximity (daily traffic count/distance to road)	160	190	60	210	69
Lead Paint (% Pre-1960 Housing)	0.016	0.089	54	0.3	17
Superfund Proximity (site count/km distance)	0.16	0.077	91	0.13	79
RMP Facility Proximity (facility count/km distance)	0.19	0.38	62	0.43	55
Hazardous Waste Proximity (facility count/km distance)	0.84	0.71	78	1.9	58
Underground Storage Tanks (count/km <sup>2</sup> )	1.9	1.7	71	3.9	58
Wastewater Discharge (toxicity-weighted concentration/m distance)	1.8	5.8	87	22	93
<b>SOCIOECONOMIC INDICATORS</b>					
Demographic Index	53%	38%	74	35%	77
Supplemental Demographic Index	13%	14%	52	14%	51
People of Color	77%	44%	83	39%	81
Low Income	28%	32%	49	31%	52
Unemployment Rate	3%	6%	43	6%	44
Limited English Speaking Households	7%	4%	80	5%	79
Less Than High School Education	14%	12%	69	12%	70
Under Age 5	5%	5%	55	6%	54
Over Age 64	9%	20%	29	17%	21
Low Life Expectancy	0%	19%	0	20%	0

\*Diesel particulate matter, air toxics cancer risk, and air toxics respiratory hazard index are from the EPA's Air Toxics Data Update, which is the Agency's ongoing, comprehensive evaluation of air toxics in the United States. This effort aims to prioritize air toxics, emission sources, and locations of interest for further study. It is important to remember that the air toxics data presented here provide broad estimates of health risks over geographic areas of the country, not definitive risks to specific individuals or locations. Cancer risks and hazard indices from the Air Toxics Data Update are reported to one significant figure and any additional significant figures here are due to rounding. More information on the Air Toxics Data Update can be found at: <https://www.epa.gov/haps/air-toxics-data-update>.

**Sites reporting to EPA within defined area:**

Superfund .....	0
Hazardous Waste, Treatment, Storage, and Disposal Facilities .....	0
Water Dischargers .....	10
Air Pollution .....	1
Brownfields .....	0
Toxic Release Inventory .....	0

**Other community features within defined area:**

Schools .....	2
Hospitals .....	0
Places of Worship .....	0

**Other environmental data:**

Air Non-attainment .....	No
Impaired Waters .....	No

Selected location contains American Indian Reservation Lands* .....	No
Selected location contains a "Justice40 (CEJST)" disadvantaged community .....	No
Selected location contains an EPA IRA disadvantaged community .....	Yes

Report for 1 mile Ring around the Area

# EJScreen Environmental and Socioeconomic Indicators Data

## HEALTH INDICATORS

INDICATOR	HEALTH VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Low Life Expectancy	0%	19%	0	20%	0
Heart Disease	0	6	0	6.1	0
Asthma	11	10.6	68	10	77
Cancer	4.5	6.1	29	6.1	16
Persons with Disabilities	8.7%	13.9%	21	13.4%	23

## CLIMATE INDICATORS

INDICATOR	HEALTH VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Flood Risk	11%	6%	82	12%	69
Wildfire Risk	31%	48%	48	14%	84

## CRITICAL SERVICE GAPS

INDICATOR	HEALTH VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Broadband Internet	5%	13%	37	14%	28
Lack of Health Insurance	13%	10%	70	9%	79
Housing Burden	No	N/A	N/A	N/A	N/A
Transportation Access	Yes	N/A	N/A	N/A	N/A
Food Desert	Yes	N/A	N/A	N/A	N/A

Footnotes

Report for 1 mile Ring around the Area



## 2022 FFIEC Geocode Census Report

Address: 6650 South Avenida Del Arrabal, Tucson, Arizona, 85756  
 MSA: 46060 - TUCSON, AZ  
 State: 04 - ARIZONA  
 County: 019 - PIMA COUNTY  
 Tract Code: 0041.18

## Summary Census Demographic Information

Tract Income Level	Middle
Underserved or Distressed Tract	No
2022 FFIEC Estimated MSA/MD/non-MSA/MD Median Family Income	\$81,100
2022 Estimated Tract Median Family Income	\$79,527
2020 Tract Median Family Income	\$68,125
Tract Median Family Income %	98.06
Tract Population	5274
Tract Minority %	70.38
Tract Minority Population	3712
Owner-Occupied Units	1640
1- to 4- Family Units	1957

## Census Income Information

Tract Income Level	Middle
2020 MSA/MD/statewide non-MSA/MD Median Family Income	\$69,466
2022 FFIEC Estimated MSA/MD/non-MSA/MD Median Family Income	\$81,100
% below Poverty Line	12.75
Tract Median Family Income %	98.06
2020 Tract Median Family Income	\$68,125
2022 Estimated Tract Median Family Income	\$79,527
2020 Tract Median Household Income	\$61,505

## Census Population Information

Tract Population	5274
Tract Minority %	70.38
Number of Families	1420
Number of Households	1941
Non-Hispanic White Population	1562
Tract Minority Population	3712
American Indian Population	81
Asian/Hawaiian/Pacific Islander Population	168
Black Population	406
Hispanic Population	2836
Other/Two or More Races Population	221

## Census Housing Information

Total Housing Units	1966
1- to 4- Family Units	1957
Median House Age (Years)	0
Owner-Occupied Units	1640
Renter Occupied Units	301
Owner Occupied 1- to 4- Family Units	1640
Inside Principal City?	YES
Vacant Units	25